

# DEVELOPMENT REVIEW BOARD REPORT



Meeting Date: August 18, 2016 Item No. 7  
General Plan Element: *Character and Design*  
General Plan Goal: *Foster quality design that enhances Scottsdale as a unique southwestern desert community.*

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## ACTION

**Alta Osborn**  
**12-ZN-2016**

**Location:** 3220 N. Scottsdale Road

**Request:** Pursuant to the Planned Block Development Overlay District (PBD) requirements of the Downtown (D) zoning district, the applicant is requesting a recommendation from the Development Review Board to the Planning Commission and City Council regarding the proposed Development Plan and Proposed Property Development Standards that are being requested as part of the zoning district map amendment application from Highway Commercial (C-3) to Downtown/ Downtown Multiple Use - Type 2, Planned Block Development Overlay, Downtown Overlay (D/DMU-2 PBD DO) for a 4.2-acre site.

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## OWNER

David L. Chapman Trust

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## ARCHITECT/DESIGNER

Biltform Architecture Group

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## ENGINEER

Sustainability Engineering Group

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## APPLICANT CONTACT

John Berry  
Berry Riddell, LLC  
480-385-2727

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## BACKGROUND

### Zoning

This site is currently zoned Highway Commercial (C-3). This zoning district is intended to permit most types of commercial activities located along major streets, including shopping and service needs.

### **Context**

Located at the intersection of N. Scottsdale Road and the southbound lane of the N. Drinkwater Boulevard couplet, the surrounding developments are a mix of multi-family residential, restaurant, hotel, and office uses.

### **Adjacent Uses and Zoning**

- North An existing restaurant and a multi-family residential development are located north of the site in the Downtown Office/Commercial–Type 2, Planned Block Development, Downtown Overlay (D/OC-2 PBD DO) and Downtown/Downtown Multiple Use–Type 2, Planned Block Development, Downtown Overlay (D/DMU-2 PBD DO) zoning districts.
- South An existing mobile home park is located south of the site in the Highway Commercial (C-3) and Single-family Residential (R1-7) zoning districts.
- East N. Scottsdale Road abuts the property to the east. An existing restaurant and hotel are located farther east in the Highway Commercial, Downtown Overlay (C-3 DO) zoning district.
- West N. 71<sup>st</sup> Street abuts the property to the west. An existing multi-family residential complex is located farther west in the Multiple-family Residential (R-5) zoning district.

### **Key Items for Consideration**

- Determine if the Planned Block Development Overlay District Criteria have been met
- Determine whether the Proposed Property Development Standards will achieve the goals and objectives of the Downtown Plan and Downtown Plan Urban Design & Architectural Guidelines
- Provide design comments to the Planning Commission and City Council regarding the proposed Development Plan

## **DEVELOPMENT PROPOSAL**

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### **Goal/Purpose of Request**

The applicant is requesting a zoning district map amendment to the Downtown/Downtown Multiple Use–Type 2, Planned Block Development, Downtown Overlay (D/DMU-2 PBD DO) zoning district, including Proposed Property Development Standards. The zoning district map amendment would allow the development of new 277-unit multi-family residential development and associated amenities. Pursuant to the Planned Block Development Overlay District (PBD) requirements of the Downtown (D) zoning district, the applicant is proposing to establish Property Development Standards and is requesting a recommendation from the Development Review Board to the Planning Commission and the City Council regarding the proposed Property Development Standards (PDS). The applicant is also seeking review of the Development Plan for the proposal, which includes the site plan, landscape plan, pedestrian and vehicular circulation plan, conceptual building elevations, transition plan, and site sections.

### **Neighborhood Communication**

Surrounding property owners within 750 feet have been notified by the City and the site has been posted with one sign. The owner’s representatives notified property owners within 750 feet of the site area and held an open house meeting on April 21, 2016. In addition, the applicant has met one-

on-one with members of the community. As of the writing of this report, staff has not received any inquiries regarding the applicant's Development Plan or Proposed Property Development Standards.

## **DEVELOPMENT REVIEW BOARD CRITERIA ANALYSIS**

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### **General Plan**

The Land Use Element of the General Plan designates the site as Mixed-Use Neighborhoods. This category includes higher density residential, office, and retail uses. Mixed-Use Neighborhoods are also characterized by being located in areas that have multiple modes of transportation available. The Downtown is a designated Growth Area that also relies on these factors. The proposed development addresses several of these goals and approaches, as identified in the project narrative (Attachment #1).

### **Downtown Character Area Plan**

This site is within the Downtown Area boundary. The development plan for this site is in compliance with the goals and policies outlined in the Downtown Character Area Plan (CAP). More specifically, the proposed development aligns with specific policies of the Land Use chapter including LU1.2., which states that it is important to maintain Downtown as a year-round, 24-hour, highly functional mixed-use center, containing areas of different densities, architectural styles, and land uses that support the needs of Scottsdale residents and visitors. The proposed development plan is for an urban, multi-family residential project consisting of 277 multi-family residential units and associated amenities that will integrate with the existing regional land uses near the E. Osborn Road and N. Scottsdale Road area.

The General Plan notes that the Downtown Area, as delineated in the Downtown CAP, is one of the most suitable areas for mixed-use developments and the proposal will align the property with the Downtown CAP Land Use Goal #1.1 (LU#1.1) that encourages revitalization in the Downtown so that it will remain competitive in local, regional, national, and international markets. Other important aspects of the Downtown CAP include Urban Design, Mobility, Arts and Culture, Economic Vitality and Public Services, and Facilities. The applicant has provided a detailed analysis of the project's merits as it relates to each of the elements of the Downtown CAP and it has been provided as Attachment #1.

### **Planned Block Development Overlay District Criteria**

In accordance with Section 6.1304(A) of the Zoning Ordinance, before the first Planning Commission hearing for a Planned Block Development (PBD) Overlay District zoning district map amendment application, the Development Review Board shall make a recommendation to the Planning Commission, regarding the proposed Development Plan (Attachments #5 – 12), based on the following criteria for development in the Type 2 Area of the Downtown Area:

- a. The Development Plan shall reflect the goals and policies of the Character & Design chapter of the Downtown Plan;
  - The Development Plan includes components such as contextually-appropriate design and pedestrian connections that reflect the goals and policies of the Character & Design chapter of the Downtown Plan. Please refer to the Project Narrative for the application (Attachment #1) for additional analysis.

- b. The site development standards and building form shall be in conformance with the Downtown Plan Urban Design & Architectural Guidelines;
  - The Proposed Property Development Standards (PDS) and building form have been designed in conformance with the Downtown Urban Design & Architectural Guidelines. Please refer to additional analysis regarding the proposed PDS below.
- c. The building form shall reflect the planned character of development within which the development will be located;
  - The development site is located at the southern end of the Downtown Area within a Downtown Multiple Use – Type 2 area. The proposed building form has been designed to reflect the planned character of development within the surrounding area. The proposal does not include an increase in building height over the base allowed. In addition, appropriate building stepbacks have been provided to ensure building massing for the proposed site is in harmony with exiting development in the area and the project is not proposing to encroach within required setbacks or stepbacks of the district. The project also provides edge buffering to the Downtown Area boundary and continuity between newly proposed and existing architecture in the surrounding area.
- d. The Development Plan shall incorporate standards for development within three hundred fifty (350) feet of the Downtown Boundary that address appropriate transitions in building heights between the proposed development and the zoning districts abutting or adjacent to the development;
  - The Development plan incorporates transitions in building heights between the proposed development and the zoning districts abutting or adjacent to the development by increasing building setbacks along the 71<sup>st</sup> Street boundary and leaving stepbacks untouched from the base requirements of the Downtown zoning.
- e. The Development Plan shall incorporate standards for development in the Downtown Regional Use – Type 2 or Downtown Medical – Type 2 Areas, and within one hundred (100) feet of the Downtown Multiple Use – Type 2 or Downtown Civic Center – Type 2 Areas, that address appropriate transitions in building heights between the proposed development and the Downtown Multiple Use – Type 2 or Downtown Civic Center – Type 2 Areas;
  - This criterion is not applicable as the site is located within a Downtown Multiple Use – Type 2 area.
- f. The Development Plan for development within one hundred (100) feet of a Type 1 Area shall incorporate standards that address appropriate landscape materials and transitions in building heights between the proposed development and the Type 1 Area;
  - This criterion is not applicable as the site is not located within one hundred feet of a Type 1 area.
- g. The Development Plan shall incorporate standards for development adjacent to public streets that include sidewalks, pedestrian linkages, building forms, and architectural features that address human scale and pedestrian orientation; and
  - The Development Plan has incorporated the Downtown zoning district requirements for development adjacent to public streets, including sidewalks, pedestrian linkages,

building forms, and architectural features. In addition, by following the guidelines of the Downtown Plan Urban Design & Architectural Guidelines the Development Plan address human scale and pedestrian orientation through the use of street-level patios and shaded walkways.

The applicant’s legislative draft of the proposed PDS are included as part of the project narrative under Attachment #1. In general the proposed PDS mimic the Property Development Standards of the Downtown zoning district, with the exception of project specific development standards and their related additional regulations as follows:

- a. Density Maximum (per acre of gross lot area);
- b. Building location; and
- c. Prevailing Setbacks for Buildings Adjacent to a Public Street (except alleys).

<b>Development Standard</b>	<b>Zoning Ordinance Requirement (Zoning Ordinance Sec. 6.1308. &amp; 5.3006.)</b>	<b>Proposed Property Development Standard</b>
<i>Density Maximum (per acre of gross lot area)</i>	50 dwelling units per gross acre	57.24 dwelling units per gross acre (achieved through bonus)
<i>Building location</i>	At least 25% of the length of a building façade shall be located at the minimum setback	Eliminate requirement
<i>Prevailing Setbacks for Buildings Adjacent to a Public Street (except alleys)</i>	Between 25 and 35 Feet	Between 20 and 60 Feet

- h. The pedestrian circulation shall be accessible and easy to navigate, and incorporate open space and pedestrian linkages to the public pedestrian circulation network.
  - The Development Plan has been designed to allow easy pedestrian access to adjacent public sidewalks along N. Scottsdale Road and N. 71<sup>st</sup> Street. In addition, public pathways for pedestrians and bicycles that connect N. Scottsdale Road and N. 71<sup>st</sup> Street are provided along the northern portion of the proposed building.

**Development Plan**

As part of the zoning district map amendment application process, the Development Review Board provides comments to the City Council regarding the design components of an applicant’s proposed Development Plan, which typically consist of a site plan, conceptual building elevations, building massing diagrams, and other information deemed necessary by the Project Coordinator. Finer details of a project’s site and building elevations design are typically refined in a subsequent Development Review Board hearing process, following the zoning district map amendment request and approval by City Council. As such, direction is typically sought from the Development Review Board at this time related to the general site plan and building elevation design that may affect broader aspects of the overall layout of the site or building design. The Development Review Board comments will be forwarded to the Planning Commission and City Council in the form of minutes

taken during the hearing. The applicant’s Development Plan is in general conformance with the base Property Development Standards of the Downtown zoning district with the exception of the proposed alternations as noted above. Staff request that the Development Review Board consider the applicant’s proposed increase in building setback along N. Scottsdale Road and specifically the treatment of the space between the building and the streetline along the N. Scottsdale Road frontage as a result of that increased setback. The increased setback is a departure from the Downtown Design Guidelines, which encourage more activity occurring at the street level.

**Development Information**

- Existing Use: Vehicle leasing, rental, sales and repair
- Proposed Use: Multi-family residential
- Parcel Size: 4.84 gross acres  
4.22 net acres  
183,705 square feet
- Gross floor area: 425,652 square feet
- Gross Floor Area Ratio Allowed: 1.4
- Gross Floor Area Ratio Proposed: None
- Open Space Required: None
- Open Space Provided: 33,778 square feet / 0.78 acres
- Building Height Allowed: 66 feet, including rooftop appurtenances
- Building Height Proposed: 56 feet, including rooftop appurtenances
- Parking Required: 382 spaces
- Parking Provided: 447 spaces
- Number of Dwelling Units Allowed: 242 units
- Number of Dwelling Units Proposed: 277 units (achieved through bonus)
- Density Allowed: 50 dwelling units per acre
- Density Proposed: 57.24 dwelling units per acre (achieved through bonus)

**STAFF RECOMMENDATION**

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**Recommended Approach:**

Staff recommends that the Development Review Board forward a recommendation of approval to the City Council regarding the Proposed Property Development Standards and Development Plan, per the attached legislative draft, finding that the provisions of Land Use and Character and Design Elements of the General Plan and Downtown Character Area Plan and the Planned Block Development Overlay District Criteria have been met.

**Proposed Next Steps:**

Following a recommendation from the Development Review Board, the zoning district map amendment request will be heard by the Planning Commission and finally by the City Council.

**RESPONSIBLE DEPARTMENT**

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**Planning and Development Services**

Current Planning Services

**STAFF CONTACT**

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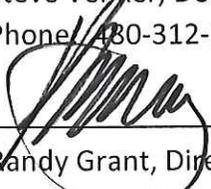
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**APPROVED BY**

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Date: 8/10/16

**ATTACHMENTS**

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1. Applicant's Narrative
2. Context Aerial
- 2A. Close-Up Aerial
3. Zoning Map – Existing
4. Zoning Map - Proposed
5. Combined Context Aerial and Site Plan
6. Site Plan
7. Pedestrian and Vehicular Circulation Plan
8. Building Elevations
9. Perspectives
10. Transition Plan
11. Site Sections and Stepback Exhibit
12. Landscape Plan

**Alta Osborn**  
3220 N. Scottsdale Road  
Rezoning Application

*Project Narrative*

**12-ZN-2016**

**Prepared for:**

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**Prepared by:**

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## I. Property Information

**Location:** North of the northwest corner of Scottsdale and Earll (the “Property”)

**Address:** 3220 N. Scottsdale Road

**Property Size:** Approximately 4.84+/- gross acres, 4.1+/- net acres

## II. Current/Proposed Zoning

### *Current*

- C-3

### *Proposed*

- D/DMU-2 PBD DO

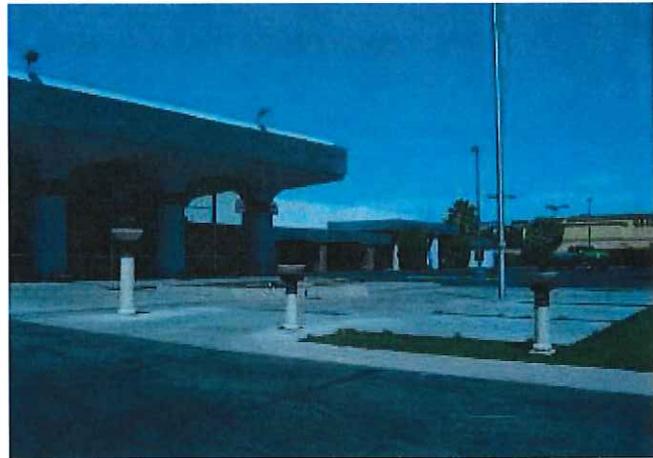
*The Downtown Multiple Use sub-district replaced the Office/Commercial sub-district per the November 2012 Downtown Ordinance text amendment*



### **III. Project Overview**

This request is for a rezoning and site plan approval for a 4.84 +/- gross acre site located north of the northwest corner of Scottsdale and Earll, which is located directly south of U.S. Egg and the recently approved Agave Residential development in Downtown Scottsdale. The site is currently zoned C-3 and occupies a partially abandoned dealership building and auto repair and body shop service facility (formerly Luxor Auto Group car dealership). The applicant intends to redevelop the site with residential development consisting of 277+/- residences (approximately 4.84+/- acres) and onsite amenities consistent with the goals and polices of the Downtown Plan adopted by City Council in 2009. The Property is surrounded by a variety of retail, entertainment, residential, employment and service related business in Downtown Scottsdale and is within close proximity to two of Scottsdale's largest employers, Honor Health and the City of Scottsdale.

#### **Existing Site Photos**



**Cultural Improvements Program & Bonus Provision for Special Improvements**

Contributions to the Cultural Improvements Program include original works of art costing a minimum of one percent (1%) of the applicable building valuation are required at the time of permitting. The developer will elect to either provide public art on site, provide or contribute an in-lieu fee to the Cultural Trust Fund to be dispersed in accordance with the public places program, or provide art on site. As is required, the developer will determine prior to Development Review Board (“DRB”) approval of the development project which alternative they will use.

Additionally, per the development plan and legislative draft of development standards for the Alta Osborn, a density of 57 du/ac is being proposed with the development plan. Density above 50 du/ac is achievable through the use of the bonus provisions as described in Zoning Ordinance sections 6.1310 and 6.1311, which states that PBD properties in the Downtown Multiple Use – Type 2 area can achieve greater densities (no maximum) as part of the development plan.

Based on the table set forth in section 6.1310. F., below is the following bonus and public benefit/special improvement calculation.

