

# DEVELOPMENT REVIEW BOARD REPORT



Meeting Date: October 6, 2016 Item No. 7  
General Plan Element: *Character and Design*  
General Plan Goal: *Foster quality design that enhances Scottsdale as a unique southwestern desert community.*

## **ACTION**

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### **District at the Quarter 8-ZN-2016**

**Location:** 15501 N. 73<sup>rd</sup> Street/Dial Boulevard & 15450 N. Greenway-Hayden Loop

**Request:** Pursuant to the requirements of the Planned Unit Development (PUD) zoning district, Zoning Ordinance Section 5.5003, the applicant is requesting a review by the Development Review Board regarding the Development Plan elements related to design compatibility, environmental responsiveness, solar shading, connectivity and open spaces, and amended development standard to the average setback requirement, and to make a recommendation to the Planning Commission and the City Council for a proposed Planned Unit Development comprised of two buildings with 581,509 square feet of building area containing 622 residential units and 22,025 square feet of non-residential space on 8.84 net acres, as part of an application for a zoning district map amendment from Industrial Park (I-1) district to Planned Unit Development District (PUD).

## **OWNER**

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## **ARCHITECT/DESIGNER**

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Architects Orange

## **ENGINEER**

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Rick Engineering Company

## **APPLICANT CONTACT**

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Dennis Newcombe  
Beus Gilbert PLLC  
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## **BACKGROUND**

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### **Zoning**

The site is currently zoned Industrial Park (I-1) district, which is intended to allow light manufacturing, aeronautical, light industrial, office, and supportive uses to sustain and enhance major employment opportunities.

The City Council, in June 2009, approved the creation of the Planned Unit Development (PUD) zoning district which promotes a mixed-use development pattern along major/minor arterial/collector streets for small- to medium-sized infill sites which are located outside of the Environmentally Sensitive Lands Overlay and the Downtown Area boundary.

The purpose of the request for a zoning district map amendment from I-1 to PUD is to take advantage of development opportunities, afforded by the PUD zoning district, that are not provided in the I-1 district. For example, the existing I-1 zoning designation allows most office uses, aviation uses, and very limited retail and service uses, but prohibits any residential uses. The requested PUD zoning designation allows all the uses that are allowed in the Commercial Office (C-O) and Planned Regional Center (PRC) zoning district, allowing for more development flexibility.

### **Context**

Located at the northeast corner of the intersection of E. Greenway-Hayden Loop and N. 73<sup>rd</sup> Street/Dial Boulevard, the surrounding developments are fairly diverse, including office, aviation, retail, service, and residential.

### **Adjacent Uses and Zoning**

- North Office uses, zoned Industrial Park (I-1) district
- South Office and mixed-use, zoned Industrial Park (I-1) district and Planned Regional Center (PRC) district
- East Office uses, zoned Industrial Park (I-1) district
- West Retail, service, and residential, zoned Highway Commercial (C-3) district and Planned Unit Development (PUD) district

### **Key Items for Consideration**

- Planned Unit Development findings
- Amendment to average setback requirements
- Treatment of signature intersection (Greenway-Hayden & 73<sup>rd</sup> Street)

## **DEVELOPMENT PROPOSAL**

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### **Goal/Purpose of Request**

#### *Request*

The applicant is requesting a recommendation to the Planning Commission on the proposed Development Plan for a Planned Unit Development. In accordance with Zoning Ordinance Section 5.5003.C.1.a., the Development Review Board shall make a recommendation to the Planning Commission based on specific criteria, which is outlined in detail below under Development Review Board Criteria Analysis.

*Proposal*

The owner proposes to demolish the existing office building and associated improvements on the site for the proposed Planned Unit Development comprised of two buildings with 581,509 square feet of building area containing 622 residential units and 22,025 square feet of non-residential space on the 8.84 net acre site. Main vehicular access will be provided through one driveway on E. Greenway-Hayden Loop and another driveway on N. 73<sup>rd</sup> Street. There is a second driveway proposed on N. 73<sup>rd</sup> Street nearest to Greenway-Hayden Loop, which is intended to serve a proposed restaurant and the other non-residential uses which consist of a fitness center, clubhouse and some office space. There are also secondary access points on both E. Greenway-Hayden Loop (southeast corner of site) and N. 73<sup>rd</sup> Street (north west corner of site) for Fire Department access only. These driveways will be gated and only accessible by the Fire Department. Parking for the site is provided in two (2) above-ground parking structures (one for each building) which are completely wrapped and screened by the proposed buildings.

Approximately 21 percent of the site is landscaped with a design appropriate for the Sonoran Desert and compatible with the surrounding area. Low water use, drought tolerant trees, shrubs and groundcovers, accented with more lush materials, flank all sides of the property and the buildings, and line the sidewalks adjacent to the public streets. Buildings have been placed on the site to provide ample landscape buffers to adjacent roadways, and new 8-foot-wide public sidewalks provide pedestrian connections to surrounding retail, restaurant, and office uses. In addition, special focus will be provided at the corner of E. Greenway-Hayden Loop and N. 73<sup>rd</sup> Street, which is designated as a Landmark Intersection in the Greater Airpark Character Area Plan, with enhanced landscaping and pedestrian amenities. The proposed architecture combines stucco, glass, and stone, in a contemporary style which includes varying roof parapets and stoops on select work-live units at the ground level.

**Neighborhood Communication**

The applicant has sent notice to all property owners within 750 feet of the site and held an open house meeting on March 17, 2016 to seek input regarding the proposed project. In addition, the applicant performed door to door outreach in the general vicinity of the proposed project. The City also sent notices to property owners within 750 feet of the site regarding the proposed project. Staff has received a few phone calls and one (1) email inquiry with general questions regarding this project.

**DEVELOPMENT REVIEW BOARD CRITERIA ANALYSIS**

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An application for the PUD District must be accompanied by a Development Plan (DP), the elements of which shall include the following:

1. Character statements, including environmental response, design principles, architectural character, site development character, and landscape character;
2. A master site plan, with a land use budget tabulation, showing the location of development components, the intensity of development, residential density, and building heights;
3. An open space plan;
4. An architectural concepts and design standards plan;
5. Basis of design reports for storm water drainage, water service, and waste water disposal;

and

6. Any additional information as necessary to process the DP such as:
  - a. A view shed analysis plan;
  - b. A pedestrian circulation plan;
  - c. A hardscape plan;
  - d. A landscape plan;
  - e. A lighting plan; and/or
  - f. A signage plan.
- *The applicant's submittal includes a DP which contains all the required elements.*

The Development Review Board shall review the DP elements and make a recommendation to the Planning Commission, based on the following considerations:

1. The design contained in the DP is compatible with development in the area that it may directly affect and the DP provides a benefit to the city and adjacent neighborhoods.
  - *The proposed site design uses existing access points, including two main entrances that are accessed from E. Greenway-Hayden Loop and N. 73<sup>rd</sup> Street. Further, pedestrian connections are being provided from the proposed site to existing properties adjacent to the site, and new 8-foot-wide sidewalks along the public right-of-way, as a direct benefit to the city and adjacent neighborhoods.*
  - *The buildings on the site have been situated to provide appropriate setbacks from public roadways and other existing buildings, and the focus of the restaurant, clubhouse, and other non-residential uses at the corner of E. Greenway-Hayden Loop and N. 73<sup>rd</sup> Street will contribute to activation of the Landmark Intersection.*
  - *All of the proposed parking is located within above-ground parking structures that are fully integrated in to the proposed buildings which will reduce the potential impervious area on the site and eliminate visibility and associated impacts on adjacent properties.*
  - *Proposed building materials and design are similar to existing buildings on neighboring properties, with colors that complement those of existing shopping and office centers that surround the site. Enhanced open space and pedestrian amenities, including shade structures, provided at the Landmark Intersection will benefit the subject property as well as the surrounding neighborhood.*
2. The DP is environmentally responsive, incorporates green building principles, contributes to the city's design guidelines and design objectives, and that any deviations from the design guidelines must be justified by compensating benefits of the DP.
  - *The site is designed to maximize efficient use of space by vertically stacking floor area, rather than spreading it horizontally across the property, which leaves room for more landscaping and several usable open space courtyard areas. Approximately 21 percent of the site is proposed to be landscaping or usable amenity areas.*
  - *All of the parking for the site has been provided in fully integrated structures to minimize impervious surfaces, reduce the heat-island effect, and fully screen from view.*
  - *Site landscaping has been designed to complement a Sonoran Desert theme and*

*implement water conservation measures by strategically locating low water use trees, shrubs, and ground covers creating a lush appearance and cooling environment.*

- *The design of proposed buildings on the site uses effective building techniques, such as solar shading, landscaping, recessed windows, building articulation, material selection and paint colors, to effectively integrate the site with the surrounding area and promote the unique character of the Sonoran Desert. Shade elements are proposed for most of the windows of the proposed buildings.*
3. The DP will not significantly increase solar shading of adjacent land in comparison with a development that could be developed under the existing zoning district.
    - *The proposed development includes a building which is comparable in height to many of the existing buildings in the area, and much shorter than some existing within the adjacent Scottsdale Quarter development. The allowed building height in the requested PUD district (48 feet) is less than what the existing I-1 district (56 feet) allows.*
  4. The DP promotes connectivity between adjacent and abutting parcels, and provides open spaces that are visible from the public right-of-way and useful to the development.
    - *The proposed development will include new 8-foot-wide sidewalks along both adjacent streets, and provides several pedestrian connections to these public sidewalks allowing for easy connectivity to adjacent developments. A landscape buffer averaging 38 feet wide surrounds the development along the public streets.*

**Amended Development Standards**

The Development Review Board may review any proposed amended development standards included in the DP and provide a recommendation to the Planning Commission on the proposed amendments. The applicant is requesting the following amended development standard for consideration:

1. Building Setbacks – Average Setback. The applicant is requesting an amendment to the average setback requirement. The Zoning Ordinance requires minimum and average setbacks as shown in the table below. The applicant is complying with the minimum setbacks, but is requesting to amend the average setback to be 38 feet, on all streets.

Street Frontage and Use	Minimum Setback Required / Proposed	Average Setback Required / Proposed
Greenway-Hayden Loop w/ residential on first floor	34 feet / No change	40 feet / 38 feet
Greenway-Hayden Loop w/ non-residential on first floor	28 feet / No change	32 feet / 38 feet
Dial Boulevard / 73 <sup>rd</sup> Street w/ residential on first floor	25 feet / No change	30 feet / 38 feet

- *The curved alignment of N. 73<sup>rd</sup> Street and E. Greenway-Hayden Loop make it more difficult to achieve the required average building setback. The proposed amendment*

*to the average setback gives the applicant flexibility to allow for additional variation in architectural relief and recesses as well as provide additional landscaping and open space adjacent to the streets.*

**Development Information**

- Existing Use: Office
- Proposed Use: Mixed-use / Residential
- Parcel Size: 10.29 gross acres  
8.84 net acres  
384,837 square feet
- Non-Residential space: 22,025 square feet
- Dwelling unit space (residential): 559,484 square feet
- Building Height Allowed: 48 feet, excluding rooftop appurtenances
- Building Height Proposed: 48 feet, excluding rooftop appurtenances
- Parking Required: 974 spaces
- Parking Provided: 1,002 spaces
- Open Space Required (Common): 44,830 square feet / 1.03 acres
- Open Space Provided (Common): 46,114 square feet / 1.06 acres
- Open Space Provided (Other): 50,288 square feet / 1.15 acres
- Open Space Provided (Total): 141,232 square feet / 3.24 acres
- Number of Dwelling Units Proposed: 622 units
- Density Proposed: 62 dwelling units per acre

**STAFF RECOMMENDATION**

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**Recommended Approach:**

Staff recommends that the Development Review Board forward to the Planning Commission a recommendation for approval of the Development Plan and requested amended development standard for The District at the Quarter, finding that the Development Review Board Considerations of the Planned Unit Development district have been met.

**Proposed Next Steps:**

The minor General Plan Amendment and Zoning District Map Amendment are scheduled for a hearing by the Planning Commission on October 19, 2016.

**RESPONSIBLE DEPARTMENT**

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**Planning and Development Services**

Current Planning Services

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**APPROVED BY**

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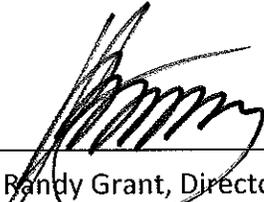
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**ATTACHMENTS**

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1. Context Aerial
- 1A. Close-Up Aerial
2. Development Plan



District at the Quarter

8-ZN-2016



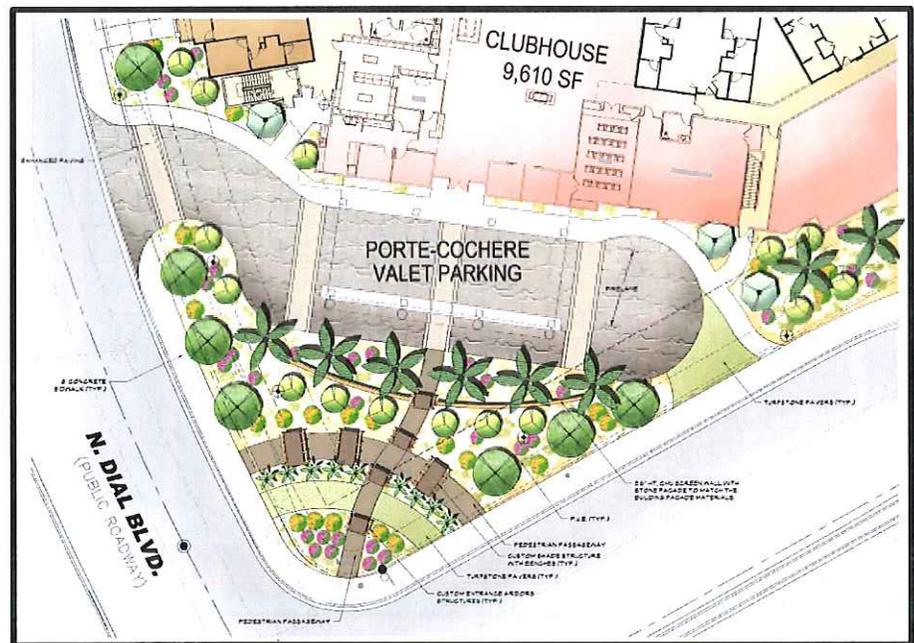
District at the Quarter

8-ZN-2016

# District at the Quarter

## Planned Unit Development & Non-Major General Plan Amendment Narrative

Northeast corner of Greenway-Hayden Loop & 73<sup>rd</sup> Street



Cases: 3-GP-2016 & 8-ZN-2016

1<sup>st</sup> Submittal: March 30, 2016

2<sup>nd</sup> Submittal: June 14, 2016

3<sup>rd</sup> Submittal: August 5, 2016

4<sup>th</sup> Submittal: September 2, 2016

# Project Narrative/Development Plan

## Non-Major General Plan Amendment & Rezoning for District at the Quarter

Case #'s:  
3-GP-2016 & 8-ZN-2016

Location: 15501 North 73<sup>rd</sup> Street

Request for a Non-Major General Plan Amendment from Airpark Mixed Use (AMU) and a small portion of Employment (EMP) to Airpark Mixed Use- Residential (AMU-R)

- and -

Rezoning from Industrial Park (I-1) to Planned Unit Development District (PUD)

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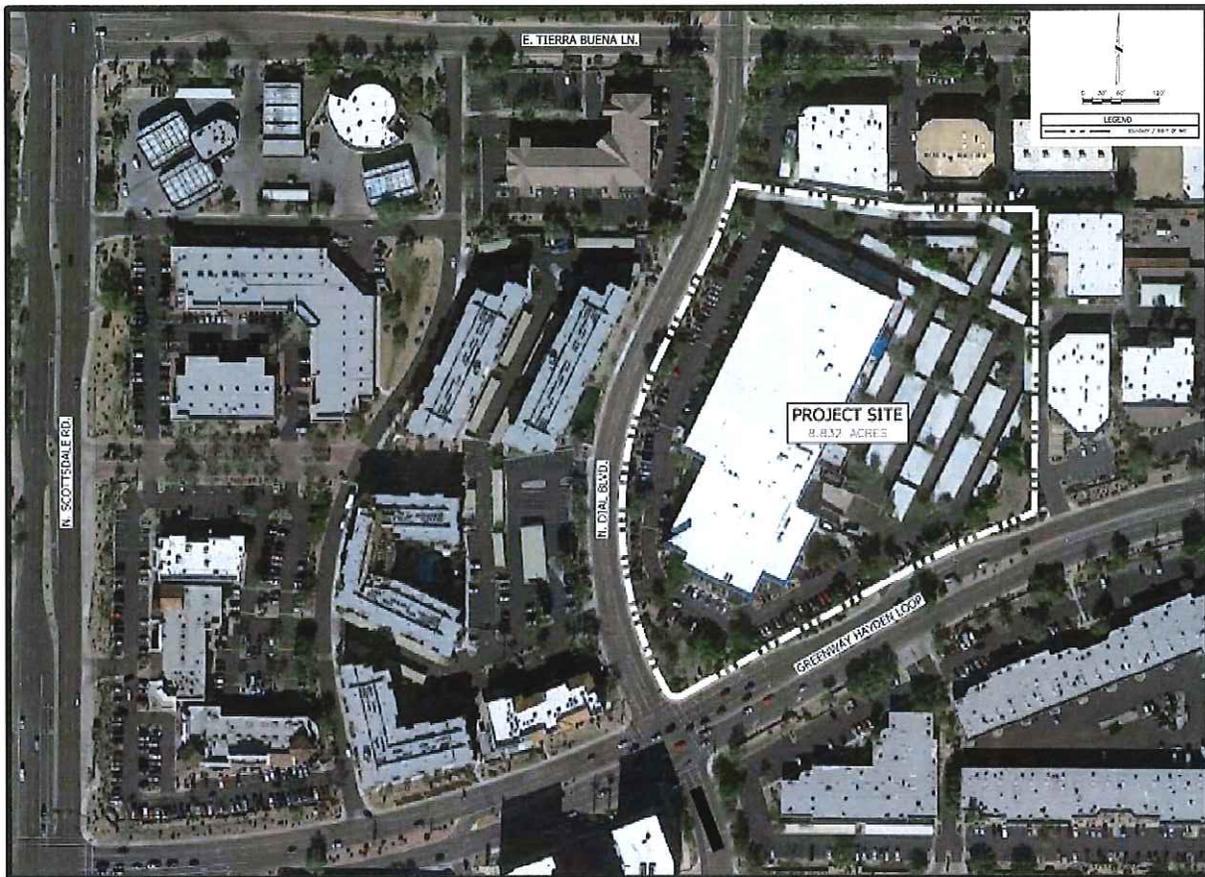
**Exhibits Continued....**

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**I. Purpose of Request:**

This request is for a Non-Major General Plan Amendment (GPA) from Airpark Mixed Use (AMU) and a small portion of Employment (EMP) to Airpark Mixed Use-Residential (AMU-R) and a rezoning from Industrial Park (I-1) to Planned Unit Development (PUD) zoning district on 10.29+/- gross acre and 8.83+/- net acre site located at 15501 North 73<sup>rd</sup> Street (a.k.a. Dial Boulevard) (the "Site"). The applicant intends to create a small mixed-use luxury residential multi-family community of approximately 622 units. (See below and also: *Dimensioned Plan Boundaries Exhibit # 1 & Aerial Exhibit # 2*)

**AERIAL**



**II. Kaplan Management Company**

The applicant, Kaplan Management Company ("Kaplan") was formed in 1978 and is based out of Houston, Texas. Kaplan is the diversified operator, owner, and developer of high quality multi-family properties in major metropolitan areas across the United States. Kaplan is active in emerging high growth submarkets, redeveloping existing multi-family housing, and repurposing commercial developments into residential communities, and holds 36 years of successful

management experience in the dynamic and evolving multi-family industry. Kaplan would be honored to bring their experience and success to this particular location in Scottsdale.

### **III. District at the Quarter (The Proposal)**

The District at the Quarter residential community will provide an exciting mixed-use project featuring upscale residences including ground level live/work units and retail/commercial (i.e. non-residential) components that blends and transitions with the retail/commercial businesses strategically located along with employment entities within the Scottsdale Airpark, which is the second largest employment center in the state of Arizona. The proposed development would further many of the goals set forth in the Greater Airpark Character Area Plan (“GACAP”) by creating a dynamic walkable community based on live, work, and play principles.

The Site is a transitional piece that defines the boundary between employment and retail/commercial uses within Scottsdale Airpark. This important and strategic location is bounded by two (2) Signature Corridors which also forms a Landmark Intersection as defined within the GACAP. As such, this Site will provide an appropriate level of mixed-use and residential live, work, and play community that will transition, activate and define the shopping/retail activities to the west/southwest, employment sector to the east, and formalize/enhance the Signature Corridors (i.e. Greenway-Hayden Loop and 73<sup>rd</sup> Street). The transition and addition of this mixed-use proposal at this location will help to continue enhancing the desirability of this location and the surrounding context. The retail/commercial (i.e. non-residential) and residential mix proposed will provide an ideal fit for this location and beyond. The natural progression of uses and development along Scottsdale Road, 73<sup>rd</sup> Street, and throughout the Airpark will continue draw people and activity to this location from surrounding areas, which in turn will build upon the placemaking and character that is envisioned at this Landmark Intersection.

The Airpark as a whole creates a unique opportunity to blend interrelated land uses and promote the live, work, and play concept, which is memorialized numerous times within the GACAP. Additionally, the nearby retail and surrounding employment core provides regional appeal for future owners of the District at the Quarter. The ease of accessibility, proximity to businesses, shopping, entertainment, recreation, and airport makes this development a compliment to its surroundings.

## CONTEXT AERIAL



### a) Existing Conditions & Context

The Site constitutes 10.29 gross acres and bordered by Greenway-Hayden Loop on the west and 73rd Street (a.k.a. Dial Boulevard) to the south. Existing businesses (I-1) border the Site to the north, east, and south. The west side is bordered by existing commercial businesses (C-3) and a recently completed multi-family development (PUD). (See above and also: *Context Aerial # 3*)

The redevelopment parcels contain an office building currently occupied by "International Cruise & Excursions" ("ICE"), a travel and vacation service company. The Site contains covered and uncovered parking surrounding the main building structure. The streets, sidewalks, and utilities surrounding the Site have all previously been built, as such this redevelopment project can be "plugged in" to this location with minimal disturbances to the area.

### b) Site Plan

The design for this Site features an enhanced Landmark Intersection including an enhanced public open space, pedestrian passageway, and rest area inviting public activity and interaction provided by shaded rest areas, enhanced paving, a variety of meandering pathways, and landscaping. The Landmark Intersection of the Site is activated with ground level uses anchored by an approximately 7,855 sq. ft. clubhouse that provides services and amenities such as: porte-

cochere/valet on-site drop-off dry cleaning service, business center, restaurant, office space, fitness center, barista station, etc. The Signature Corridor of Greenway-Hayden Loop will also feature an approximately 5,354 sq. ft. fitness center located on the ground level facing Greenway-Hayden Loop along with seven (7) live/work units located on both Greenway-Hayden Loop and 73<sup>rd</sup> Street.

A main vehicular access drive bisects the Site and allows for two (2) vehicular entry connections located at the mid-block of 73rd Street and Greenway-Hayden Loop. Additionally, this vehicular access drive will serve as access to the internal parking garages and provide for fire lane emergency access. The secondary fire lane/enhanced setback along the perimeter of the Site will be gated and serve dual purposes (i.e. building setback, vehicular pavers, landscaping, turfstone, dog walk/wash areas, walking area, and fire safety access). The site design consists of two (2) (i.e. 2 phases) 4-story luxury residential buildings that enclose a total of five (5) interior courtyards, featuring various amenities, open space, and landscaping. These outdoor activity areas provide for both active and passive recreation, relaxation, and an abundance of open space for the future residents. The main building structures are navigated through a series of pedestrian passages that lead throughout the Site and ultimately to the outer pedestrian sidewalk network and the surrounding area. The north and east boundaries of the Site are nicely buffered by a 27'-40' building setback, vehicular pavers, landscaping, turfstone, dog walk/wash areas, walking area, and fire safety access from the proposed units. (See *Site Plan Exhibit #4*)

### **c) Retail/Commercial (Non-Residential) Component**

The retail/commercial (i.e. non-residential) component of this mixed-use development is strategically located at the Landmark Intersection of the Site in the form of a highly amenitized clubhouse (7,855 sq. ft.), restaurant (7,035 sq. ft.), office space (1,781 sq. ft.), and fitness center (5,354 sq. ft.) that will continue along Greenway-Hayden Loop with seven (7) live/work units proposed as well along both Greenway-Hayden Loop and 73<sup>rd</sup> Street. This project provides design and uses that cater to the pedestrian at the ground level particularly at the Landmark Intersection, which supports pedestrian interest and walking as well as enhancing the streetscape. These uses will be also open to the public that will generate additional interest and vibrancy.

The amount of retail/commercial (i.e. non-residential) proposed is adequate and supported within the context of the existing overall retail/commercial in the surrounding area. There is already a sufficient amount of retail/commercial in this area, and proposed in the future, that an increase in residential density is important towards balancing and supporting these uses as well as live, work, and play lifestyle concept. It is worth noting that there is currently 116,000 sq. ft. of retail and 250,000 sq. ft. of office space within the Scottsdale Airpark North; which this development would be a part of. Within walking distance of the Site, there is: 437,000 sq. ft. at Kierland Commons, 400,000 sq. ft. at Scottsdale Quarter, 700,000 at Scottsdale Promenade, and 100,000 at Zocallo. Thus, if one considers a modest 5% vacancy rate, which is typical, then that would equate to nearly 100,000 sq. ft. of vacant retail space, which at this time includes neighboring Crescent's ground floor space being vacant. To that end, the amount of retail/commercial proposed with this development (i.e. 22,025 sq. ft.), we believe, is more than adequate to properly serve the future

residents of this development and to provide a proper balance with the existing and future population, employment, and retail base within this area.

**d) Building Architecture**

The building architecture features varied massing, varying roof parapets, architectural features, stoops, materials and façade detailing found in many contemporary luxury multi-family developments found in Scottsdale and the Southwest region. The building massing includes a series of towers and recessed facades and patios. The massing and detailing emphasize the promotion of pedestrian activity including a walkable street level building design lined with lighting, trees, and shade structures such as: porte-cochere, awnings, and roof overhangs. The color scheme is a blend of shades of brown, white, and tan with some complementary accent colors found within the Sonoran Desert color palette and consistent and appropriate with the surrounding building context. (See *Architectural Plan Set Exhibit #5*)

Maximum building height is 48' with all rooftop mechanical equipment and screening, stairwell bulkheads, as well as rooftop deck enclosure walls and fall protection railing that exceed that building height, limited to under 30% total of the roof area for each building. (See *Building Cross Sections, Height Diagrams, and Height Exception Area Exhibit #8*)

**e) Landscape Theme**

The project's landscape theme will reduce overall water intake include by utilizing native trees, shrubs, accent plants, groundcover, and minimal turf. The two (2) Signature Corridors (i.e. Greenway-Hayden Loop and 73<sup>rd</sup> Street) abutting the Site has sidewalks lined with trees and shrubs providing a pleasant/cool environment to walk. The main vehicular access drive and open space areas will also contain a variety of native shrubs and landscaping providing for a friendly, enjoyable, useable, and shaded environment for residents to traverse and use the property. As such, the plan includes a total of five (5) internal courtyards which all feature a variety of landscaping and amenities such as a swimming pool, fountains, fire pit, putting green, televisions, barbeques, and shaded seating and dining areas which are all connected by a system of winding walkways. (See *Landscape Plan Set Exhibit # 6*)

**IV. The Development Plan (DP)**

Pursuant to the City's Zoning Ordinance for PUD rezoning requests the following four (4) items to be addressed within the required Development Plan ("DP"). They are:

- (1) The design contained in the DP is compatible with development in the area.

The Site sits on the northeast corner of Green Hayden Loop and 73<sup>rd</sup> Street. The proposal is for a planned, medium density, mixed-use apartment project with an appropriate/sustainable level of supporting retail/commercial (non-residential) situated at this Landmark Intersection. The clubhouse and adjoining fitness center as well the option of live/work units will also help to

residents of this development and to provide a proper balance with the existing and future population, employment, and retail base within this area.

**d) Building Architecture**

The building architecture features varied massing, varying roof parapets, architectural features, stoops, materials and façade detailing found in many contemporary luxury multi-family developments found in Scottsdale and the Southwest region. The building massing includes a series of towers and recessed facades and patios. The massing and detailing emphasize the promotion of pedestrian activity including a walkable street level building design lined with lighting, trees, and shade structures such as: porte-cochere, awnings, and roof overhangs. The color scheme is a blend of shades of brown, white, and tan with some complementary accent colors found within the Sonoran Desert color palette and consistent and appropriate with the surrounding building context. (See *Architectural Plan Set Exhibit #5*)

Maximum building height is 48' with all rooftop mechanical equipment and screening, stairwell bulkheads, as well as rooftop deck enclosure walls and fall protection railing that exceed that building height, limited to under 30% total of the roof area for each building. (See *Building Cross Sections, Height Diagrams, and Height Exception Area Exhibit #8*)

**e) Landscape Theme**

The project's landscape theme will reduce overall water intake include by utilizing native trees, shrubs, accent plants, groundcover, and minimal turf. The two (2) Signature Corridors (i.e. Greenway-Hayden Loop and 73<sup>rd</sup> Street) abutting the Site has sidewalks lined with trees and shrubs providing a pleasant/cool environment to walk. The main vehicular access drive and open space areas will also contain a variety of native shrubs and landscaping providing for a friendly, enjoyable, useable, and shaded environment for residents to traverse and use the property. As such, the plan includes a total of five (5) internal courtyards which all feature a variety of landscaping and amenities such as a swimming pool, fountains, fire pit, putting green, televisions, barbecues, and shaded seating and dining areas which are all connected by a system of winding walkways. (See *Landscape Plan Set Exhibit # 6*)

**IV. The Development Plan (DP)**

Pursuant to the City's Zoning Ordinance for PUD rezoning requests the following four (4) items to be addressed within the required Development Plan ("DP"). They are:

- (1) The design contained in the DP is compatible with development in the area.

The Site sits on the northeast corner of Green Hayden Loop and 73<sup>rd</sup> Street. The proposal is for a planned, medium density, mixed-use apartment project with an appropriate/sustainable level of supporting retail/commercial (non-residential) situated at this Landmark Intersection. The clubhouse and adjoining fitness center as well the option of live/work units will also help to

continue to build upon the live, work, and play concept in this area by providing for amenities and recreation, business needs, etc. at the pedestrian environment subsequently anchored by an enhanced Landmark Intersection with seating, special paving, and landscaping. The enhanced pedestrian Landmark Intersection and the services proposed at this corner (i.e. clubhouse with porte-cochere/valet restaurant, office space, fitness center, etc.) will further promote the lively street scene expected at this Landmark Intersection and along the two (2) Signature Corridors (i.e. Greenway-Hayden and 73<sup>rd</sup> Street), which will continue/enhance the activity in this area and create a synergetic node.

The proposed project is compatible with and contributes to its surrounding uses, which has evolved towards a more active, dynamic, and vibrant corner. The proposed development by Kaplan, as part of this Landmark Intersection, and together with the existing mixed-use corners adjacent, creates the desired effect envisioned for this Landmark Intersection and along these two (2) Signature Corridors, which are: transitioning development away from Scottsdale Road, attracting new development into the airpark, connecting pedestrians without the use of cars, internalizing/structured parking, creating an identity/gateway, and increasing the critical mass of people to support the businesses in the area on a daily/nightly basis. There are two (2) adjacent recent mixed-use developments that help to complete two (2) of the corners of this Landmark Intersection which provide for residential living within a mixed-use environment while being within a different context/lifestyle of living. These developments are concentrated at the corners and help to complete the Landmark Intersection. Crescent Scottsdale Quarter is a 5-story apartment complex over supporting retail uses within the successful Scottsdale Quarter. The other is immediately to the west across 73<sup>rd</sup> Street called Liv North Scottsdale, which is approximately the same height of 48'. The proposed development by Kaplan and these two (2) other projects are responding to the demand for housing to support the surrounding retail/commercial/employment uses. The proposed development plan of approximately 60.4 du/ac would be of comparable intensity by providing a density option roughly within the middle of Liv North and Crescent (i.e. if we focus only on the ground area devoted to the residential use). With that being said, the District at the Quarter complies in overall height, setbacks, high quality architecture, and pedestrian/vehicular connectivity to seamlessly blend within the area, but also a unique residential development option.