277 units – Requested unit count (57 du/ac)

242 units – Allowed unit count (50 du/ac)

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35 units – Requested Bonus

The total cost of special improvements for a bonus of 35 units = \$388,016.30\*  
(\$11,086.18 per unit based on formula)

*\*Calculation to be confirmed with City Staff*

Wood Partners will provide these required public benefits through additional Cultural Improvement Program contributions beyond the base amount required by the PBD overlay district. Per section 6.1309. A., and independent of the special improvements for bonus density, Wood Partners will comply with the Cultural Improvements Program by providing 1% of the building valuation as noted above.

## **IV. 2001 General Plan**

The General Plan sets forth a collection of goals and approaches intended to integrate the Guiding Principles into the planning process and provide as a framework for proposed development and the built environment.

### *General Plan - Six Guiding Principles:*

Per the City's 2001 General Plan, six Guiding Principles articulate how the appropriateness of a land use change to the General Plan is to be qualified. These six Guiding Principles are as follows:

1. Value Scottsdale's Unique Lifestyle & Character (Character & Design, Land Use)
2. Support Economic Vitality
3. Enhance Neighborhoods (Housing, Neighborhoods)
4. Preserve Meaningful Open Space
5. Seek Sustainability
6. Advance Transportation (Mobility)

These six Guiding Principles are further explained below through the different Elements of the General Plan.

### **2001 General Plan Goals & Policies**

- ***Character & Design:***

***Goal 1: Determine the appropriateness of all development in terms of community goals, surrounding area character, and the specific context of the surrounding neighborhood.***

***-Bullet 2: Enrich the lives of all Scottsdale citizens by promoting safe, attractive, and context compatible development.***

***-Bullet 4: Ensure that all development is a part of and contributes to the established or planned character of the area of the proposed location. Character can cross land uses and zoning to include community regions containing a mixture of housing, employment, cultural, educational, commercial and recreational uses. The overall type of character type that these uses are a part of describes the pattern and intensity of how these uses fit together.***

***The **Downtown** is a highly functional mixed-use center, containing areas of different densities and the architectural styles that emphasize regional and specialty retail, office, residential and hotel uses.***

**Response:** Downtown zoning allows for the ability to seek higher densities in a mixed-use setting to encourage an urban lifestyle and appropriate balance of land uses with pedestrian connectivity. The site currently occupies the abandoned Luxor Auto Group car dealership which is mostly vacant with the exception of automotive repair on the western portion of the site. The Property is surrounded by a variety of retail, entertainment, multifamily, employment and service related business in the Downtown Scottsdale and is within close proximity to two of Scottsdale's largest employers, Honor Health and the City of Scottsdale. Additionally, this property is directly adjacent to Scottsdale Road a major north-south arterial with nearby access to public transportation (bus route 72) and is only 2 miles away from the Loop 101, both of which provide regional access. This site is ideally situated in a Downtown setting that offers all ingredients for higher density residential development in a mixed use setting.

***Goal 2: Review the design of all development proposals to foster quality design that enhances Scottsdale as a unique southwestern desert community.***

***-Bullet 5: Promote development that respects and enhances the unique climate, topography, vegetation and historical context of Scottsdale's Sonoran Desert environment, all of which are considered amenities that help sustain our community and its quality of life.***

***-Bullet 6: Promote, evaluate and maintain the Scottsdale Sensitive Design Principles that when followed will help improve and reinforce the quality of design in our community.***

**Response:** The proposed site layout, architectural character and landscaping design respects the unique climate, vegetation and Downtown context of Scottsdale. See Scottsdale Sensitive Design Principles VIII. below for detailed responses regarding each principle.

***Goal 4: Encourage "streetscapes" for major roadways that promote the City's visual quality and character, and blend into the character of the surrounding area.***

***-Bullet 2: Design Downtown/Urban areas to concentrate on those elements that will provide pedestrian comfort, such as arcade-covered walkways, shade, decorative paving and landscaping, so that a comfortable setting can be created for this use-intensive area.***

***-Bullet 10: Other visually significant roadways include roadways with buffered setbacks and roadways with specific streetscape design themes. Each of these designated roadways have individual design guideline policies.***

**Response:** Pedestrian circulation along both the perimeter and internal to the residential community is an important feature of this Downtown project, as numerous retail, restaurant, entertainment, and cultural uses are within walking distances from this site. Special consideration has been given to the frontage along Scottsdale Road with over

one-half of the ground level building area to be designated as residential support uses (ie: fitness, clubhouse), direct unit access/stoops and outdoor seating/gathering space along Scottsdale Road. The ground level residences along all edges of the development (north, south, east & west) will have direct access to pedestrian connections and the streetscape to encourage walkability and street interaction. Additionally, quality hardscape surfaces, appropriately placed pedestrian furniture and landscape canopies will be provided along Scottsdale Road contributing towards the activation of Scottsdale Road.

The sidewalk along the Scottsdale Road frontage is planned to be 8' in width to encourage walkability and to follow Scottsdale Road Streetscape Design Guidelines that encourage wider sidewalks near intersections and gathering zones for greater pedestrian capacity. Shading at the northern public space will generally be provided by Sweet Acacia, Mulga Acacia and Palo Brea trees with shrubs and groundcover comprised of Red Fairy Duster, Texas Sage, Desert Marigold and Twisted Yucca to name a few. The Scottsdale Road sidewalk will tie to secondary sidewalks providing access to the street facing units.

Along 71<sup>st</sup> Street there is also planned to be an 8' wide attached sidewalk with secondary sidewalks providing direct access to street facing units. The landscape palette for the 71<sup>st</sup> Street frontage is planned to contain Fruitless Olive and Allee Elm trees which will provide colorful canopies in addition to shading. The plant palette along the sidewalks will provide year-round color and texture inviting and encouraging increased pedestrian activity and alternative modes of transportation. Additionally, street furniture in the form of benches, trash cans and planters will be thoughtfully located along the walkway to allow respite areas within the pedestrian corridor.

The northern pedestrian connection (running east-west through the site), which is directly adjacent to Agave Residential and U.S. Egg, will be surfaced with a combination of grass-crete and decorative pavers providing fire access and pedestrian access through the site as well as access for the residential units that have direct first floor access.

***Goal 5: Build upon the significant role the arts have played in shaping our community's image and lifestyle by maximizing the potential of public art to enrich the daily lives of people that live in or visit Scottsdale.***

***-Bullet 11: Encourage private participation in public art through the donation of artwork to the city and the placement of artwork on private property that can be publically viewed.***

***-Bullet 12: Celebrate the dominant lifestyle or character of an area of the city by using art.***

**Response:** Regarding the one percent (1%) art requirement, the developer will elect to either provide public art on site provide or contribute an in-lieu fee to the Cultural Trust Fund to be dispersed in accordance the with public places program or provide art on site.

As is required, the developer will determine prior to DRB approval of the development project which alternative they will use. If the developer determines to utilize the required art funds on the Property, the goal will be to integrate and enhance the public realm with interactive art opportunities further promoting the lifestyle and character of Downtown Scottsdale through art.

***Goal 6: Recognize the value and visual significance that landscaping has upon the character of the community and maintain standards that result in substantial, mature landscaping that reinforces the character of a city.***

- Bullet 1: Require substantial landscaping be provided as part of new development or redevelopment.*
- Bullet 2: Maintain the landscaping materials and pattern within a character area.*
- Bullet 3: Encourage the use of landscaping to reduce the effects of heat and glare on buildings and pedestrian areas as well as contribute toward better air quality.*
- Bullet 4: Discourage plant materials that contribute substantial air-borne pollen.*
- Bullet 5: Encourage landscape designs that promote water conservation, safe public settings, erosion protection, and reduce the “urban heat island” effect.*
- Bullet 6: Encourage the retention of mature landscape plant materials.*

**Response:** The landscape character of the proposed Alta Osborn property is that of an ornamental desert with a variety of plants that will provide year-round color, shade, and texture for the site. The proposed plant palette incorporates hardy plants known to thrive in the intense heat and sun of the desert climate. Plant selection and intentional planting design will allow Alta Osborn to use water efficiently throughout the site. The proposed planting design limits lush and dense planting patterns to areas with the most human interaction such as entries, amenity areas and pedestrian areas.

***Goal 7: Encourage sensitive outdoor lighting that reflects the needs and character of different parts of the City.***

- Bullet 2: Encourage lighting designs that minimize glare and lighting intrusions into neighborhood settings.*
- Bullet 3: Encourage creative and high quality designs for outdoor lighting fixtures and standards that reflect the character of the local context.*
- Bullet 5: Allow for lighting systems that support active pedestrian uses and contribute to public safety.*

**Response:** Lighting will be designed in a manner that is respectful of the surrounding context while maintaining safety for future residents. Lighting designs will be commensurate with the quality architectural style proposed for Alta Osborn; low-level with no glare and intrusion on neighboring properties. Lighting will be placed in a thoughtful way to provide safe pedestrian wayfinding at night and highlight paths leading to both street frontages (71<sup>st</sup> and Scottsdale Road).

- **Land Use:**

**Goal 4: Maintain a balance of land uses that support a high quality of life, a diverse mixture of housing and leisure opportunities and the economic base needed to secure resources to support the community.**

***-Bullet 1:*** Allow for diversity of residential uses and supporting services that provide for the needs of the community.

***-Bullet 4:*** Provide a variety of housing types and densities and innovative development patterns and building methods that will result in greater housing affordability.

**Response:** The Downtown District (Multiple Use Type 2 category) supports a variety of major employment and service related uses including, but not limited to, multiple story residential, office, commercial retail, and support services consistent with the intensity of uses which are typically found in the Downtown Scottsdale. Additionally, revitalizing and redeveloping properties is a critical part of the economic vitality of the community as it matures. Integrating additional housing options in Downtown is essential for the continuing economic growth and sustainability of the city as a whole. This project exemplifies this revitalization component by taking underutilized and partially vacant automotive buildings into a thriving residential community with direct access to supporting services and transportation. Thus, providing greater housing options and increasing quality of life for residents.

**Goal 5: Develop land use patterns that are compatible with and support a variety of mobility opportunities/choices and service provisions.**

***- Bullet 1:*** Integrate the pattern of land use and mobility systems in ways that allow for shorter and fewer automobile trips and greater choices for mobility.

***-Bullet 2:*** Encourage non-motorized (pedestrian and bicycle) access/circulation within and to mix-use centers to reduce reliance on the automobile.

**Response:** The Property is located along Scottsdale Road, a major north/south arterial, and is approximately 2 miles west of the Loop 101, both of which provide regional access. The nearby bus shelter (adjacent to Agave Residential to the north) will provide access to the local and regional transportation systems. By creating a comfortable and inviting pedestrian experience along the adjacent streets, this proposed project will encourage alternative modes of transportation, such as by foot, bicycle, bus and/or trolley. Encouraging alternative means of transportation is important as the Property is located near important employment and cultural destinations. Alta Osborn is located 1 block west of the Honor Health Osborn campus (Scottsdale's largest employer) and approximately 6 blocks away from the City of Scottsdale library, Center of the Arts, and City offices. Additionally, the Indian Bend Wash (a multi-use path network) is located approximately ¾ miles from the site.

Pedestrian access is an important factor in activating the streetscape. Within the 24' fire lane to the north of the Property will provide the main pedestrian access through the site from Scottsdale Road to 71<sup>st</sup> street; however, a secondary private access to the south will be provided through a corridor that connects to the internal parking area. Additional pedestrian access will be provided from all ground level apartments to the adjacent streetscape or sidewalk connection. The internal parking area will also incorporate a bicycle parking area as well as 2 bicycle racks at the northeast portion of the Property for residents and visitors.

- ***Economic Vitality:***

**Goal 7: *Sustain the long-term economic well-being of the city and its citizens through redevelopment and revitalization efforts.***

***-Bullet 2:* Encourage and support the renovation and reuse of underutilized or vacant parcels/buildings/shopping centers.**

***-Bullet 6:* Promote residential revitalization to maintain quality housing and thus maintain quality of life and stability of the local economy.**

**Response:** The redevelopment of this underutilized and partially vacant site will contribute towards the economic stability of Scottsdale by providing housing options in the heart of Downtown near abundant retail, entertainment, support services and major employers (Honor Health and City of Scottsdale). Integrating housing in a Downtown core brings physical and economic synergy, which will enliven and enhance the city center consistent with the goals and policies of the Downtown Plan.

- ***Housing:***

**Goal 4: *Encourage housing development that provides for “live, work, and play” relationships as a way to reduce traffic congestion, encourage economic expansion and increase overall quality of life for our residents.***

***-Bullet 1:* Encourage a variety of housing densities throughout Scottsdale, with mixed-use in areas of major employment and transit hubs, to offer greater live-work choices to a broader economic range of households.**

***-Bullet 6:* Consider a variety of strategies to increase housing intensity and diversity in appropriate locations, such as around commercial areas, near transit centers and major employers.**

**Response:** Downtown zoning allows for the ability to seek higher densities in a mixed-use setting to encourage an urban lifestyle and appropriate balance of land uses with pedestrian connectivity. The site currently occupies the abandoned Luxor Auto Group car dealership which is mostly vacant with the exception of automotive repair on the

western portion of the site. The Property is surrounded by a variety of retail, entertainment, multifamily, employment and service related business in the Downtown Scottsdale and is within close proximity to two of Scottsdale's largest employers, Honor Health and the City of Scottsdale. Additionally, this property is directly adjacent to Scottsdale Road a major north-south arterial with nearby access to public transportation (bus route 72) and is only 2 miles away from the Loop 101, both of which provide regional access. This site is ideally situated in a Downtown setting that offers all ingredients for higher density residential development.

- ***Neighborhoods:***

**Goal 4: *Preserve and enhance the unique sense of neighborhood found in diverse areas of Scottsdale through neighborhood conservation.***

***-Bullet 7: Create, preserve, and enhance pedestrian, vehicular, and alternative transportation mode connections and links between neighborhoods and other neighborhood-supporting land uses throughout the community.***

**Response:** The Alta Osborn community will provide a comfortable and safe streetscape with adequate access, shade and visibility. Since there are two existing bus shelters within a quarter of a mile of the site, it is not necessary to provide an additional shelter; however, installing tasteful, comfortable seating along Scottsdale Road. Trees and shrubs will be used to create shade and a comfortable microclimate as well provide visual relief that will enhance the pedestrian experience on the streets and encourage the use of the sidewalks to get from one destination to another.

**Goal 5: *Promote and encourage context-appropriate new development in established areas of the community.***

***-Bullet 1: Encourage new development efforts toward existing developed areas in Scottsdale.***

***-Bullet 2: Promote the use of existing infrastructure as an incentive to encourage more infill development within the community.***

**Response:** The Alta Osborn community will provide new development on an underutilized and partially vacant parcel in Downtown. The infill development will utilize existing infrastructure systems (roads, utilities, etc.). This application is driven by the site's surrounding mix of land uses and a strong market demand for residential in Downtown.

- ***Growth Areas***

**Goal 2: *Make automobile transit and other multimodal circulation more efficient.***

*-Bullet 1: Encourage physical planning and design techniques that facilitate the access to and use of transit services and pedestrian amenities.*

**Response:** The Property is located along Scottsdale Road a major north/south arterial and is approximately 2 miles west of the Loop 101, both of which provide regional access. The nearby bus shelter (adjacent to Agave Residential to the north) will provide access to the local and regional transportation systems. This proposed project will also encourage alternative modes of transportation, such as by foot, bicycle and/or trolley as it is located near important employment and cultural destinations. Alta Osborn is located 1 block west of the Honor Health Osborn campus (Scottsdale's largest employer) and approximately 6 blocks away from the City of Scottsdale library, Center of the Arts, and City offices. Additionally, the Indian Bend Wash (a multi-use path network) is located approximately ¾ miles from the site.

**Goal 6: *Integrate public (civic) art into the visual character of designated growth areas.***

**Response:** Contributions to the Cultural Improvements Program include original works of art costing a minimum of one percent (1%) of the applicable building valuation are required at the time of permitting. The developer will elect to either provide public art on site provide or contribute an in-lieu fee to the Cultural Trust Fund to be dispersed in accordance the ith public places program or provide art on site. As is required, the developer will determine prior to DRB approval of the development project which alternative they will use. If the developer determines to utilize the required art funds on the Property, the goal will be to integrate and enhance the public realm with interactive art opportunities.

- ***Community Mobility:***

**Goal 8: *Emphasize live, work and play land use relationships to optimize the use of citywide systems and reduce the strain on regional and local/neighborhood systems.***

*-Bullet 2: Encourage the development of redevelopment of areas that support a balance of live, work and play land use relationships and alternative modes of transportation that reduces reliance on the automobile.*

**Response:** As mentioned above, the Property is directly adjacent to Scottsdale Road and 2 miles west of the Loop 101, both of which provide regional access. The proposed development accomplishes a range of goals including the integration of high quality, vibrant architecture and innovative site planning, creating pedestrian presence with

ground level activity, and providing housing in the Downtown employment core thereby reducing trip generation during peak hours. The nearby bus stop will provide direct access to the local and regional transportation systems.

**Goal 11:** *Provide opportunities for building “community” through neighborhood mobility.*

***-Bullet 10:*** *Emphasize strong pedestrian orientation (e.g. shaded safe paths, links to civic spaces) to foster a strong sense of community.*

**Response:** The proposed 8’ sidewalk combined with well-placed shade trees will generate a more comfortable and inviting pedestrian space along the streetscape. This will allow use of the sites circulation systems regardless of solar orientation. Direct pedestrian access from ground level apartments will allow residents to immediately engage the pathways after visiting local retailers providing for a more urban experience.

## **V. Downtown Plan Overview**

The Downtown Multiple Use Type 2 land use designation, set forth in the Downtown Plan adopted in June 2009, directly correlates with the mixed-use concept envisioned for this site. The Downtown Multiple Use Type 2 category supports a variety of major employment and service related uses including, but not limited to, multiple story residential, office, commercial retail, and support services consistent with the intensity of uses which are typically found in the Downtown Scottsdale. Additionally, revitalizing and redeveloping properties is a critical part of the economic vitality of the community as it matures. Thus, the proposed and future redevelopment of the site is essential for the continuing economic growth and sustainability of the city as a whole.

The proposed D/DMU-2 PBD DO zoning district is consistent with the General Plan/Downtown Plan land use designation for the subject Property. The Downtown Plan’s goals and policies which relate specifically to the proposed Development Plan for subject Property are identified as follows:

- **LAND USE**

**GOAL LU 1:**  
***MAINTAIN DOWNTOWN SCOTTSDALE AS THE COMMERCIAL, CULTURAL, CIVIC, AND SYMBOLIC CENTER OF THE COMMUNITY TO ENSURE A VIBRANT MIX OF MUTUALLY SUPPORTIVE LAND USES.***

***Policy LU 1.2.***

*Maintain Downtown as a year-round, 24-hour highly functional mixed use center, containing areas of different densities, architectural styles and land uses that support the needs of Scottsdale’s residents and visitors.*

**Response:** The proposed urban residential development will further strengthen Downtown's urban environment and 24-hour mixed use character by bringing additional roof tops into the urban core where there is established commercial, entertainment, and employment land uses. Note Character & Design Goal 4 (and subsequent goals & policies) above regarding the specific design elements that will create a vibrant, interactive urban development on the Property.

**GOAL LU 2:**

***PROMOTE THE DEVELOPMENT OF DOWNTOWN AS A COLLECTION OF MIXED-USE URBAN NEIGHBORHOODS.***

***Policy LU 2.1.***

*As a General Plan designated Mixed-Use Neighborhood, Downtown Scottsdale should offer efficiencies of strong access to multiple modes of transportation and major regional access and services, focus on human scale development, accommodate higher density housing combined with complementary office or retail uses, and accommodate vertical mixed-use structures.*

***Policy LU 2.2.***

*Support interconnected, pedestrian oriented urban neighborhoods that are comprised of a balanced mix of activities and land uses within optimal walking distance.*

**Response:** The rezoning request is for a parcel in Downtown Scottsdale west of the Honor Health Osborn Medical Center and with direct access to Scottsdale Road (a major north-south transportation corridor) that is integrated in a horizontally mixed-use setting with abundant onsite amenities including a pool area within a resort setting, common courtyard, 7,000+/- s.f. clubhouse, and 3,000+/- s.f. fitness facility not to mention the surrounding commercial including U.S. Egg, Olive Garden, Courtyard by Marriott, Lo-Lo's Chicken & Waffles,

D/DMU-2 PBD DO zoning will allow for residential and future mixed-use development on the Property by allowing for a wider range of development standards than currently allowed under existing zoning categories. The Property is located directly on Scottsdale Road (a major north-south arterial roadway) and 2 miles west of the Loop 101, both of which provide regional access. The location of the proposed residential development will lend itself to various modes of transportation, such as by foot, bicycle and/or trolley as Alta Osborn is located 1 block west of the Honor Health campus (Scottsdale's largest employer) and approximately 6 blocks away from the City of Scottsdale library, Center of the Arts, and City offices. Additionally, the Indian Bend Wash (a multi-use path network) is located approximately ¾ miles from the site.

Additionally, since there are already two bus shelters within a quarter-mile of the site it is not necessary to provide an additional shelter; however, installing comfortable and inviting seating along Scottsdale Road, as well as trees and shrubs, will create shade and a comfortable microclimate. This will act as a sensory relief from the built, urban

environment. These practices will enrich the pedestrian experience on the streets and encourage the use of the sidewalks to get from one destination to the next.

***Policy LU 2.5.***

*Encourage new development, redevelopment and infill that strengthens Downtown Scottsdale's mix of uses and activities through the development of mutually supportive land uses within the **Downtown Multiple Use** urban neighborhoods. Multiple Use areas include the majority of the properties within the overall Downtown Plan boundary. New development and redevelopment within the Multiple Use areas adjacent to the Arizona Canal should incorporate vertically mixed land uses that activate the Canal with both visitors and residents year-round.*

***Policy LU 2.6.***

*Maintain, enhance, and expand the development of a **Downtown Medical Mixed-Use** urban neighborhood that supports the Downtown Medical Campus with its high influx of employees and clients. Components of this neighborhood should include vertical mixed-use development that serves the efficiency needs of the hospital, including research and development, and other medical activities. The incorporation of land uses that support the medical focus of this neighborhood such as housing, hotels, service oriented businesses, child care, assisted living and long term care facilities are also envisioned. The greatest intensity of Downtown development may be accommodated in this urban neighborhood.*

**Response:** Redevelopment of this Property will build on the City's goal of strengthening Downtown's mix of uses and activities. This project will bring 277+/- residences to the southern edge of the Downtown boundary on a site that is within close proximity to two major Scottsdale employers, Honor Health and City of Scottsdale. Although the site is not located within the Downtown Medical sub-district, its adjacency is important to note as it will provide housing options supportive of the employees of the hospital and nearby medical offices.

**GOAL LU 4:**

***ENCOURAGE DOWNTOWN LAND USE DEVELOPMENT FLEXIBILITY.***

***Policy LU 4.1***

*Retain, expand, or modify as necessary, flexibility in Downtown zoning, development standards and incentives to achieve the goals of the Downtown Plan.*

***Policy LU 4.5***

*To assist the community in achieving the vision of the Downtown Plan, the provision of Downtown public amenities and benefits should be provided when development bonuses such as increased floor area, (ratio of usable building area to parcel size), greater density, greater height, transfer of development rights, and/or street/alley abandonment, are being considered. Potential public amenities and benefits may include, but are not limited to:*

- *New or expanded public realm and open space areas;*
- *Pedestrian enhancements and amenities;*
- *Mobility enhancements;*
- *Transit enhancements;*
- *Sensitive edge buffering;*
- *Revitalization of Downtown properties;*
- *Infrastructure improvements;*
- *Additional public parking and shared parking facilities;*
- *Historic preservation; and/or*
- *Work force housing.*

**Response:** The PBD (Planned Block Development) overlay requested with this zoning application will allow for greater flexibility with the development standards for the Property. Redevelopment and revitalization of this Property includes new work force housing (for nearby employers such as Honor Health and City of Scottsdale), allocation of new open space, integration of pedestrian amenities and stronger pedestrian connectivity, mobility enhancements, sensitive edge buffering, and structured parking. Special consideration has been given to the frontage along Scottsdale Road with over one-half of the ground level building area to be designated as residential support uses (ie: fitness, clubhouse) and outdoor seating/plaza space will be provided along Scottsdale Road. All of the ground level residences along Scottsdale Road, 71<sup>st</sup> Street and along the north and south will have direct access to the streetscape and/or sidewalk connections to the street. Additionally, quality hardscape surfaces and appropriately placed pedestrian furniture and landscape canopies will be provided along the perimeter and throughout the community thereby contributing towards the activation of the street frontages. The landscape palette for Scottsdale Road will be consistent with Scottsdale Road Streetscape Design Guidelines.

The proposed housing will provide additional options in the Downtown employment core for nearby employers, as mentioned above, with a range of price points and options that will appeal to essential workers (ie: police officers, firefighters, teachers, nurses, medical technicians, and working professionals). However, specific price points have not determined at this time.

**GOAL LU 5:**  
***ENHANCE, EXPAND, AND CREATE NEW DOWNTOWN PUBLIC REALM AND OPEN SPACE AREAS.***

***Policy LU 5.1.***

*Provide for more open space areas within the Downtown that may include central gathering places, a series of smaller, intimate spaces, as well as active and passive recreational use opportunities.*

**Response:** As noted above, the ground level of Scottsdale Road will be activated with plazas along Scottsdale Road, non-residential uses (fitness and clubhouse), ground level residences along Scottsdale Road and 71<sup>st</sup> Street with direct access to the streetscape, an

east-west pedestrian connection along the north and south, quality hardscape surfaces, pedestrian furniture/seating, and landscape canopies to enhance the pedestrian “open space” experience.

The proposed 8’ sidewalk combined with well-placed shade trees will generate a more comfortable and inviting pedestrian space along the streetscape. The use of landscape elements and planting to clearly define and identify spaces for pedestrian activity and vehicular activity is key to creating a safe pedestrian environment. The proposed plan integrates these pedestrian spaces through the use of street furniture in the form of benches, trash cans and planters will be thoughtfully located along the walkway to allow respite areas within the pedestrian corridor.

***Policy LU 5.6***

*Improve, expand or create public realm and open space areas that can be enhanced by art and interactive opportunities.*

**Response:** Contributions to the Cultural Improvements Program include original works of art costing a minimum of one percent (1%) of the applicable building valuation are required at the time of permitting. The developer will elect to either provide public art on site provide or contribute an in-lieu fee to the Cultural Trust Fund to be dispersed in accordance with the public places program or provide art on site. As is required, the developer will determine prior to DRB approval of the development project which alternative they will use. If the developer determines to utilize the required art funds on the Property, the goal will be to integrate and enhance the public realm with interactive art opportunities.

**GOAL LU 6:**

***PROMOTE DIVERSITY IN DOWNTOWN HOUSING OPTIONS.***

***Policy LU 6.1.***

*Develop a variety of housing types such as apartments, condominiums, lofts, town homes, patio homes, and live/work units.*

**Response:** The Alta Osborn development provides an urban residential living experience with numerous onsite amenities and a lifestyle that integrates with the existing Downtown fabric and collection of uses (entertainment, arts, employment, retail, support services) contributing towards the variety of housing options available to the residents of Scottsdale.

**GOAL LU 7:**

***PROMOTE A MIX OF LAND USES THAT PROMOTE A SUSTAINABLE DOWNTOWN.***

***Policy LU 7.2***

*Encourage development to make cost effective uses of existing urban resources such as infrastructure systems, underutilized sites, buildings and transportation networks.*

**Response:** The redevelopment of this site from the existing and underutilized automotive uses to a vibrant urban development bring housing and synergy to the southern edge of Downtown Scottsdale will utilize existing infrastructure systems and transportation networks that run through and adjacent to Property consistent with the City's goals for Downtown redevelopment.

- **CHARACTER & DESIGN**

**GOAL CD 1:**

***THE DESIGN CHARACTER OF ANY AREA SHOULD BE ENHANCED AND STRENGTHENED BY NEW DEVELOPMENT THAT PROMOTES CONTEXTUAL COMPATIBILITY.***

***Policy CD 1.1***

*Incorporate, as appropriate; in building and site design the distinctive qualities and character of the surrounding, and/or evolving, context.*

***Policy CD 1.2***

*Encourage creative and innovative civic and prominent developments to establish new urban design character and contextual interpretations of the Downtown, for areas where development patterns are fragmented or are in transition.*

***Policy CD 1.4***

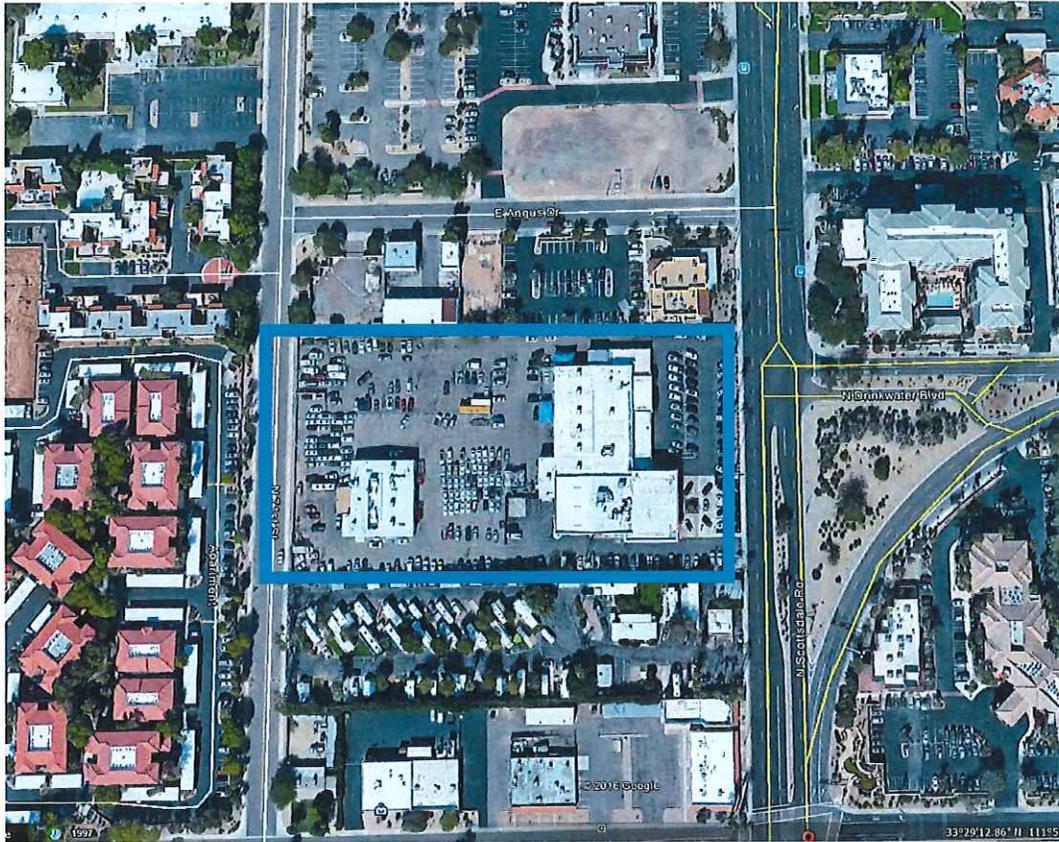
*Promote Downtown urban and architectural design that is influenced by, and responds to, the character and climate of the Sonoran Desert.*

***Policy CD 1.5:*** *Encourage urban and architectural design that addresses human scale, and provides for pedestrian comfort.*

***Policy CD 1.7:*** *Maintain pedestrian oriented small town character and human scale in the Downtown Core areas of Main Street, Marshall Way, 5<sup>th</sup> Avenue, Craftsman Court, and Old Town. Incorporate similar elements of pedestrian character and scale at the street level throughout all of Downtown.*

***Policy CD 1.8:*** *Encourage new development and revitalization that is composed of complementary and/or supportive design relationships with the established urban form, as well as design relationships with the established urban form, as well as design features and materials of the neighborhood within which they are sited.*

## Existing Property Condition – LACKING PEDESTRIAN CONNECTIVITY



**Response:** The building design for the Alta Scottsdale preserves and enhances the existing architectural character of the Downtown Scottsdale while creating a unique architectural style and providing appropriate scale, massing and hierarchy within the existing Downtown setting. The proposed development also provides continuity between the newly proposed and existing architecture in the surrounding area, providing contextually appropriate redevelopment and visual fluidity along Scottsdale Road.

Alta Osborn proposes a high quality multifamily residential community to the growing south end of Scottsdale's thriving Downtown. The project consists of a wrap-style building design (residential units wrapping above ground parking structure), ranging from two-stories along Scottsdale Road and 71<sup>st</sup> Street to four-stories with fifth floor mezzanine in limited areas. Amongst the internal parking area will also be a secured bicycle parking area as well as bike shop for residents. There are two internal courtyards with a wide range of amenities and a common open space amount reaching approximately 20,000 s.f. in courtyard space.

Alta Osborn seamlessly combines pedestrian oriented design at the ground level with front door stoops to each first floor unit defining the streetscape along Scottsdale Road and 71<sup>st</sup> Street, and also along the south drive and north pedestrian-way. This approach will activate all four sides of the building, allowing both convenient access and

opportunities for outdoor living on patios defined by low walls with metal railing details. Sensitivity to context is addressed by use of brownstone-type articulation along the east and west elevations of the project. This articulation emulates an appropriate urban housing model, which provides a distinctive and memorable streetscape for the project. Exterior building materials include a generous use of brick veneer, synthetic cut stone at key accent areas, metal canopies and railings, and sand finished stucco rendered in various complementary colors. The contemporary Southwest architecture is defined by an intricate light colored grid, wrapping the building both horizontally and vertically, and projecting across patios for visual interest and shade elements.