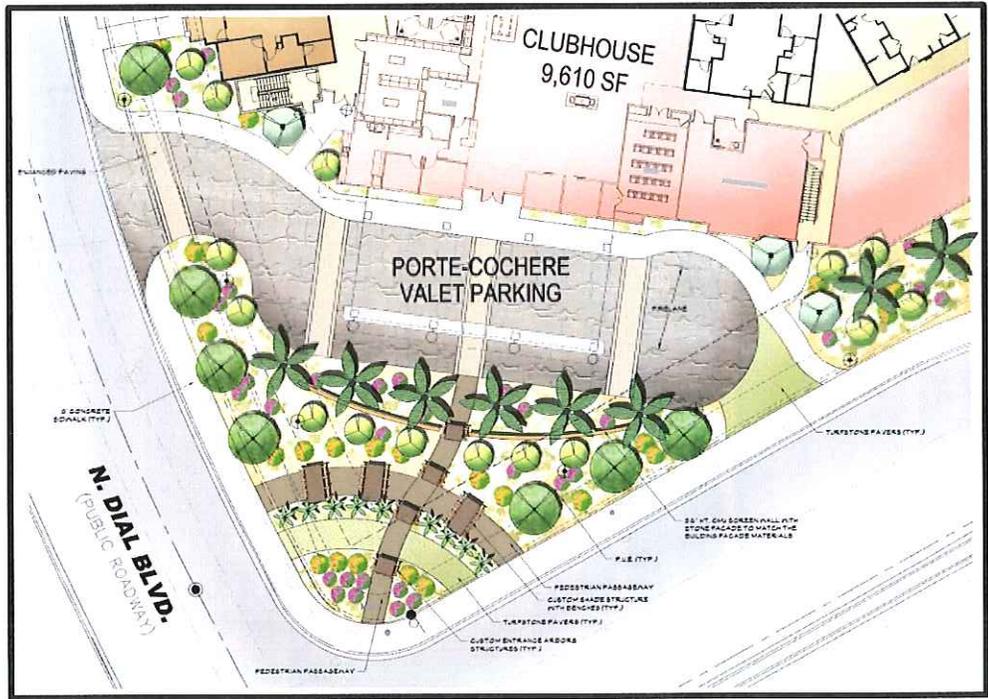
The project construction will be built in two (2) phases. (See *Phasing Plan Exhibit # 7*) The anticipated timing of construction for Phase One is expected to begin in 2017 with Phase Two anticipated to begin approximately 18-22 months later or approximately by early 2019. Phase One of construction will include the majority of non-residential floor area uses and provide for the enhanced public pedestrian area associated at the Landmark Intersection. (See next page and also: *Corner Landmark Intersection Enlargement within the Landscape Plan Set Exhibit # 6*) Phase Two will complement the first and continue the dynamic vision of the project, as well as supporting the development standards of the PUD zoning. With the public amenities and non-residential floor area uses being completed in the Phase One, it ensures that the intent of the PUD, General Plan, and Character Area Plan will be met and provide a substantial public benefit along with implementing the components of a lively mixed-use area and identifiable node.

There are five (5) total usable open space areas integrated into the DP that continue the theme of the surrounding area, which include the main swimming pool. The swimming pool is the largest open space area being approximately 12,000 sq. ft. and the smallest internal usable open space being approximately 2,600 sq. ft. It should also be noted that a dog walk/wash areas will be provided abutting the perimeter of the property (i.e. Phase Two). Finally, the entire Site is connected by winding walkways which link up to public sidewalks, bike paths, and ultimately to the surrounding area.

- (2) The DP is environmentally responsive, incorporates green building principles, contributes to the city's design guidelines and design objectives, and that any deviations from the design guidelines must be justified by compensating benefits of the DP.

The proposed development is environmentally responsive and also provides exceptional public benefits in many ways by including an enhanced common open space and public pedestrian area at the Landmark Intersection (see below) featuring seated and shaded seating, varied pathway connections, landscaping, pedestrian scaled design, and pedestrian amenities encouraged in the Scottsdale Design Guidelines.

**LANDMARK INTERSECTION: PEDESTRIAN REST AREA AND PASSAGEWAY**



The five (5) usable common open space areas are strategically located throughout the Site to provide areas for the residents to recreate and contemplate. The open space at the Landmark Intersection includes approximately 4,000 sq. ft. of public usable common. The Site also has a total of 46,114 sq. ft. of total common open space area (excluding street frontage area or

parking lot landscaping) or 10.3% and 96,402 sq. ft. total landscape space provided (including all open space, street frontage, and parking landscape areas) or 21.5% for the Site. It should be noted that both phases of development will exceed the required 10% open space. Again, these open space areas provide areas of contemplation, recreation (passive/active), cooling, and visual interest both on-site and off-site. The landscape palette is in keeping with a Sonoran Desert theme and water conservation measures by strategically locating low water use trees, shrubs, groundcovers, etc. to create a lush appearance that cools the environment both internally and externally (i.e. along the street frontages). Turf is strategically located on-site within a few of the areas designated for internal/activities in order to optimize comfort and use. Finally, the entire Site is interconnected by walkways which link up to public sidewalks and bike paths encouraging multimodal options.

The architectural character is a contemporary design which will utilize the most current building materials to provide for an energy efficient development. The building masses are broken up into two (2) main buildings. The south building which faces Greenway-Hayden Loop is varied in its setback an average of 38' while maintaining the required minimum 34' setback for residential and 28' setback for retail/commercial. Due to 73<sup>rd</sup> Street curving the west façade of this building bows and creates challenges with building design and as such the average setback is 38' while maintaining the required minimum 25' setback. The architectural elevations are broken up vertically by varying the roof lines, alternating between flat parapets and flat roof overhangs, awnings, porte-cochere, fitness center bump out, and live/work units. These architectural treatments help vary the roof lines vertically. Within the flat parapet areas of the building the walls of the building stepback horizontally creating wide recessed areas that provide space for awnings, patios, and visual massing reliefs. This horizontal relief occurs approximately every 100 lineal feet with massing changes, the bump out of the fitness center, live/work units, porte-cochere, and the natural curvature of the streets. The proposed design provides a strong base with a material change from stucco, limestone, glass, steel awnings, ground floor fitness center, live/work units and stoops located in strategic locations to "ground" the design. The midsection and top utilized the same materials with the top units accentuated by patio areas as well as bulkhead areas protruding towards the top of the buildings to provide access to the roof decks for those units along with alternating flat parapets and roof overhangs. The overarching intent is to create a compatible development design for the area while also being a unique, high quality, and visually appealing for one to want to live and play while being in close proximity to work, entertainment, restaurants, etc. thus reducing traffic and pollution while also maintaining long term economic success.

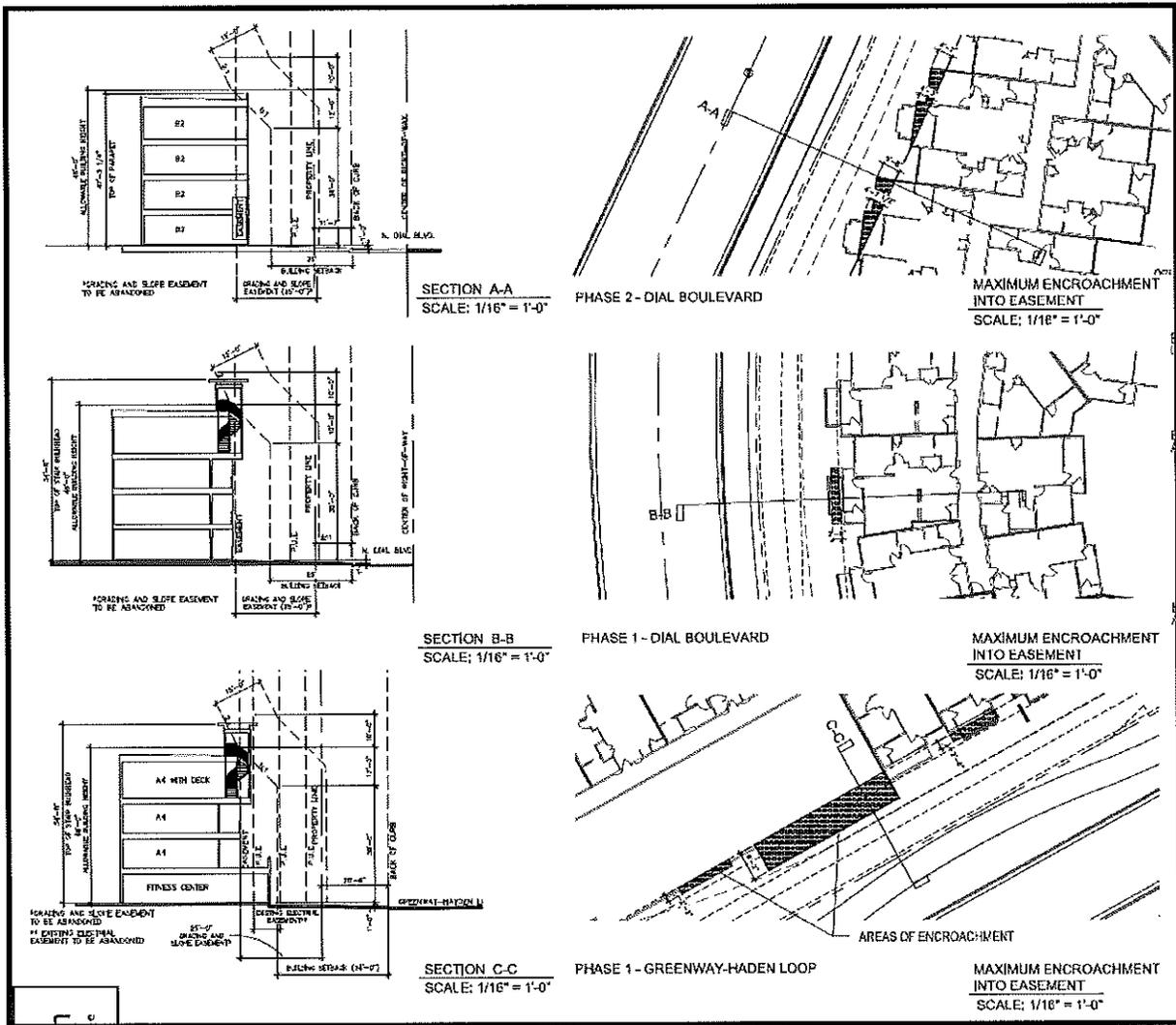
➤ **Building Stepback Deviation**

The District at the Quarter development proposes a deviation from the city's building stepback provision due to the surrounding context (i.e. Liv North Scottsdale, Crescent Scottsdale Quarter, and large streets abutting the Site). There are only a few locations along Greenway-Hayden Road and 73<sup>rd</sup> Street where the bulkhead feature containing a staircase to the upper floor roof deck area for that particular unit or roof parapet penetrates the required stepback. Thus, this minimal encroachment into the building stepback provision only occurs above the

3<sup>rd</sup> story and minimally towards the top of the roof, which is predominately the bulkhead area that provides access to the roof deck for individual units as well as the screen walls and fall protection railing for those roof decks.

The other instance is the roof parapet not meeting the stepback provision along 73<sup>rd</sup> Street, which is due to the curve in the street. Section A-A, Section B-B, and Section C-C are three (3) particular instances illustrated on the next page. (See below and also: *Building Cross Sections, Height Diagrams, and Height Exception Area Exhibit #8*)

### BUILDING CROSS SECTIONS



In addition, the following ten (10) design reasons that contribute to supporting the building setback exception proposed along Greenway-Hayden Loop and 73<sup>rd</sup> Street. (See *Amended Standards Legislative Draft Exhibit #10*)

1. The architecture provides relief and recesses that creates impression of “movement” along the street frontages.
2. Incorporation of ground plain interest/activity for the pedestrian such as: The corner enhancement, clubhouse, fitness center, stoops, walkways, and main/secondary entries along the street frontages as well as outdoor areas (areas of respite) created due to the curvature/undulation of the street and building too.
3. Pop-outs such as: awnings, balconies, etc. provide visual relief to the building massing.
4. The curvature of the streets forces the buildings back providing more openness along the street frontages.
5. Repeated architectural elements throughout such as: a recessed base, similar roof forms, and awnings provide pedestrian scale and reduce the need for a setback.
6. The building is designed as a composition of smaller parts. The building design reduces its perceived height and bulk by dividing the building mass into smaller-scale components (i.e. base, middle, and top). The proposed design has a strong base with a material changes from stucco, limestone, glass, steel awnings, and stoops as well live/work units fitness center bump outs located in strategic locations. The midsection and top provide for similar materials as the base, but provide for massing changes, recesses, balconies, roof deck areas for individual units, and topped with alternating flat parapets and roof overhang areas. The building also differentiates its top story by developing a lighter color scheme and thus provides for a more grounded base.
7. Large or long continuous wall planes are avoided within the proposed design. The architectural elevations are broken up vertically by varying the rooflines by alternating between flat parapets, roof deck areas, and elevated architectural features. This horizontal relief occurs approximately every 100 lineal feet with the bump out of the fitness center, live/work units, porte-cochere, and the natural curvature of the streets.
8. Building surfaces are created with a change of wall planes that provide strong shadow and visual interest; thus, reducing the massing.
9. Recessed balconies create a sense of depth in the building wall, contrasting surfaces exposed to the sun with those in the shadow. The strong, alternating light and dark pattern proposed in the design is a distinctive quality.
10. Projections are used to emphasize important architectural elements such as entrances, bays, stoops, stair towers, roof overhangs, balconies, porte-cochere, and verandas.

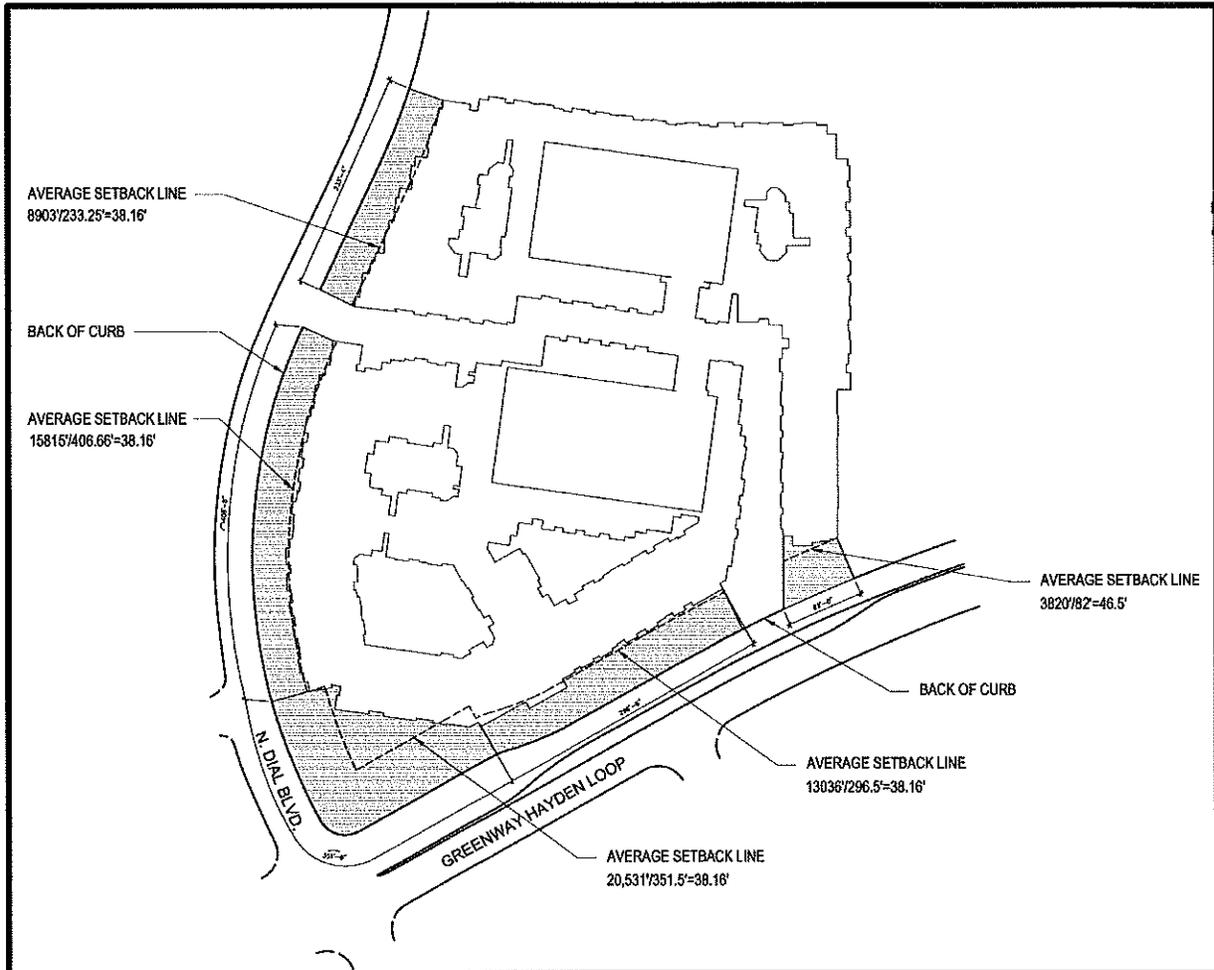
### ➤ Average Building Setbacks Deviation

In order to provide the contemporary/clean lined architectural character as well as to design around the two (2) curved streets an average building setback deviation is being requested for both Greenway-Hayden and 73<sup>rd</sup> Street. The Site's constraints and the ability to push the buildings closer to the street frontages is hinder, but where possible every effort has been made to push the buildings towards these streets or provided for alternative means of "connecting" the development to the streets, such as: stoops, walkways, and live/work units in order to activate the street frontages as much as possible. With that being said the south building which faces Greenway-Hayden Loop is varied in its setback and provides for the ground floor fitness center and live/work units with an average of 38' (i.e. City of Scottsdale Zoning Ordinance requires 40' average for residential and 32' average for retail/commercial) while maintaining the required minimum 34' setback for residential and 28' setback for retail/commercial. With regards to 73<sup>rd</sup> Street and its significant curve the building massing bows and creates challenges as to how close the building can get to the street. As such, the building/site design creates an average setback of 38' (i.e. City of Scottsdale Zoning Ordinance requires 30') while maintaining the required minimum 25' setback. However, stoops, walkways, and live/work units are being provided to help connect the development to the street frontage. In addition, the average setbacks deviations proposed will allow for enhanced landscaping as well as being compatible with the abutting developments to the east, west and south. (See next page and also: *Average Building Setbacks Exhibit # 9*)

In addition, the following five (5) design reasons that contribute to supporting the average building setback exception proposed along Greenway-Hayden Loop and 73<sup>rd</sup> Street. (See *Amended Standards Legislative Draft Exhibit #10*)

1. The architecture provides relief and recesses that creates impression of "movement" along the street frontages while not being directly abutting the street frontages.
2. Incorporation of ground plain interest/activity for the pedestrian such as: The corner enhancement and clubhouse, fitness center, porte-cochere, and main/secondary entries along the street frontages as well as the addition of stoops, walkways, and live/work units help connect the development to the streets.
3. Pop-outs such as: awnings, balconies, fitness center, etc. provide visual relief and interest along the street frontages and help push the buildings forward.
4. The curvature of the streets forces the buildings back providing more landscaping and areas of respite creating a more pleasant/shaded walkable environment, which is not out of character with the abutting developments to the east, west, and south.
5. Projections are used to emphasize important architectural elements and discoveries for the pedestrian and street frontages, such as: entrances, bays, stoops, porte-cochere, fitness center building projection, and verandas that draw people in and around the property.

## AVERAGE BUILDING SETBACKS



- (3) The DP will not significantly increase solar shading of adjacent land in comparison with a development that could be developed under the existing zoning district.

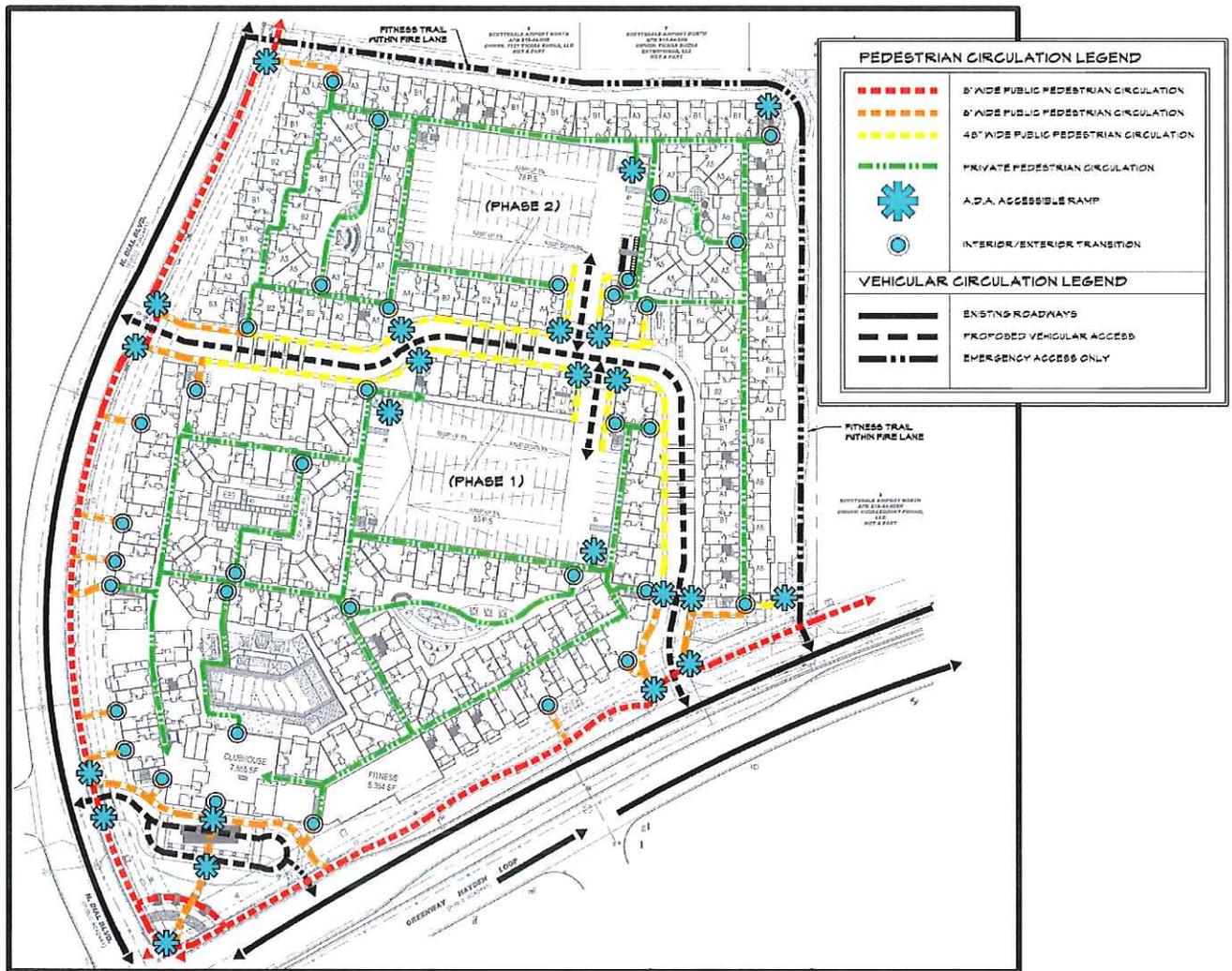
Comparable heights, uses and generous setbacks created by the abutting streets and internal circular access drive aisle do not significantly increase solar shading of adjacent land in comparison with a development that could be developed under the existing I-1 zoning district. Moreover, the I-1 zoning district allows up to 52' in building height, which will be comparable to the proposed development.

- (4) The DP promotes connectivity between adjacent and abutting parcels, and provides open spaces that are visible at the public right-of-way and useful to the development.

The proposed development provides for an enhanced Landmark Intersection featuring usable and common open space, with public shaded seating and landscaping features that exemplify

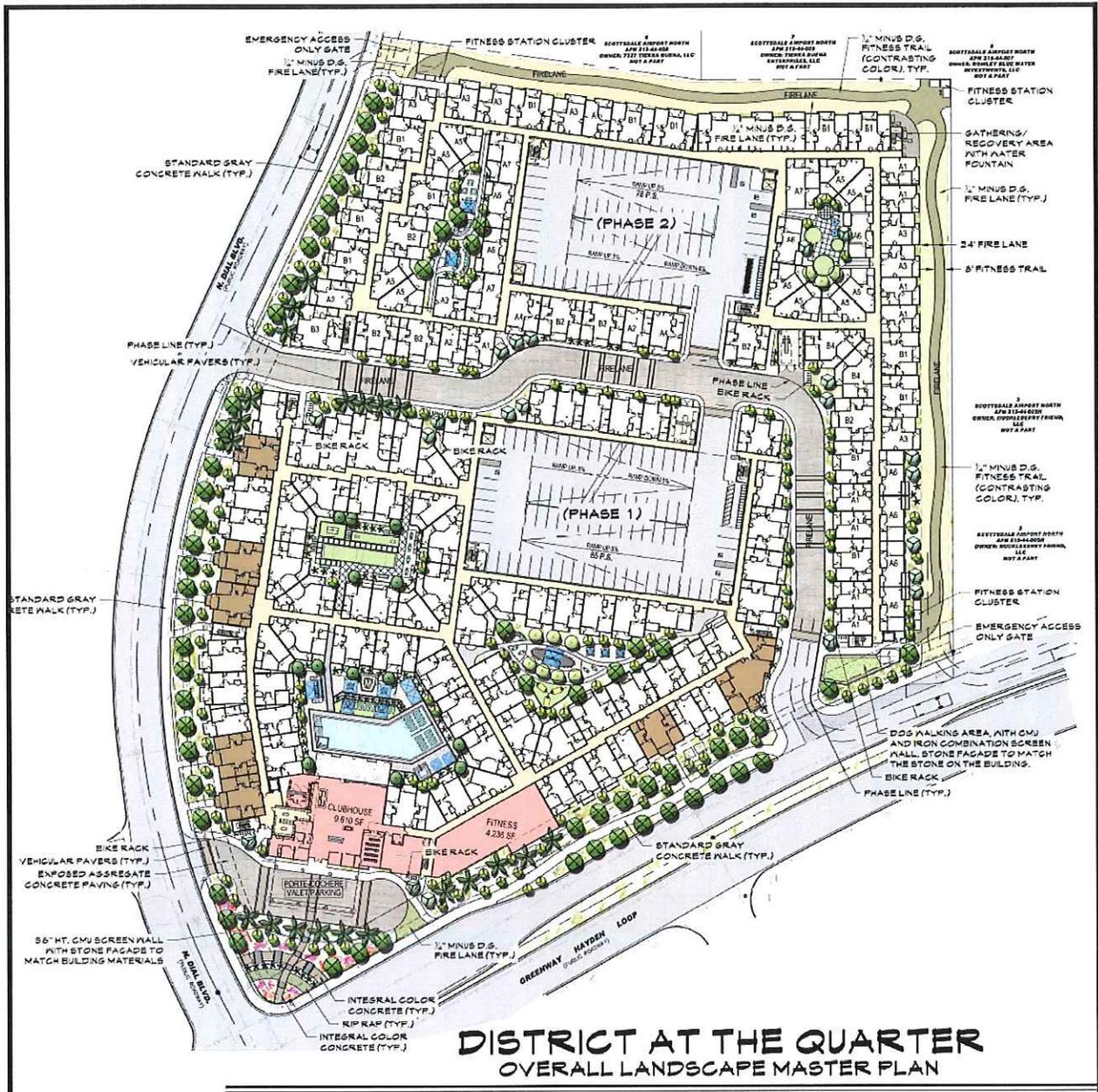
the promotion of connectivity between abutting parcels. The enhanced open space street frontage landscaping along both major roadways and corner bordering this Site creates an inviting path and enhanced enjoyable streetscape for all modes of transportation. Furthermore, with the addition of individual unit roof decks along 73<sup>rd</sup> Street and Greenway-Hayden Loop will help enhance visibility and activity (i.e. "eyes on the street") along these major pathways and nearby uses currently located along this Signature Corridor as well as in the future along 73<sup>rd</sup> Street as it becomes more multimodal and development occurs further north. Thus, these open space areas and enhanced connections provide areas of contemplation, recreation (passive/active), cooling, visual interest and connections to the surrounding area. (See below and also: *Pedestrian and Vehicular Circulation within the Landscape Plan Set Exhibit # 6*)

### PEDESTRIAN & VEHICULAR CIRCULATION



The landscape palette is in keeping with a Sonoran Desert theme with strategically locating trees, shrubs, groundcovers, etc. to create a lush appearance that cools the environment both internally and externally (i.e. along the streets). The entire Site is connected by internal walkways which link up to public sidewalks and bike paths encouraging multimodal options. Vehicular access is centrally located to provide ease of access to 73<sup>rd</sup> Street and to circulate around the building to Greenway-Hayden Loop for resident access and fire access. (See below and also: Overall Master Landscape Plan within the Landscape Plan Set Exhibit # 6)

## MASTER LANDSCAPE PLAN



## V. Non-Major General Plan Determination

The requested GPA is in conformance with a Non-Major General Plan Amendment, based upon the criteria set forth in the General Plan. A detailed explanation of how this proposal is consistent with the 2001 General Plan and the Greater Airpark Character Area Plan is provided below.

### 1. *Change in Land Use Category*

A change in land use category on the land use plan that changes the land use character from one type to another as delineated in the land use category table (page 20 of the General Plan).

**Response:** The current 2001 General Plan designation is Employment with a Regional Use District Overlay, and a requested GPA land use change to the Mixed Use category with a Regional Use District Overlay. Both of these land use categories are located within Group E, and therefore do not constitute a Major GPA.

### 2a. *Area of Criteria Change*

A change in the land use designation that includes the following gross acreages:

\*Planning Zones A1, A2, B: 10 acres or more.

\*Planning Zones C1, C2, C3, D, E1, and E3: 15 acres or more.

**Response:** The designated Planning Zone for the Site is Zone B.

*Excerpt from the 2001 General Plan:*

“Zone B includes the Scottsdale Airpark, the fastest growing and one of the three largest employment centers in the metropolitan area”.

This project in keeping with the mission and values of the General Plan and community, and in addition the overage being minimal creates a certain exception for a Non-Major GPA within item #2 of the “area of change criteria.” The Site totals approximately 8.84 net and 10.29 gross acres. The excessive amount of public right-of-way along Greenway-Hayden Loop (55’ right-of-way, half street) and 73<sup>rd</sup> Street (30’ right-of-way, half street) of approximately 1.45 combined acres creates a situation where the Site “technically” exceeds the 10 acre gross acreage by a nominal amount (.30 acres) without creating an increase in infrastructure needs. (See next page and also: *Right-of-Way Exhibit # 11*)

Moreover, in keeping with this area being a “large employment center” additional housing strategically located to support the continued growth of this employment center is critical. Thus, this proposal is optimally located to tap into the network of the existing employment, infrastructure, transportation system and amenities closeby.



The proposed mixed-use project uses and design elements including the Landmark Intersection area of this project closely align with and reinforce the goals of Scottsdale's Mission, among them:

- "Promoting the livability of the community and enhancing and protecting neighborhoods and ensuring and sustaining the quality of life for all residents and visitors."
- "Enhancing and protecting neighborhoods"
- "Ensuring and sustain the quality of life for all residents and visitors."

This request provides a mix of uses, passive and active open spaces, and public benefits that are desired to enhance, protect, and sustain quality of life for the immediate neighborhood. The development provides many of the elements reflective of the goals and policies of the Scottsdale Design Guidelines, GACAP, and General Plan that speak to the intent of creating livable communities and a high quality of life desired by citizens.

Furthermore, the General Plan continues within the mission values of the land use element that contribute to Scottsdale's identity and provide a balance of uses adequate for the live, work, and play theme. This proposal makes meaningful contributions to these land use element values:

- "Land uses should provide for an unsurpassed quality of life for both its citizens and visitors."
- "Land uses should contribute to the unique identity that is Scottsdale."
- "Land uses should work in concert with transportation systems in order to promote choice and reduce negative impacts upon the lifestyle of citizens and the quality of the environment."
- "Land uses should provide opportunities for the design of uses to fit and respect the character, scale and quality of uses that exist in the community."

The uses and public benefits of this lively development furthers the goals and policies listed above by creating a live, work, and play development that enhances the public realm surrounding this area providing improvements to the Signature Corridors and Landmark Intersection, and activating the corner with ground level uses and architecture. These elements collectively meet the future intent of the area as identified in the Scottsdale Design Guidelines, GACAP, and General Plan. Therefore in keeping with the mission values of the General Plan this request will not need rise to the level of a major amendment.

### **3. Character Area Criteria**

Character areas have been added to the city's planning process in order to recognize and maintain the unique physical, visual and functional conditions that occur in distinct areas across the community. The city recognizes that these form a context that is important to the lifestyle, economic well-being and long term viability of the community. These areas

are identified by a number of parameters including but not limited to building scale, open space types and patterns, age of development and topographic setting.

If a proposal to change the land use category has not been clearly demonstrated by the applicant to comply with the guidelines and standards embodied within an approved character area plan it will be considered a major amendment.

**Response:** Based on the justification and written responses below regarding the GACAP, the proposed GPA would not constitute a major amendment. The current employment designation will be enhanced with a portion of mixed-use. This location as indicated in GACAP is intended to provide a future Signature Corridor, Landmark Intersection, and is located within the shopping and activity center area. This indicates the appropriateness of the proposed mixed-use residential development in this strategic location, current trends that are important to the Airpark lifestyle, economic well-being, and long term viability.

#### 4. **Water/ Wastewater Infrastructure Criteria**

If a proposal to change the planned land use category results in the premature increase in the size of a master planned water transmission or sewer collection facility, it will qualify as a major amendment.

**Response:** The proposed change in land use does not result in a premature increase in the water and sewer plan, therefore it will not constitute a major amendment. At this time the development team has no plans to change or upgrade any water or sewer infrastructure.

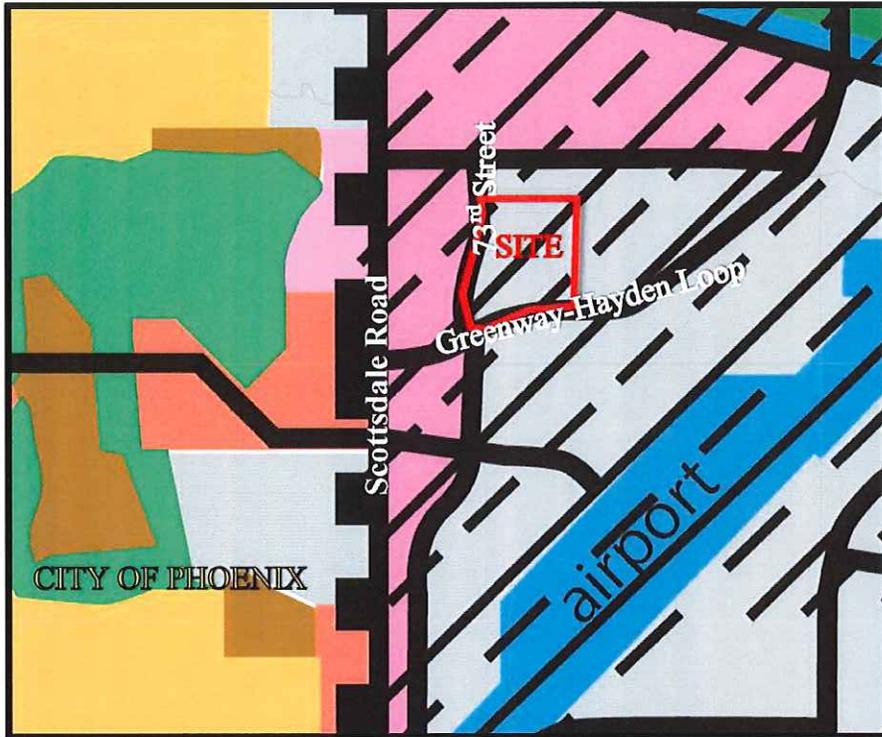
### VI. **2001 General Plan/Greater Airpark Character Area Plan (GACAP)**

#### **2001 General Plan**

The current 2001 General Plan designation is Employment with a Regional Use District Overlay, and a requested GPA land use change to the Mixed Use category with a Regional Use District Overlay. (See next page and also: *2001 General Plan Land Use Existing & Proposed Exhibit # 13*)

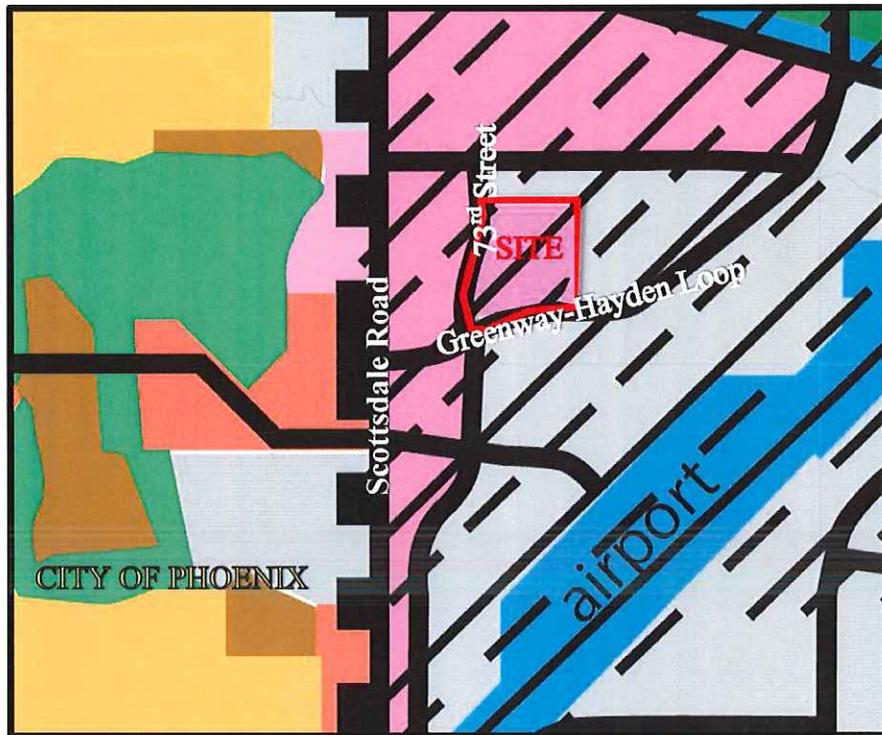
The General Plan sets forward collective goals and approaches of the community with the intent to integrate Guiding Principles into the planning process and provide as a framework for proposed development and the built environment. The goals are, however, not intended to be stationary or inflexible, which the General Plan clearly recognizes with this statement: "The General Plan is designed to be a broad, flexible document that changes as the community needs, conditions and direction change." With this statement in mind, this proposed non-major general plan amendment not only meets but exceeds the goals and approaches established in the General Plan. In addition, it is important to understand that the current General Plan is 15 years old and the evolving lifestyles and economic development trends of today must meet the needs of a growing and changing city and area.

**2001 GENERAL PLAN LAND USE EXISTING/ PROPOSED**



2001 General Plan Existing Land Use

- Commercial
- Office
- Employment
- Natural Open Space
- Developed Open Space (Parks)
- Developed Open Space (Golf Courses)
- Cultural/Institutional or Public Use
- Rural Neighborhoods
- Suburban Neighborhoods
- Urban Neighborhoods
- Mixed-Use Neighborhoods
- Resorts/Tourism
- Shea Corridor
- Mayo Support District
- Regional Use District
- McDowell Sonoran Preserve
- Recommended Study Bounds
- City Boundary



2001 General Plan Proposed Land Use

- Commercial
- Office
- Employment
- Natural Open Space
- Developed Open Space (Parks)
- Developed Open Space (Golf Courses)
- Cultural/Institutional or Public Use
- Rural Neighborhoods
- Suburban Neighborhoods
- Urban Neighborhoods
- Mixed-Use Neighborhoods
- Resorts/Tourism
- Shea Corridor
- Mayo Support District
- Regional Use District
- McDowell Sonoran Preserve
- Recommended Study Bounds
- City Boundary

## General Plan Elements:

### Land Use:

**Goal 3, Bullet Point 2:** Encourage the location of more intense mixed-use centers and regional employment cores along regional networks while incorporating appropriate transitions to adjoining land uses.

**Response:** The location of this mixed use project is appropriately located along a major regional network at the convergence of two (2) Signature Corridors that form a Landmark Intersection and along with adjacent mixed-uses form a “gateway” to the Scottsdale Airpark from surrounding areas. The Landmark Intersection is the appropriate designation for the proposed intensity which provides pedestrian scaled uses and services supported by a balance of supporting uses. In addition, the location will support and enhance both the existing and proposed residential and non-residential uses in this location and transition between the employment center and surrounding retail. Appropriate transitions have been incorporated by way of design solutions, building construction, setbacks, noise disclosures, and avigation easements. This development is replacing the old sidewalk surrounding the site and creating a more walkable and enjoyable streetscape that helps to complete the streetscapes by creating a street scene similar to the adjacent LIV development.

**Goal 4:** Maintain a balance of land uses that support a high quality of life, a diverse mixture of housing and leisure opportunities and the economic base needed to secure resources to support the community.

**Response:** This mixed use development proposes appropriately balanced uses that are consistent with the needs and character of the surrounding developments and area uses and encourage a high quality lifestyle with many leisure opportunities and resources that support the surrounding community. The residential and non-residential uses at this location will support the quality of life and lifestyle desired in the area by providing residential units with some additional non-residential uses to help the balance of uses that support the surrounding community.

- Allow for a diversity of residential uses and supporting services that provide for the needs of the community.

**Response:** This proposed development allows for a unique residential housing opportunity with supporting services and amenities within the area. The residential and non-residential uses proposed at this location provides for a balance and an appropriate level of unique mixed-use development that complement and complete the surrounding area providing for the needs of community and affords for an exceptional lifestyle.

- Ensure the highest level of services and public amenities are provided to the citizens of Scottsdale at the lowest costs in terms of property taxes and travel distances.

**Response:** High quality and desirable services and amenities are included within this development. Furthermore, the location of the development and uses within walking distance of this development provide residents and nearby citizens the highest level of services with minimal travel. The clubhouse, restaurant, and fitness center of the live/work element included in this proposal provide for a balance of uses and amenities that promote a healthy lifestyle by encouraging walking and reduced travel. There is a balance of non-residential and commercial uses included that support the development and attract surrounding pedestrians to this Landmark Intersection location which is the intent of a walkable mixed use development.

- Support jobs/housing balance by integrating housing, employment, and supporting infrastructure in mixed-use centers located at appropriate locations.

**Response:** This development will support the jobs and housing balance by providing an appropriate balance of commercial and non-residential uses and services along with the residential population base to support new and existing employment and services. The designated Landmark Intersection is the ideal location for live, work, and play based mixed-use development that activates the surrounding area with walkable streetscapes and connections while utilizing existing infrastructure. In addition the option for ground floor walk-up live/work units are proposed along both Signature Corridors further integrating the live/work concept and activating the streetscape, similar to LIV.

- Provide a variety of housing types and densities and innovative development patterns and building methods that will result in greater housing affordability.

**Response:** This development provides a variety of residential housing options through various floor plans including ground floor live/work units. Additionally, the density proposed within this development is comparable and falls within the middle of a residential density option within the immediate area (i.e. LIV and Crescent).

- Maintain a citywide balance of land uses that support changes in community vision/dynamics (established by future community visioning processes) over time.

**Response:** The proposed development provides housing opportunities that support the future of the community and Scottsdale Airpark by enhancing the employment center's appeal to employers while also providing an appropriate and transitional mixed-use development in this particular location. The development provides a balance of uses to the area and non-residential services, notably, at the pedestrian scale of the Landmark Intersection which is the intent of the community's future vision and goals of the Landmark Intersection.

**Goal 7, Bullet Point 5:** Incorporate open space, mobility, and drainage networks while protecting the area's character and natural systems.

**Response:** Approximately, 10.5% of the Site has been reserved as useable open space. Private and public, passive and active forms of open space are found throughout the site plan. The Site also provides a total landscape space (including all open space, street frontage, and parking landscape areas) of 20% for the Site. It should be noted that both phases of development will exceed the required 10% open space. Again, these open space areas provide areas of contemplation, recreation (passive/active), cooling, and visual interest both on-site and off-site. Mobility and connections through the site and surrounding area are enhanced with meandering pathways, landscaping, enhanced streetscapes, and ground level uses. Native landscaping, open spaces, and drainage have been incorporated in an environmentally sensitive manner with consideration of the character in this area.

**Goal 8, Bullet Point 2:** Develop and reinforce links (i.e. trails, paths, open space, transit, and streets) within and between residential, retail, employment, recreational and other public land uses.

**Response:** Paths, connections, and open spaces have been carefully connected throughout and surrounding the site to provide for comfortable connections and alternative modes of transportation to surrounding land uses.

**Goal 9:** Provide a broad variety of land uses that create synergy within mixed-use neighborhoods.

- Incorporate a diverse range of residential and non-residential uses and densities within mixed-use neighborhoods.

**Response:** The proposed development adds a diverse range of residential and non-residential uses alike, with a different density option/density for residents within a larger mixed-use neighborhood. This development adds a complimentary balance of uses of non-residential uses at a location designated for mixed-use development with amenities and uses appropriately situated at the ground level of the Landmark Intersection.

- Promote residential uses that support the scale and function of retail, commercial and employment uses within these neighborhoods, including the use of mixed-use structures (retail or office on lower level and residential uses on upper levels).

**Response:** The residential/commercial uses proposed will be supportive of the existing retail, commercial, and employment uses within these neighborhoods and complete the mixed-use nature of this Landmark Intersection as envisioned. In addition, non-residential floor area uses and ground level live/work units are proposed along the Signature Corridors.

- Encourage compact mixed-use, pedestrian oriented development patterns, at urban densities, that limit the demand for parking and unnecessary automobile trips, and support alternative modes of mobility.

**Response:** This project utilizes existing infrastructure already in place and takes advantage of this prime location for such a development because of the surrounding employment, retail, and services. With a broad variety of employment and uses within walking distance, this development encourages and taps into non-residential uses on site and within walking distance of this location and enhances the streetscapes, which will encourage walkability and alternative modes of transportation which limits automobile trips.

### **Growth Areas Element**

**Goal 3, Bullet Point 2:** Provide open spaces in designated growth areas that encourage public gathering, enhance aesthetics, preserve viewsheds, and serve as buffers between uses of significantly differing function and intensity.

**Response:** Open spaces have been strategically laid out within the development. The Landmark intersection and pedestrian area of this development includes amenities and open spaces that encourage public gathering and activity. The open space areas along the street frontages and enhanced design aesthetics will help to preserve viewsheds that also serve as buffers from inside and outside the development and provide for a public benefit for the area. This development strengthens the design and character of the Greater Airpark and provides enhanced streetscapes that promote a safe, comfortable, and aesthetically pleasing pedestrian environment.

### **Housing Element**

**Goal 3, Bullet Point 1:** Encourage and establish appropriate incentives for development of aesthetically pleasing housing that will accommodate a variety of income levels and socioeconomic needs.

**Response:** The proposed development will be aesthetically pleasing and will allow for another housing opportunity within this immediate area.

**Goal 3, Bullet Point 6:** Consider incentives that encourage the development of diverse housing types, including smaller, more affordable units.

**Response:** A range of floor plans have been included with the development that provides various levels of living options (i.e. 1 and 2 bedrooms with roof access for some units and ground level live/work units) as well as amenities (i.e. concierge service).

**Goal 4, Bullet Point 1:** Encourage a variety of housing densities throughout Scottsdale, with mixed-uses in areas of major employment and transit hubs, to offer greater live/work choices to a broader economic range of households.

**Response:** This development provides a different level option in housing density while also providing a mix of uses located one of the state's largest employment centers and further enhancing the live/work philosophy.

**Goal 4, Bullet Point 5:** Support the use of future innovations in technology and telecommunications as a way to remain flexible to changing demographics, community profiles and quality of life choices, including opportunities like home based businesses, telecommuting, on-line shopping, etc.

**Response:** This development supports the use of future innovations in technology that provide opportunities for "work from home" based businesses of the present and future while providing for a high quality of life. The development will utilize state of the art Wi-Fi technology and contains on-site amenities and services within the business center that support future technology workers and can help to eliminate automobile use. The on-site business center has a conference center with meeting space and can assist in business needs such as printing all located within the development. In addition, live/work units are proposed along both Signature Corridors allowing for opportunities for home based businesses and lifestyle choices.

**Goal 4, Bullet Point 6:** Consider a variety of strategies to increase housing intensity and diversity in appropriate locations, such as around commercial areas, near transit centers or major employment.

**Response:** Located between a heavily commercial area and one of the largest employment centers in the state, at a Landmark Intersection, and with existing transit, this site is ideal for an increase in housing intensity.

**Goal 4, Bullet Point 7:** Explore opportunities for new or redeveloped housing to serve the employment base.

**Response:** The redevelopment of this site with existing infrastructure is an ideal opportunity for housing to serve the existing and future employment base in the area.

**Goal 4, Bullet Point 9:** Work to adjust the housing mix based on changing demographics and economics of the city.

**Response:** The changing economics and demographics of the city and particularly the future of the Scottsdale Airpark are evolving towards a live, work, and play philosophy which allows for "work from home" options and the Site is in an ideal location to capitalize on that philosophy.

## Community Mobility Element

**Goal 8** Emphasize live, work, and play land use relationships to optimize the use of citywide systems and reduce the strain on regional and local/neighborhood systems.

**Response:** This mixed-use development will contribute to the existing live, work, and play theme seen in adjacent developments in this location, and particularly at the GACAP designated Landmark Intersection. The mixed-use nature and design of the proposal provides for walkability and encourages alternative modes of transportation to reduce automobile trips and ultimately the strain on regional and local/neighborhood systems.

**Goal 8, Bullet Point 3:** Encourage, where appropriate, mixed use developments that physically incorporate residential, shopping and work environments within one area or project and place strong emphasis on connectivity with non-motorized access (pedestrian oriented development).

**Response:** This project provides an appropriate mixed-use development located at the GACAP designated Landmark Intersection and various commercial/retail services and business related opportunities. Furthermore, the development is designed with the intent to reduce automobile trips and encourage pedestrian oriented development by including ground level uses, live/work units, and services and enhancing the streetscapes to become walkable, comfortable, and aesthetically pleasing.

**Goal 11:** Provide opportunities for building “community” through neighborhood mobility.

- Encourage the sensitive integration of live, work and play land uses and their physical links within and between neighborhoods to emphasize sense of place.
- Provide a high level of service for pedestrians through facilities that are separated and protected from vehicle travel (e.g., placing landscaping between curbs and sidewalks).
- Emphasize strong pedestrian orientation (e.g. shaded safe paths, links to civic spaces) to foster a strong sense of community.

**Response:** The design for this development provides the opportunity to build community through neighborhood mobility by placing an amenitized and public open space at the intersection and uses and enhanced streetscapes that encourage activity and friendly neighborhood interaction. Design consideration includes separated sidewalks, shade, seating, bicycle parking, and safe pathways that foster a strong sense of community.

## General Plan – Six Guiding Principles:

The following request is for a non-major GPA to the land use category and map contained in the Land Use Element of the City of Scottsdale 2001 General Plan. Six guiding principles indicate the appropriateness of a land use change to the General Plan. These six Guiding Principles are:

1. Value Scottsdale’s Unique Lifestyle & Character.
2. Support Economic Vitality.
3. Enhance Neighborhoods.
4. Preserve meaningful Open Space.
5. Seek Sustainability.
6. Advance Transportation.

### Responses:

1. Value Scottsdale’s Unique Lifestyle & Character:

**Response:** Scottsdale is world renowned for a tourism and resort lifestyle. The live, work, and play theme extends the concept of the unique Scottsdale lifestyle by offering an opportunity to utilize the surrounding amenities, services, entertainment, recreation, relaxation, business, and travel within walking distance to a place of residence. With the close proximity to the TPC Golf Course, Scottsdale Airpark, and Scottsdale Quarter among many other businesses this proposed development offers a modern, active, and exciting lifestyle rooted in a location that provides access to some of the best amenities the Scottsdale lifestyle has to offer. From professionals to families, this location provides the location, lifestyle, and services that appeal to everyone.

2. Support Economic Vitality:

**Response:** This proposed development will generate sales tax revenue from new tenants, property taxes, and indirect revenues spent within the area at nearby businesses, which generates a substantial new economic benefit to the city and Airpark. The current employment base would not generate as great of economic benefit as a new community of residents who will generally spend a good portion of their income within the Airpark and Scottsdale. Moreover, increased residents within an area provides for greater opportunities for new employers to locate to the Airpark, with close proximity to housing, retail, entertainment/ recreation.

3. Enhance Neighborhoods:

**Response:** The proposed project will best utilize the Site, which is located at a designated future Signature Corridor. This project will enhance pedestrian activity and connectivity of various areas, as well as bring to the area an active residential community and support services. The economic spin-off related to the new community provides revenue to

surrounding property owners businesses through shopping, dining, airport travel, and nearby recreation and entertainment.

4. Preserve Meaningful Open Space:

**Response:** This project will dedicate a total of approximately 10.5% of useable open space. The most meaningful being the public benefit of an enhanced Landmark Intersection pedestrian rest area and passageway. This space provides areas for public rest, interaction, and connections. The Signature Corridors of this site include landscaped street frontage open space that creates a comfortable walking experience and enjoyable streetscape. In addition, five (5) internal interconnected courtyards throughout the development will contain substantial amenities. In addition, the Site will have beautiful natural landscaping that enhances open space and showcases the beauty of the surrounding lush Sonoran desert landscape palette.

5. Seek Sustainability:

**Response:** Many features of District at the Quarter will indicate the commitment to sustainability. The density and design of this dense and urban multi-family development uses less land area and encourages a more mobile lifestyle other than via an automobile (e.g. walking, biking, and transit). Other design considerations include native landscaping which will reduce water use.

6. Advance Transportation:

**Response:** A mix of uses and nearby services encourages alternative modes of transportation through walking and bicycling. Furthermore, the development of an exciting and active street scene in conjunction with improved landscaping will activate the intersection and both roadways connecting to nearby destinations while providing pedestrian amenities such as shade, seating, and bicycle parking.

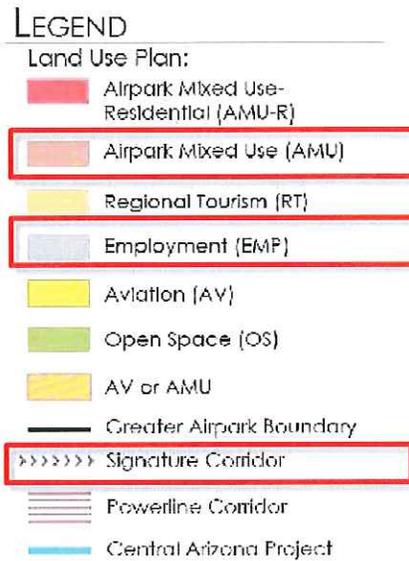
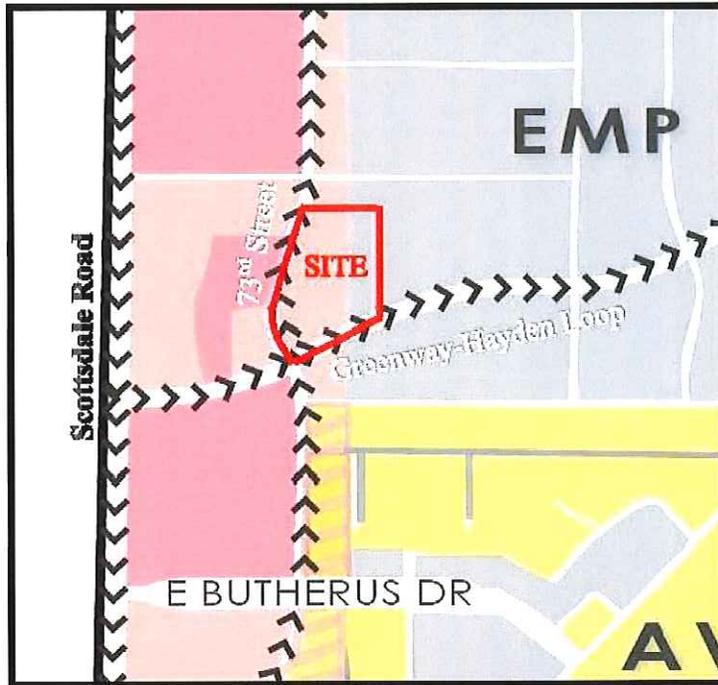
### **Greater Airport Character Area Plan (GACAP)**

The GACAP was adopted in October 2010 (last amended: December 2014) by Scottsdale's City Council. The purpose of the GACAP is to establish "the vision for the Greater Scottsdale Airport and provide the basis for Greater Airport decision-making over a twenty-year timeframe." To achieve this, the City established a series of goals and policies to provide a framework for future development. The GACAP is divided into eight chapters each with its own focus and vision: Land Use, Neighborhood & Housing, Aviation, Community Mobility, Economic Vitality, Environmental Planning, Character & Design, and Public Service Facilities. The following paragraphs (i.e. responses) will highlight how this application meets the goals and policies of the GACAP.

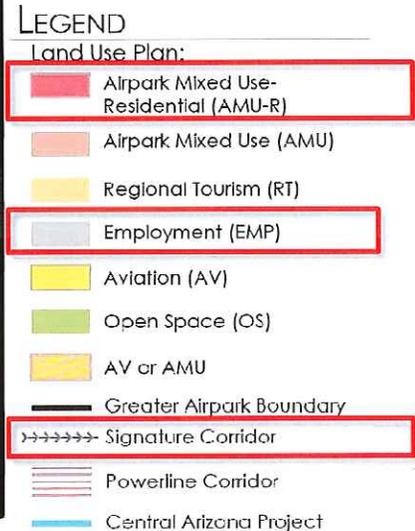
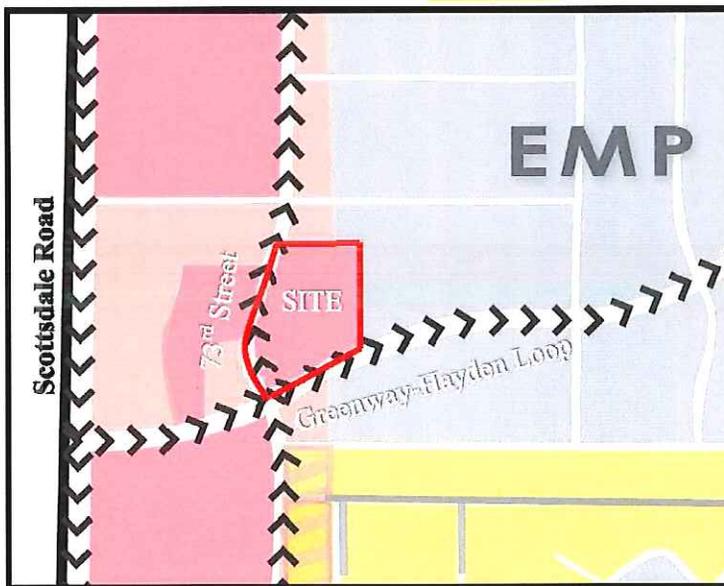
The request for a Non-Major GPA on the Site from AMU and a small portion of EMP to AMU-R will allow for a Multi-Family Residential land use on this Site that is partially designated for mixed-use.

A small portion of the Employment land use boundary for the Airpark runs north and south and happens to fall on the eastern edge of the redevelopment parcel. This entire Site is owned and developed as one parcel and should be developed under the same plan, as such, a minimal portion of the Employment land use would be changed to accommodate the proposed development and pushed slightly east towards a more appropriate location for employment. (See below and also: *GACAP Land Use – Existing & Proposed, Exhibit # 14*)

**SCOTTSDALE AIRPARK LAND USE – EXISTING**

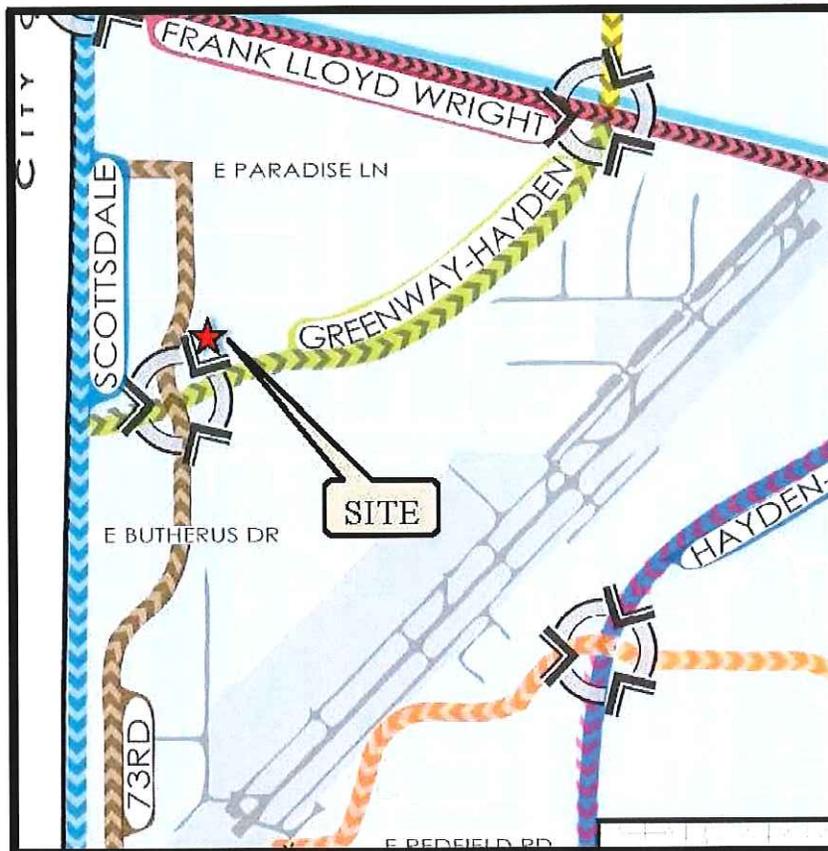


**SCOTTSDALE AIRPARK LAND USE – PROPOSED**



As designated in the GACAP Signature Corridors Map, the Site is located along 73<sup>rd</sup> Street and Greenway-Hayden Loop, both of which are designated as Signature Corridors of the Greater Airpark. (See below and also: *Airpark Signature Corridors Exhibit # 15*) The Greater Airpark Transit Connections Map designates the Site within the “shopping/ housing” activity center which also contains a potentially future transit route on Greenway-Hayden Loop. The Pedestrian and Bicycle Connectivity shows both major roadways bordering the Site as future pedestrian and bicycle corridors, further illustrating the concept that this Site should be utilized for more residential and mixed-use development that will support the planned pedestrian activity. (See next pages and also: *Airpark Future Transit Connections Exhibit # 16 & Airpark Pedestrian & Bicycle Connectivity Exhibit #17*)

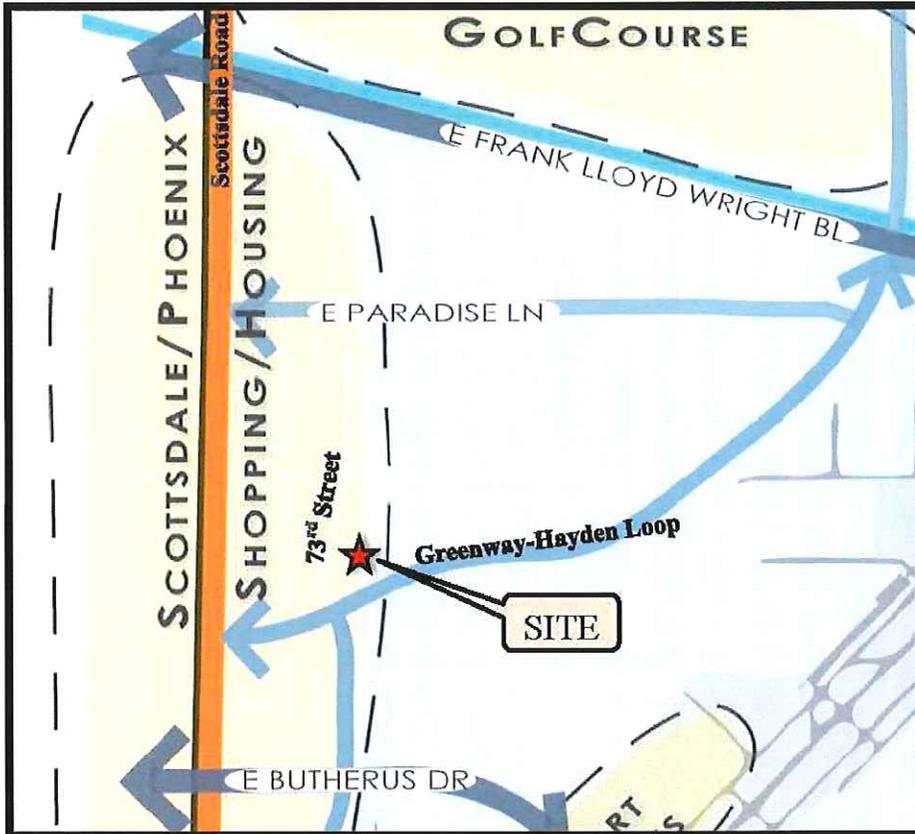
**SCOTTSDALE AIRPARK SIGNATURE CORRIDORS**



**LEGEND**

- Signature Corridors
- Greater Airpark Boundary
- Powerline Corridor
- Central Arizona Project
- Signature Corridors
- Landmark Intersections

**SCOTTSDALE AIRPARK FUTURE TRANSIT CONNECTIONS**

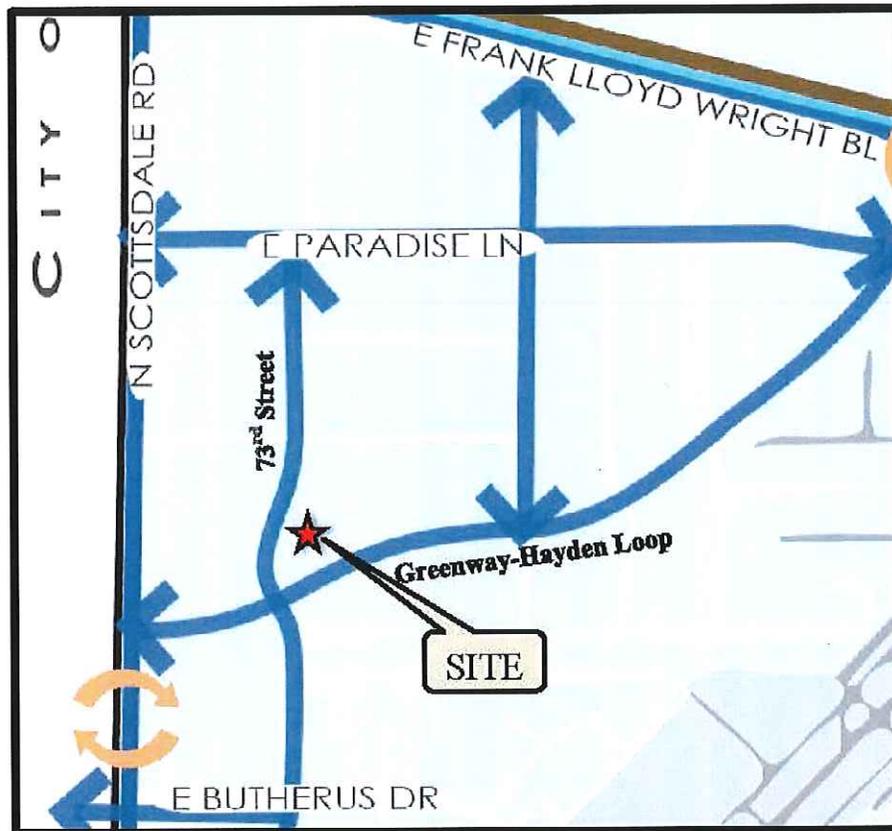


**LEGEND**

Conceptual Transit Connections

-  Greater Airpark Boundary
-  Powerline Corridor
-  Central Arizona Project
-  Existing Transit Route
-  Potential Future Transit Route
-  Future High Capacity Transit Corridor
-  Activity Centers
-  Park and Ride

**SCOTTSDALE AIRPARK PEDESTRIAN & BICYCLE CONNECTIVITY**

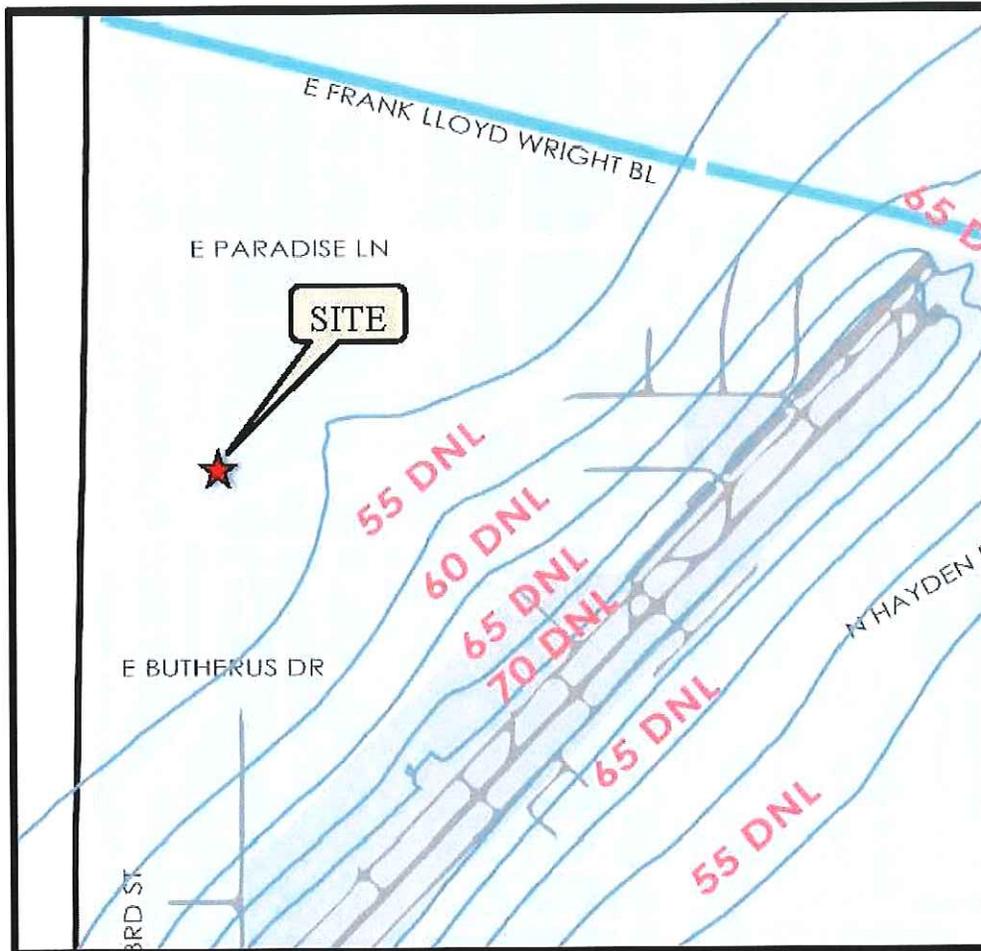


**LEGEND**

- Conceptual Greater Airpark Pedestrian/Bicycle Connectivity
- Greater Airpark Boundary
- Powerline Corridor
- Central Arizona Project Aqueduct
- Pedestrian/ Bicycle Corridors
- Trails
- ↻ Important Pedestrian Linkage

Additionally, the proposed redevelopment Site is located outside of the 55 DNL line, which is appropriate for residential land uses. (See next page and also: *Airpark Noise Contours Exhibit # 18*) New construction will feature appropriate noise attenuation through the use of upgraded building materials and insulation, disclosure to residents and an aviation easement in conformance with the City's requirements. Flight paths for both helicopter and fixed wing planes do not cross over this property, rather they route to the north, northeast, south and southwest. This Site is clear of most flight path tracks as compared to other properties located within the airpark.