Along Scottsdale Road this mix of uses will include a fitness center, a clubhouse and business center, and direct unit access to the street frontages. In total, over one-half of the ground level building area along Scottsdale Road will be designated as residential support uses (ie: fitness, common area, etc.) with a commercial storefront appearance that ties into the existing architecture of the surrounding area engaging the streetscape both visually and physically. The state-of-the art, 7,000+/- s.f. clubhouse is a lifestyle center for the residents, with a large-scale social room, kitchen for entertaining, business center, conference room, leasing offices and a 3,000+/- s.f. fitness facility with a separate yoga room. These uses will be designated for the residents of Alta Osborn and not open to the general public.

#### **GOAL CD 2:**

***DEVELOPMENT SHOULD SENSITIVELY TRANSITION IN SCALE, HEIGHT, AND INTENSITY AT THE DOWNTOWN PLAN BOUNDARY AND BETWEEN ADJOINING URBAN NEIGHBORHOODS OF DIFFERING DEVELOPMENT TYPES.***

##### ***Policy CD 2.1.***

*The scale of existing development surrounding the Downtown Plan boundary should be acknowledged and respected through a sensitive edge transition buffer, established on a location specific basis, that may include transitional development types, landscape buffers, and sensitive architectural design solutions to address building mass and height.*

**Response:** The overall project massing steps down from four & five stories (fifth story mezzanine only) to two stories to help create a logical transition from the Downtown boundary (71<sup>st</sup> Street) and the existing lower-scale multifamily development to the west (two and three-story apartments). Additional setbacks are provided along 71<sup>st</sup> as well; where a 20' minimum setback is required, 26' to 42' is provided. Along Scottsdale Road, a 20' minimum setback is required and a 43' to 59' setback is provided. Additional building undulation has been provided along the Scottsdale Road frontage for the brownstone-style character and direct/stoop access.

The ground level will also be activated with an open plaza-like feel along Scottsdale Road, ground level residences along Scottsdale Road & 71<sup>st</sup> Street with direct access to the streetscape, an east-west pedestrian pathway along the north, quality hardscape

surfaces, pedestrian furniture/seating, and landscape canopies to enhance the pedestrian experience.

A critical design quality that is considered in this building design is human scale. Human scale provides for both a higher quality pedestrian environment, which is created with a variety of small-scale buildings elements rather than a single large unarticulated building, and a sense of “home” for the residents.

**GOAL CD 3:**  
***DOWNTOWN DEVELOPMENT SHOULD RESPECT AND RESPOND TO THE UNIQUE CLIMATE AND CONTEXT OF THE SOUTHWESTERN SONORAN DESERT.***

***Policy CD 3.1***

*Enhance outdoor pedestrian comfort through microclimate design that incorporates a variety of shade conditions, landscape, and features that are drought tolerant, as well as offer attractive spaces, and passively cooler temperatures.*

***Policy CD 3.2***

*Public realm and site design should incorporate techniques for efficient water use. Water, as a landscape element or design feature, should be used judiciously and placed in locations with high pedestrian activity.*

**Response:** The landscape character of the proposed Alta Osborn property is that of an ornamental desert with a variety of plants that will provide year-round color, shade, and texture for the site. The proposed plant palette incorporates hardy plants known to thrive in the intense heat and sun of the desert climate. Plant selection and intentional planting design will allow Alta Osborn to use water efficiently throughout the site. The proposed planting design limits lush and dense planting patterns to areas with the most human interaction such as entries, amenity areas and pedestrian areas.

**GOAL CD 4:**  
***STRENGTHEN PEDESTRIAN CHARACTER AND CREATE STRONG PEDESTRIAN LINKAGES.***

***Policy CD 4.1***

*Develop an attractive, interconnected network of safe and walkable pedestrian linkages to, within, and between, the various Downtown urban neighborhoods.*

***Policy CD 4.2*** *Development should demonstrate consideration for the pedestrian by providing access and connections to adjacent developments.*

**Response:** Pedestrian circulation along both the perimeter and internal to the residential community is an important feature of this Downtown project, as numerous retail, restaurant, entertainment, and cultural uses are within walking distances from this site.

Special consideration has been given to the frontage along Scottsdale Road with over one-half of the ground level building area to be designated as residential support uses (ie: fitness, clubhouse), direct unit access/stoops and outdoor seating/gathering space along Scottsdale Road. The ground level residences along all edges of the development (north, south, east & west) will have direct access to pedestrian connections and the streetscape to encourage walkability and street interaction. Additionally, quality hardscape surfaces, appropriately placed pedestrian furniture and landscape canopies will be provided along this edge thereby contributing towards the activation of the Scottsdale Road frontage.

## **GOAL CD 5**

### ***CREATE COHERENT AND CONSISTENT STREET SPACES.***

#### ***Policy CD 5.1.***

*Create a unified public realm through the design of Downtown's streets, building setback areas, and building frontages. Setbacks may only be compromised after clear demonstration that other significant public benefits are achieved.*

#### ***Policy CD 5.2.***

*Street spaces are components of the overall public realm and should be logically interconnected with other pedestrian spaces.*

#### ***Policy CD 5.3***

*Streetscapes should provide continuity among adjacent uses through comprehensive sustainable landscape improvements, shade elements, decorative paving, street furniture, public art, and integrated infrastructure elements.*

**Response:** The proposed 8' sidewalk combined with well-placed shade trees will generate a more comfortable and inviting pedestrian space along the streetscape. This will allow use of the sites circulation systems regardless of solar orientation. Direct pedestrian access from ground level apartments will allow residents to immediately engage the pathways after visiting local retailers providing for a more urban experience.

In addition to a safe and comfortable pedestrian environment, connectivity and access is an important factor in activating the streetscape. The 24' fire lane to the north of the Property will provide the main pedestrian access through the site from N Scottsdale Road to 71<sup>st</sup> street; however, a secondary private access to the south will be provided through a corridor that connects to the internal parking area. Additional pedestrian access will be provided from all ground level apartments to the adjacent streetscape or sidewalk connection. These routes will allow for numerous patterns of pedestrian circulation across the site.

**GOAL CD 6**

**INCORPORATE A REGIONAL LANDSCAPE PALETTE THAT COMPLEMENTS DOWNTOWN'S URBAN CHARACTER.**

***Policy CD 6.1.***

*Downtown landscape elements should project a desert oasis design character, providing an abundance of shade, color, varied textures and forms.*

***Policy CD 6.2.***

*Landscape materials should complement Downtown's built environment, land uses, and activities through the careful selection of plant materials with regard to scale, density, placement, and arrangement.*

**Response:** The proposed plant materials palette complements the character of the urban built environment surrounding the site. Incorporating softer less formal planting that will create more attractive and inviting spaces for pedestrians into a palette that retains a more formal, defined feel will create the appropriate balance and transition between the urban hardscape and more interactive areas.

**GOAL CD 8**

**IMPLEMENT HIGH QUALITY DESIGN IN DOWNTOWN ARCHITECTURE.**

***Policy CD 8.5.***

*Encourage the integration of art into Downtown urban design and architecture.*

**Response:** The development will comply with the Scottsdale Road Streetscape Design Guidelines utilizing plant materials that were approved with Agave Residential (38-DR-2015) to the north and the proposed L'Esperance development (13-ZN-2016) to the south. The developer is considering the two options of including public art on the site or contributing the 1% towards the Cultural Trust Fund to be determined prior to DRB approval.

The proposed landscape character will complement the proposed developments north and south of this site. Through the development process, the applicant and its consultants will coordinate with the adjacent developments to ensure a complementary use of proposed landscape materials in accordance with the Scottsdale Road Streetscape Design Guidelines. Similar planting patterns and groupings as well as complementary textures, colors and plant varieties will create strong aesthetic connections between different spaces while still maintaining a unique and identifiable character.

**GOAL CD 9**

**DEVELOPMENT SHOULD INCORPORATE SUSTAINABLE BUILDING PRACTICES AND PRODUCTS.**

***Policy CD 9.1.***

*Development should strive to incorporate sustainable planning, design strategies, building techniques, and the use of durable indigenous materials, to minimize environmental and maintenance impacts, and endure over time.*

***Policy CD 9.2.***

*Encourage green building design strategies such as building orientation, passive solar response, natural day lighting, passive cooling techniques, and the integration of regional plant materials.*

***Policy CD 9.3.***

*Promote the use of energy efficient systems, construction methods, and alternative energy sources.*

**Response:** Sustainable strategies and building techniques, which minimize environmental impact and reduce energy consumption, will be emphasized. The developer intends to incorporate sustainable design elements into the building design and plans to develop in accordance with green building standards where possible.

• **MOBILITY**

**GOAL M 1:**

***DEVELOP COMPLETE STREETS THROUGH PUBLIC AND PRIVATE INFRASTRUCTURE INVESTMENTS AND IMPROVEMENTS.***

***Policy M 1.1.***

*Develop a well-connected Downtown circulation grid, comprised of narrower streets, improved alleys, smaller blocks, and newly constructed local street segments, to enhance connectivity for all mobility modes.*

***Policy M 1.2.***

*Provide interconnectivity within large development parcels that provide pedestrian and bicycle connections through large projects.*

**Response:** See CD 4.1 above. Additionally, the project will provide pedestrian and bicycle connectivity via an east-west pathway along the northern portion of the project site. This will allow residents along 71<sup>st</sup> Street to have greater access to Scottsdale Road, existing businesses, including nearby restaurants, without going north via Osborn Road or south via Earll Drive. A minimum 8' sidewalk is being provided in this location with a direct line of sight to make this connection point a smooth and visible access for pedestrians.

The Alta Osborn community will provide a comfortable and safe streetscape with adequate access, shade and visibility. Since there are already two existing bus shelters within a quarter-mile of the site, it is not necessary to provide an additional shelter; however, installing tasteful, comfortable seating along Scottsdale Road, as well as trees

and shrubs, will create shade and a comfortable microclimate. This will act as a sensory relief from the built, urban environment. These practices will enrich the pedestrian experience on the streets and encourage the use of the sidewalks to get from one destination to the next.

**GOAL M 2:**

***CREATE COMPLETE, COMFORTABLE, AND ATTRACTIVE PEDESTRIAN CIRCULATION SYSTEMS.***

***Policy M 2.1.***

*Design the public realm to include enhanced requirements for sidewalk space.*

*Provide adequate space for meeting and passing other pedestrians, including queuing space to support desired levels of activity.*

**Response:** The proposed 8' sidewalk combined with well-placed shade trees will generate a more comfortable and inviting pedestrian space along the streetscape. This will allow use of the sites circulation systems regardless of solar orientation. Direct pedestrian access from ground level apartments will allow residents to immediately engage the pathways after visiting local retailers providing for a more urban experience.

In addition to a safe and comfortable pedestrian environment, connectivity and access is an important factor in activating the streetscape. The 24' fire lane to the north of the Property will provide the main pedestrian access through the site from N Scottsdale Road to 71<sup>st</sup> street; however, a secondary private access to the south will be provided through a corridor that connects to the internal parking area. Additional pedestrian access will be provided from all ground level apartments to the adjacent streetscape or sidewalk connection. These routes will allow for numerous patterns of pedestrian circulation across the site.

**GOAL M 3:**

***CREATE A HIERARCHY OF PEDESTRIAN SPACES WITHIN THE OVERALL DOWNTOWN.***

***Policy M 3.1.***

*Certain areas of Downtown should be developed as Pedestrian Places.*

***Policy M 3.2.***

*Downtown should become a Pedestrian Supportive environment.*

**Response:** See CD 4.1 above. Successful Downtown revitalization and redevelopment focuses on creating synergy as a result of mixed-use development (horizontally and vertically) and quality pedestrian level design and linkages. One of the primary draws to creating this quality pedestrian environment is locating residential in the Downtown core. Alta Osborn being located on the southern end of Downtown ("Southern Gateway") will provide as an anchor for this highly walkable pedestrian environment of Downtown Scottsdale. The range of surrounding land uses coupled with the pedestrian

enhancements noted above will increase foot traffic in, out and around this site as well as throughout Downtown.

Providing gathering space for residents and pedestrians through ground level improvements including enhanced hardscape, landscape, gathering spaces and outdoor furniture will revitalize an area that currently feels suburban in nature with single story commercial dealership buildings, abundant pavement and minimal pedestrian improvements. The design elements proposed for Alta Osborn will greatly enhance the character of the Scottsdale Road street frontage in this location. Activating the pedestrian experience and enhancing the streetscape improvements in combination with the proposed residential use reinforces the vibrant Downtown synergy that the City strongly promotes in both the Downtown Plan and Downtown Urban Design & Architectural Guidelines.

The Alta Osborn community will provide additional pedestrian activity to the streets of Downtown Scottsdale. The pedestrian connections throughout and across the site reflect that of an urban environment. By internalizing the parking, the Alta Osborn property has removed the pedestrian obstacle of the parking lot from the streetscape and reinforced the ideas of other alternative means of transportation. Direct access from ground level apartments to the streets and sidewalk connections reinforces the desire to engage the community through a close proximity of pedestrian activity.

- **ECONOMIC VITALITY**

***GOAL EV 2:***

***PROMOTE PRIVATE INVESTMENT AND ATTRACT NEW DEVELOPMENT TO DOWNTOWN.***

***Policy EV 2.3***

*Encourage private reinvestment through flexible policies, and incentives, that work with the private sector to respond to functional and economic changes in the Downtown.*

**Response:** In the spirit of promoting the Downtown Plan, the rezoning request allows for the Downtown community to continue to grow and revitalize. Flexibility is a fundamental part of revitalizing a property and the requested PBD overlay allows for site specific development standards essential to redevelopment of the site.

Over the last year Scottsdale, similar to other parts of the Phoenix metro area, has experienced a boom in multifamily development in or near their urban core. Industry experts indicate pent-up demand is attracting everyone from college students to boomers to locate in the hub of activity vs. the once more sought after suburbs. This is in part due to the change in mindset about home ownership after the last market downturn. Additionally, millennials are attracted to smaller units, which are easier to maintain and within close proximity to support services, restaurants, jobs, nightlife and entertainment found in Downtown.

**GOAL EV 3:**

***ESTABLISH PUBLIC REGULATIONS AND PROCESSES THAT ENCOURAGE CREATIVITY AND FLEXIBILITY IN BUILDING AND SITE DESIGN.***

***Policy EV 3.2.***

*Create, and/or amend, zoning and land use regulations to allow for flexible design as Downtown spaces are developed or redeveloped.*

**Response:** Redevelopment of the subject property supports and upholds the goals and policies of the Downtown Plan. This rezoning request is consistent with the Downtown Plan and promotes revitalization of Scottsdale's Downtown community.

**VI. Planned Block Development (PBD)/Development Plan**

*The purpose of the PBD Overlay District is to allow for development flexibility in the Downtown Area to assist the City in achieving the Downtown Plan, developing more Downtown Area public amenities, and adding land uses that would further promote the Downtown Area as a twenty-four (24) hour community.*

**Response:** The PBD designation has been requested as part of this zoning application to allow for greater development flexibility on the site. In keeping with the PBD requirements, a Development Plan has been established to layout parameters for site planning, architecture, and landscaping. The intent of the Development Plan is to define an established style, character, and design quality for the site, while maintaining opportunities for specific needs and a creative identity through future approvals by the DRB. The Development Plan is intended to invoke a sense of quality and character that ensure compatible development with Scottsdale's Downtown urban character. As part of the Development Plan the applicant has created a set of Property Development Standards consistent with the Downtown Ordinance. There are only three modifications as permitted by Downtown Multiple Use –Type 2 sub-district.

- 1) Density: A proposed density of 57 du/ac (50 du/ac is the base maximum).
- 2) Building Location: To remove the building location requirement, which states that 25% of the building shall be at the setback line.
- 3) Prevailing Setback for Buildings: Modification to the prevailing setback range along both Scottsdale Road and 71<sup>st</sup> Street to allow for greater flexibility in building setback as follows: from 25-35 feet to 20-60 feet.

**Sec. 6.1304. PBD Overlay District criteria.**

A. *Before the first Planning Commission hearing on a PBD Overlay District application, the Development Review Board shall make a recommendation to the Planning Commission regarding the Development Plan based on the following criteria.*

- ~~1. Criteria for a PBD Overlay District application in a Type 1 Area:~~  
~~a. The Development Plan shall reflect the goals and policies of the Character & Design chapter of the Downtown Plan; and~~  
~~b. The site development standards and building form shall be in conformance with the Downtown Plan Urban Design & Architectural Guidelines.~~

**Response:** Not applicable.

2. *Criteria for a PBD Overlay District application in the Type 2 Area:*  
*a. The Development Plan shall reflect the goals and policies of the Character & Design chapter of the Downtown Plan;*

**Response:** See Downtown Plan Overview section above.

- b. The site development standards and building form shall be in conformance with the Downtown Plan Urban Design & Architectural Guidelines;*

**Response:** See Downtown Plan Urban Design & Architectural Guidelines section below.

- c. The building form shall reflect the planned character of development within which the development will be located;*

**Response:** The proposed development has sensitive edge buffering through the implementation of appropriate setbacks and stepbacks per the Downtown Ordinance. The proposed development also provides continuity between newly proposed and existing architecture in the surrounding area. See Downtown Plan Urban Design & Architectural Guidelines section below for more detail.