## SCOTTSDALE AIRPARK NOISE CONTOURS



GACAP Definition (emphasis added):

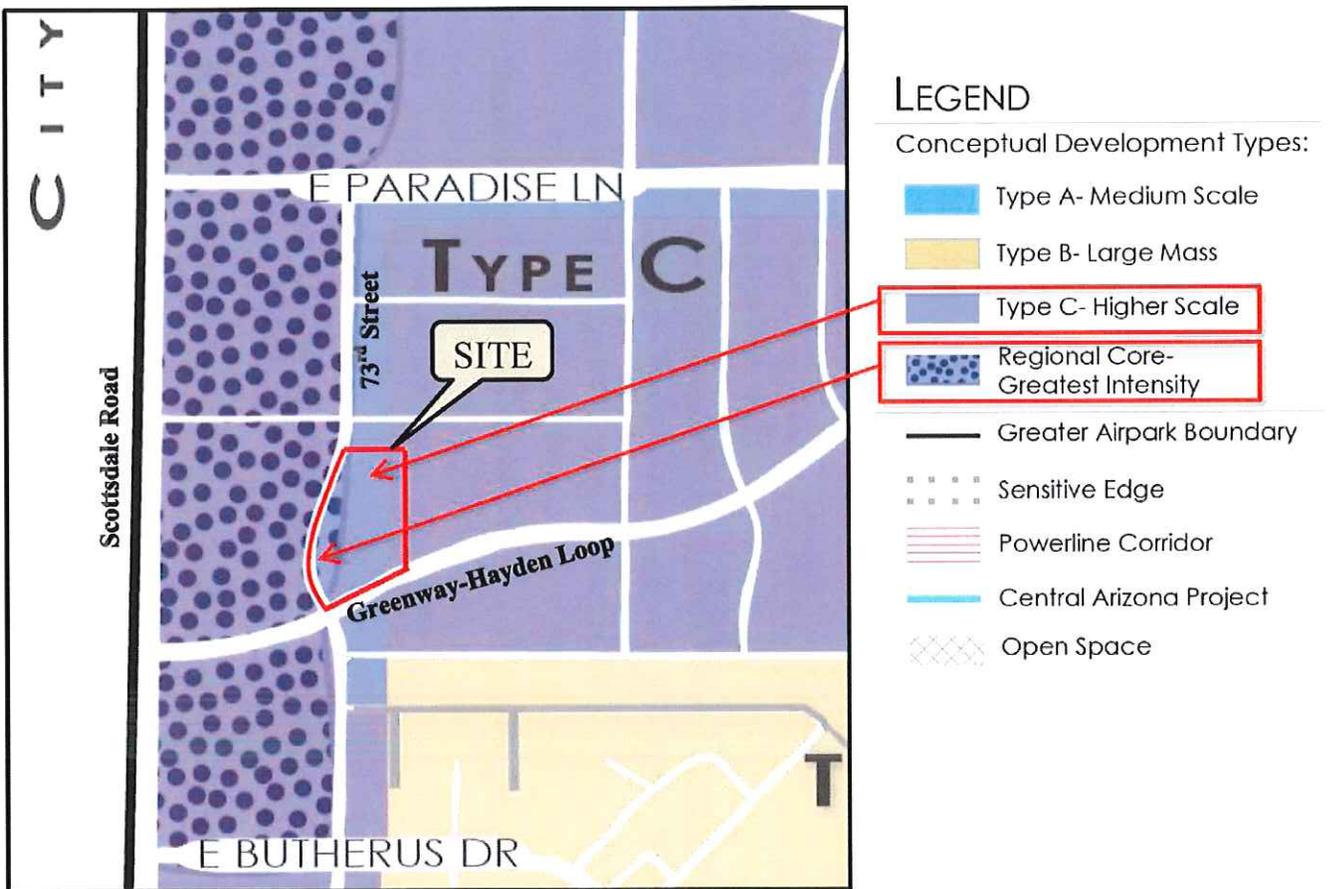
***Airpark Mixed Use Residential** areas are appropriate for the greatest variety of Residential (AMU-R) land uses in the Greater Airpark. Appropriate uses may include a combination of personal and business services, employment, office, institutional, cultural amenities, retail, hotel, and higher density residential. Developments in AMU-R areas should be pedestrian-oriented, have access to multiple modes of transportation, and should be located outside of the Airport's 55 DNL contour. Residential and other sensitive uses should be a lesser component of development and include adequate sound attenuation. Design of residential uses in the area south of the Central Arizona Project Aqueduct should support businesses and tourism uses, such as time-shares, multi-family rental units, and corporate housing.*

The Conceptual Development Type Map designates the majority of the property as “Type C-Higher Scale” and a small portion also as “Regional Core” and they are defined in the GACAP as follows (emphasis added):

Type C development represents medium to higher scale development which supports pedestrian activity in the Greater Airpark. Type C is encouraged in areas with access to multiple modes of transportation, served by regional transportation networks (i.e. freeways or transit corridors), and where the scale will complement the area’s character. Type C development is appropriate in areas next to both Types C1 and B development. Type C is not recommended immediately adjacent to the Scottsdale airport.

The Regional Core designation denotes areas appropriate for the greatest development intensity in the Greater Airpark to support regional land uses. Regional core is only appropriate in Type C areas that are or will be served by high-capacity transit or a freeway. Regional Core areas should not be located adjacent to single-family residential areas or within Type A or B development areas (emphasis added). (See below and also: Airpark Development Types Exhibit # 19)

**DEVELOPMENT TYPES**



**Airpark Development Types Exhibit**

**a) Land Use**

***Policy LU 1.1 Maintain and expand the diversity of land uses in the Greater Airpark.***

**Response:** The proposal to add a multifamily land use adds an appropriate residential component in a prime location of the Scottsdale Airpark. Characteristics of successful mixed-use developments include a range of land uses and promote the “live, work, and play” philosophy. The proposed development accomplishes a range of goals including helping partially to enhance the regional core, integrating high quality, vibrant architecture and site planning to the area, and creating pedestrian synergy that will complement the surrounding context.

***Policy LU 1.2 Support a mix of land uses within the Greater Airpark that promote a sense of community and economic efficiency, such as clustering similar/ supportive uses and incorporating residential intended for the area’s workforce, where appropriate.***

**Response:** The Airpark is predominately an employment center. Integrating the proposed multifamily land use will provide a residential rental housing opportunity for residents of Scottsdale in an employment and service core area of the City. The location of the property not only provides an opportunity for housing of workers in and adjacent to the employment core, but also residents seeking connectivity to the retail, restaurants, and businesses established in the nearby developments that will enhance their sustainability. District at the Quarter will promote an integrated mix of uses within the broader area’s existing uses contributing towards the live, work, play goals identified in the GACAP.

***Policy LU 1.3 Promote development intensities supportive of existing and future market needs.***

**Response:** The proposal promotes development intensities consistent with the present area context and appropriately responds to the needs of future development intensity planning for the Landmark Intersection and the Site is within the Airpark Type C development and Regional Core designations.

***Policy LU 4.5 Greater visual variety and architectural interest should be considered in the design of the Greater Airpark’s tallest buildings (Regional Core), particularly at the pedestrian level.***

**Response:** District at the Quarter intends to create a unique luxury 4-story residential rental community with Southwestern contemporary elevations complementary to the character of the adjacent Scottsdale Airpark. The site design and architecture will further cater to the pedestrian scale particularly with the incorporation of the ground level commercial component and public pedestrian area of the project.

**Policy LU 4.7** *Encourage greater visual variety between employment/commercial land uses and residential neighborhoods, and avoid continuous building shapes and mass adjacent to residential neighborhoods.*

**Response:** The unique design demonstrates rich character and architectural visual variety that create a distinct and appropriate transition between the proposed development and adjacent employment land uses. The building and site planning design provides pedestrian scale massing along the Signature Corridor and a hierarchy of masses and sensitivity to pedestrian scale.

**Goal LU 5** Encourage Greater Airpark development flexibility.

**Policy LU 5.1** *Update and provide greater flexibility in development regulations to achieve the goals of the Greater Airpark Character Area Plan and encourage revitalization in the area.*

**Response:** The AMU-R zoning will provide the flexibility in development standards to accommodate residential and mixed land uses. This will further promote the type of mixed-use development and character that revitalizes the area and creates synergy that defines land uses, lifestyle and activities, and furthers the economic goals of the Scottsdale Airpark while providing enhancements and amenities for the public benefit.

**Policy LU 5.2** *Greater Airpark public amenities and benefits should be provided by the private sector when development bonuses, such as increased floor area, greater intensity, greater height, development standard flexibility, and/or street abandonment are considered. Potential public amenities and benefits may include, but are not limited to:*

- *New/expanded usable open space areas;*
- *Linkages to planned or existing trails and/or paths;*
- *Transit and/or other mobility enhancements, including bicycle and pedestrian amenities;*
- *Workforce housing (where appropriate);*
- *Infrastructure improvements;*
- *Regional tourism facilities or accommodations;*
- *Green building standards, such as LEED certification;*
- *Gateway and/or Landmark Intersection enhancements;*
- *Integrated daycare facilities;*
- *Public/shared/park-and-ride parking facilities;*
- *Net-zero/significant energy efficiency and/or on-site alternative energy generation for multiple properties; and/or*
- *Area stormwater infrastructure.*

**Response:** This development provides many public benefits most notably providing an enhanced Landmark Intersection for public use that serves as a “gateway” to the Airpark. This enhanced Landmark Intersection space features shade, seating, various meandering pathways, public open space, landscaping, and ground level uses and services which produce a public benefit in the form of placemaking for this intersection and area. In addition, the development provides various passive and active open space areas, and enhanced Signature Corridor streetscapes with various interesting connections through the site and to surrounding areas. This development provides public benefits and amenities that promote walkability and alternative modes of transportation through improved streetscapes, pedestrian scaled services and uses, bicycle parking, and ground level live/work units that help activate the streetscapes and intersection providing visually interesting and comfortable public spaces and an exceptional walkable community.

*Goal 6 Promote the Greater Airpark as a mixed-use economic and aviation-based employment center that is complementary to Downtown Scottsdale, the city’s premier cultural, civic, and residential mixed-use core.*

**Response:** The mixed-use, non-residential floor area uses, amenities, and residential population base support the economic activity and boost revenue of this major employment center while strengthening the synergy and achieving the future goals of the Scottsdale Airpark. The mixed-use development proposed is ideal for this particular intersection due to the many attributes identified with the GACAP for this intersection (i.e. Landmark Intersection, transit, bike, etc.) of the airpark.

*Policy LU 6.4 Enforce and modify development standards and building codes to enhance compatibility of residential uses with aviation and employment, and buffer existing industrial and aviation uses from new and existing residential development.*

**Response:** The proposed development buffers primarily lower intensity employment and boutique industrial which includes predominately closed buildings (i.e. Piano Gallery) to the north and east, making the transition of the proposed high density residential apartment development use more easily compatible with the proposed mixed-use project. In addition to design solutions such as building construction, and setbacks, and other measures (see below) are being taken to ensure the compatibility of uses. Examples of Policy LU 6.4 illustrate how the residential use is compatible in the area:

- Sound and attenuation measures will be included in the development.
- A maximum height of 48 feet and 58 feet for rooftop mechanical stairwell and patio enclosure elements, not exceeding 30% of the roof are, is included.
- Notification will be given of proximity to Airport.
- Tenants will be required to sign noise disclosures and aviation easements.
- The proposed development will be in compliance for lighting standards set forth by the Federal Aviation Administration and Scottsdale Design Standards and Policies Manual.

**Policy LU 6.5** *In accordance with the Airport's Part 150 Noise Compatibility Study, avigation easements and fair disclosure statements are required for all new and redevelopment projects in the Greater Airpark.*

**Response:** Avigation easements and fair disclosure statements will be required for new tenants.

**Goal 7** *Develop an interconnected network of Signature Corridors to support the Greater Airpark as a place for meeting, creating, shopping, learning, as well as working.*

**Response:** This development creates and helps interconnect the network of two (2) Signature Corridors which will be enhanced by the mixed-use nature of pedestrian features and ground level uses located at the Landmark Intersection, surrounding spaces, and supporting commercial/retail and employment services.

**Policy LU 7.1** *Encourage growth along corridors with the greatest potential for activity, new development, revitalization, tourist attractions, and enhanced multi-modal connections.*

**Response:** The proposed mixed-use development encourages growth, activity, and revitalization at the intersection of two (2) Signature Corridors along 73<sup>rd</sup> Street and Greenway-Hayden Loop. This proposal provides a vibrant and active residential community at this integral location within the Airpark, which is positioned within walking distance of shopping and restaurants along Scottsdale Road and within walking proximity of employment land uses further promoting pedestrian oriented activity.

**Policy LU 7.2** *Promote a greater mix of uses along identified Signature Corridors, which complement and are compatible with each respective land use designation.*

**Response:** Expanding on the statement above, this project will integrate multi-family residential and mixed uses at the intersection of two (2) Signature Corridors, further broadening the mix of uses within the area. Walk up style live/work units are proposed along the Signature Corridor that support the mixed-use component as well as employment for this area.

**Policy LU 7.3** *Encourage and incentivize revitalization along Signature Corridors, particularly south of the Central Arizona Project Aqueduct.*

**Response:** The proposed redevelopment encourages and incentivizes revitalization along both the Signature Corridors of 73<sup>rd</sup> Street and Greenway-Hayden loop which is located south of the Central Arizona Project Aqueduct.

**Policy LU 7.4** *Integrate entertainment, education uses, cultural uses, and tourist attractions along Signature Corridors.*

**Response:** The redevelopment of the Signature Corridors bordering the site combined with the non-residential floor area uses support pedestrian interest, activity, and connections to the nearby

educational, cultural, and tourism based uses of the surrounding area. This project will serve to enhance the proximity of the residential population base that supports the surrounding entertainment, tourism, and education based uses. The enhanced Landmark Intersection and the Greenway-Hayden Loop Signature Corridor provide active ground level uses in the form of a clubhouse and fitness center that encourages community interaction and contemplation.

**Goal LU 8** *Create an interconnected network of meaningful open spaces within the Greater Airpark.*

**Response:** This development as the “gateway” to the Airpark enhances and encourages the pedestrian based activity and connections including utilizing alternative modes of transportation that create a meaningful network of connections to open spaces within the development and surrounding area. The enhanced open space and pedestrian scale and amenities of the Landmark Intersection will serve as an inviting gateway.

**Policy LU 8.3** *Promote public/private partnerships in the design of development plans that provide functional urban open spaces, such as plazas and pocket parks, within and between projects.*

**Response:** A large number of functional open spaces, amenities, and open space has been included in this high quality design and development, most notably, the enhanced Landmark Intersection with public open space and pedestrian elements that will engage the public.

**b) Neighborhood & Housing**

**Goal NH 2** *Create complete neighborhoods within the Greater Airpark, through the development of urban dwelling types and mixed- use developments, while being respectful of the Greater Airpark as an aviation-based employment center.*

**Response:** This project offers various dwelling types not found in surrounding housing options and in a location ideal for a mixed use development. This mixed use project creates and compliments the surrounding developments to create a complete neighborhood and respect and compliment the employment area of the Airpark to the east. This project furthers the intent of a mixed-use development in this location while providing for neighborhood commercial/ retail services and live/work units complimentary to the Airpark and the LIV development across 73<sup>rd</sup> Street.

**Policy NH 2.1** *Encourage developments, in Airpark Mixed Use Future Land Use Areas (AMU and AMU-R), to provide support services for current and future Greater Airpark residents, such as local markets, drugstores, and other essential services.*

**Response:** The subject Site is located within an Airpark Mixed Use Future Land Use Area and as proposed would provide a mixed-use development featuring a variety of commercial and non-residential uses and floor area such as: retail/commercial clubhouse component with restaurant, office, and fitness center along with seven (7) live/work units.

**Policy NH 2.3** *Incorporate gathering spaces and recreational opportunities into the design of mixed- use development to support a high quality of life for Greater Airpark residents.*

**Policy NH 2.4** *Promote opportunities for parks, open space, and trail connections within new mixed-use development and as a part of the redevelopment of existing property.*

**Response:** This centrally located project with its enhanced pedestrian corner has the ability to draw visitors from across the Valley. Living in this area has the unique benefits of those who fly regularly from Scottsdale Airport, attend events at nearby Westworld, or hike the McDowell Mountains and nearby trails. The development provides a mix of uses and design elements that cater to the pedestrian and providing urban character and gathering spaces through the use of building design, connectivity, landscaping, open spaces, hardscape and lighting. The proposed site plan calls for an enhanced public pedestrian area and amenities, a total of five (5) courtyards, a clubhouse, and private areas providing high quality and active and passive open space and recreation for residents and the public to connect and converse on a daily basis. The development also provides a network of sidewalks that tie to outer perimeter sidewalks and into the greater pedestrian network. The walkable nature of this development along with the public Landmark Intersection and Signature Corridors provide connectivity and economic vitality that supports the continued growth within the area and the future connections.

**Policy NH 3.1** *Encourage thoughtful and creative residential development that enhances, supports, and is sensitive to Airport operations and the Greater Airpark's identity as an employment center.*

**Response:** As previously discussed, the proposed uses will support the work, live, play philosophy emphasized throughout the GACAP. Special measures will be taken to create buildings that are sensitive and compatible to the nearby Airport operations.

**Policy NH 3.2** *Incorporate residential into Airpark Mixed Use- Residential Future Land Use Areas to reduce traffic congestion, improve air quality, and provide opportunities for workforce housing where:*

- *Dwellings units will not be directly adjacent to industrial uses that could be in conflict with residential uses;*
- *Dwellings will not lie within the 55 day-night average noise level (DNL) or higher areas established by the FAA; and*
- *Multi-modal transportation options will be incorporated into residential design.*

**Response:** The proposed mixed-use multi-family community will add residential to an area ideally suited for residential development. The proposed dwelling units have a buffer from existing industrial uses, by way of an internal roadway, setbacks, and landscaping. Residences will be well aware of Airpark and employment uses within the surrounding area. The proposed dwelling units are also located well outside the 55 DNL line (see *Airpark Noise Contours Exhibit # 4*) and

appropriate sound attenuation, building materials, and insulation will be utilized. Multi-modal transportation options will be available to residents including, but not limited to walking, bicycling, driving and public transit.

- **Policy NH 3.3** *Support the integration of workforce housing and a diversity of dwelling types within the Greater Airpark.*
- **Policy NH 3.3.1** *Encourage residential development for a variety of income groups, in appropriate Greater Airpark locations, and in surrounding areas.*
- **Policy NH 3.3.2** *Develop a variety of urban dwelling types, including condominiums, apartments, townhomes, lofts, time-shares, patio homes, and work-live and/or live/work units.*
- **Policy NH 3.3.3** *Encourage live/work units in Airpark Mixed Use-Residential and Regional Tourism Future Land Use Areas in the Greater Airpark.*

**Response:** The proposed development will provide a unique retail/commercial and residential mixed-use experience for the business owners of Scottsdale. As noted above and throughout the GACAP, the work/live concept is promoted and supported in AMU-R areas. The proposed GPA will create a development that facilitates an important transition of uses and provides opportunity for live/work units.

**c) Community Mobility**

**Policy CM 4.2** *Improve pedestrian and bicycle connections from adjacent neighborhoods to Greater Airpark destinations.*

**Response:** The Signature Corridors adjacent to the property include 73<sup>rd</sup> Street and Greenway-Hayden Loop. These streets provide a framework for pedestrian and bicycle connections within the immediate area. Furthermore, the development plan will create a walk-friendly environment for its residents with internal pedestrian connections between multi-family buildings, on site amenities, and adjoining land uses.

**Goal CM 6** *Enhance pedestrian and bicyclist access and activity for Greater Airpark residents, visitors, and employees.*

**Policy CM 6.5** *Design corridors that accommodate and attract pedestrians and bicyclists, particularly in Airpark Mixed Use Future Land Use Areas and along Signature Corridors.*

**Policy CM 6.6** *Design safe, comfortable, and aesthetically-pleasing Greater Airpark pedestrian and bicyclist facilities through the incorporation of universally accessible designs, coordinated street lighting, visually-interesting landscape treatments, shading, bicycle lanes, and public art integrated into facility design.*

**Goal CM 7** *Promote sustainable transportation options that meet the needs of the current and future Greater Airpark community.*

**Goal CM 7.1** *Incorporate site design features that promote more access to those walking, cycling, or taking public transit, such as ground-floor retail and parking located in the rear of buildings, particularly along Signature Corridors and within Airpark Mixed Use Future Land Use Areas.*

**Policy CM 7.2** *Promote more sustainable modes of passenger transportation, such as alternative fuel vehicles, walking, biking, and/or other future technologies.*

**Response:** The Greater Airpark designates this property as “Shopping/Housing” along a future transit connection route, two (2) Signature Corridors, and a Landmark Intersection. (See *Airpark Signature Corridors Exhibit # 15, Airpark Future Transit Connections Exhibit # 16, and Airpark Pedestrian & Bicycle Connectivity Exhibit #17*) In response, the site plan has been designed in a manner that pays particular attention to pedestrians and bicyclists along the perimeter of the Site, by providing safe and aesthetically pleasing streetscapes while utilizing existing bike lanes.

The mixed use nature of District at the Quarter promotes pedestrian mobility and activity with ground floor retail/commercial, active non-residential floor area uses, enhanced streetscapes with pedestrian amenities, shade and landscaping, and a distinctive public gathering space at the intersection of the two (2) Signature Corridors to form a Landmark Intersection. These elements together complete an enhanced streetscape that is pleasing, interesting, safe, and comfortable while providing for pedestrian access that easily transitions into the surrounding area.

Integration of housing within the greater area and a mixed use development at this Airpark location increases the efficiency of movement by people by locating them close to employment and retail services and minimizing vehicular trips creating a sustainable walkable community. The design and location of this development promotes walkability and alternative modes of transportation including biking infrastructure and easy access to nearby transit. Furthermore, parking is located towards the back and internal to the development, in order to activate the streets with pedestrian amenities, public space, and ground floor mixed-uses.

#### **d) Economic Vitality**

**Policy EV 1.1** *Develop and implement long-term economic development strategies that maintain and enhance city revenue streams in order to balance the area’s revenue generation with the cost of services and ensure financial stability now and in the future.*

**Response:** The proposed work, live, play land use concept achieves this policy on several levels. The construction of new multi-family development will generate significant building permit fees and revenue for the City of Scottsdale as well as increased sales tax revenue for the City. Secondly, the integration of approximately 622 residential units in the Scottsdale Airpark area will

increase retail sales and sales tax revenue for the City. Lastly, the proposed residential community will provide on-site jobs for management and maintenance.

The integration of new development will enhance the City's revenue streams and provide financial benefits to Scottsdale businesses and Airpark. Residents and business owners require services, goods, food, and entertainment and draw from local businesses and resources. By creating a walkable environment that already has abundant retail and service opportunities nearby, a land use balance will be created which will strengthen the City's long-term economic stability. The Scottsdale Airpark as an economic engine generates hundreds of millions of dollars per year, and billions of dollars of spin-off financial benefits. The inclusion of this mixed use development will increase the money spent by residents and workers living, working, and playing within the Airpark. This project provides the activity and subsequent revenue to ensure long term financial stability and precedent for surrounding investment.

**Policy EV 2.5** *Aggressively market the Greater Scottsdale Airpark as an ideal destination to work, live, and play.*

**Response:** The residential and commercial/retail components proposed under this application complements the existing mixed-use character of the Scottsdale Airpark of which is particularly found at and planned for at this intersection. The Airpark as a whole creates a unique opportunity to foster interrelated land uses and promote the live, work, and play concept, which is memorialized numerous times in the GACAP. The surrounding retail development, airport, and nearby employment core provide regional appeal for future residents. This application is driven by the Site's surrounding land uses and strong market demand for multi-family residential in this area. Business and housing trends indicate a fundamental shift away from traditional homeownership and business operations as a result of the growing tech generation (i.e. millennials) and their desire for true work/live experience an alternative mixed-use, high quality (i.e. design/tech oriented/services/amenities) development options fit their lifestyle.

**e) Environmental Planning**

**Policy EP1.3** *Promote landscape design and irrigation methods that contribute to water and energy conservation.*

**Response:** The landscaping for the proposed development will be in accordance with the city approved landscape palette and blends with surrounding developments in a cohesive and coherent manner. With a judicious use of water as a design element, there has been an intentional preference to surround the project with native plants that show off the natural Sonoran beauty. The project will proudly showcase the combination of natural shapes, textures, and materials from this region and incorporate shade structures.

**Policy EP 4.8** *Building design should respect and enhance the Sonoran Desert context of the Greater Airpark using building orientation, landscape buffers, colors, textures, material and lighting.*

**Policy EP 5.4** *Encourage landscape improvements that limit the amount of turf area and make optimal use of indigenous adapted desert plants.*

**Response:** To further elaborate on the statements above, the buildings have been designed in a manner to respond to the Sonoran Desert climate through the use of shading, landscaping, recessed windows, articulation, material selection, textures, paint colors, scale and massing in balance with the surrounding community. The development proposal promotes a rich desert landscape palette in a contemporary theme that celebrates the unique character and quality of life of the Sonoran Desert while providing an attractive context appropriate setting for the buildings.

***f) Character & Design***

**Policy CD 1.1** *Promote innovative, high quality design using specific design criteria associated with each Future Land Use Area in the Greater Airport:*

**Airpark Mixed Use Future Land Use Areas (AMU & AMU-R)**

*The character of these areas is pedestrian-oriented, urban, and human-scale and features a variety of open spaces, gathering areas, and multi-modal transportation options. Multi-modal transportation should include bicycle and transit access connected to a pedestrian network to encourage social contact and interaction among the community. Design elements should be oriented toward people, such as the provision of shelter and shade for the pedestrian, active land uses at the ground floor/ street level, and a variety of building forms and facade articulation to visually shorten long distances. A variety of textures and natural materials is encouraged to provide visual interest and richness, particularly at the pedestrian level. Design of this Future Land Use Area should be based on a small city block layout with mid-block connections to promote greater walkability. The public realm may be activated through building and site design, orientation toward the street, high-activity uses on the street level, and the integration of public art.*

**Response:** The proposed development achieves this policy in the following ways:

- Mixed-use live, work, play philosophy enhanced with proposed combination of uses.
- Building design includes context appropriate massing, architecture and materials including pedestrian scale design at floor level.
- Special attention given to pedestrian linkages both internal to the Site and along the perimeter to emphasize connectivity.
- Compatibility with surrounding context.
- Site and building design focused on Sonoran Desert climate through the use of shading, recessed windows, articulation, material selection, textures, paint colors, scale and massing.

- Open space maximized; development provides abundant open space with approximately 10.3% (excluding street frontage area or parking lot landscaping) useable open space (i.e. 10% required) with approximately 20% (including all open space, street frontage, and parking landscape areas). This also includes dog walk/wash areas too.

In summation, there will be many gathering areas where people can come together in an environmentally friendly passive and/or active setting.

**Policy CD 1.2** *Lighting should be designed to minimize glare, conserve energy, and accent the respective Future Land Use Area character.*

**Response:** The on-site lighting will be designed in a manner to minimize glare and conserve energy while respecting and remaining consistent with the surrounding land uses. One of the lighting goals will be to provide appropriate low-level pedestrian scale lighting (bollard and foot lighting) for pedestrians walking at night. The lighting will be integrated with the abundant desert landscaping proposed with this development.

**Goal CD 2** *Create vibrant Signature Corridors in the Greater Airpark to provide a distinct identity and design theme in the area.*

**Response:** The Signature Corridors bordering this site are designed to create a comfortable, safe, and aesthetically pleasing streetscape with passive open spaces along the street frontage and anchored by an enhanced Landmark Intersection pedestrian area with public pedestrian amenities and open space. Various design measures are included such as shade, seating, bicycle parking, landscaping, and ground level live/work units along both Signature Corridors.

**Policy CD 2.1.3 Greenway-Hayden Signature Corridor (emphasis added)**

*The Greenway-Hayden Signature Corridor should serve as the “Main Street” of the Greater Airpark, north of the runway, and should reference multi-modal connections from other cities to major area destinations, such as the Tournament Players Club Golf Course and Scottsdale Sports Complex. The public realm should animate the street during all seasons. Design elements could include large window displays, banners, integrated signage, passive cooling elements, covered patios, shade, public art, bicycle/pedestrian connections, and gathering areas.*

**Response:** The proposed development will highlight Greenway-Hayden Loop as a Signature Corridor and provide design elements that cater to the pedestrian through the use of building character, design, signage, walkway connectivity, landscape separated sidewalks, hardscape and lighting. This corridor connection orients pedestrian towards the Landmark Intersection pedestrian areas and open space as well as the clubhouse and fitness center located at and designed for the ground level containing large glass windows and activity. This corridor streetscape is designed to cater to the pedestrian serving as an inviting connection to the surrounding areas and public spaces and amenities that provide shelter, shade, and a variety of

building articulation to visually shorten longer building expanses. Furthermore, the Signature Corridor streetscape will contain walkways and stoop elements for optional ground level live/work units from the sidewalk that further build upon community interaction. (See *Corner Landmark Intersection Enlargement within the Landscape Plan Set Exhibit # 6 & Airpark Signature Corridors Exhibit # 15*)

**Policy CD 2.1.7 73<sup>rd</sup> Signature Corridor (emphasis added)**

*The 73rd Signature Corridor is a major pedestrian corridor in the Greater Airpark. Design elements along this street should include aviation themes and human-scale building orientation. Shade is an important element and may be incorporated through the built environment, as well as natural shading. Hardscape elements could include bollards, foot lighting, and sophisticated fencing that secure taxi lanes from pedestrian activity. Low-lying vegetation, such as vines and shrubs, are important landscape features that should soften the continuous building mass characteristics of land uses on the east side of the corridor.*

**Response:** The proposed development will emphasize 73<sup>rd</sup> Street as a Signature Corridor and provide design elements that are pedestrian oriented through the use of building and pedestrian scaled design, shaded rest areas, signage, landscape separated walkways, live/work stoops, hardscape, and lighting that enhance the streetscape. This Signature Corridor provides a safe, comfortable, and aesthetically pleasing streetscape connection that encourages walking and orients pedestrians towards ground level uses, public spaces, and the nearby surrounding areas. A variety of building articulation visually shortens long building expanses. Additionally, the completion of this development and walkable streetscape will improve and encourage activity and mobility further along this north to south corridor as well as drawing pedestrians in and through the development. The walkways and stoop elements at the street level will reinforce and help to complete the street scene similar to the adjacent LIV development. The completion will promote pedestrian access to shopping, retail, and businesses to the south, and current and future destinations to the north such as the ultimate redevelopment of the Cracker-Jax site.

**Policy CD 2.2 Signature Corridor streetscapes should provide continuity among adjacent uses through a comprehensive landscape design, including decorative paving, street furniture, public art, and integrated infrastructure improvements.**

**Response:** The Signature Corridors included in this development are designed to create an enhanced streetscape theme featuring passive open spaces, shade, and decorative paving that provide continuity to the adjacent and surrounding uses and create a safe and comfortable experience.

**Policy CD 2.3 In designated Signature Corridors, encourage pedestrian- and transit-oriented development, with parking and automobile access in the rear of the development, and short access paths to transit.**

**Response:** This development is designed to create a pedestrian oriented environment at this location of two (2) Signature Corridors featuring streetscapes, automobile parking located internally (i.e. garages), and short and comfortable pathways to nearby transit.

**Policy CD 2.4** *Incorporate multi-modal access along Signature Corridors.*

**Response:** With the addition of enhanced streetscapes and their ultimate connectivity to transit and surrounding area that promote walking, bicycle parking has also been included in the front of the development.

**Policy CD 2.5** *Frame designated Landmark Intersections with prominent buildings, community landmarks, high quality architecture, and enhanced streetscape treatments to enhance Greater Airpark identity and assist with wayfinding.*

**Response:** The Landmark Intersection will be a prominent and memorable location framed by the ground level uses high quality architecture and enhancement of the Landmark Intersection public open space features and amenities that assist in wayfinding/placemaking and contribute to the identity of the immediate intersection and area.

**Policy CD 2.6** *Where Signature Corridors intersect, and particularly at designated Landmark Intersections, incorporate distinct, landmark architecture, which incorporates elements of the intersecting design themes.*

**Response:** The proposal includes an enhanced and distinctive public pedestrian area at the Landmark Intersection that includes decorative paving, street furniture, shade, seating, and pedestrian level uses and distinctive architecture incorporating elements of the intersecting design themes. The Landmark Intersection will be a safe, comfortable, and distinctive which will be a memorable and pleasing addition to the area.

**VII. PUD Criteria**

**Section 5.5003** of the Zoning Ordinance states that the development proposals shall comply with the following criteria:

**A. PUD Zoning District Approval Criteria,**

1. *As part of the approval or modified approval of an application for a PUD district, the Planning Commission shall recommend and the City Council shall find that the following criteria have been met:*
  - A. *The proposed development promotes revitalization, the goals, policies and guidelines of the General Plan, Area Plans and Design Guidelines.*

**Response:** The proposed development accomplishes a range of goals including the public benefit of the revitalization of two (2) Signature Corridors streetscapes and Landmark Intersection/“gateway”, integrating high quality, vibrant architecture and site planning to the area, and creating pedestrian synergy that will complement the surrounding area. The proposed development meets and furthers the goals and policies of the General Plan and GACAP as discussed in this narrative. For example, the development of enhanced common open spaces of the Landmark Intersection and streetscapes of the Site.

*B. The proposed development’s uses, densities, or development standards would not otherwise be permitted by the property’s existing zoning.*

**Response:** The proposed development would not be permitted under the existing I-1 zoning designation.

*C. The proposed development will be compatible with adjacent land uses and promotes the stability and integrity of abutting or adjacent residential neighborhoods.*

**Response:** The proposed development is compatible with adjacent land uses and maintains the integrity of the Airpark employment core by providing a balance between residential and employment uses. Adjacent multi-family residential to the project is compatible and of similar character.

*D. That there is adequate infrastructure and City services to serve the development.*

**Response:** There are adequate infrastructure and City services to serve the development.

*E. That the proposal meets the following location criteria:*

*i. The proposed development is not located within any areas zoned environmentally sensitive lands ordinance (ESL) nor within the boundaries of the Downtown Plan.*

**Response:** The property is not located within the ESL area or within the boundaries of the Downtown Plan.

*ii. The proposed development fronts onto a major or minor arterial and/or major collector street as designated in the City’s transportation master plan.*

**Response:** The proposed development fronts Greenway-Hayden Loop, a minor arterial and 73<sup>rd</sup> Street a minor collector.

## VIII. Conclusion

In summary, the applicant is seeking a Minor GPA and rezoning on an approximately 10.29+/- gross acre and 8.83+/- net acre site located at 15501 North 73rd Street to create a unique luxury multi-family residential and mixed use community with approximately 622 units and 22,025 sq. ft. commercial/ retail (i.e. non-residential) use to enhance upon the work, live, and play environment encouraged within this area of the Greater Airpark Character Area Plan.

As mentioned above, workers, millennials, and professionals alike desire a work/live lifestyle option that is different than a traditional workplace and household environment. One that affords them a flexible schedule live, work, and play concept like the cutting edge experience proposed for District at the Quarter; a development that promotes a mix of land uses, walkability/bike riding, reduced trip generation, environmental responsibility, amenities, and professional synergy.

The proposed mix of uses will enhance the local and regional economic base. The property is surrounded by a variety of employment, recreation, entertainment, office, instructional/education and service related business in the Scottsdale Airpark and there is a strong demand for this unique work/live concept.

This is a very unique and exciting project that will not only be a success, but will exemplify the vision that the city of Scottsdale hopes to achieve in the area.

# EXHIBITS

# Exhibit 1

## Dimensioned Plan Boundaries



# Exhibit 2

Aerial



Aerial Exhibit

# Exhibit 3

Context Aerial



**DISTRICT AT THE QUARTER**  
**KAPLAN ACQUISITIONS, LLC**

CONTEXT  
 SITE PLAN **A1.2**  
 GRAPHIC SCALE

8-03-16

# Exhibit 4

## Site Plan



# Exhibit 5

## Architectural Plan Set



**3-GP-16/8-ZN-16**

**08/05/16**

**DISTRICT AT THE QUARTER**  
KAPLAN ACQUISITIONS, LLC

PERSPECTIVE  
CORNER OF DIAL BLVD AND GREENWAY HAYDEN LOOP  
NOT TO SCALE

**A2.1**  
8-03-16



**biluform**  
architecture

# DISTRICT AT THE QUARTER

KAPLAN ACQUISITIONS, LLC

PERSPECTIVE  
DIAL BOULEVARD  
NOT TO SCALE

**A2.2**  
8-03-16





TYPICAL UNITS ON SOUTH SIDE  
SOLAR SHADING - JUNE - 21ST (SOLAR NOON)



LOCATION OF ENLARGED AREAS OF SOUTH ELEVATION

# DISTRICT AT THE QUARTER

KAPLAN ACQUISITIONS, LLC

NOTES  
ALL FRAME-OUTS WALL PROJECTIONS ARE TAKEN FROM CORRESPONDING ELEVATION PLANE MARKED AS +0.

TYPICAL ELEVATION WORK SHEET  
DIAL BOULEVARD  
NOT TO SCALE

A2.4

8-03-16



LIVE WORK UNIT PATIO ALONG GREENWAY-HAYDEN AND DIAL BLVD. WITH ACCESS TO MAIN SIDEWALK.

TYPICAL BUILDING ENTRY ALONG GREENWAY-HAYDEN AND DIAL BLVD.



LOCATION OF ENLARGED AREAS OF EAST ELEVATION

NOTES  
 ALL FRAME-OUTS WALL PROJECTIONS ARE TAKEN FROM CORRESPONDING ELEVATION PLANE MARKED AS +0.

TYPICAL ELEVATION WORK SHEET  
 DIAL BOULEVARD  
 NOT TO SCALE

A2.5

8-03-16

DISTRICT AT THE QUARTER  
 KAPLAN ACQUISITIONS, LLC



COOL DECEMBER  
DUNN EDWARDS - DEW383  
BODY COLOR

ALMOND  
DUNN EDWARDS - DEC753  
BODY COLOR

COVERED IN PLATINUM  
DUNN EDWARDS - DE6367  
BODY COLOR

COUNTRY BEIGE  
FRENCH LIMESTONE  
CORONADO STONE



HICKORY  
DUNN EDWARDS - DEC759  
CORNICHE



COCOA POWDER  
DUNN EDWARDS - DET631  
RAILINGS/DOORS/CANOPIES



BRONZE  
WINDOW/STOREFRONT FRAMES



SAMPLE LOCATION

HICKORY  
ALMOND  
HICKORY  
BRONZE  
COOL DECEMBER  
COUNTRY BEIGE  
HICKORY



HICKORY  
ALMOND  
COOL DECEMBER  
COCOA POWDER  
COOL DECEMBER  
COOL DECEMBER  
COVERED IN PLATINUM  
ALMOND  
COUNTRY BEIGE  
BRONZE

biliform  
architecture

# DISTRICT AT THE QUARTER

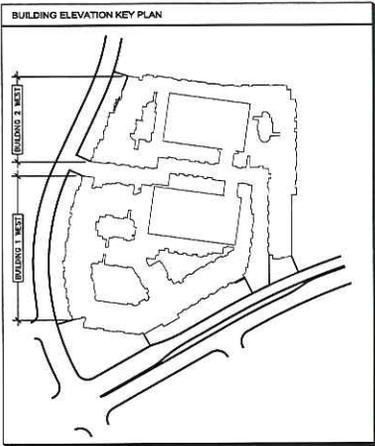
KAPLAN ACQUISITIONS, LLC

BUILDING COLOR AND MATERIALS  
DIAL BOULEVARD  
NOT TO SCALE

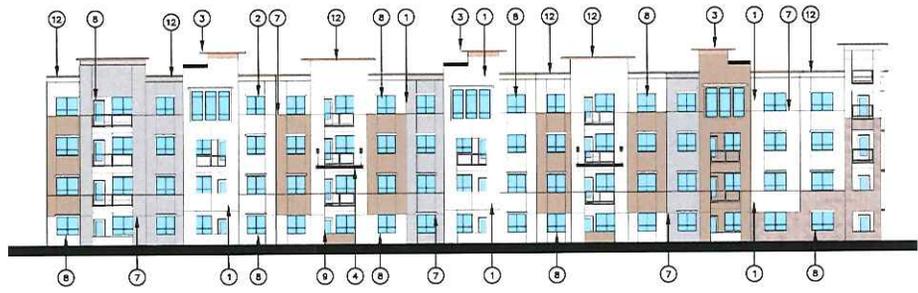
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8-03-16

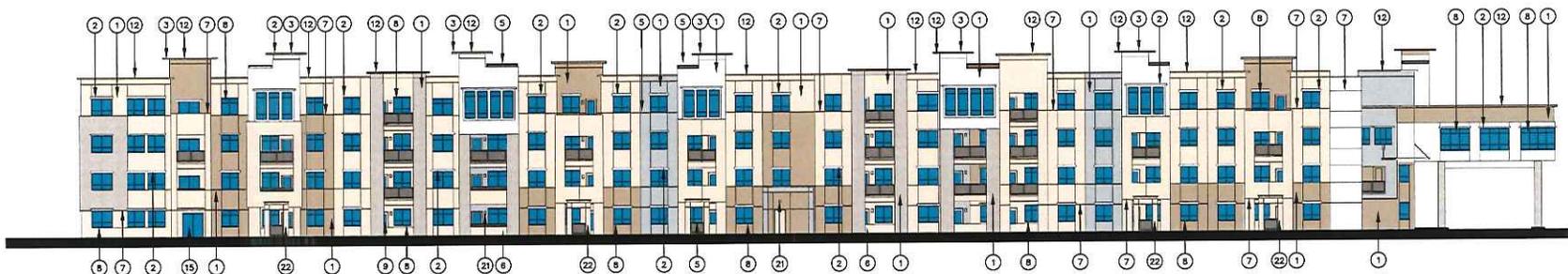
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- KEY NOTES**
1. STUCCO FINISH
  2. 8" DEEP STUCCO FINISH EYEBROW - SHADING DEVICE
  3. SOLARIUM UNIT AT THE 4TH FLOOR
  4. METAL SHADE CANOPY - SHADING DEVICE
  5. DECORATIVE METAL RAILING 442" A.F.F.
  6. PATIO WALL WITH STUCCO FINISH
  7. STUCCO CONTROL JOINT
  8. WINDOW UNIT, RECESSED 2" MINIMUM
  9. FRENCH DOOR
  10. FOLDING OR TELESCOPING DOOR SYSTEM
  11. STOREFRONT GLAZING
  12. METAL PARAPET CAP - 42" MINIMUM, HEIGHT MEASURED FROM FINISHED ROOF DECK
  13. METAL DOOR - PAINTED ACCENT COLOR
  14. STAIR BULKHEAD
  15. SLIDING GLASS PATIO DOOR
  16. TOP OF ROOF DECK, BEYOND @ +46'-0"
  17. 6" WALL POP-OUT
  18. ROOF TOP DECK
  19. PROJECT SIGNAGE
  20. 4" STUCCO POP-OUT WINDOW SURROUND
  21. EXTERIOR LIGHT
  22. LIVE / WORK ENTRY



BUILDING #2 - WEST ELEVATION - DIAL BOULEVARD



BUILDING #1 - WEST ELEVATION - DIAL BOULEVARD

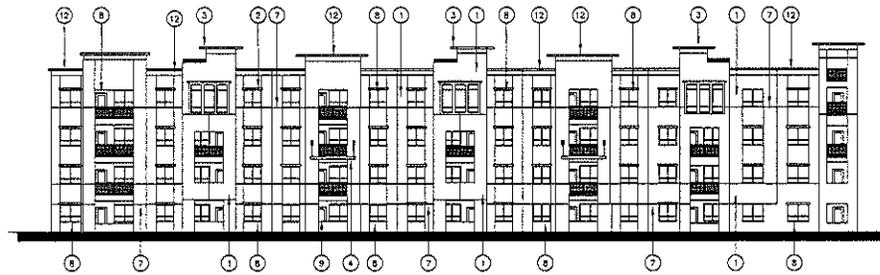
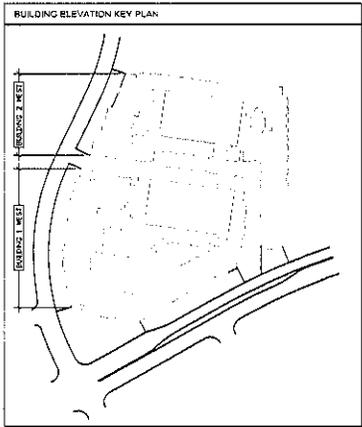


**DISTRICT AT THE QUARTER**  
 KAPLAN ACQUISITIONS, LLC



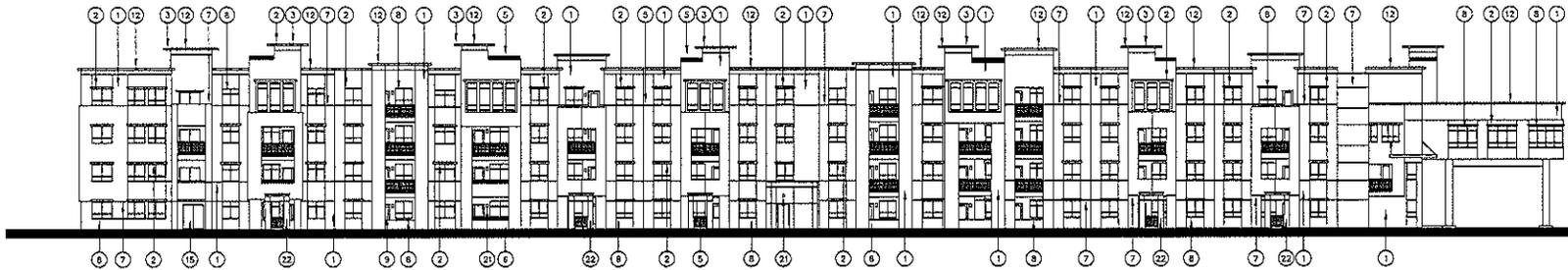
BUILDING ELEVATIONS  
 BUILDING #1 ELEVATIONS  
 1/16" = 1'-0"

**A7.1**  
 8-03-16



BUILDING #2 - WEST ELEVATION - DIAL BOULEVARD

- KEY NOTES**
1. STUCCO FINISH
  2. 8" DEEP STUCCO FINISH CYEBROW - SHADING DEVICE.
  3. SOLARIUM LAMP AT THE 4TH FLOOR.
  4. METAL SHADE CANOPY - SHADING DEVICE.
  5. DECORATIVE METAL RAILING +42" A.F.F.
  6. PATIO WALL WITH STUCCO FINISH
  7. STUCCO CONTROL JOINT
  8. WINDOW UNIT, RECESSED 2" MINIMUM.
  9. FRENCH DOOR.
  10. FOLDING OR TELESCOPING DOOR SYSTEM.
  11. STONEFRONT GLAZING.
  12. METAL PARASOL CAP - 42" MINIMUM HEIGHT MEASURED FROM FINISHED ROOF DECK.
  13. METAL DOOR - PAINTED ACCENT COLOR.
  14. STAIR RAILHEAD.
  15. SLIDING GLASS PATIO DOOR.
  16. TOP OF ROOF DECK BEYOND @ +46'-0".
  17. 8" WALL POP-OUT.
  18. ROOF TOP DECK.
  19. PROJECT SIGNAGE.
  20. 4" STUCCO POP-OUT WINDOW SURROUND.
  21. EXTERIOR LIGHT.
  22. LIVE / WORK ENTRY



BUILDING #1 - WEST ELEVATION - DIAL BOULEVARD

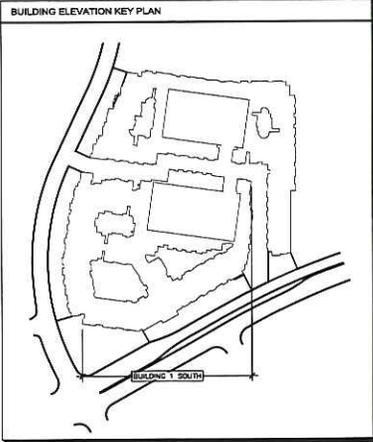


DISTRICT AT THE QUARTER  
KAPLAN ACQUISITIONS, LLC

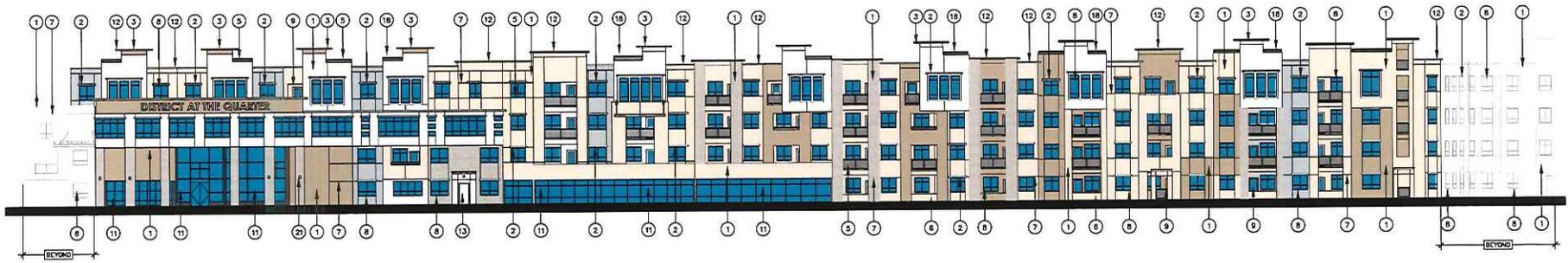


BUILDING ELEVATIONS  
BUILDING #1 ELEVATIONS  
1/16" = 1'-0"

A7.2  
8-03-16



- KEY NOTES**
1. STUCCO FINISH
  2. 6" DEEP STUCCO FINISH EYEBROW - SHADING DEVICE
  3. SOLARIUM UNIT AT THE 4TH FLOOR
  4. METAL SHADE CANOPY - SHADING DEVICE
  5. DECORATIVE METAL RAILING 4x12" A.F.F.
  6. PATIO WALL WITH STUCCO FINISH
  7. STUCCO CONTROL JOINT
  8. WINDOW UNIT, RECESSED 2" MINIMUM
  9. FRENCH DOOR
  10. FOLDING OR TELESCOPING DOOR SYSTEM
  11. STOREFRONT GLAZING
  12. METAL PARAPET CAP - 42" MINIMUM, HEIGHT MEASURED FROM FINISHED ROOF DECK
  13. METAL DOOR - PAINTED ACCENT COLOR
  14. STAIR BULKHEAD
  15. SLIDING GLASS PATIO DOOR
  16. TOP OF ROOF DECK BEYOND @ +46'-0"
  17. 6" WALL POP-OUT
  18. ROOF TOP DECK
  19. PROJECT SIGNAGE
  20. 4" STUCCO POP-OUT WINDOW SURROUND
  21. EXTERIOR LIGHT
  22. LIVE / WORK ENTRY



BUILDING #1 - SOUTH ELEVATION - GREENWAY HAYDEN LOOP



**DISTRICT AT THE QUARTER**  
 KAPLAN ACQUISITIONS, LLC

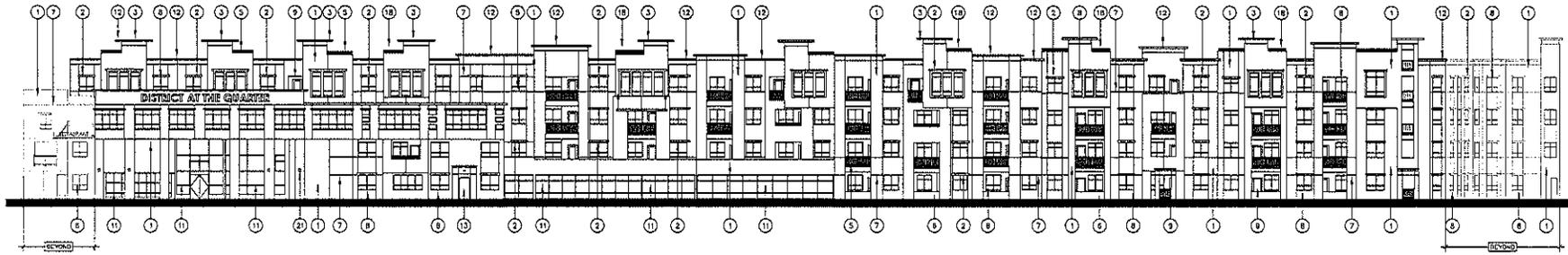


BUILDING ELEVATIONS  
 BUILDING #1 ELEVATIONS  
 1/16" = 1'-0"

**A7.3**  
 8-03-16



- KEY NOTES**
1. STUCCO FINISH
  2. 6" DEEP STUCCO FINISH EYEBROW - SHADING DEVICE
  3. SOLARIUM UNIT AT THE 4TH FLOOR
  4. METAL SHADE CANOPY - SHADING DEVICE
  5. DECORATIVE METAL RAILING +42" A.F.F.
  6. PATIO WALL WITH STUCCO FINISH
  7. STUCCO CONTROL JOINT
  8. WINDOW UNIT, RECESSED 2" MINIMUM
  9. FRENCH DOOR
  10. FOLDING OR TELESCOPING DOOR SYSTEM
  11. STOREFRONT GLAZING
  12. METAL PARAPET CAP - 42" MINIMUM HEIGHT MEASURED FROM FINISHED ROOF DECK
  13. METAL DOOR - PAINTED ACCENT COLOR
  14. STAR BULKHEAD
  15. SLIDING GLASS PATIO DOOR
  16. TOP OF ROOF DECK BEYOND @ +46'-0"
  17. 8" WALL POP-OUT
  18. ROOF TOP DECK
  19. PROJECT SIGNAGE
  20. 4" STUCCO POP-OUT WINDOW SURROUND
  21. EXTERIOR LIGHT
  22. LINC / WORK ENTRY



BUILDING #1 - SOUTH ELEVATION - GREENWAY HAYDEN LOOP

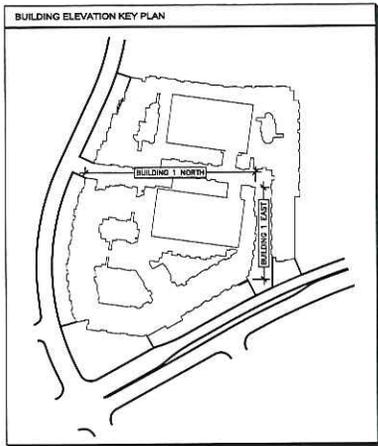


DISTRICT AT THE QUARTER  
KAPLAN ACQUISITIONS, LLC

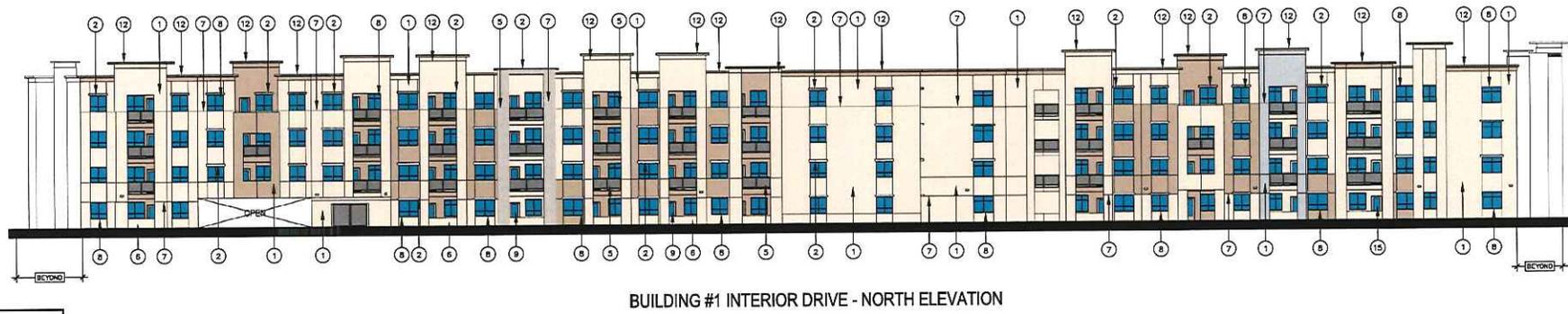
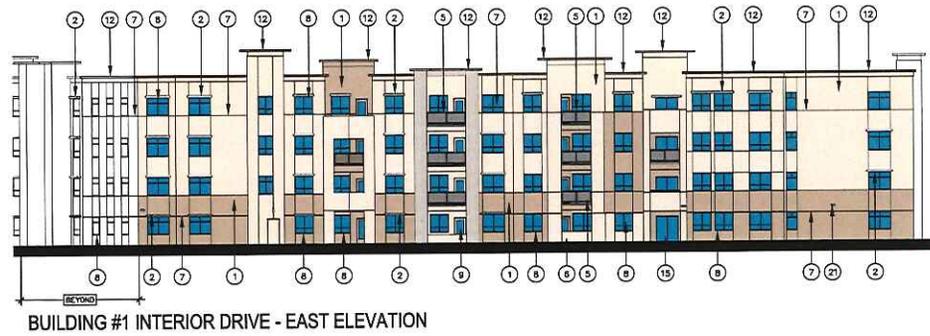


BUILDING ELEVATIONS  
BUILDING #1 ELEVATIONS  
1/16" = 1'-0"

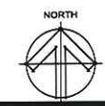
A7.4  
8-03-16



- KEY NOTES**
1. STUCCO FINISH
  2. 8" DEEP STUCCO FINISH EYEBROW - SHADING DEVICE
  3. SOLARIUM UNIT AT THE 4TH FLOOR
  4. METAL SHADE CANOPY - SHADING DEVICE
  5. DECORATIVE METAL RAILING +42" A.F.F.
  6. PATIO WALL WITH STUCCO FINISH
  7. STUCCO CONTROL JOINT
  8. WINDOW UNIT, RECESSED 2" MINIMUM
  9. FRENCH DOOR
  10. FOLDING OR TELESCOPING DOOR SYSTEM
  11. STOREFRONT GLAZING
  12. METAL PARAPET CAP - 42" MINIMUM HEIGHT MEASURED FROM FINISHED ROOF DECK
  13. METAL DOOR - PAINTED ACCENT COLOR
  14. STAIR BULKHEAD
  15. SLIDING GLASS PATIO DOOR
  16. TOP OF ROOF DECK BEYOND @ +46'-0"
  17. 6" WALL POP-OUT
  18. ROOF TOP DECK
  19. PROJECT SIGNAGE
  20. 4" STUCCO POP-OUT WINDOW SURROUND
  21. EXTERIOR LIGHT
  22. LIVE / WORK ENTRY

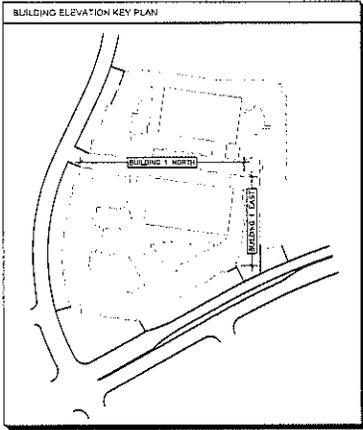


**DISTRICT AT THE QUARTER**  
 KAPLAN ACQUISITIONS, LLC

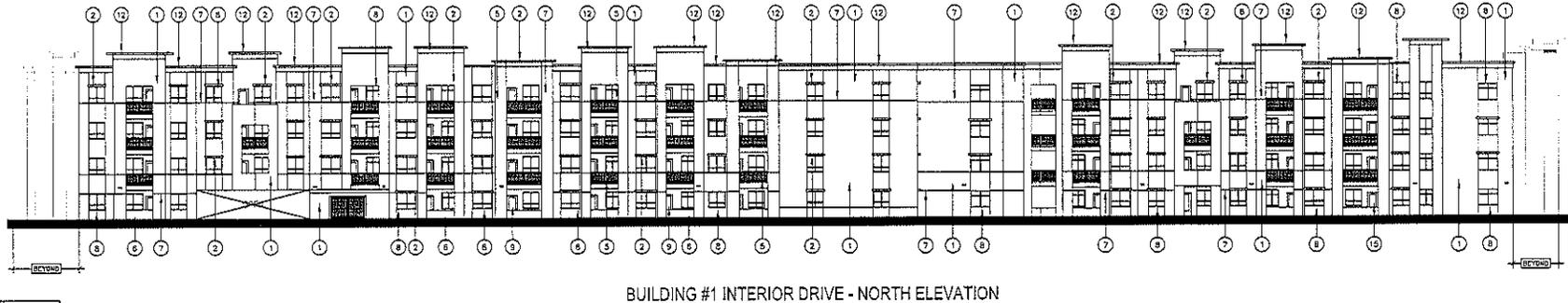
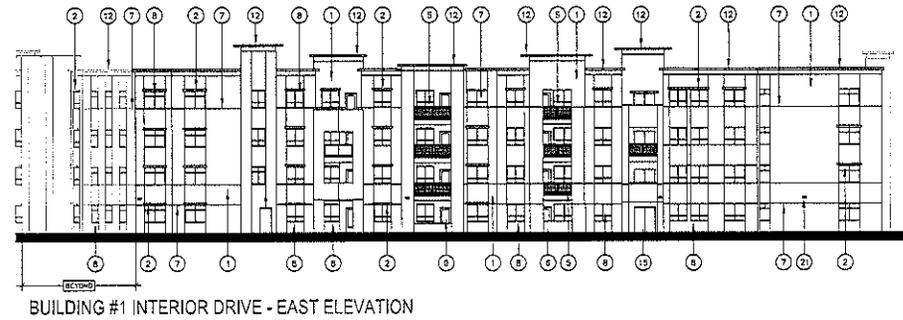


BUILDING ELEVATIONS  
 BUILDING #1 ELEVATIONS  
 1/16" = 1'-0"

**A7.5**  
 8-03-16



- KEY NOTES**
1. STUCCO FINISH
  2. 8" DEEP STUCCO FINISH EYEBROW - SHADING DEVICE
  3. SOLARIUM UNIT AT THE 4TH FLOOR
  4. METAL SHADE CANOPY - SHADING DEVICE
  5. DECORATIVE METAL RAILING +42" A.F.F.
  6. PATIO WALL WITH STUCCO FINISH
  7. STUCCO CONTROL JOINT
  8. WINDOW UNIT, RECESSED 2" MINIMUM
  9. FRENCH DOOR
  10. FOLDING OR TELESCOPING DOOR SYSTEM
  11. STOREFRONT GLAZING
  12. METAL PARAPET CAP - 42" MINIMUM HEIGHT MEASURED FROM FINISHED ROOF DECK
  13. METAL DOOR - PAINTED ACCENT COLOR
  14. STAIR BULKHEAD
  15. GLIDING CLASS PATIO DOOR
  16. TOP OF ROOF DECK BEYOND @ +44'-0"
  17. 8" WALL POP-OUT
  18. ROOF TOP DECK
  19. PROJECT SIGNAGE
  20. 4" STUCCO POP-OUT WINDOW SURROUND
  21. EXTERIOR LIGHT
  22. LINC / WORK ENTRY