- d. The Development Plan shall incorporate standards for development within three hundred fifty (350) feet of the Downtown Boundary that address appropriate transitions in building heights between the proposed development and the zoning districts abutting or adjacent to the development;*

**Response:** The overall project massing steps down from five stories to two stories to help create a logical transition from the Downtown boundary (71<sup>st</sup> Street) and the existing lower-scale development to the west. Additional setbacks

are provided along 71<sup>st</sup> as well; where a 20' minimum setback is required, 26' to 42' is provided. Along Scottsdale Road, a 20' minimum setback is required and a 43' to 59' setback is provided.

*e. The Development Plan shall incorporate standards for development in the Downtown Regional Use - Type 2 or Downtown Medical - Type 2 Areas, and within one hundred (100) feet of the Downtown Multiple Use - Type 2 or Downtown Civic Center - Type 2 Areas, that address appropriate transitions in building heights between the proposed development and the Downtown Multiple Use - Type 2 or Downtown Civic Center - Type 2 Areas;*

**Response:** See property development standards attached herein (following the Conclusion). Proposed deviations from the based Downtown Ordinance requirements are as follows: 1) To increase the base density from 50 du/ac to 57 du/ac. 2) To remove the building location requirement which states that 25% of the building shall be at the setback line. 3) To modify the prevailing setback requirement from 25-35 feet to 20-60 feet to allow for a larger setback along the street frontages for the proposed residential development. These amendments allow the development to provide a greater buffer to adjacent properties appropriate for this southern Downtown site; thereby creating an opportunity to activate the street frontage with direct pedestrian access, quality hardscape materials and landscape canopies.

*f. The Development Plan for development within one hundred (100) feet of a Type 1 Area shall incorporate standards that address appropriate landscape materials and transitions in building heights between the proposed development and the Type 1 Area;*

**Response:** Not applicable.

*g. The Development Plan shall incorporate standards for development adjacent to public streets that include sidewalks, pedestrian linkages, building forms and architectural features that address human scale and pedestrian orientation; and*

*h. The pedestrian circulation shall be accessible and easy to navigate, and incorporate open space and pedestrian linkages to the public pedestrian circulation network.*

**Response:** See pedestrian circulation plan, which identifies the proposed linkages in conformance with the Downtown Plan.

*B. In addition to the criteria used by the City Council to review a zoning district map amendment application, the Planning Commission shall make a recommendation to the City Council, based on the following applicable criteria:*

*1. Standard criteria:*

*a. The proposed development supports the land use elements of General Plan and the Downtown Plan.*

**Response:** The proposed development plan supports the land use elements of the General Plan and Downtown Plan. See the Downtown Plan Overview section above.

*2. Criteria to add land uses to Table 5.3005.D., Land Uses for Each Sub-district of the Downtown District:*

*a. Each proposed land use helps maintain a balance of land uses in the Downtown Area in accordance with the Downtown Plan.*

*b. Each proposed land use is compatible with the adjacent development, and strengthens the mix of land uses and activities in the Downtown Area.*

*c. Each proposed land use substantially implements the pedestrian oriented, twenty-four (24) hour downtown community goals of the Downtown Plan.*

**Response:** No additional land uses are being added.

*3. Criteria to achieve bonus(es):*

*a. The proposed Development Plan reflects noteworthy investments to provide public benefits, improve the quality of life in the community, and assist in achieving the goals and policies of the General Plan, Downtown Plan and City objectives, primarily in the immediate vicinity of the neighborhood where the development will be located.*

**Response:** A bonus for density is being proposed with this application with this application. As described on page 5 above the public benefit/special improvements will be provided with additional funds towards the Cultural Improvements Program.

## **VII. Downtown Plan Urban Design & Architectural Guidelines** **Conformance**

### ***A. Site Development***

#### ***A1 – Relationship of New to Existing Development***

The proposed development has sensitive edge buffering through the implementation of appropriate setbacks and stepbacks per the Downtown Ordinance. The proposed development also provides continuity between the newly proposed and existing architecture in the surrounding area, providing contextually appropriate redevelopment and visual fluidity along Scottsdale Road. See Site Plan and Site Sections for setback and stepbacks.

#### ***A2 – Active Street Frontages***

Active street frontages are provided along Scottsdale and 71<sup>st</sup> Street with the integration of fitness uses, patios, direct unit access to street frontages, sidewalks and shade trees for the pedestrians and shading elements on the buildings. In addition to landscaping, windows and doors will be shaded by metal canopies and wall grid projections. See Scottsdale Road Street Elevation Exhibit and 71<sup>st</sup> Street Elevation Exhibit.

#### ***A3 – Courtyards and Passages***

The community is designed with a large active courtyards that includes a resort style pool, cabanas and spa. The courtyard is designed for active and passive areas with the goal being resident social connectivity. See Pool Courtyard Enlargement and Courtyard Enlargement.

#### ***A4 – Parking Facilities***

The development is served by an internal parking structure. The parking structure includes secured bike storage. The pedestrian connection from the internal parking structure to both Scottsdale Road and 71<sup>st</sup> Street will be provided through the building. See Site Plan and Pedestrian and Vehicular Circulation Plan.

#### ***A5 – Building Equipment & Services***

All services and loading areas will be obscured from public street view. Mechanical and electrical equipment and appurtenances will be concealed from public view and neighboring properties. See Site Plan for electrical equipment locations.

#### ***A8 – The Continuity of Street Spaces***

*-and-*

#### ***A9 – The Building Setback Zone***

Alta Osborn is designed to create an active street frontage. This mixed-use setting will contain a clubhouse, a resort style pool/spa, a fitness center, business center, and an outdoor seating/plaza space along Scottsdale Road. All of the ground level residences along Scottsdale Road, 71<sup>st</sup> Street and along the north and south will have direct access to the streetscape and/or sidewalk connections to the street. Additionally, quality hardscape surfaces and appropriately placed pedestrian furniture and landscape canopies will be

provided along the perimeter and throughout the community thereby contributing towards the activation of the street frontages and continuity of the street spaces. The landscape palette for Scottsdale Road will be consistent with Scottsdale Road Streetscape Design Guidelines.

Further, a critical design quality that has been considered in this building design is human scale. Human scale provides for both a higher quality pedestrian environment, which is created with a variety of small-scale buildings elements rather than a single large unarticulated building, and a sense of “home” for the residents.

See Site Plan and Site Sections for setbacks and stepbacks.

### ***A10 – The Linkage of Neighboring Developments***

Pedestrian circulation along both the perimeter and internal to the residential community is an important feature of this Downtown project, as numerous retail, restaurant, entertainment, and cultural uses are within walking distances from this site. Special consideration has been given to the frontage along Scottsdale Road with over one-half of the ground level building area to be designated as residential support uses (ie: fitness, clubhouse), direct unit access/stoops and outdoor seating/gathering space along Scottsdale Road. The ground level residences along all edges of the development (north, south, east & west) will have direct access to pedestrian connections and the streetscape to encourage walkability and street interaction. Additionally, quality hardscape surfaces, appropriately placed pedestrian furniture and landscape canopies will be provided along this edge thereby contributing towards the activation of the Scottsdale Road frontage. See Pedestrian and Vehicular Circulation Plan.

## **B. Building Form**

### ***B1- Reduction of Apparent Size and Bulk***

The building design for Alta Osborn preserves and enhances the existing architectural character of the Downtown Scottsdale while creating a unique architectural style and providing appropriate scale, massing and hierarchy within the existing Downtown setting.

### ***B2 – Covered Walkways***

The street facing elevations along both Scottsdale and 71<sup>st</sup> will provide shaded environments for the pedestrian through the placement of trees and building overhangs. See Scottsdale Road Street Elevation Exhibit and 71<sup>st</sup> Street Elevation Exhibit.

## **C. Architectural Character**

### ***C1 - Proportion and Scale***

The proportion and scale of the proposed development is in conformance with the Downtown design guidelines. The architecture provides variation, movement, and hierarchy of massing to create visual interest and varied proportions. See Elevations and Perspectives.

## ***C2 – Building Materials***

*-and-*

## ***C3 – Architectural Detail***

The proposed materials, colors, textures and detailing are indicative of Downtown Scottsdale and include a wide range of elements reflecting the vibrancy and character of this urban Sonoran Desert location while maintaining a warm earth tone palette with appropriate accents. See Building Colors and Materials sheet.

## ***D. Landscape Character***

### ***D1- Streets***

The street trees proposed will be consistent with the Downtown design guidelines providing appropriate spacing, theming and shade for the pedestrians. See Planting Plans.

### ***D2- Site Spaces***

This building is a courtyard style project with special emphasis creating resident social interaction. The overall project massing has been developed in a configuration to create multiple areas that optimize the buildings opportunities for natural light and ventilation.

### ***D3- Plant Selection***

Plant selection will include low-water use, desert appropriate trees with a range of sizes to provide hierarchy in scale, year round color and a variety of textures. See Planting Plans.

## **VIII. Scottsdale Sensitive Design Principles**

The Character and Design Element of the General Plan states that “Development should respect and enhance the unique climate, topography, vegetation and historical context of Scottsdale's Sonoran desert environment, all of which are considered amenities that help sustain our community and its quality of life.” The City has established a set of design principles, known as the Scottsdale's Sensitive Design Principles, to reinforce the quality of design in our community. The following Sensitive Design Principles are fundamental to the design and development of the Property.

- 1. The design character of any area should be enhanced and strengthened by new development.***

**Response:** The contemporary building character and stepped massing is complementary to the surrounding development including the Ten-Wine Lofts, Agave Residential the Courtyard by Marriott hotel and several newer projects throughout Downtown. The proposed development will utilize a variety of desert appropriate textures and building finishes, incorporate architectural elements that provide solar shading and overhangs, and celebrate the Southwestern climate by creating outdoor living spaces for each unit,

recreational amenities, and gathering places for its residents. See Elevations and Perspectives.

2. *Development, through appropriate siting and orientation of buildings, should recognize and preserve established major vistas, as well as protect natural features.*

**Response:** Although the setting of this Downtown site is urban in character and does not have natural features such as washes and natural area open space, the developer has taken special consideration in maintaining providing meaningful open space and recreational amenities for the residents (approximately 20,000 s.f. for the courtyard areas). See Pool Courtyard Enlargement and Courtyard Enlargement. See Site Plan.

3. *Development should be sensitive to existing topography and landscaping.*

**Response:** The property is a Downtown redevelopment site. All landscaping will consist of low-water use desert appropriate landscaping materials. See Planting Plans.

4. *Development should protect the character of the Sonoran Desert by preserving and restoring natural habitats and ecological processes.*

**Response:** The proposed redevelopment will preserve and restore natural habitats and ecological processes through the inclusion of additional desert appropriate landscaping (as well as integration of native plants). Additional landscaping will contribute to the urban habitat for wildlife and improved air quality. Also, desert appropriate plants will be able to withstand the variations of the local climate and as they mature they will become self-sustaining relative to water demand. See Planting Plans.

5. *The design of the public realm, including streetscapes, parks, plazas and civic amenities, is an opportunity to provide identity to the community and to convey its design expectations.*

**Response:** Pedestrian circulation along both the perimeter and internal to the community is an important feature of this Downtown project, as numerous retail, restaurant, resort/hotel, and entertainment uses are within walking distances from this site (see pedestrian circulation plan). The design of these public spaces will be in the character of surrounding developments and will comply with the Downtown Design Guidelines. See Pedestrian and Vehicular Circulation Plan.

6. *Developments should integrate alternative modes of transportation, including bicycles and bus access, within the pedestrian network that encourage social contact and interaction within the community.*

**Response:** The Property is within walking distance to an established Downtown core and within close proximity to Honor Health's 44-acre Downtown/Osborn campus and

approximately one-half a mile of the City of Scottsdale government offices (including city hall, library and civic center). As such, the site plan has been designed with an emphasis on pedestrian connectivity enhancing the connectivity land use goals for this area. Developing housing in Downtown along the Scottsdale Road corridor within close proximity to a range of employment, retail and entertainment uses with established transportation options reduces the number and distance of automobile trips, improves air quality, thereby enhancing the quality of life for surrounding residents. See Pedestrian and Vehicular Circulation Plan. See Site Plan for incorporation of secured bike storage in the parking structure.

7. *Development should show consideration for the pedestrian by providing landscaping and shading elements as well as inviting access connections to adjacent developments.*

**Response:** The proposed development will incorporate design elements that respect human-scale, providing shade and shelter through building, site and landscape design. A detailed pedestrian circulation plan is provided with the application showing connectivity to/from/along the site to the north, south, east and west and well internal connections. See Pedestrian and Vehicular Circulation Plan and Planting Plans.

8. *Buildings should be designed with a logical hierarchy of masses.*

**Response:** The proposed use of building massing is compatible to surrounding developments and is appropriately placed back from Scottsdale Road and 71<sup>st</sup> Street. Building articulation and stepped massing promote a natural hierarchy. See the cross-section and stepback exhibits for more details.

9. *The design of the built environment should respond to the desert environment.*

**Response:** The proposed development will utilize a variety of desert appropriate textures and building finishes, incorporate architectural elements that provide solar shading and overhangs, and celebrate the Southwest climate by creating abundant outdoor living spaces and amenities for its residents.

10. *Developments should strive to incorporate sustainable and healthy building practices and products.*

**Response:** Sustainable strategies and building techniques, which minimize environmental impact and reduce energy consumption, will be emphasized. The developer intends to incorporate sustainable design elements into the building design and plans to develop in accordance with green building standards where possible.

11. *Landscape design should respond to the desert environment by utilizing a variety of mature landscape materials indigenous to the arid region.*

**Response:** Context appropriate, mature arid-region plant materials will be utilized with the redevelopment of the Property. The desert character will be upheld through the careful selection of plant materials in terms of scale, density, and arrangement. See Planting Plans.

12. *Site design should incorporate techniques for efficient water use by providing desert adapted landscaping and preserving native plants.*

**Response:** The proposed development will maintain a low-water use plant palette (see landscape plan). Context appropriate desert plant materials will be utilized with the development of the Property, consistent with the established vegetative pattern along Scottsdale Road. See Planting Plans.

13. *The extent and quality of lighting should be integrally designed as part of the built environment.*

**Response:** Lighting will be designed in a manner that is respectful of the surrounding context while maintaining safety for future residents. See Lighting Plan and Photometric Plan.

14. *Signage should consider the distinctive qualities and character of the surrounding context in terms of size, color, location and illumination.*

**Response:** Project identification will be contextually appropriate and processed under a separate approval and permit process.

## **IX. Conclusion**

Downtown Scottsdale is a diverse community where residents enjoy an enhanced standard and quality of life. The support services and range of employment opportunities found within Downtown contribute to Scottsdale's well-earned reputation as a premier destination and place to live and work. Redevelopment of the Property will play a meaningful role in the fabric of Scottsdale's Downtown by implementing the goals and policies set forth in the General Plan and Downtown Plan. Wood Partners is dedicated to reinvesting in the Downtown core by promoting a development that will continue to improve the urban community character in a mixed use setting.

## Alta Osborn - Property Development Standards

A. *Maximums for building height, GFAR and density, are shown on Table A.1.*

<b>Table A.1 Building Height, Gross Floor Area Ratio (GFAR), Density Maximums</b>			
Sub-district and Development Type	Building Height Maximum <sup>(1)</sup>	GFAR Maximum (up to 2.0 with bonuses) <sup>(2)</sup>	Density Maximum per acre of gross lot area
Downtown Multiple Use – Type 2	66 feet	1.3-2.0	57 dwelling units
Note: 1. Includes rooftop appurtenances 2. See Table 5.3008.B.			

B. *Setbacks from public streets, except alleys.*

1. The minimum setback from public streets (except alleys) is shown in Table B.1. The setback is measured from the back of curb.