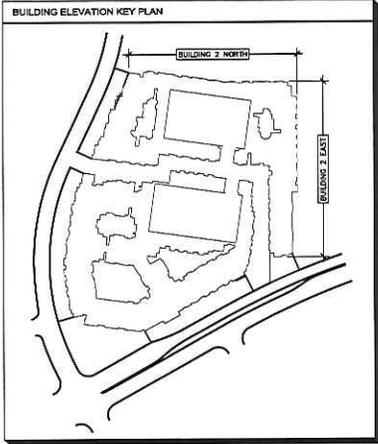


DISTRICT AT THE QUARTER  
KAPLAN ACQUISITIONS, LLC

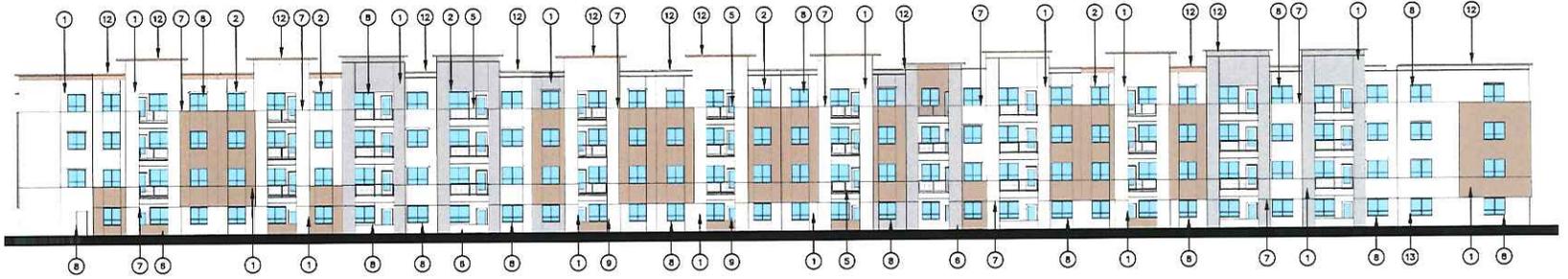


BUILDING ELEVATIONS  
BUILDING #1 ELEVATIONS  
1/16" = 1'-0"

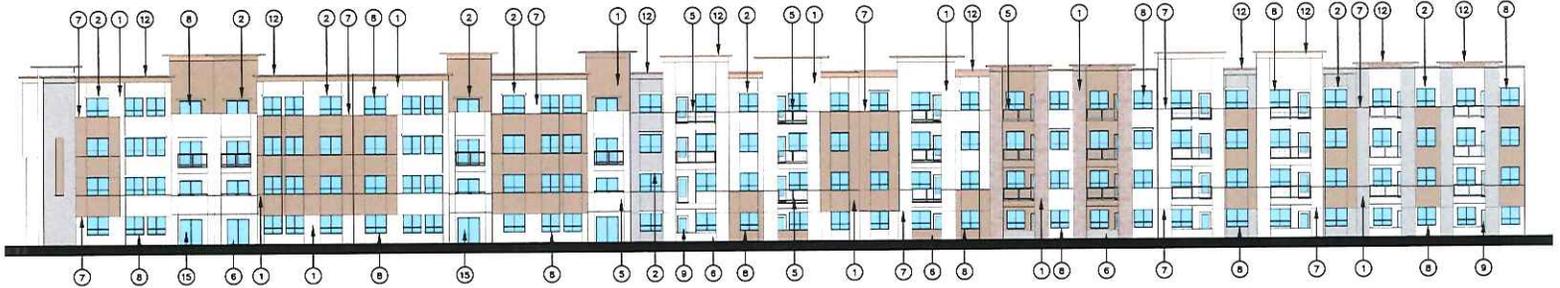
A7.6  
8-03-16



- KEY NOTES**
1. STUCCO FINISH
  2. 8" DEEP STUCCO FINISH EYEBROW - SHADING DEVICE
  3. SOLARIUM UNIT AT THE 4TH FLOOR.
  4. METAL SHADE CANOPY - SHADING DEVICE.
  5. DECORATIVE METAL RAILING +42" A.F.F.
  6. PATIO WALL WITH STUCCO FINISH
  7. STUCCO CONTROL JOINT
  8. WINDOW UNIT, RECESSED 2" MINIMUM.
  9. FRENCH DOOR.
  10. FOLDING OR TELESCOPING DOOR SYSTEM.
  11. STOREFRONT GLAZING.
  12. METAL PARAPET CAP - 42" MINIMUM HEIGHT MEASURED FROM FINISHED ROOF DECK.
  13. METAL DOOR - PAINTED ACCENT COLOR.
  14. STAIR BULKHEAD.
  15. SLIDING GLASS PATIO DOOR.
  16. TOP OF ROOF DECK BEYOND @ +46'-0".
  17. 8" WALL POP-OUT.
  18. ROOF TOP DECK.
  19. PROJECT SIGNAGE.
  20. 4" STUCCO POP-OUT WINDOW SURROUND.
  21. EXTERIOR LIGHT
  22. LIVE / WORK ENTRY



BUILDING #2 - NORTH ELEVATION



BUILDING #2 - EAST ELEVATION

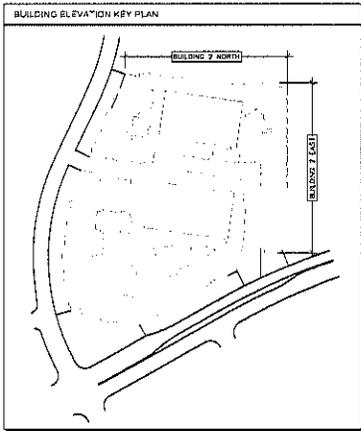
**DISTRICT AT THE QUARTER**  
**KAPLAN ACQUISITIONS, LLC**



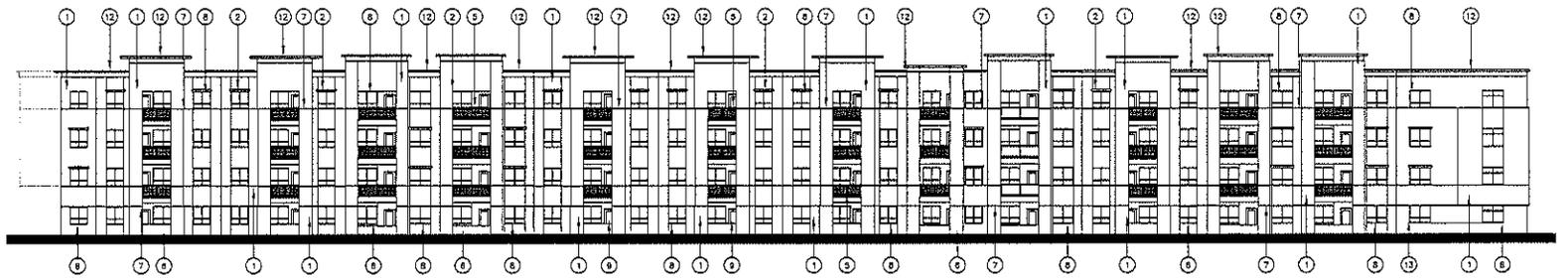
**BUILDING ELEVATIONS**  
**BUILDING #2 ELEVATIONS**  
 1/16" = 1'-0"

**A7.7**  
 8-03-16

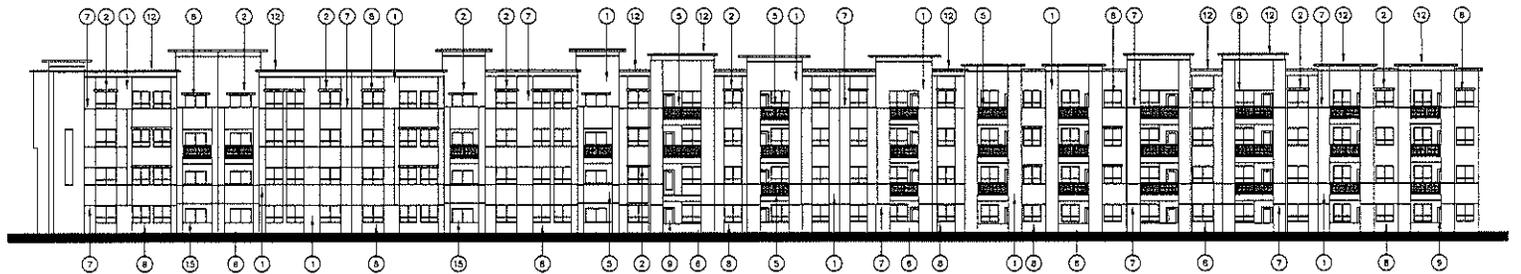




- KEY NOTES**
1. STUCCO FINISH
  2. 8" DEEP STUCCO FINISH EYEBROW - SHADING DEVICE.
  3. SOLARIUM UNIT AT THE 4TH FLOOR.
  4. METAL SHADE CANOPY - SHADING DEVICE.
  5. DECORATIVE METAL RAILING +42" A.F.F.
  6. PATIO WALL WITH STUCCO FINISH
  7. STUCCO CONTROL JOINT
  8. WINDOW UNIT, RECESSED 2" MINIMUM.
  9. FRENCH DOOR.
  10. FOLDING OR TELESCOPING DOOR SYSTEM.
  11. STOREFRONT GLAZING.
  12. METAL PARAPET CAP - 42" MINIMUM, HEIGHT MEASURED FROM FINISHED ROOF DECK.
  13. METAL DOOR - PAINTED ACCENT COLOR.
  14. STAIR BUMHEAD.
  15. SLIDING GLASS PATIO DOOR.
  16. TOP OF ROOF DECK BEYOND @ +46'-0".
  17. 8" WALL POP-OUT.
  18. ROOF TOP DECK.
  19. PROJECT SIGNAGE.
  20. 4" STUCCO POP-OUT WINDOW SURROUND.
  21. EXTERIOR LIGHT
  22. LIVE / WORK ENTRY



BUILDING #2 - NORTH ELEVATION



BUILDING #2 - EAST ELEVATION

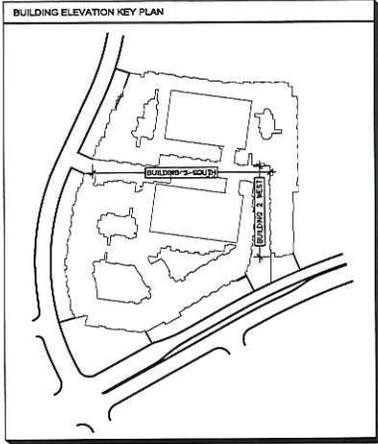


**DISTRICT AT THE QUARTER**  
**KAPLAN ACQUISITIONS, LLC**

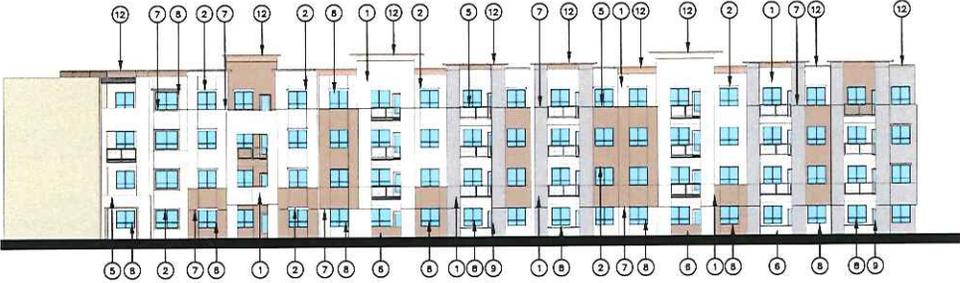


BUILDING ELEVATIONS  
 BUILDING #2 ELEVATIONS  
 1/16" = 1'-0"

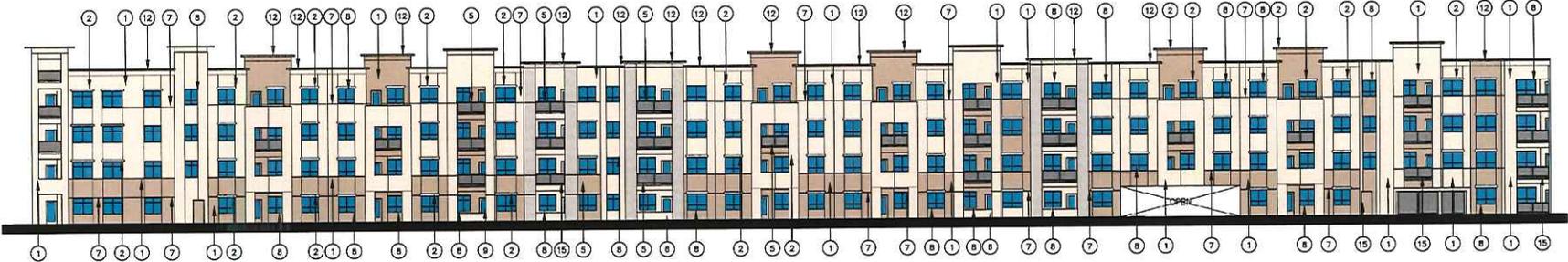
**A7.8**  
 8-03-16



- KEY NOTES**
1. STUCCO FINISH
  2. 8" DEEP STUCCO FINISH EYEBROW - SHADING DEVICE
  3. SOLARIUM UNIT AT THE 4TH FLOOR
  4. METAL SHADE CANOPY - SHADING DEVICE
  5. DECORATIVE METAL RAILING 44" A.F.F.
  6. PATIO WALL WITH STUCCO FINISH
  7. STUCCO CONTROL JOINT
  8. WINDOW UNIT, RECESSED 2" MINIMUM
  9. FRENCH DOOR
  10. FOLDING OR TELESCOPING DOOR SYSTEM
  11. STOREFRONT GLAZING
  12. METAL PARAPET CAP - 42" MINIMUM, HEIGHT MEASURED FROM FINISHED ROOF DECK
  13. METAL DOOR - PAINTED ACCENT COLOR
  14. STAIR BULKHEAD
  15. SLIDING GLASS PATIO DOOR
  16. TOP OF ROOF DECK BEYOND @ +48'-0"
  17. 6" WALL POP-OUT
  18. ROOF TOP DECK
  19. PROJECT SIGNAGE
  20. 4" STUCCO POP-OUT WINDOW SURROUND
  21. EXTERIOR LIGHT
  22. LIVE / WORK ENTRY



BUILDING #2 INTERIOR DRIVE - WEST ELEVATION



BUILDING #2 INTERIOR DRIVE - SOUTH ELEVATION



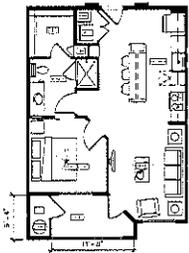
**DISTRICT AT THE QUARTER**  
 KAPLAN ACQUISITIONS, LLC



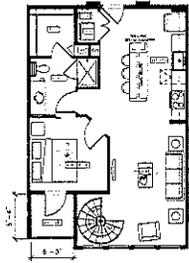
BUILDING ELEVATIONS  
 BUILDING #2 ELEVATIONS  
 1/16" = 1'-0"

**A7.9**  
 8-03-16

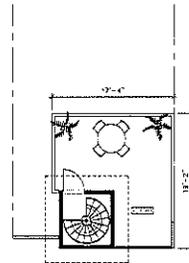




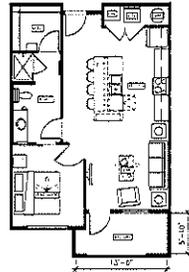
**UNIT A1**  
FIRST - FOURTH FLOOR  
GROSS FLOOR AREA: 432 SF  
NET GROSS FLOOR AREA: 421 SF



**UNIT A1 ROOFDECK**  
FOURTH FLOOR  
GROSS FLOOR AREA: 112 SF  
NET GROSS FLOOR AREA: 101 SF



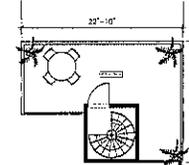
**UNIT A1 ROOFDECK - 2ND FLOOR**  
UPPER FLOOR  
GROSS FLOOR AREA: 242 SF  
NET GROSS FLOOR AREA: 231 SF



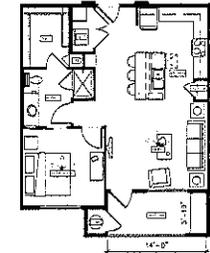
**UNIT A2**  
FIRST - FOURTH FLOOR  
GROSS FLOOR AREA: 432 SF  
NET GROSS FLOOR AREA: 421 SF



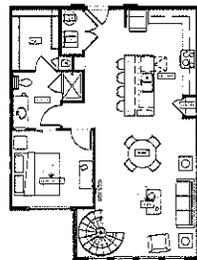
**UNIT A2 ROOFDECK**  
FOURTH FLOOR  
GROSS FLOOR AREA: 104 SF



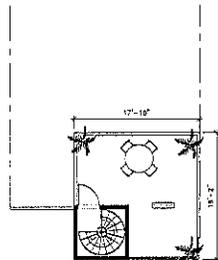
**UNIT A2 ROOFDECK - 2ND FLOOR**  
UPPER FLOOR  
GROSS FLOOR AREA: 316 SF  
NET GROSS FLOOR AREA: 305 SF



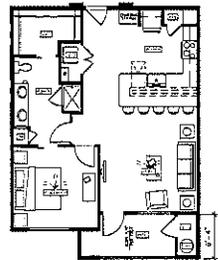
**UNIT A3**  
FIRST - FOURTH FLOOR  
GROSS FLOOR AREA: 432 SF  
NET GROSS FLOOR AREA: 421 SF



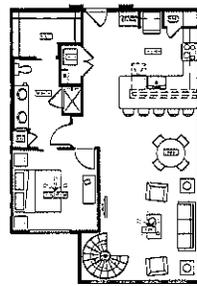
**UNIT A3 ROOFDECK**  
FOURTH FLOOR  
GROSS FLOOR AREA: 104 SF



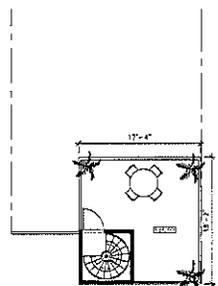
**UNIT A3 ROOFDECK - 2ND FLOOR**  
UPPER FLOOR  
GROSS FLOOR AREA: 242 SF  
NET GROSS FLOOR AREA: 231 SF



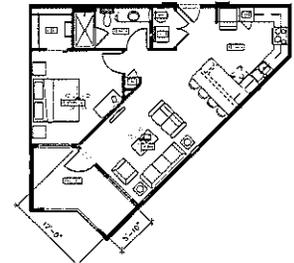
**UNIT A4**  
FIRST - FOURTH FLOOR  
GROSS FLOOR AREA: 432 SF  
NET GROSS FLOOR AREA: 421 SF



**UNIT A4 ROOFDECK**  
FOURTH FLOOR  
GROSS FLOOR AREA: 104 SF



**UNIT A4 ROOFDECK - 2ND FLOOR**  
UPPER FLOOR  
GROSS FLOOR AREA: 242 SF  
NET GROSS FLOOR AREA: 231 SF



**UNIT A5**  
FIRST - FOURTH FLOOR  
GROSS FLOOR AREA: 432 SF  
NET GROSS FLOOR AREA: 421 SF



# DISTRICT AT THE QUARTER

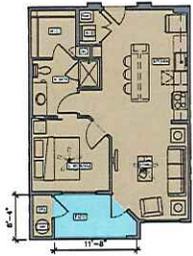
## KAPLAN ACQUISITIONS, LLC

FLOOR PLAN WORK SHEET  
UNIT PLANS

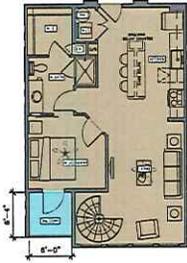
**A4.1**

1/8" = 1'-0"

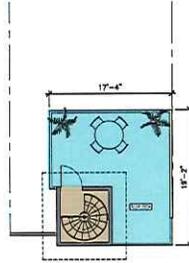
8-03-16



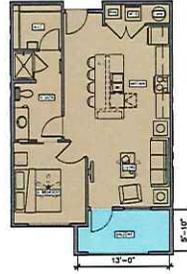
**UNIT A1**  
FIRST - FOURTH FLOORS  
GROSS FLOOR AREA: 832 SF  
PATIO OR EXTERIOR PRIVATE AREA: 101 SF



**UNIT A1 ROOFDECK**  
FOURTH FLOOR  
GROSS FLOOR AREA: 142 SF  
PATIO OR EXTERIOR PRIVATE AREA: 43 SF



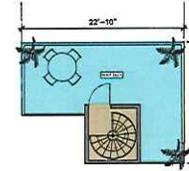
**UNIT A1 ROOFDECK - 2ND FLOOR**  
UPPER FLOOR  
PATIO OR EXTERIOR PRIVATE AREA: 240 SF



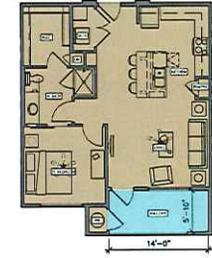
**UNIT A2**  
FIRST - FOURTH FLOORS  
GROSS FLOOR AREA: 832 SF  
PATIO OR EXTERIOR PRIVATE AREA: 101 SF



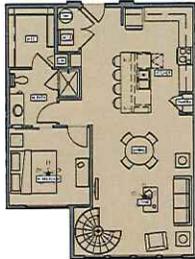
**UNIT A2 ROOFDECK**  
FOURTH FLOOR  
GROSS FLOOR AREA: 338 SF



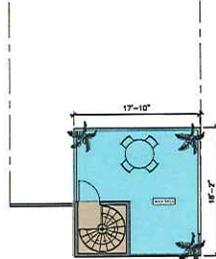
**UNIT A2 ROOFDECK - 2ND FLOOR**  
UPPER FLOOR  
PATIO OR EXTERIOR PRIVATE AREA: 345 SF



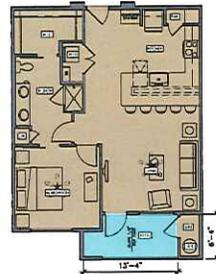
**UNIT A3**  
FIRST - FOURTH FLOORS  
GROSS FLOOR AREA: 832 SF  
PATIO OR EXTERIOR PRIVATE AREA: 81 SF



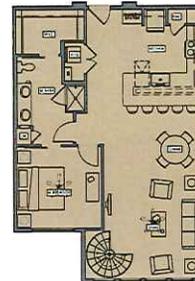
**UNIT A3 ROOFDECK**  
FOURTH FLOOR  
GROSS FLOOR AREA: 832 SF



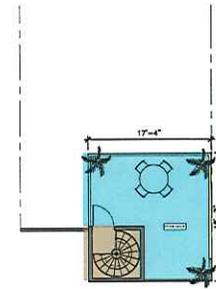
**UNIT A3 ROOFDECK - 2ND FLOOR**  
UPPER FLOOR  
PATIO OR EXTERIOR PRIVATE AREA: 244 SF



**UNIT A4**  
FIRST - FOURTH FLOORS  
GROSS FLOOR AREA: 831 SF  
PATIO OR EXTERIOR PRIVATE AREA: 79 SF



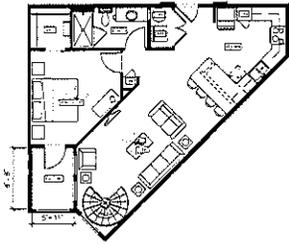
**UNIT A4 ROOFDECK**  
FOURTH FLOOR  
GROSS FLOOR AREA: 338 SF



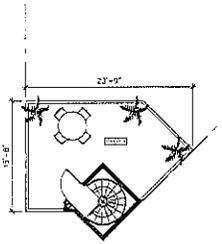
**UNIT A4 ROOFDECK - 2ND FLOOR**  
UPPER FLOOR  
PATIO OR EXTERIOR PRIVATE AREA: 244 SF



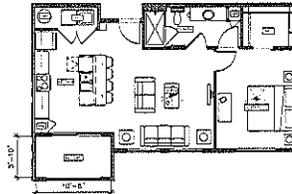
**UNIT A5**  
FIRST - FOURTH FLOORS  
GROSS FLOOR AREA: 783 SF  
PATIO OR EXTERIOR PRIVATE AREA: 110 SF



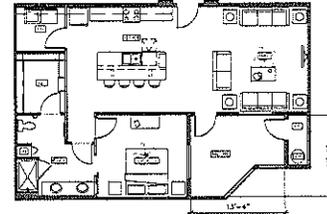
**UNIT A5 ROOFDECK**  
FOURTH FLOOR  
CIRCULAR AREA:  
110 SF  
TOTAL FLOOR AREA:  
152 SF



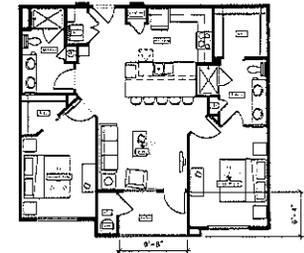
**UNIT A5 ROOFDECK - 2ND FLOOR**  
UPPER FLOOR  
TOTAL FLOOR AREA:  
146 SF



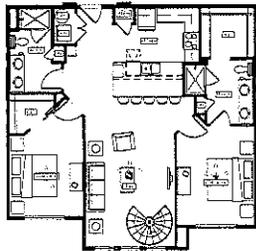
**UNIT A6**  
FIRST - FOURTH FLOORS  
CIRCULAR AREA:  
100 SF  
TOTAL FLOOR AREA:  
112 SF



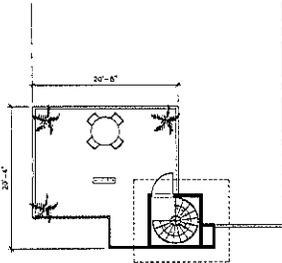
**UNIT A7**  
FIRST - FOURTH FLOORS  
CIRCULAR AREA:  
103 SF  
TOTAL FLOOR AREA:  
107 SF



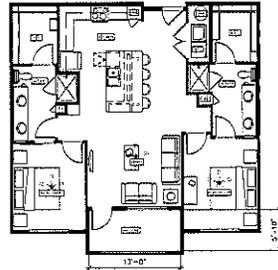
**UNIT B1**  
FIRST - FOURTH FLOORS  
CIRCULAR AREA:  
146 SF  
TOTAL FLOOR AREA:  
141 SF



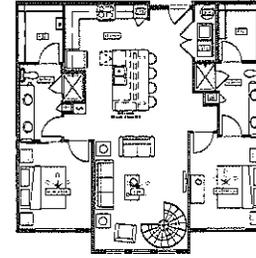
**UNIT B1 ROOFDECK**  
FOURTH FLOOR  
TOTAL FLOOR AREA:  
171 SF



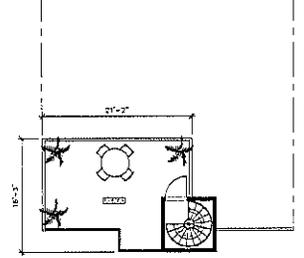
**UNIT B1 ROOFDECK - 2ND FLOOR**  
UPPER FLOOR  
TOTAL FLOOR AREA:  
141 SF



**UNIT B2**  
FIRST - FOURTH FLOORS  
CIRCULAR AREA:  
131 SF  
TOTAL FLOOR AREA:  
116 SF



**UNIT B2 ROOFDECK**  
FOURTH FLOOR  
TOTAL FLOOR AREA:  
127 SF



**UNIT B2 ROOFDECK - 2ND FLOOR**  
UPPER FLOOR  
TOTAL FLOOR AREA:  
133 SF

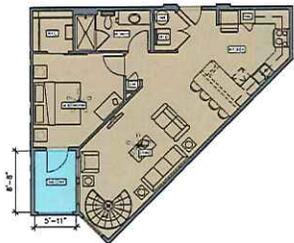


# DISTRICT AT THE QUARTER

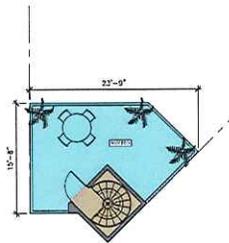
## KAPLAN ACQUISITIONS, LLC

FLOOR PLAN WORK SHEET  
UNIT PLANS  
1/8" = 1'-0"

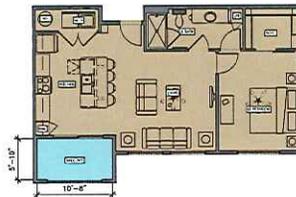
**A4.2**  
8-03-16



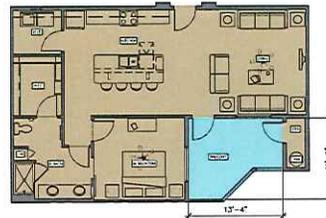
**UNIT A5 ROOFDECK**  
FOURTH FLOOR  
GROSS FLOOR AREA 816 SF  
PATIO OR EXTERIOR PRIVATE AREA 84 SF



**UNIT A5 ROOFDECK - 2ND FLOOR**  
UPPER FLOOR  
PATIO OR EXTERIOR PRIVATE AREA 248 SF



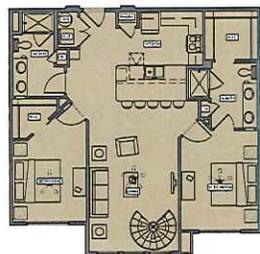
**UNIT A6**  
FIRST - FOURTH FLOORS  
GROSS FLOOR AREA 788 SF  
PATIO OR EXTERIOR PRIVATE AREA 88 SF



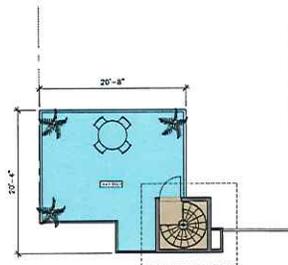
**UNIT A7**  
FIRST - FOURTH FLOORS  
GROSS FLOOR AREA 875 SF  
PATIO OR EXTERIOR PRIVATE AREA 109 SF



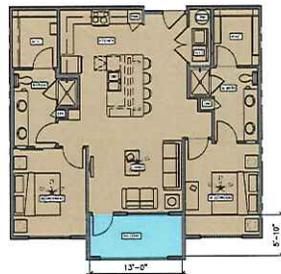
**UNIT B1**  
FIRST - FOURTH FLOORS  
GROSS FLOOR AREA 1,080 SF  
PATIO OR EXTERIOR PRIVATE AREA 84 SF



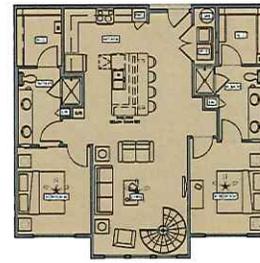
**UNIT B1 ROOFDECK**  
FOURTH FLOOR  
GROSS FLOOR AREA 1,174 SF



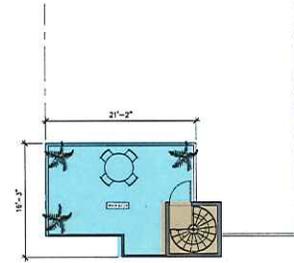
**UNIT B1 ROOFDECK - 2ND FLOOR**  
UPPER FLOOR  
PATIO OR EXTERIOR PRIVATE AREA 307 SF



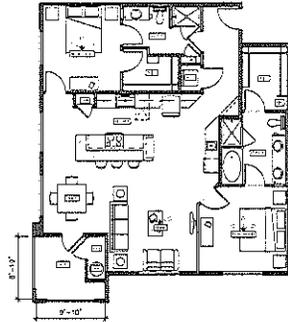
**UNIT B2**  
FIRST - FOURTH FLOORS  
GROSS FLOOR AREA 1,125 SF  
PATIO OR EXTERIOR PRIVATE AREA 71 SF



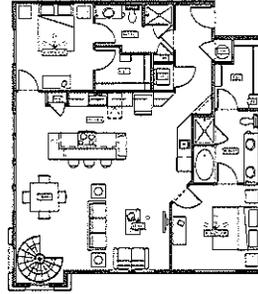
**UNIT B2 ROOFDECK**  
FOURTH FLOOR  
GROSS FLOOR AREA 1,216 SF



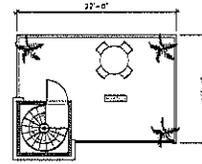
**UNIT B2 ROOFDECK - 2ND FLOOR**  
UPPER FLOOR  
PATIO OR EXTERIOR PRIVATE AREA 253 SF



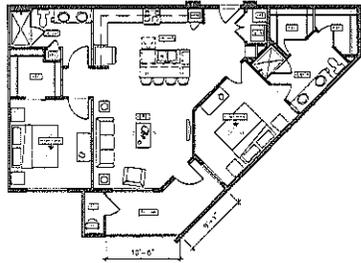
**UNIT B3**  
 FIRST + FOURTH FLOOR  
 ABOVE STAIR AREA  
 WITH 20' EXPOSED PRIVATE AREA  
 1,241 SF  
 46.17



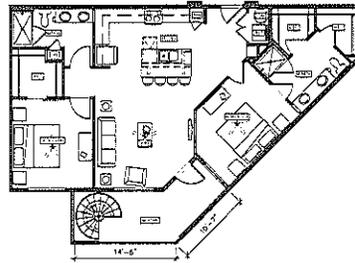
**UNIT B3 ROOFDECK**  
 FOURTH FLOOR  
 FLOOR DECK AREA  
 124 SF



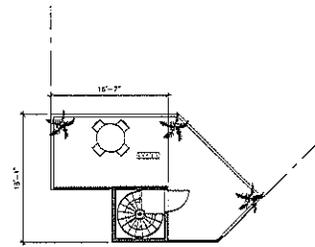
**UNIT B3 ROOFDECK - 2ND FLOOR**  
 UPPER A FLOOR  
 WITH 10' EXPOSED PRIVATE AREA  
 22.71



**UNIT B4**  
 FIRST + FOURTH FLOOR  
 2ND - 3RD AREA  
 WITH 10' EXPOSED PRIVATE AREA  
 1,111 SF  
 41.27



**UNIT B4 ROOFDECK**  
 FOURTH FLOOR  
 2ND FLOOR AREA  
 WITH 10' EXPOSED PRIVATE AREA  
 1,111 SF  
 41.27



**UNIT B4 ROOFDECK - 2ND FLOOR**  
 UPPER A FLOOR  
 WITH 10' EXPOSED PRIVATE AREA  
 33.17



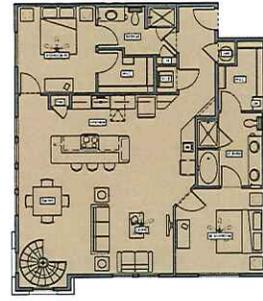
**DISTRICT AT THE QUARTER**  
**KAPLAN ACQUISITIONS, LLC**