<b>Table B.1. Minimum Setback for Buildings Adjacent to Public Streets, except alleys</b>	
Street	Minimum Building Setback
Scottsdale Road in Downtown Multiple Use sub-district	20 feet
All other public streets and public street segments in the Type 2 Area	20 feet

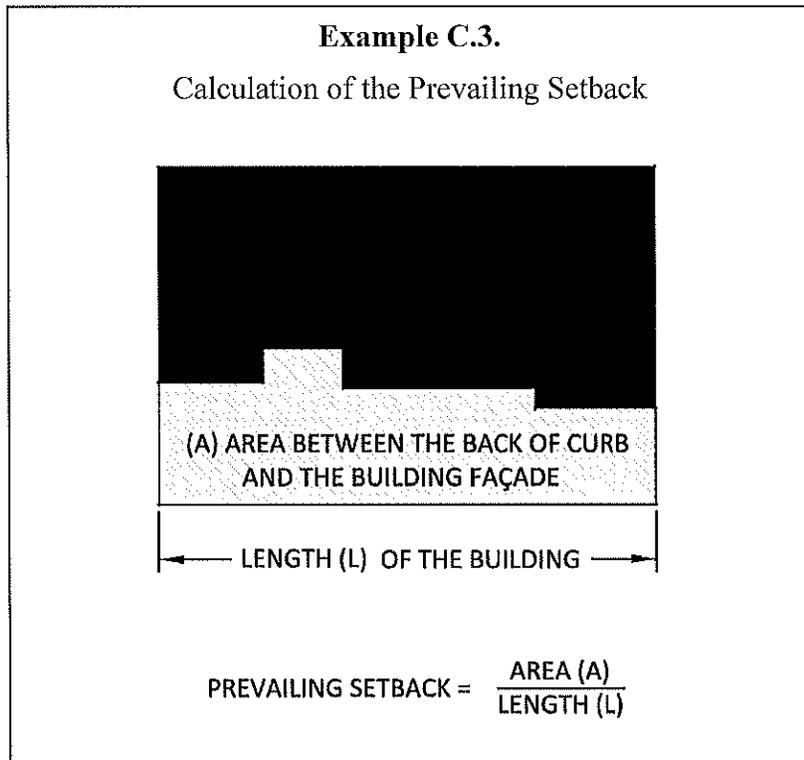
2. The adjustment of front yard requirements in Article VII. does not apply.

C. *Building location.*

1. In a Type 2 Area, a building with a building façade length of 200 feet or more shall be located to achieve a prevailing setback shown in Table C.2. The building façades on a corner lot are calculated separately, and not added together.

Table C.2. Prevailing Setbacks for Buildings Adjacent to a Public Street (except alleys)	
Street	Prevailing Setback
All public street and public street segments*	Between 20 and 60 Feet

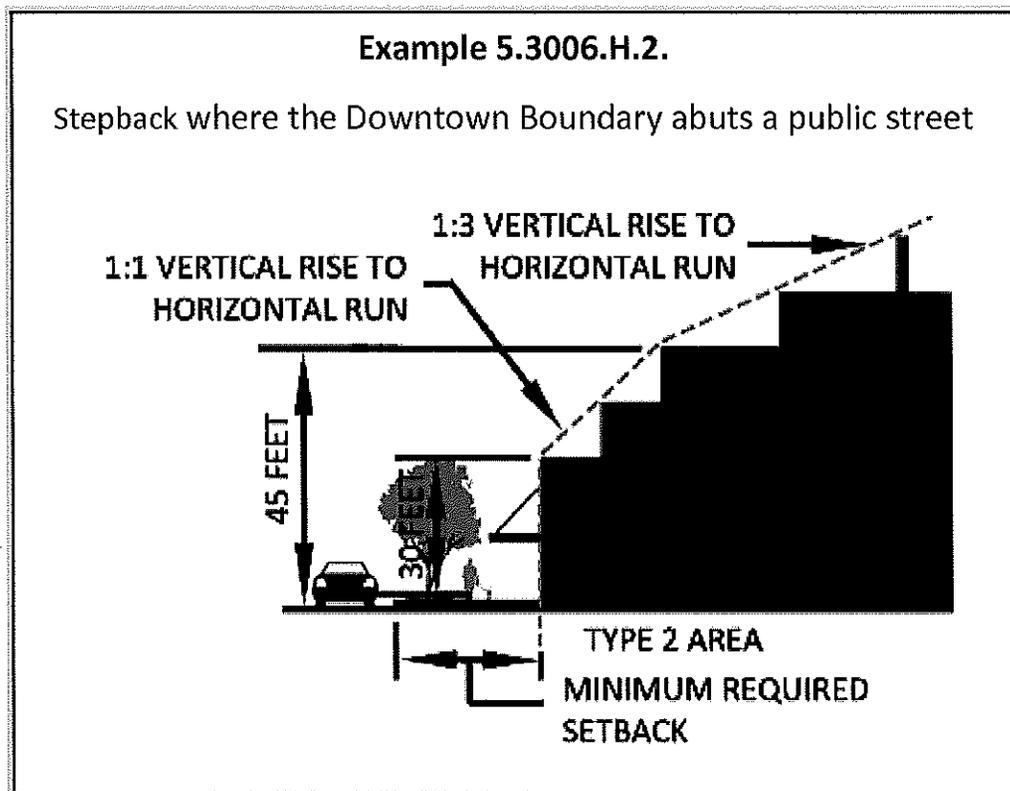
- The prevailing setback is equal to the area between the back of curb and the building facade, divided by the length of the building, as shown in Example C.3.



- Private outdoor living space.
  - All dwelling units shall include private outdoor living space located beside the dwelling unit.
  - Each private outdoor living space shall be at least six (6) feet deep and sixty (60) square feet in area.

E. *Stepbacks.*

1. Downtown Multiple Use – Type 2 Areas: The stepback plane shall incline at a ratio of 1:1, beginning 30 feet above (i) the minimum setback from the public street (except alleys), and (ii) all other property lines, to 45 feet; and beginning at 45 feet, incline at a ratio of 2:1.
2. Downtown Boundary—additional requirements for property in a Type 2 Area: Where the Downtown Boundary abuts a public street (except alleys), the stepback plane shall incline at a ratio of 1:1, beginning thirty (30) feet above the minimum setback from the public street (except alleys) to forty-five (45) feet; and beginning at forty-five (45) feet, incline at a ratio of 1:3.



F. *Exceptions to setback, prevailing setback and stepback standards.*

1. Except as provided in Subsection F.5. below, certain exceptions to setback and stepback standards are allowed if the Development Review Board finds the exceptions conform to:
  - a. The Downtown Plan and Downtown Plan Urban Design & Architectural Guidelines; and
  - b. The sight distance requirements of the Design Standards and Policy Manual.

2. Subject to design approval by the Development Review Board, the following exceptions to setback and stepback standards are allowed:
  - a. A maximum of five (5) feet for cornices, eaves, parapets and fireplaces.
  - b. A maximum of seven (7) feet for canopies and other covers over sidewalks, balconies and terraces.
  - c. Balcony walls and railings with a maximum inside height of forty-five (45) inches.
  - d. Uncovered balconies, uncovered terraces and patios at and below grade.
  - e. Covered sidewalks and uncovered terraces directly above a sidewalk.
  - f. A maximum of three (3) feet for covered balconies.
3. Subject to design approval by the Development Review Board, in a Type 2 Area, a maximum 15 feet exception to stepback and setback standards above the first floor (not specified in F.2. above), is allowed for projections that:
  - a. Are less than 50 percent of the length of the segment of the building facade where the projections occur; and
  - b. Are less than 33 percent of the surface area of the segment of the building façade where the projections occur.
4. Subject to design approval by the Development Review Board, an exception to the stepback standard is allowed for stairwells and elevator shafts.
5. Exceptions to setback or stepback standards are not allowed:
  - a. To cross a property line; however, exceptions that encroach into the public street may be allowed, subject to the Scottsdale Revised Code.
  - b. To increase the maximum building height.

G. *Shaded sidewalks.*

1. The property owner shall provide shaded sidewalks that conform to the Downtown Plan Urban Design & Architectural Guidelines, subject to Development Review Board approval.

H. *Signs.*

1. The provisions of Article VIII. shall apply.

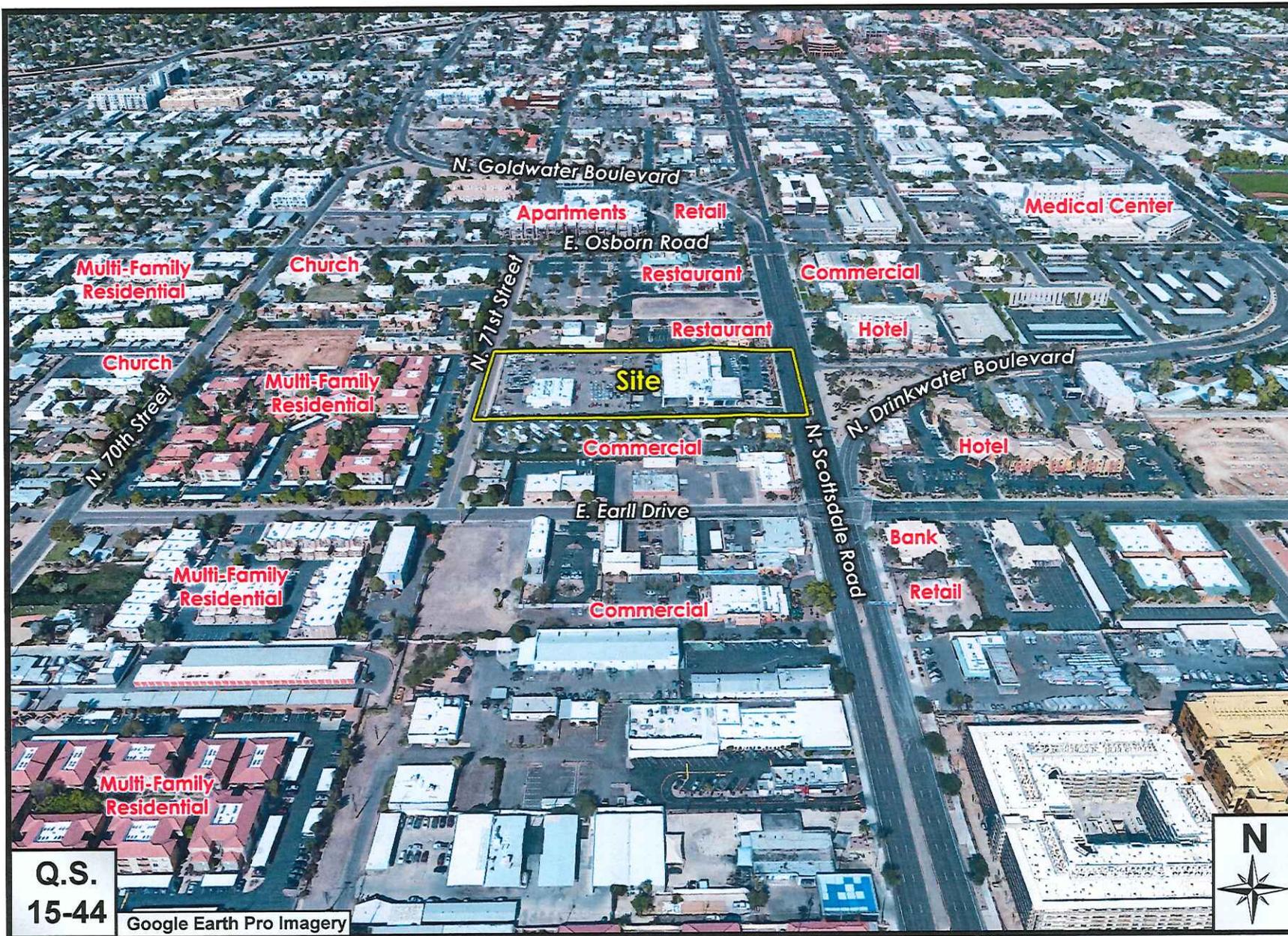
I. *Off-street parking.*

1. The provisions of Article IX. shall apply.

2. Vehicle parking is prohibited in the required setback specified in Table 5.3006.C.
3. The underground portion of a parking structure may be built to the property line.
4. A development with dwelling units that is required to provide:
  - a. Fifty (50) to two hundred (200) parking spaces for the dwelling units, shall provide at least ninety (90) percent of those parking spaces in a parking structure, podium parking, or tuck-under parking.
  - b. Two hundred one (201) or more parking spaces for the dwelling units, shall provide at least ninety (90) percent of those parking spaces in a parking structure, excluding podium parking and tuck-under parking.
5. The Development Review Board may approve an above-ground parking structure, podium parking and tuck-under parking adjacent to a public street if it finds that such parking conforms to the Downtown Plan and Downtown Plan Urban Design & Architectural Guidelines.