FLOOR PLAN WORK SHEET  
 UNIT PLANS  
 1/8" = 1'-0" **A4.3**

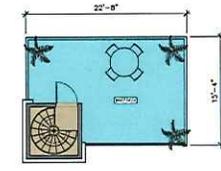
8-03-16



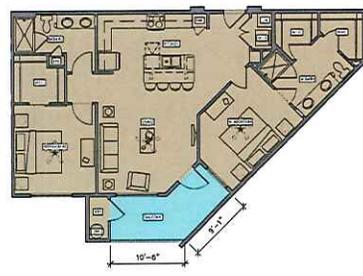
**UNIT B3**  
 FIRST - FOURTH FLOORS  
 GROSS FLOOR AREA: 1,294 SF  
 PATIO OR EXTERIOR PRIVATE AREA: 48 SF



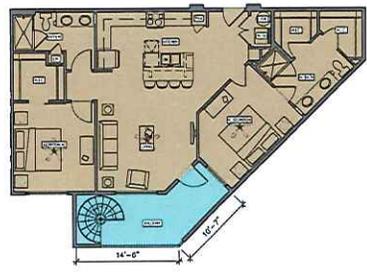
**UNIT B3 ROOFDECK**  
 FOURTH FLOOR  
 GROSS FLOOR AREA: 1,294 SF



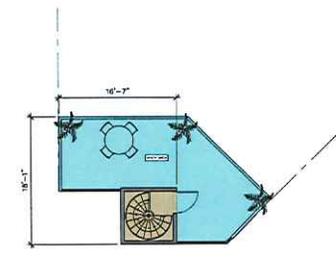
**UNIT B3 ROOFDECK - 2ND FLOOR**  
 UPPER FLOOR  
 PATIO OR EXTERIOR PRIVATE AREA: 223 SF



**UNIT B4**  
 FIRST - FOURTH FLOORS  
 GROSS FLOOR AREA: 1,121 SF  
 PATIO OR EXTERIOR PRIVATE AREA: 123 SF



**UNIT B4 ROOFDECK**  
 FOURTH FLOOR  
 GROSS FLOOR AREA: 1,121 SF  
 PATIO OR EXTERIOR PRIVATE AREA: 146 SF



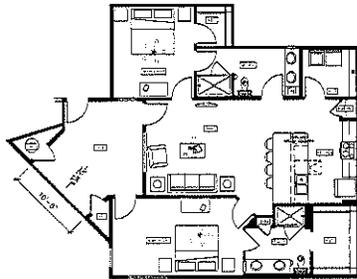
**UNIT B4 ROOFDECK - 2ND FLOOR**  
 UPPER FLOOR  
 PATIO OR EXTERIOR PRIVATE AREA: 340 SF



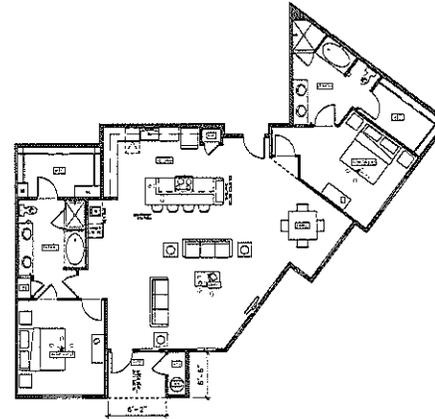
# DISTRICT AT THE QUARTER

## KAPLAN ACQUISITIONS, LLC

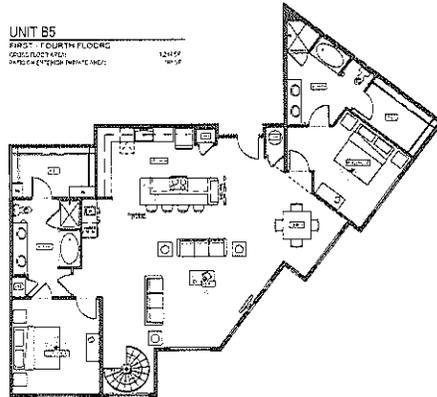
FLOOR PLAN WORK SHEET  
 UNIT PLANS  
 1/8" = 1'-0" **A4.3C**  
 8-03-16



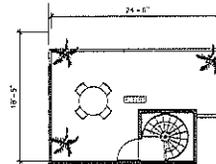
**UNIT B5**  
 FIRST - FOURTH FLOOR  
 GROSS FLOOR AREA: 12,145 SF  
 NET AREA OF EXTERIOR FINISHES: 10,112 SF



**UNIT B6**  
 FIRST - THIRD FLOOR  
 GROSS FLOOR AREA: 13,462 SF  
 NET AREA OF EXTERIOR FINISHES: 11,424 SF



**UNIT B6 ROOFDECK**  
 FOURTH FLOOR  
 GROSS FLOOR AREA: 1,142 SF  
 NET AREA OF EXTERIOR FINISHES: 612 SF



**UNIT B6 ROOFDECK - 2ND FLOOR**  
 UPPER FLOOR  
 GROSS FLOOR AREA: 1,491 SF

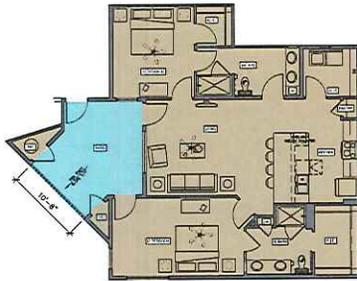


**DISTRICT AT THE QUARTER**  
**KAPLAN ACQUISITIONS, LLC**

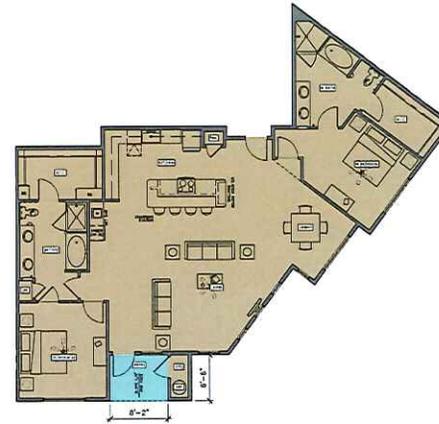
FLOOR PLAN WORK SHEET  
 UNIT PLANS  
 1/8" = 1'-0"

**A4.4**

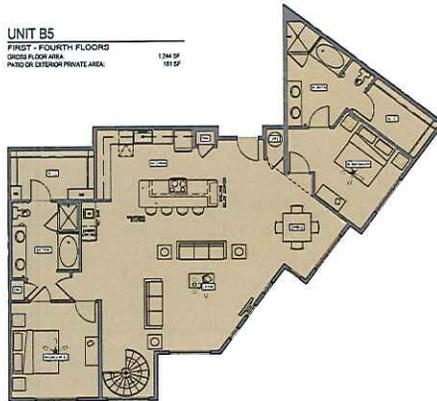
B-03-16



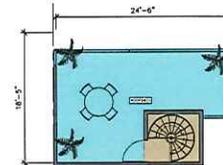
**UNIT B5**  
 FIRST - FOURTH FLOORS  
 GROSS FLOOR AREA: 1,294 SF  
 PATIO OR EXTERIOR PRIVATE AREA: 181 SF



**UNIT B6**  
 FIRST - THIRD FLOORS  
 GROSS FLOOR AREA: 1,707 SF  
 PATIO OR EXTERIOR PRIVATE AREA: 83 SF



**UNIT B6 ROOFDECK**  
 FOURTH FLOOR  
 GROSS FLOOR AREA: 1,642 SF  
 PATIO OR EXTERIOR PRIVATE AREA: 181 SF



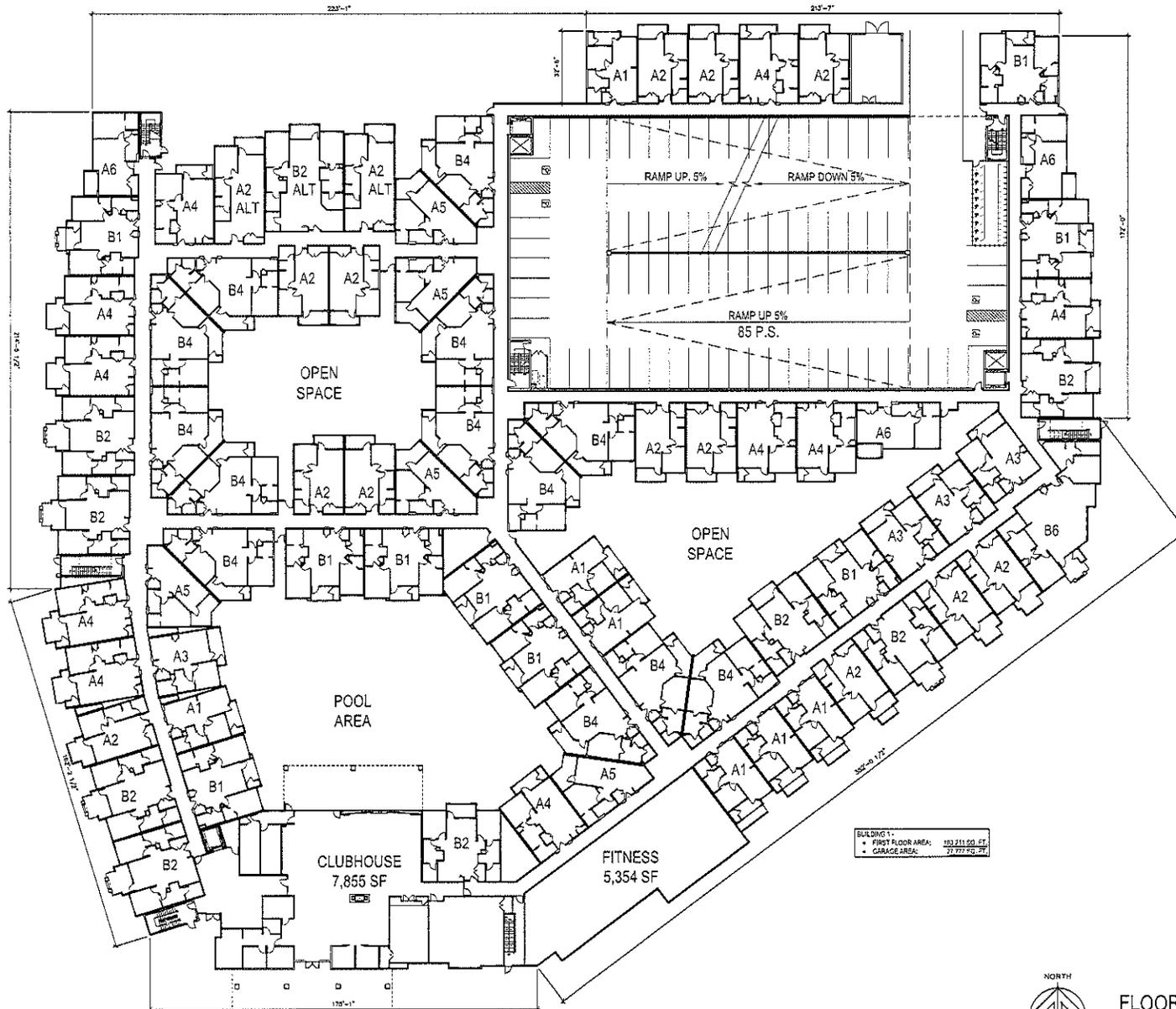
**UNIT B6 ROOFDECK - 2ND FLOOR**  
 UPPER FLOOR  
 PATIO OR EXTERIOR PRIVATE AREA: 308 SF



# DISTRICT AT THE QUARTER

## KAPLAN ACQUISITIONS, LLC

FLOOR PLAN WORK SHEET | **A4.4C**  
 UNIT PLANS  
 1/8" = 1'-0" 8-03-16



BUILDING 1  
 \* FIRST FLOOR AREA: 183,211 SQ. FT.  
 \* GARAGE AREA: 75,777 SQ. FT.



# DISTRICT AT THE QUARTER

## KAPLAN ACQUISITIONS, LLC



FLOOR PLAN WORK SHEET  
 BUILDING 1 - 1ST FLOOR  
 3/64" = 1'-0"

# A5.1.1

8-03-16



**DISTRICT AT THE QUARTER**  
KAPLAN ACQUISITIONS, LLC

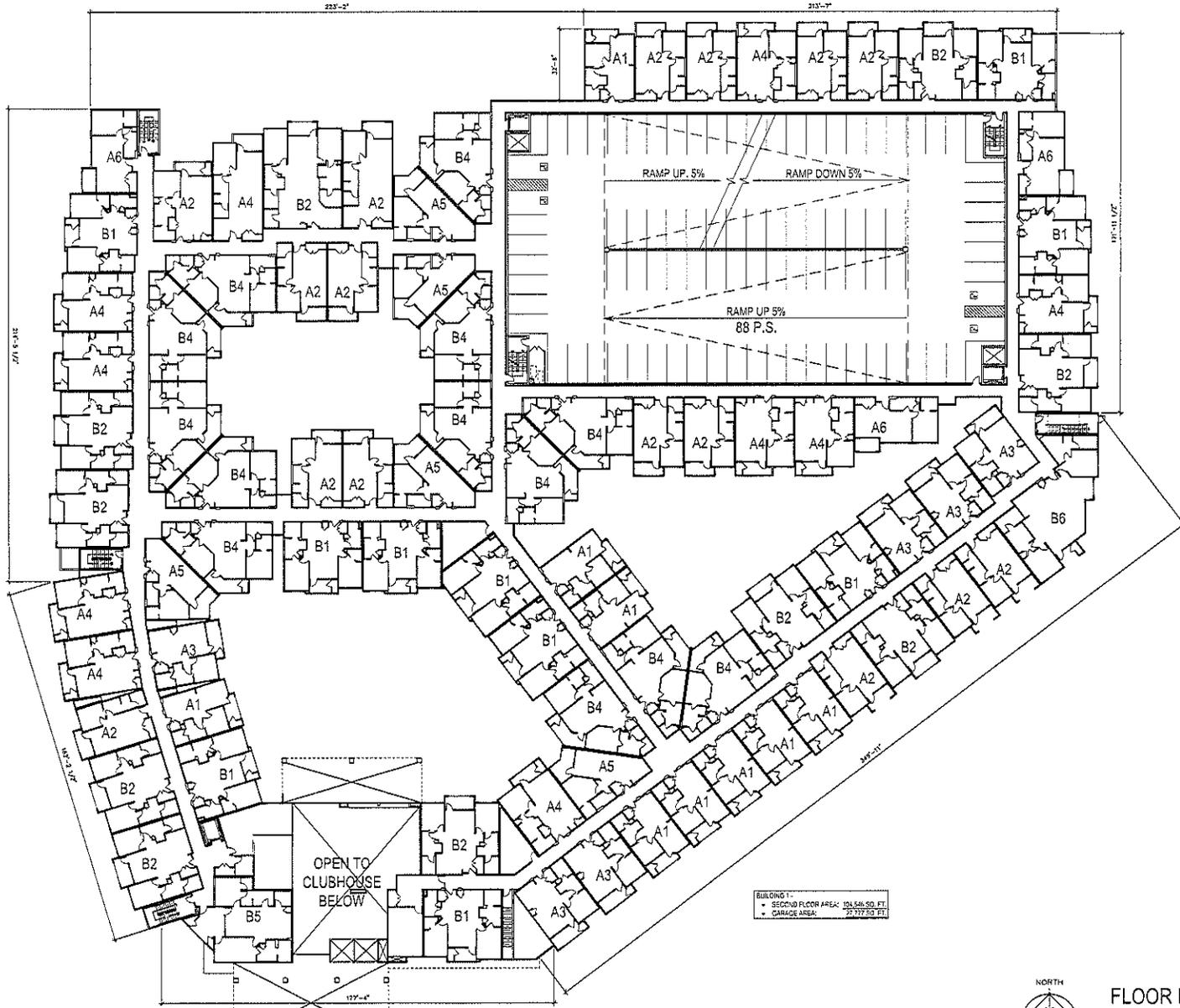


FLOOR PLAN WORK SHEET  
BUILDING 1 - 1ST FLOOR

**A5.1.1C**

3/64" = 1'-0"

8-03-16

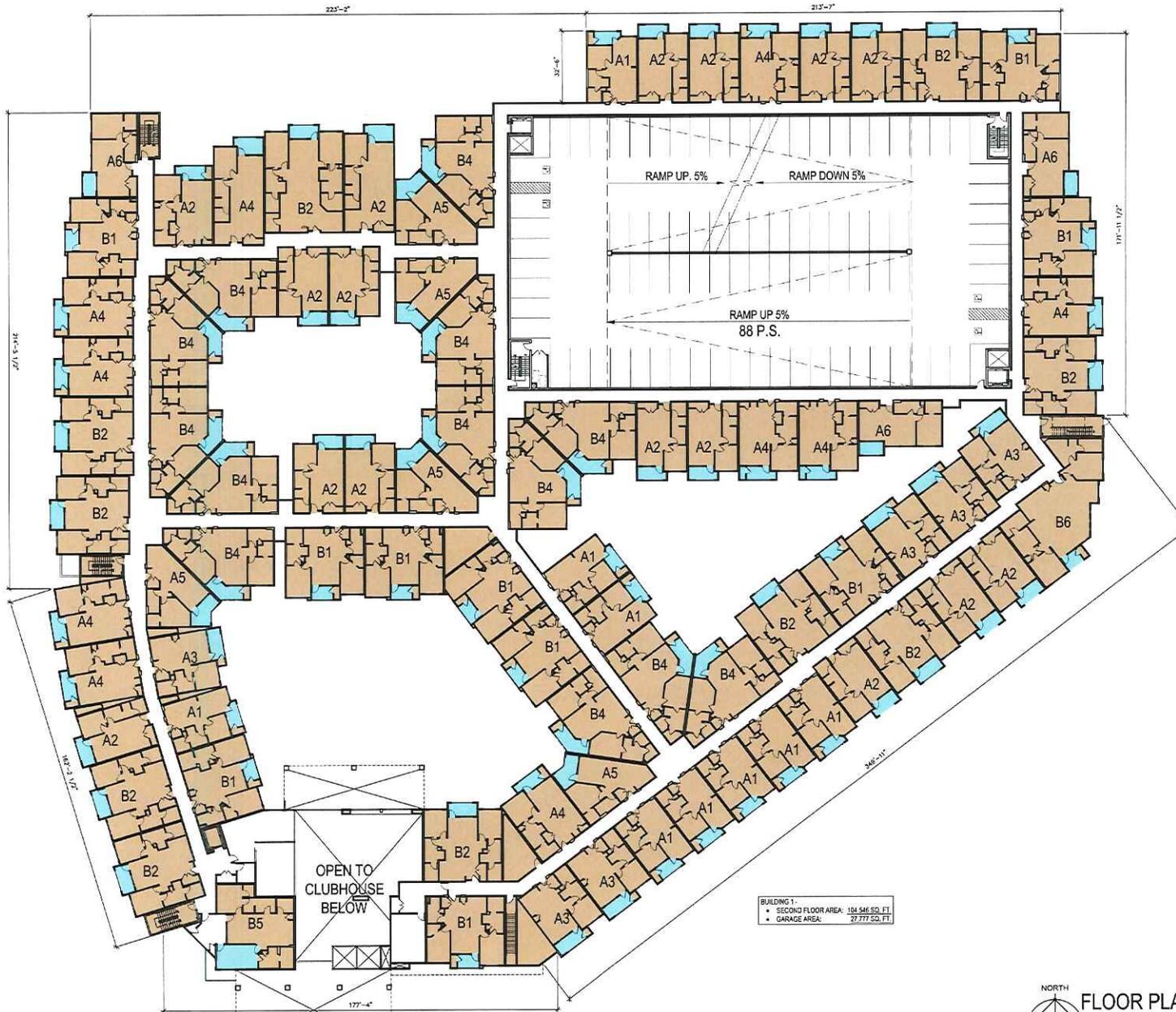


**DISTRICT AT THE QUARTER**  
**KAPLAN ACQUISITIONS, LLC**



FLOOR PLAN WORK SHEET | **A5.1.2**  
 BUILDING 1 - 2ND FLOOR

3/8" = 1'-0" 8-03-16



**DISTRICT AT THE QUARTER**  
KAPLAN ACQUISITIONS, LLC



FLOOR PLAN WORK SHEET  
BUILDING 1 - 2ND FLOOR  
3/64" = 1'-0"

**A5.1.2C**  
8-03-16





**DISTRICT AT THE QUARTER**  
KAPLAN ACQUISITIONS, LLC



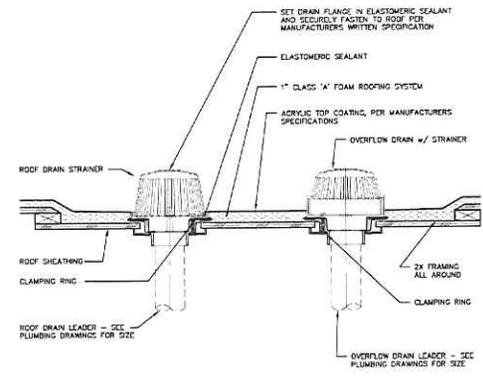
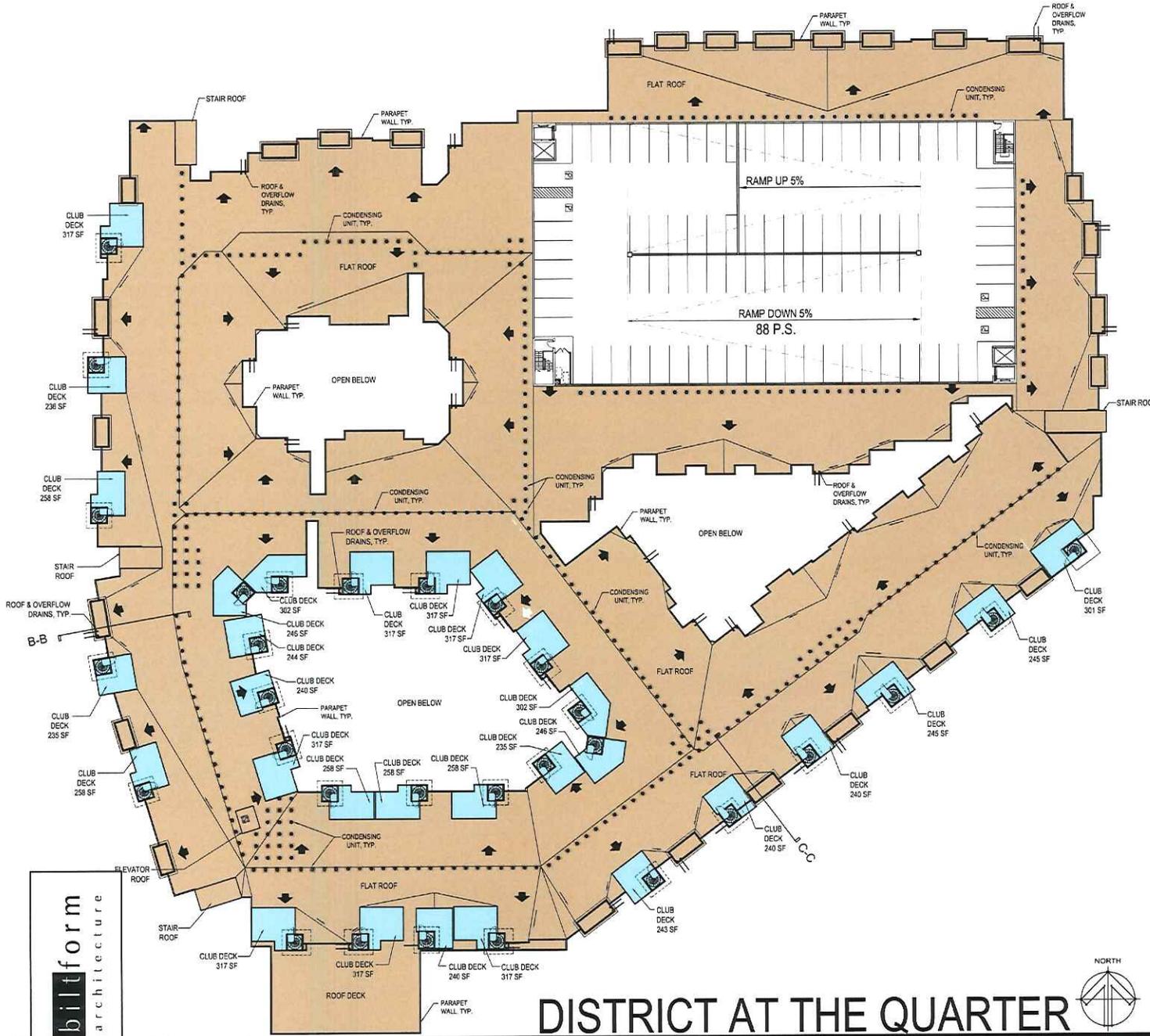
FLOOR PLAN WORK SHEET  
BUILDING 1 - 3RD FLOOR  
3/64" = 1'-0"

**A5.1.3C**  
8-03-16

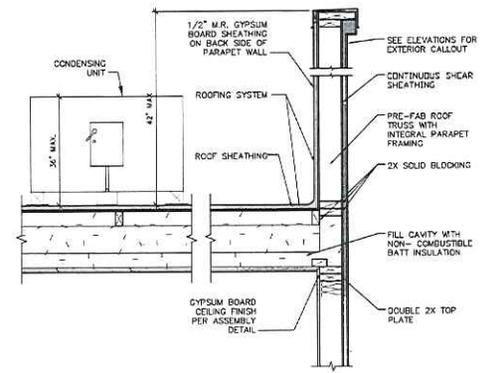








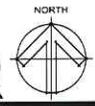
1 TYPICAL ROOF DRAIN AND OVERFLOW DRAIN DETAIL  
1/2" = 1'-0"



2 PARAPET WALL HEIGHT CLEARANCE AT CONDENSING UNIT  
3/4" = 1'-0"

**biltform**  
architecture

**DISTRICT AT THE QUARTER**  
KAPLAN ACQUISITIONS, LLC



FLOOR PLAN WORK SHEET **A5.1.5C**  
BUILDING 1 - ROOF  
3/64" = 1'-0" 8-03-16





**BUILDING 2 -**  
 • FIRST FLOOR AREA: 81,738 SQ. FT.  
 • GARAGE AREA: 25,686 SQ. FT.

**DISTRICT AT THE QUARTER**  
**KAPLAN ACQUISITIONS, LLC**



FLOOR PLAN WORK SHEET  
 BUILDING 2 - 1ST FLOOR  
 3/84" = 1'-0"

**A5.2.1C**  
 8-03-16





**BUILDING 2 -**  
 • SECOND FLOOR AREA: 83,392 SQ. FT.  
 • GARAGE AREA: 25,666 SQ. FT.

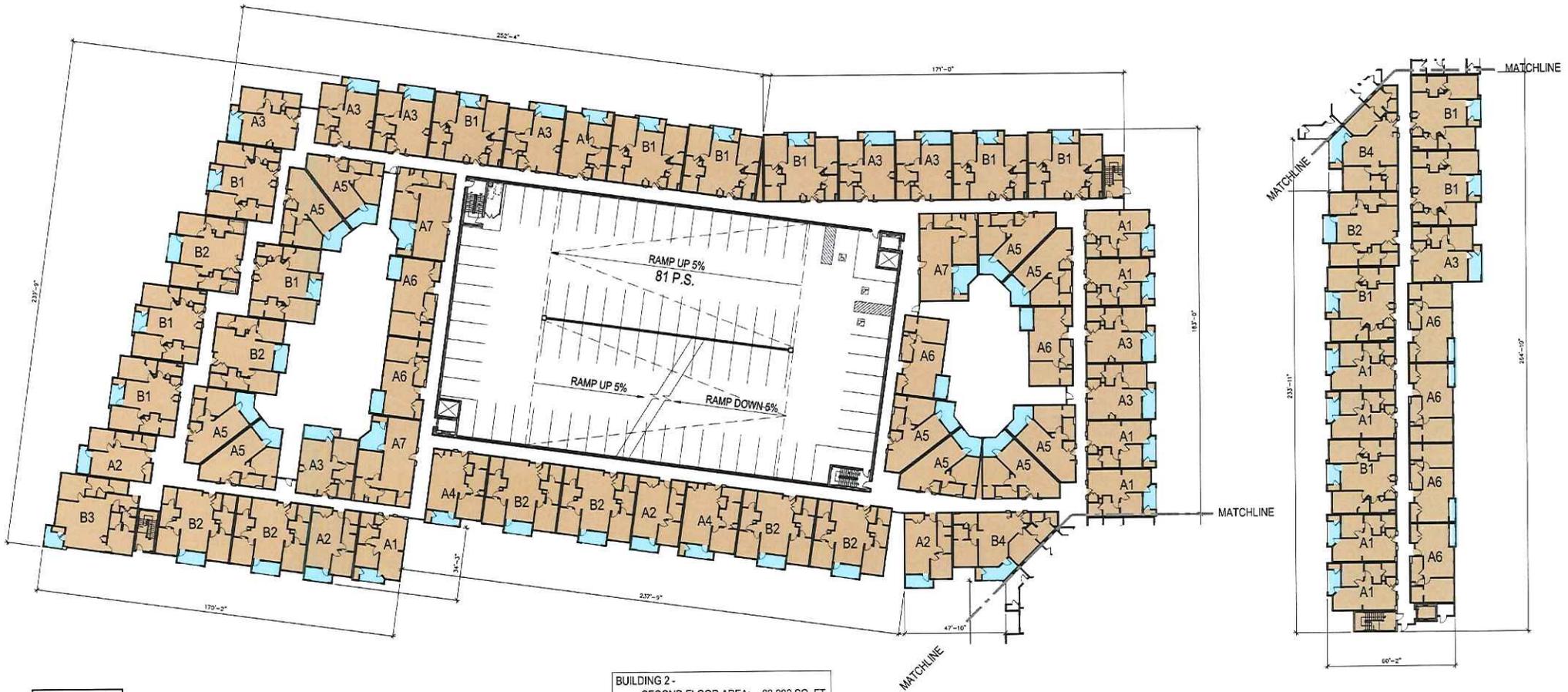


# DISTRICT AT THE QUARTER

## KAPLAN ACQUISITIONS, LLC



FLOOR PLAN WORK SHEET **A5.2.2**  
 BUILDING 2 - 2ND FLOOR  
3/64" = 1'-0" 8-03-16



**BUILDING 2 -**

- SECOND FLOOR AREA: 83,392 SQ. FT.
- GARAGE AREA: 25,686 SQ. FT.