*J. Landscaping.*

1. The provisions of Article X. shall apply.



Q.S.  
15-44

Google Earth Pro Imagery

Alta Osborn

12-ZN-2016

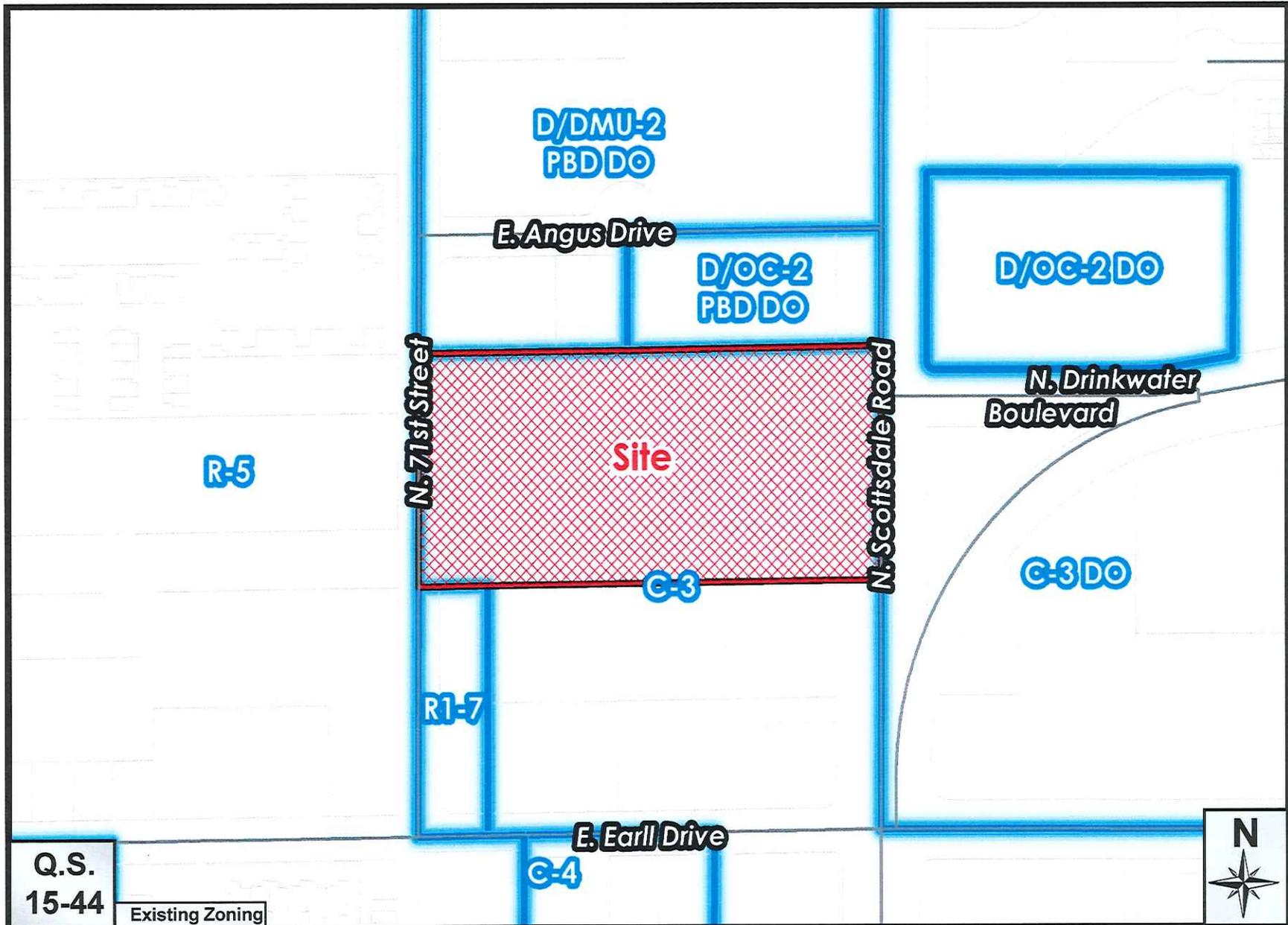


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Google Earth Pro Imagery

Alta Osborn

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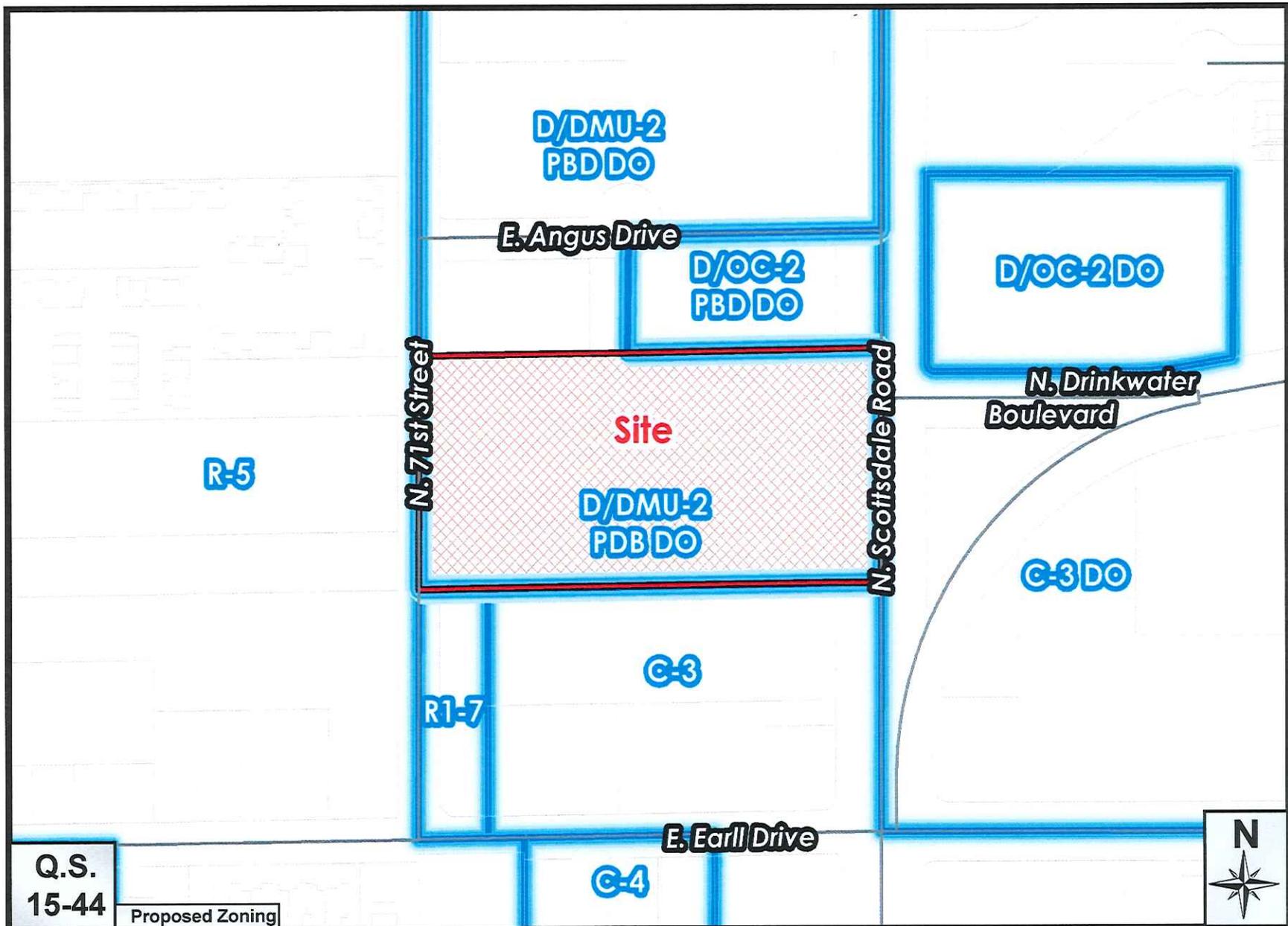


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Existing Zoning

Alta Osborn

12-ZN-2016

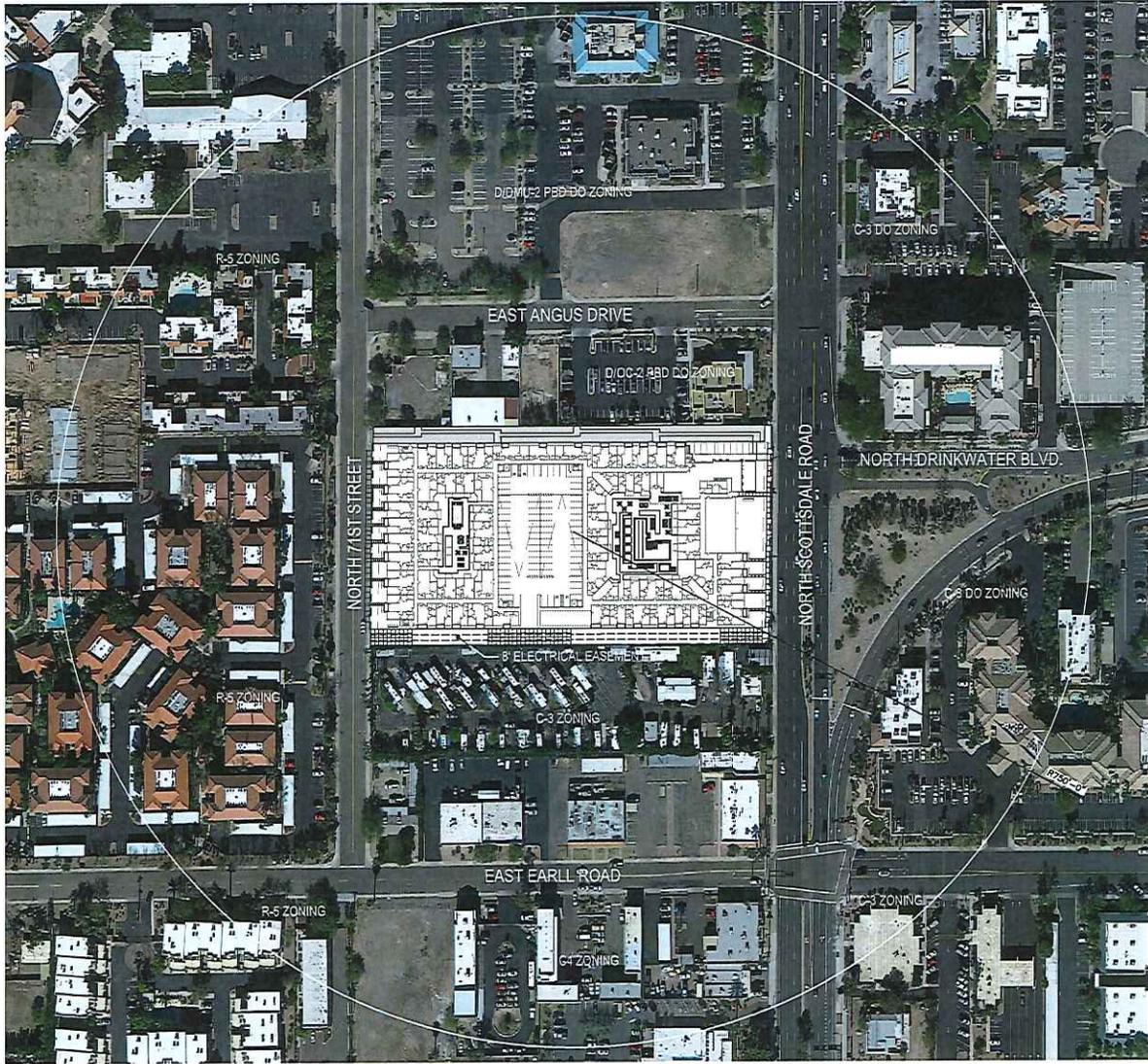


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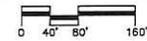
Proposed Zoning

Alta Osborn

12-ZN-2016



# ALTA OSBORN WOOD PARTNERS

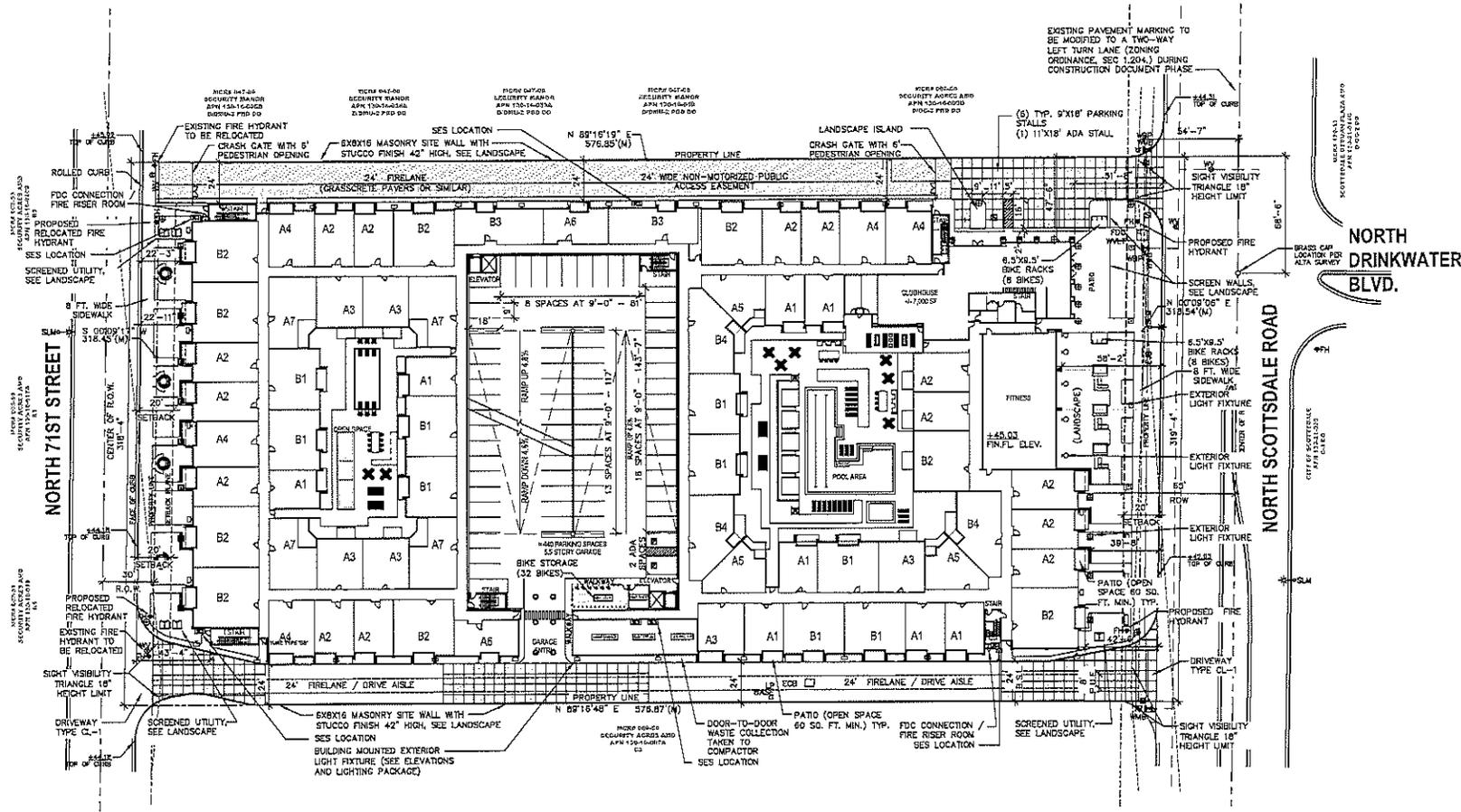


CONTEXT AERIAL | A1.2  
GRAPHIC SCALE | 4-26-16

**bil**form  
architecture



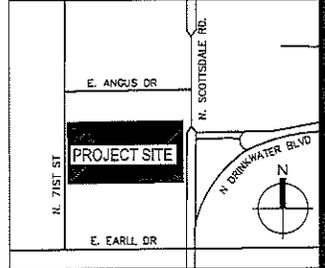
bilform architecture  
GROUP, LLC  
11160 North Central Expressway, Suite 111  
Phoenix, Arizona 85020  
Phone: 602.552.0200  
www.bilform.com



**PROJECT DATA**

SITE AREA GROSS:	210,779.22 S.F. / 4.839 AC.
SITE AREA NET:	183,705.29 S.F. / 4.217 AC.
ZONING:	EXISTING: C-3 PROPOSED: D/DMU-2 PBD 00
APN NO.:	130-16-006A
PROPOSED USE:	MULTI-FAMILY RESIDENTIAL
BUILDING HEIGHT MAX. ALLOWABLE:	85 FEET
BUILDING HEIGHT PROVIDED:	56 FEET
PRIVATE OPEN SPACE PER UNIT TYPE:	60 S.F. MIN. (6 FOOT CLEAR DEPTH X 10 CLEAR WIDTH)
NUMBER OF RESIDENTIAL UNITS:	
ONE BEDROOM UNITS:	172 (60%)
TWO BEDROOM UNITS:	92 (37%)
THREE BEDROOM UNITS:	13 (3%)
TOTAL:	277 D.U.
DENSITY:	57.24 D.U. PER GROSS ACRE
	277 UNITS/4,839 GROSS LOT AREA
GROSS FLOOR AREA PROVIDED:	425,652 S.F.
BIKE PARKING REQ'D:	1 SPACE PER 10 VEH. SPACES 447 P.S./10 = 45 SPACES
BIKE PARKING PROVIDED:	48 SPACES
OPEN SPACE PROVIDED:	COURTYARDS: 18,063 S.F. SCOTTSDALE ROAD FRONTAGE: 9,453 S.F. 71ST STREET FRONTAGE: 6,292 S.F.
TOTAL:	33,778 S.F.
PARKING REQ'D:	(172) 1 BED UNITS X 1 = 172 P.S. (105) 2/3 BED UNITS X 2 = 210 P.S.
TOTAL:	382 P.S.
PARKING PROVIDED:	OPEN SPACES = 7 P.S. GARAGE/COVERED SPACES = 440 P.S.
TOTAL:	447 P.S.
ACCESSIBLE PARKING REQ'D:	4% X 447 P.S. = 18 P.S.
ACCESSIBLE PARKING PROVIDED:	OPEN = 1 P.S. COVERED = 17 P.S.
TOTAL:	18 P.S.

**VICINITY MAP**



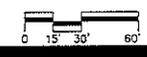
- SHEET NOTES:**
- (a) UNOBSTRUCTED VERTICAL CLEARANCE MIN. 13'-6" (ORD. 4045, 503.2.1)
  - (b) KEY SWITCH/PRE-EMPTION SENSOR REQUIRED. (ORD. 4045, 503.6.1)
  - (c) FIRE LANE SURFACE SHALL SUPPORT 83,000 LBS CVW. (OS&PM, 2-1.802(3))
  - (d) NO FENCE/WALL OVER 6' IN HEIGHT.
  - (e) SPRINKLER SYSTEM TO BE INSTALLED IN ACCORDANCE TO NFPA 13.



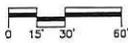
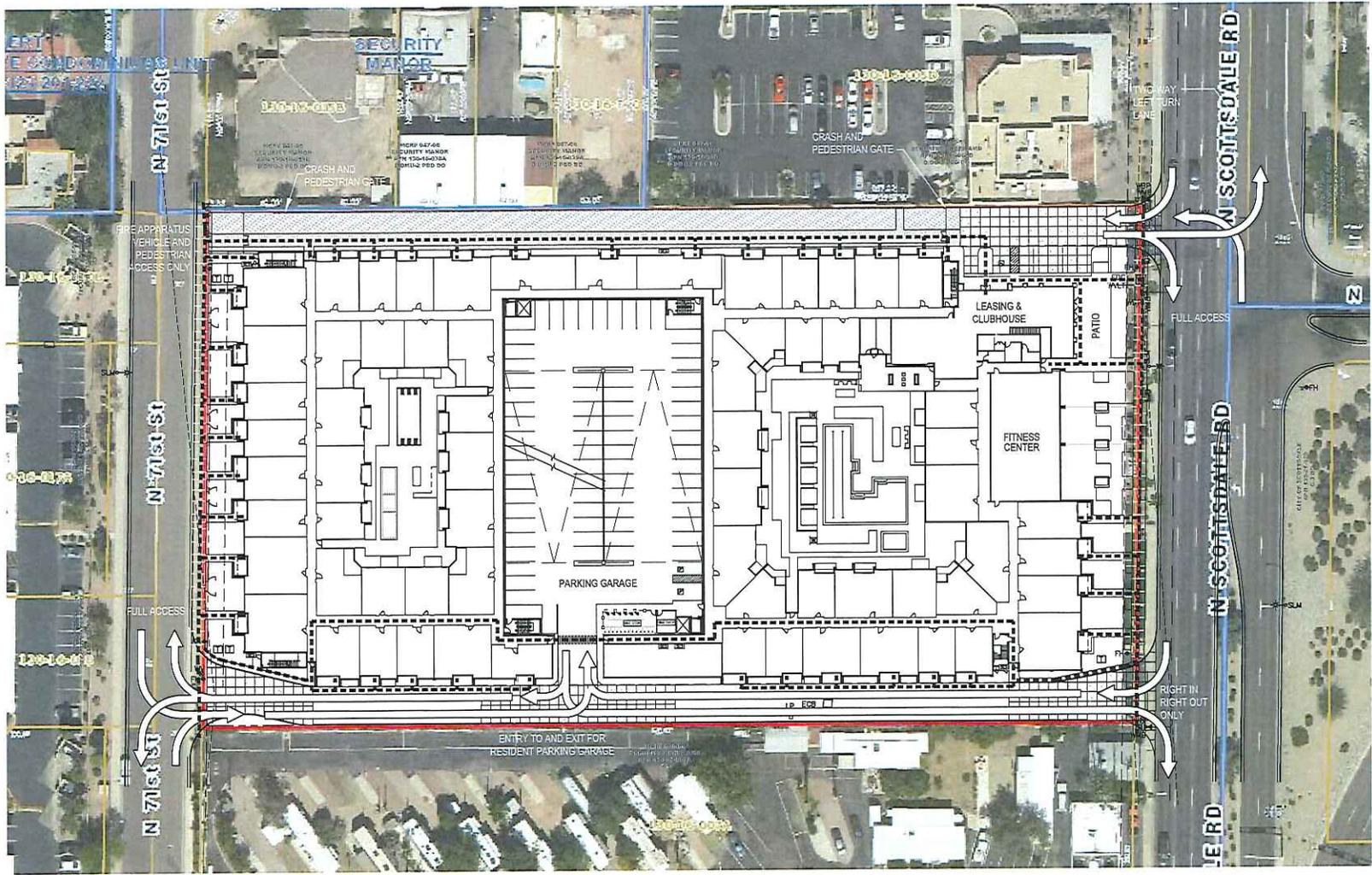
biliform architecture  
Group, LLC  
15800 N. 20th Ave, Suite 100, Scottsdale, AZ 85259  
Phone: 480.342.0000  
www.biliform.com



**ALTA OSBORN  
WOOD PARTNERS**



**SITE PLAN A1.3**  
SCALE: 1"=30'-0"  
6-22-16



# ALTA OSBORN WOOD PARTNERS

PEDESTRIAN AND VEHICULAR  
CIRCULATION PLAN  
GRAPHIC SCALE

**A1.6**  
6-22-16



billform architecture  
group, LLC  
11400 North 25th Street, Suite 111  
Phoenix, Arizona 85020  
Phone: 602.281.0020  
www.billform.com

KEYNOTES

- (A) 6'(W) PEDESTRIAN GATE
- (B) 6'(H) CRASH GATE
- (C) BENCH SEATING
- (D) MASONRY TREE PLANTER
- (E) PARKING SCREEN WALL
- (F) PERIMETER WALL
- (G) PRECAST CONCRETE PLANTER & SEATING



Scottsdale Road

ATTACHMENT #8



71st Street



billform architecture  
 Group, LLC  
 11450 North Central Expressway, Suite 11  
 Phoenix, Arizona 85020  
 Phone: 602.966.0000  
 www.billform.com

# ALTA OSBORN

## WOOD PARTNERS

Streetscape Elevations date: 06.21.16

**andersonbaron**  
 plan · design · achieve  
 50 N. McClintock Drive, 1101  
 Chandler, Arizona 85226  
 ph. 480.699.7956 f. 480.699.7986



- KEYNOTES**
- A** 6'(W) PEDESTRIAN GATE
  - B** 6'(H) CRASH GATE
  - C** BENCH SEATING
  - D** MASONRY TREE PLANTER
  - E** PARKING SCREEN WALL
  - F** PERIMETER WALL



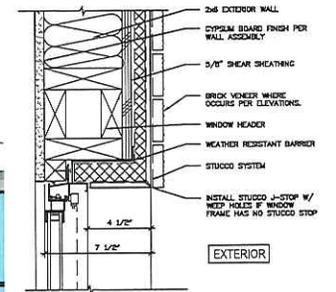
# ALTA OSBORN WOOD PARTNERS

Streetscape Elevations  
Clubhouse Enlargement

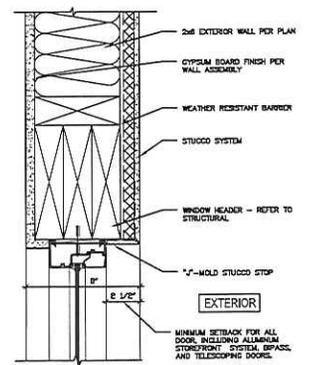
date: 06.21.16  
 **andersonbaron**  
 plan · design · achieve  
 50 N. McClintock Drive, Ste. 110  
 Chandler, Arizona 85226  
 ph. 480.699.7956 f. 480.699.7986



bilitform architecture  
 GROUP, LLC  
 11400 North Central Expressway, Suite 111  
 Phoenix, Arizona 85028  
 phone: 602.281.0200  
 www.bilitform.com



RECESSED WINDOW HEADER  
3"=1'-0"



RECESSED DOOR HEADER  
3"=1'-0"

**NOTES**  
ALL FRAME-OUTS WALL PROJECTIONS ARE TAKEN FROM CORRESPONDING ELEVATION PLANE MARKED AS +0.

LOCATION OF ENLARGED AREAS

# ALTA OSBORN

## WOOD PARTNERS

TYPICAL ELEVATION WORKSHEET **A2.3**  
NOT TO SCALE 6-22-15

**biliform**  
architecture

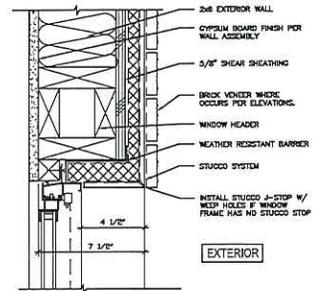
biliform architecture  
group, LLC  
11540 north cove creek road, suite 11  
placentia, california 92670  
Phone: (949) 220-0000  
www.biliform.com



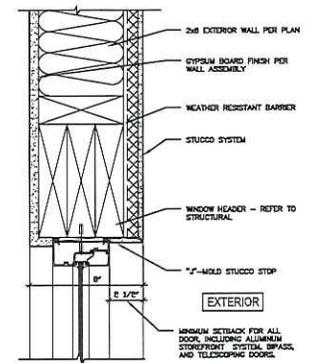
LOCATION OF ENLARGED AREAS

# ALTA OSBORN

## WOOD PARTNERS



RECESSED WINDOW HEADER  
3\"/>



RECESSED DOOR HEADER  
3\"/>

**NOTES**  
ALL FRAME-OUTS WALL PROJECTIONS ARE TAKEN FROM CORRESPONDING ELEVATION PLANE MARKED AS +0.

TYPICAL END ELEVATION WORKSHEET **A2.4**  
NOT TO SCALE 6-22-16

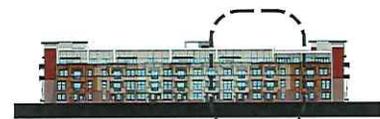
**biliform**  
architecture

biliform architecture  
GROUP, LLC  
11400 north cove creek road, suite 11  
phoenix, arizona 85020  
Phone 602.250.0000  
www.biliform.com



TYPICAL UNITS ON SOUTH SIDE  
SOLAR SHADING - JUNE - 21ST (SOLAR NOON)

NOTES  
ALL FRAME-OUTS WALL PROJECTIONS ARE TAKEN FROM  
CORRESPONDING ELEVATION PLANE MARKED AS +0.



LOCATION OF ENLARGED AREAS  
WEST ELEVATION

# ALTA OSBORN

## WOOD PARTNERS

TYPICAL ELEVATION  
WORKSHEET **A2.5**  
NOT TO SCALE 6-22-16



biliform architecture  
5704 17<sup>th</sup> S.E. LLC  
11410 north cave creek road, suite 11  
plains, alberta T5 0Z0  
Phone: 403.232.0000  
www.biliform.com



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architecture



LOCATION OF ENLARGED AREAS  
EAST ELEVATION

**NOTES**  
ALL FRAME-OUTS WALL PROJECTIONS ARE TAKEN FROM  
CORRESPONDING ELEVATION PLANE MARKED AS +0.

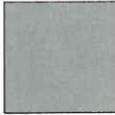
# ALTA OSBORN

## WOOD PARTNERS

**CLUBHOUSE ELEVATION  
WORKSHEET** **A2.6**  
NOT TO SCALE 6-22-16

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GROUP, LLC  
11480 North Cave Creek Road, Suite 11  
Phoenix, Arizona 85020  
Phone: (602) 250-0000  
www.biliform.com

FAWN BRINDLE  
SHERWIN WILLIAMS-SW7640  
BROWNSTONE & CONTEMPORARY  
BODY COLOR



TOWER TAN  
SHERWIN WILLIAMS-SW7704  
BROWNSTONE BODY COLOR



HAPPY TRAILS  
DUNN EDWARDS-DET444  
BROWNSTONE BODY COLOR



SPACIAL WHITE  
SHERWIN WILLIAMS-SW6259  
TRIM COLOR



THUNDER GRAY  
SHERWIN WILLIAMS-SW7645  
DOORS



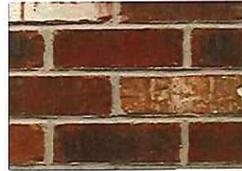
ROOKWOOD RED  
SHERWIN WILLIAMS-SW2802  
ACCENT STAIR TOWER COLOR



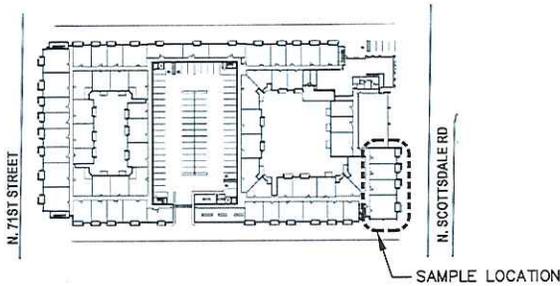
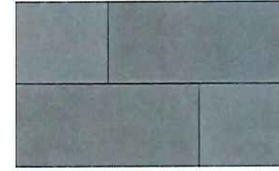
TRICORN BLACK  
SHERWIN WILLIAMS-SW6258  
RAILING & CANOPIES



HERITAGE HILLS  
THINBRICK 451  
PHOENIX BRICKYARD



CUSTOM COLOR  
SMOOTH LIMESTONE  
CORONADO STONE



TRICORN BLACK  
ROOKWOOD RED  
FAWN BRINDLE  
SPACIAL WHITE  
HAPPY TRAILS  
TOWER TAN  
FAWN BRINDLE  
TRICORN BLACK  
THUNDER GRAY  
HERITAGE HILLS



SPACIAL WHITE  
CORONADO STONE  
TRICORN BLACK  
FAWN BRINDLE  
HERITAGE HILLS  
FAWN BRINDLE



biform architecture  
group, LLC.  
11460 north care creek road suite 101  
scottsdale arizona 85250  
phone 602.215.5200  
www.biform.com

# ALTA OSBORN

## WOOD PARTNERS

BUILDING COLOR AND MATERIALS  
SCOTTSDALE ROAD  
NOT TO SCALE

**A2.7**  
6-22-16

ATTACHMENT #9



bilform  
architecture

bilform architecture  
LLC  
15460 north care creek road, suite 11  
phoenix, arizona 85020  
Phone: (602)265-0000  
www.bilform.com

# ALTA OSBORN

WOOD PARTNERS

PERSPECTIVE | A3.1  
SCOTTSDALE ROAD  
NOT TO SCALE | 6-22-16



bltform  
architecture

bltform architecture  
group, llc  
11410 north central expressway, suite 111  
phoenix, arizona 85020  
phone: 602.941.0230  
www.bltform.com

# ALTA OSBORN

## WOOD PARTNERS

PERSPECTIVE | A3.2  
71ST STREET  
NOT TO SCALE | 6-22-16



biltform architecture  
GROUP, LLC  
15410 NORTH CENTRAL EXPRESSWAY, SUITE 111  
PHOENIX, ARIZONA 85020  
PHONE: 480.241.0200  
WWW.BILTFORM.COM

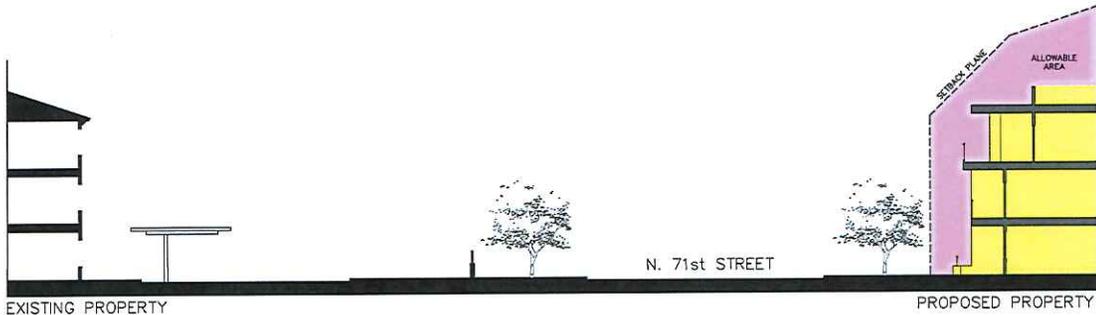
# ALTA OSBORN

## WOOD PARTNERS

PERSPECTIVE  
CLUBHOUSE  
NOT TO SCALE

# A3.3

6-22-16



Ⓐ 71ST STREET SECTION



TRANSITIONAL SITE PLAN

ATTACHMENT #10



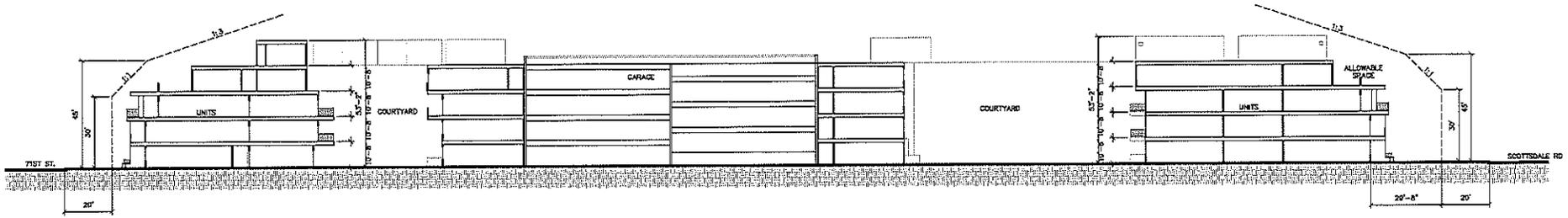
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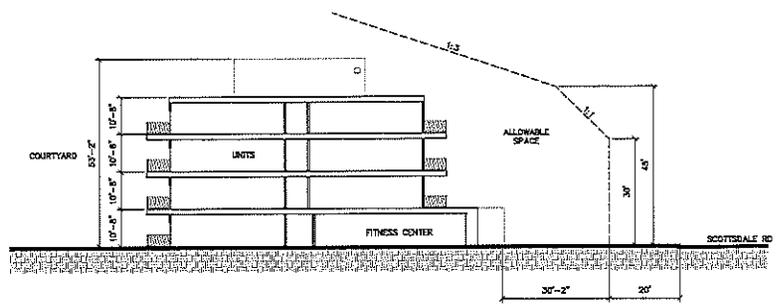
blltform architecture  
group, LLC  
11420 North Cave Creek Road - Suite 11  
Phoenix, Arizona 85026  
Phone 602.283.4200  
www.blltform.com

# ALTA OSBORN WOOD PARTNERS

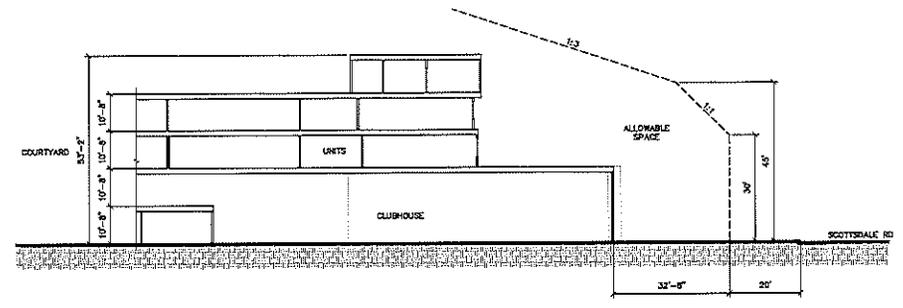
A1.5  
4-25-16



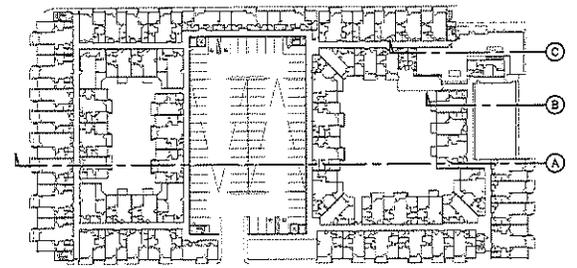
(A) SITE SECTION  
1/8\"/>



(B) SITE SECTION  
1/8\"/>



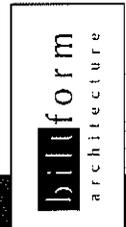
(C) SITE SECTION  
1/8\"/>



KEY PLAN

# ALTA OSBORN WOOD PARTNERS

SITE SECTIONS | A1.4  
AS NOTED | 4-28-16



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11500 North Central Expressway - Suite 111  
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Phone: 602.246.9533  
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