# DISTRICT AT THE QUARTER

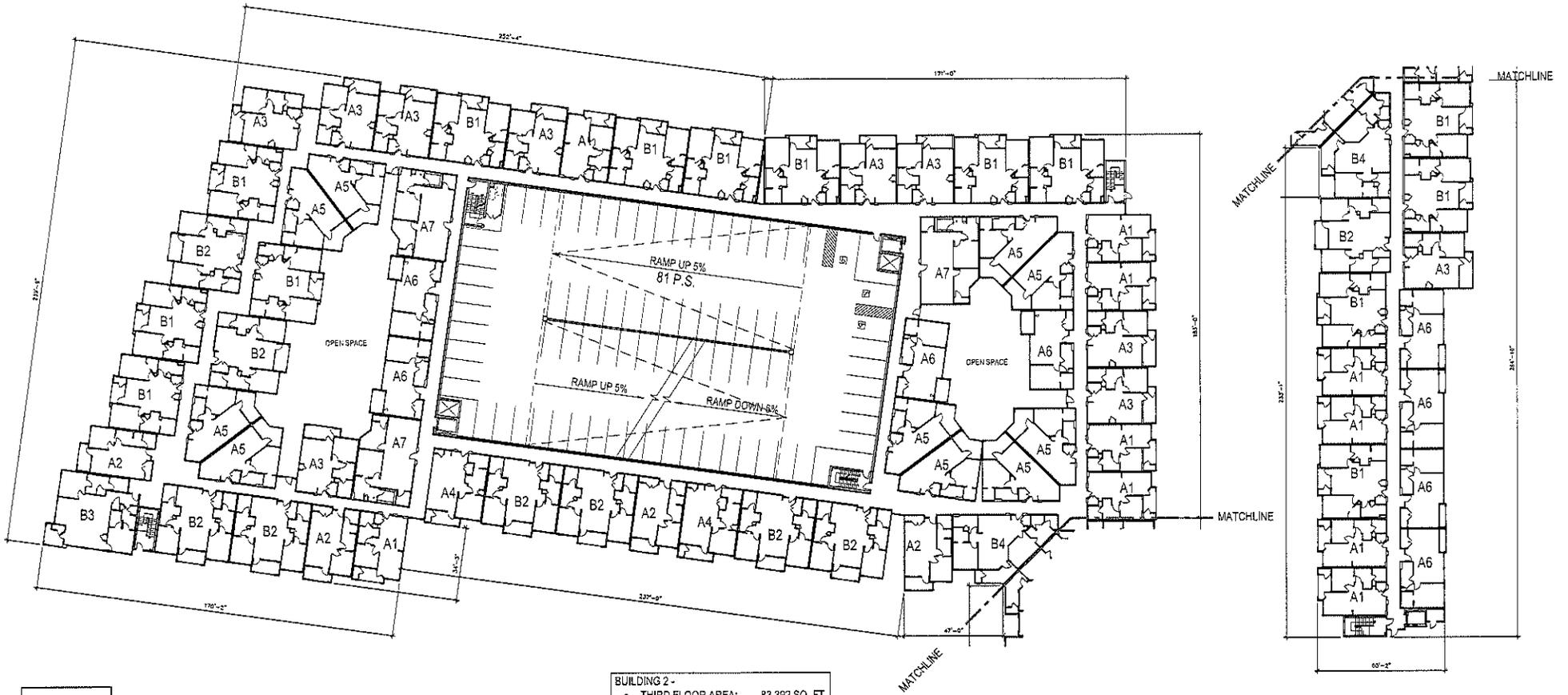
## KAPLAN ACQUISITIONS, LLC



FLOOR PLAN WORK SHEET  
 BUILDING 2 - 2ND FLOOR  
 3/64" = 1'-0"

# A5.2.2C

8-03-16



**BUILDING 2 -**  
 • THIRD FLOOR AREA: 83,392 SQ. FT.  
 • GARAGE AREA: 25,686 SQ. FT.

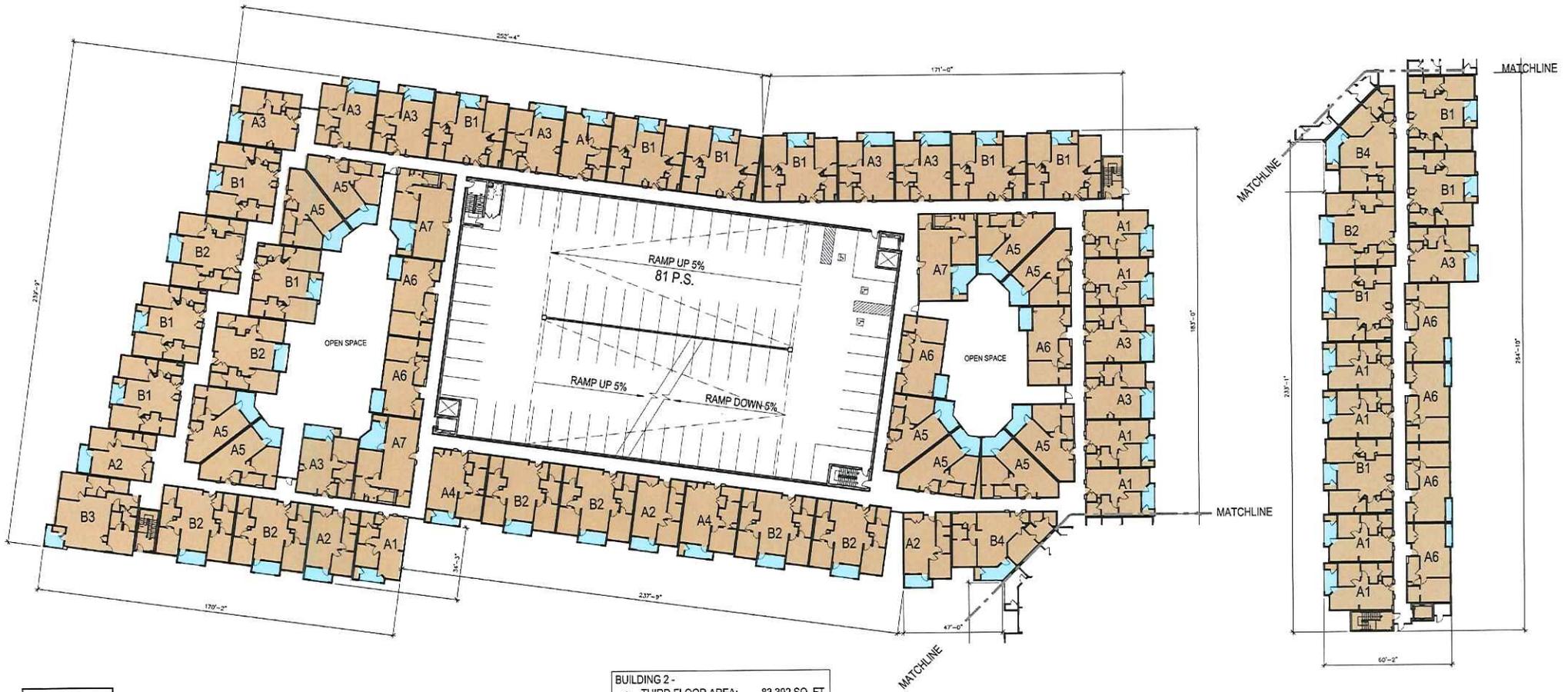


# DISTRICT AT THE QUARTER

## KAPLAN ACQUISITIONS, LLC



FLOOR PLAN WORK SHEET **A5.2.3**  
 BUILDING 2 - 3RD FLOOR  
 3/64" = 1'-0" 8-03-16



**BUILDING 2 -**  
 • THIRD FLOOR AREA: 83,392 SQ. FT.  
 • GARAGE AREA: 25,686 SQ. FT.

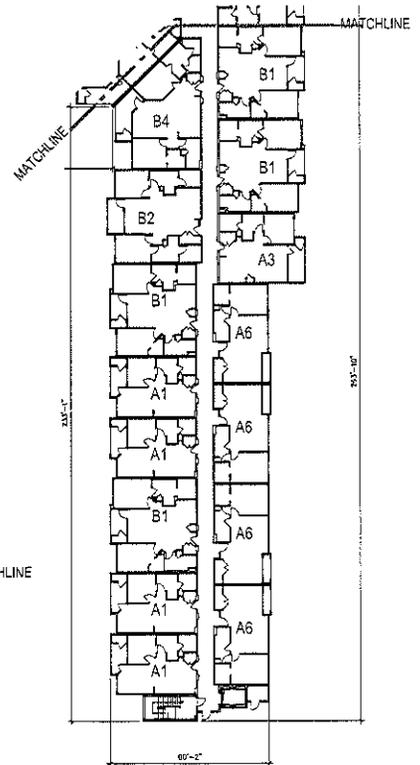
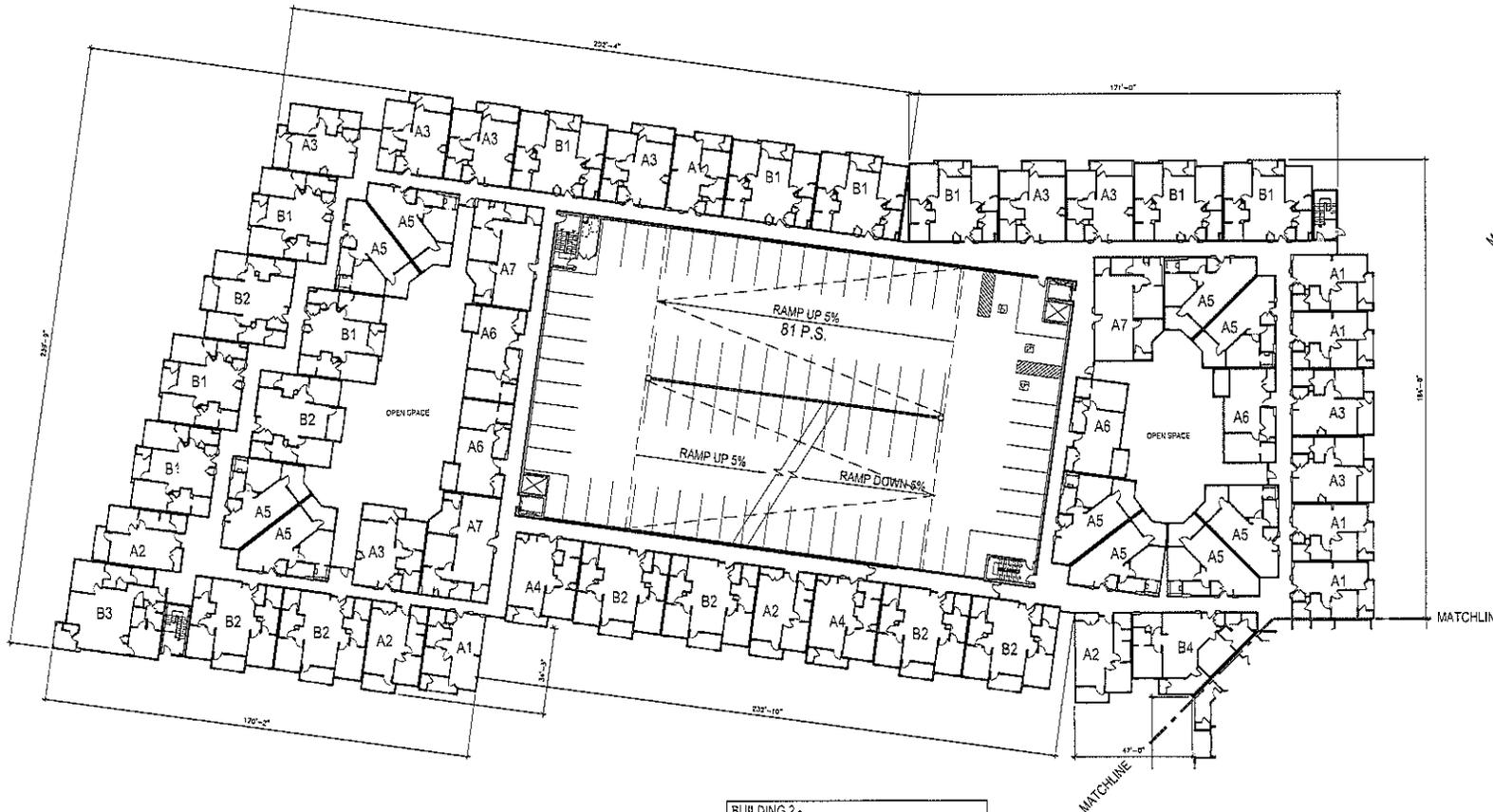


**DISTRICT AT THE QUARTER**  
 KAPLAN ACQUISITIONS, LLC



FLOOR PLAN WORK SHEET  
 BUILDING 2 - 3RD FLOOR  
 3/64" = 1'-0"

**A5.2.3C**  
 8-03-16



**BUILDING 2 -**  
 • FOURTH FLOOR AREA: 83,392 SQ. FT.  
 • GARAGE AREA: 25,686 SQ. FT.



# DISTRICT AT THE QUARTER

## KAPLAN ACQUISITIONS, LLC



FLOOR PLAN WORK SHEET | **A5.2.4**  
 BUILDING 2 - 4TH FLOOR  
3/64" = 1'-0" 8-03-16



**BUILDING 2 -**

- FOURTH FLOOR AREA: 83,392 SQ. FT.
- GARAGE AREA: 25,686 SQ. FT.



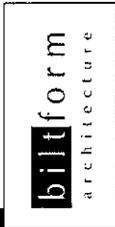
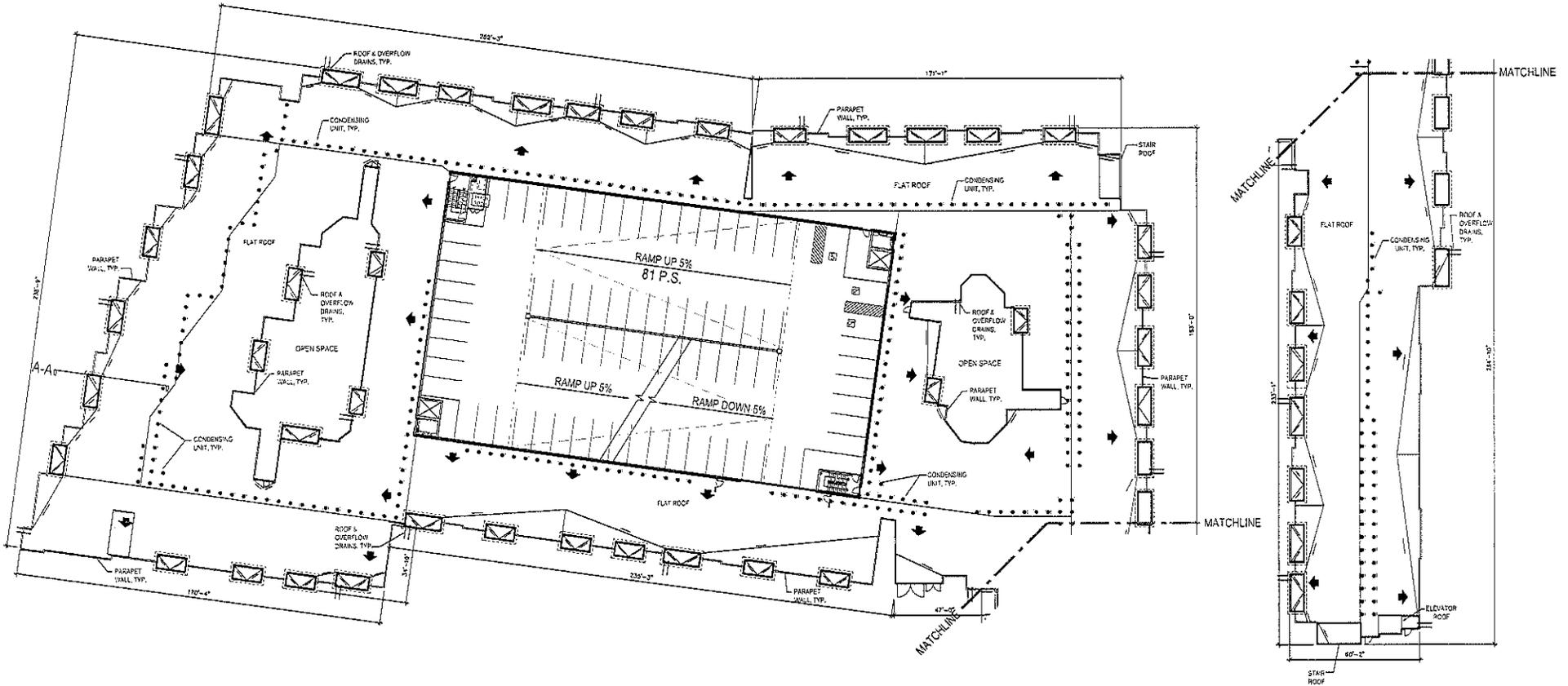
# DISTRICT AT THE QUARTER

## KAPLAN ACQUISITIONS, LLC



FLOOR PLAN WORK SHEET  
 BUILDING 2 - 4TH FLOOR  
 3/64" = 1'-0"

**A5.2.4C**  
 8-03-16

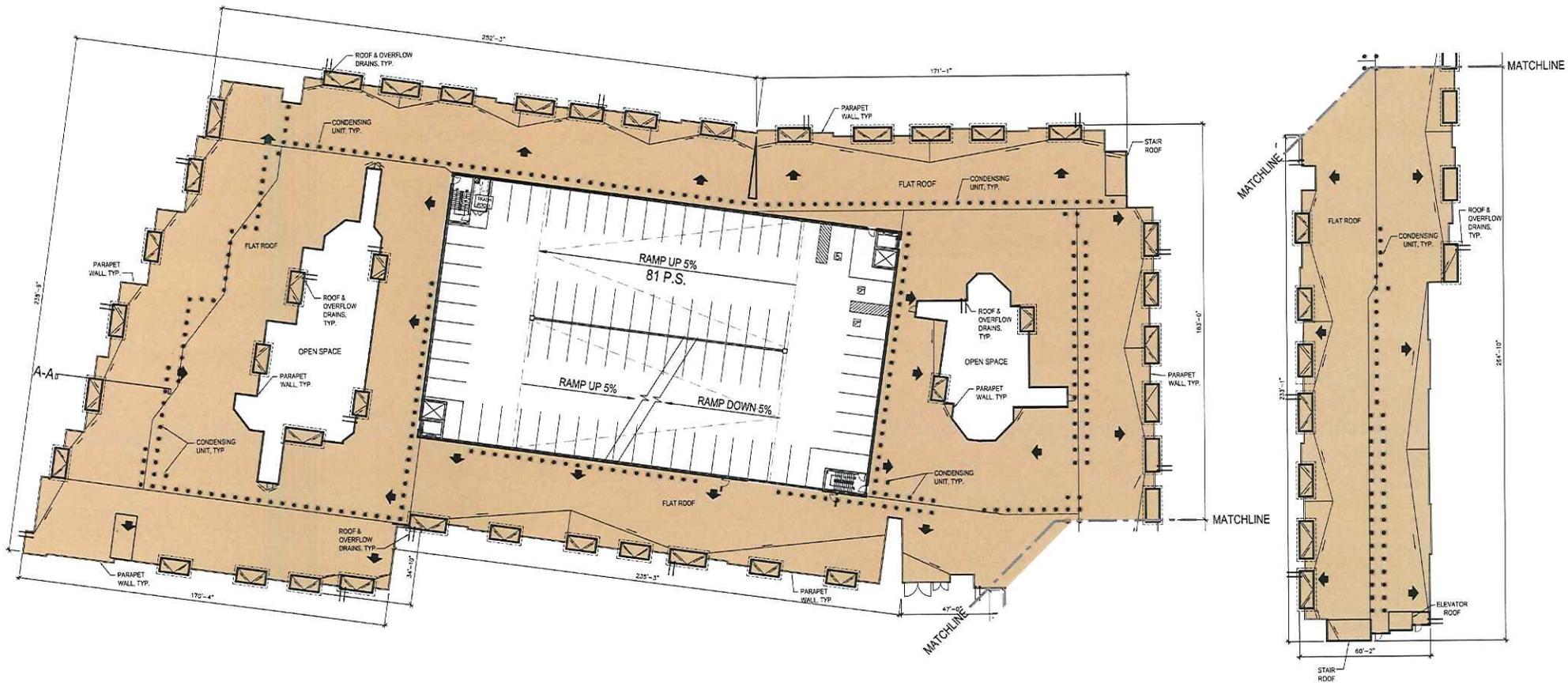


DISTRICT AT THE QUARTER  
KAPLAN ACQUISITIONS, LLC



FLOOR PLAN WORK SHEET  
BUILDING 2 - ROOF  
3/64" = 1'-0"

A5.2.5  
8-03-16

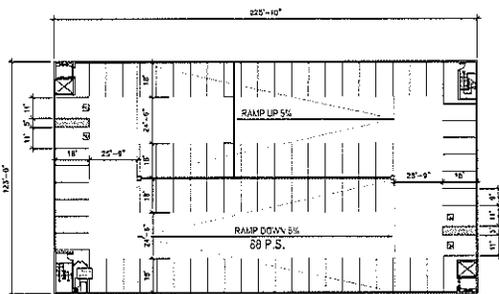


DISTRICT AT THE QUARTER  
KAPLAN ACQUISITIONS, LLC

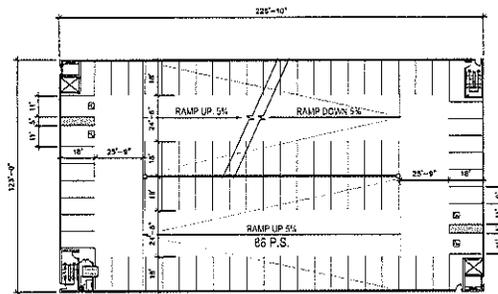


FLOOR PLAN WORK SHEET  
BUILDING 2 - ROOF  
3/16" = 1'-0"

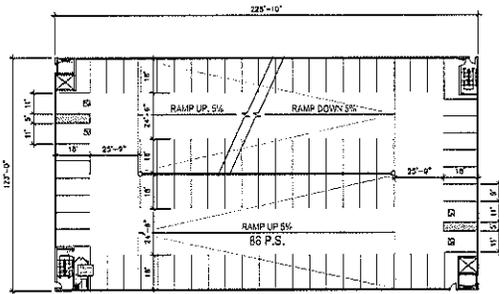
A5.2.5C  
8-03-16



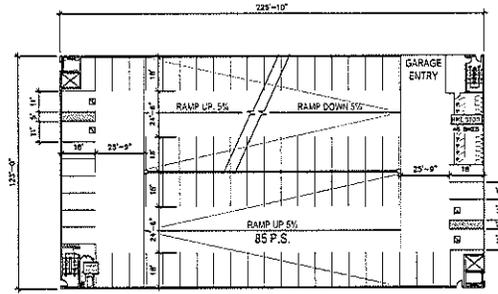
MEZZANINE LEVEL



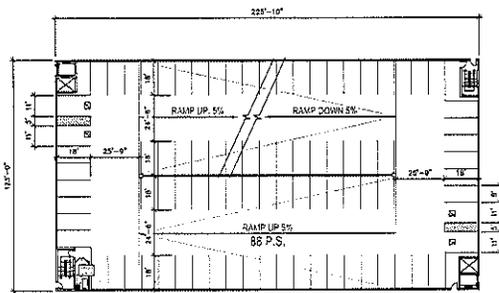
SECOND LEVEL



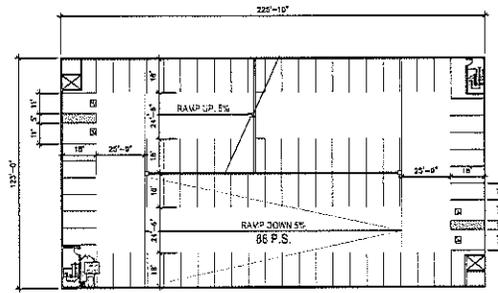
FOURTH LEVEL



GRADE LEVEL



THIRD LEVEL



LOWER LEVEL

PARKING SUMMARY - BUILDING 1	
(PHASE 1) TOTAL PARKING:	
REQUIRED: TABLE 8.103.B:	
1 BEDROOM OR LESS (1 SPACES)	182 x 2 = 182
2 BEDROOM OR MORE (2 SPACES)	140 x 2 = 280
TOTAL REQUIRED:	472 P.S.
PROVIDED:	
GARAGE PARKING (COVERED)	525 P.S.
VEHICLE PARKING (OPEN SPACES)	0 P.S.
TOTAL PROVIDED:	525 P.S.
ACCESSIBLE PARKING REQUIRED:	4% X 525 P.S. = 21 P.S.
ACCESSIBLE PARKING PROVIDED:	COVERED = 21 P.S.
BICYCLE PARKING: (1) SPACES PER 10 VEHICLE SPACES; SEC.8.103	
(472/10)=47.2	48 SPACES PROVIDED



# DISTRICT AT THE QUARTER

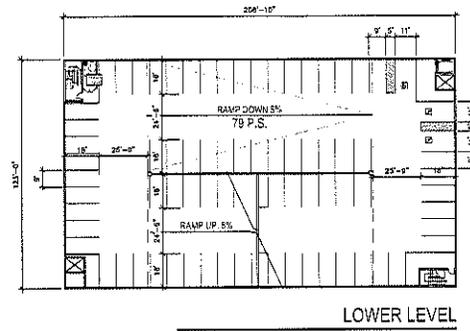
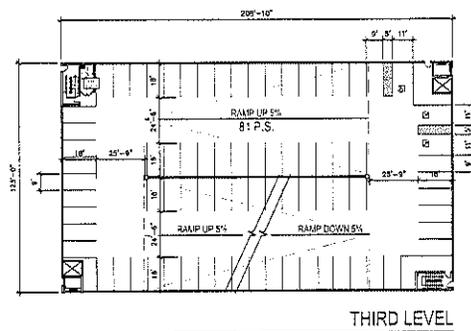
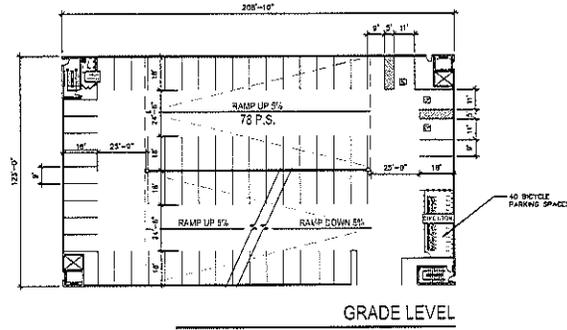
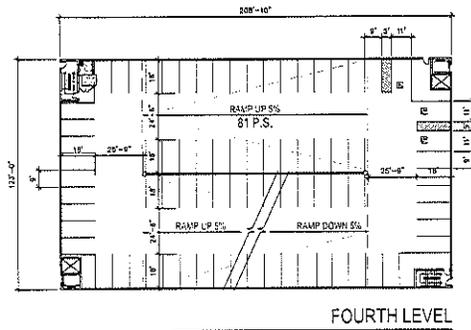
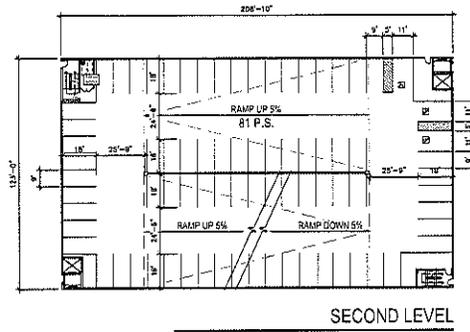
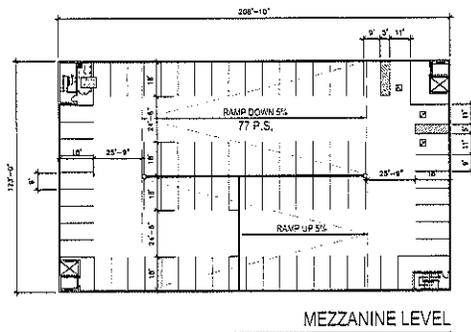
## KAPLAN ACQUISITIONS, LLC

GARAGE PLAN WORK SHEET  
BUILDING 1 - 1ST LEVEL

# A6.1

3/6" = 1'-0"

8-03-16



PARKING SUMMARY - BUILDING 2	
[PHASE 2] TOTAL PARKING:	
REQUIRED: TABLE 8.103.B:	
1 BEDROOM OR LESS (1 SPACE)	187 X 1 = 187
2 BEDROOM OR MORE (2 SPACES)	103 X 2 = 206
TOTAL REQUIRED:	393 P.S.
PROVIDED:	
GARAGE PARKING (COVERED)	477 P.S.
VISITOR PARKING (OPEN SPACES)	477 P.S.
TOTAL PROVIDED:	477 P.S.
ACCESSIBLE PARKING REQUIRED: 4% X 423 P.S. = 17 P.S. COVERED = 17 P.S.	
BICYCLE PARKING: (1) SPACES PER 10 VEHICAL SPACES; DEC.8.103 (36"x110"x3) 40 SPACES PROVIDED	

# DISTRICT AT THE QUARTER

## KAPLAN ACQUISITIONS, LLC

GARAGE PLAN WORK SHEET  
BUILDING 2  
1" = 30'-0"

# A6.2

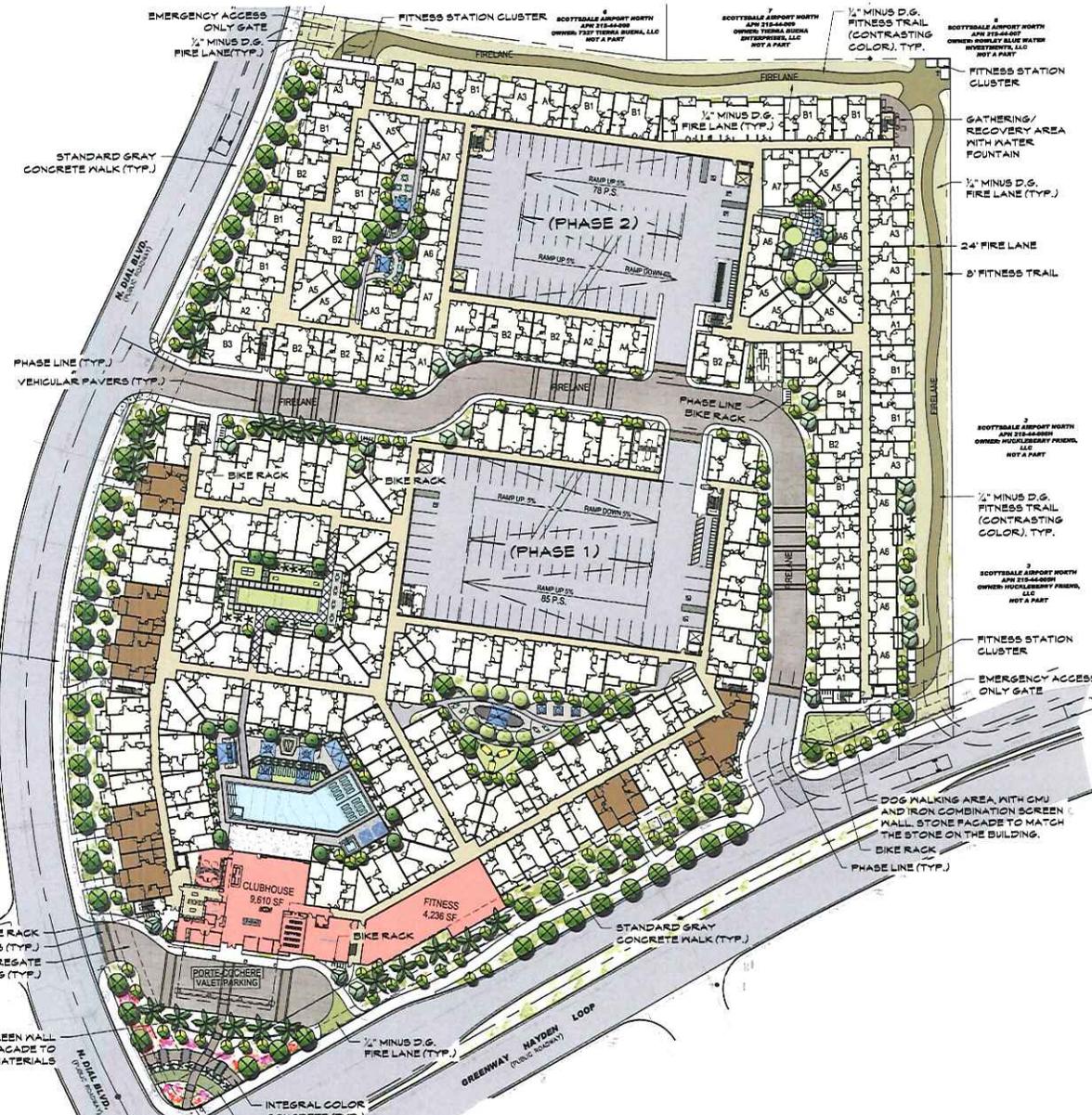
8-03-16

# Exhibit 6

## Landscape Plan Set



McGough Group



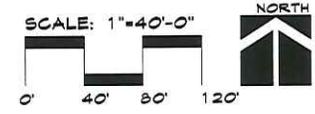
### CONCEPTUAL PLANT PALETTE

- TREES (24" BOX MIN. CALIFERS TO MEET ANA STANDARDS.)**
- MILGA (AGACIA ANURA)
  - SIBBOG TREE (DALBERGIA SIBBOG)
  - TEXAS EDONY (EDENOPSIS EDANG)
  - PURPLE LEAF PLUM (PRUNUS CERASIFERA)
  - PALO VERDE THORNLESS HYBRID (PARKINSONIA HYBRID)
  - KEEY FISH FEATHER (PISTACIA CHINENSIS RED FISH)
  - MASTIC TREE (PISTACIA LENTIGOS)
  - TREE OLIVE (OLEA SP.)
  - LIVE OAK SP. 1.50" (QUERCUS VIRGINIANA)
  - NEOSGITE SPECIES (PROSOPIS SPECIES)
  - TEXAS MOUNTAIN LAUREL (SOPHORA SEQUENDIFLORA)
  - CITRUS TREES (CITRUS SPECIES)
- PALM TREES**
- DATE PALM (PHOENIX DACTYLIFERA)
  - MEDITERRANEAN FAN PALM (CHAEMEROPS HUMILIS)
- SHRUBS - 5 GAL. MIN.**
- BOUGAINVILLEA (BOUGAINVILLEA SPECIES)
  - DESERT BIRD OF PARADISE (CAESALPIA SPECIES)
  - PARKY DUSTER (CALLIANDRA SPECIES)
  - GASSIA (GASSIA SPECIES)
  - EMU BUSH (EREMOPHILA SPECIES)
  - TEXAS BASS (LEUCOPHYLLUM SPECIES)
  - DESERBASS (MULLENBERGIA SPECIES)
  - JOJOBA (SIMMONDSIA CHINENSIS)
  - BALIA RUELLIA (RUELLIA PENINSULARIS)
  - SALVIA (SALVIA SPECIES)
  - ARIZONA YELLOW BELLS (TEGOWIA SPECIES)
  - ORANGE JUBILEE (TEGOWIA SPECIES)
  - DIARM MYRTLE (MYRTUS COMMUNIS)
  - LITTLE JOHN BOTTLEBRUSH (CALLISTEMON SPECIES)
- ACCENT PLANTS - 5 GAL. MIN.**
- AGAVE (AGAVE SPECIES)
  - ALOE (ALOE SPECIES)
  - DESERT SMOOTH SPOONS (DASYLIRON SPECIES)
  - EUPHORBIA (EUPHORBIA SPECIES)
  - HEDGEHOG CACTUS (ECHINOGERIS ENGELMANNI)
  - BARREL CACTUS (PEROGASTIUS MELIZZINI)
  - GIANT HESPERALOE (HESPERALOE FUNIFERA)
  - RED YUGGA (HESPERALOE PARVIFLORA)
  - PRICKLY PEAR (OPUNTIA SPECIES)
  - LADY BLUFFER (PEDILANTHUS MACROCARPUS)
  - YUGGA (YUGGA SPECIES)
- GROUNDCOVER - 5 GAL. MIN.**
- DESERT CARPET AGACIA (AGACIA REDOLENS DESERT CARPET)
  - ASPARAGUS FERN (ASPARAGUS SPRENGER)
  - EREMOPHILA GLADRA OUTBACK SUNRISE
  - VERBENA (GLANDULARIA SPECIES)
  - LANTANA (LANTANA SPECIES)
  - RENE ROSEMARY (ROSMARINUS SPECIES)
  - YELLOW DOT (MEDIOLA TRILOBATA)
- DECOMPOSED GRANITE**
- 3/4" D.GREENED - 2" DEPTH MIN. COLOR TO BE DETERMINED
- ARTIFICIAL TURF**

- NOTES:**
- DUE TO PLANT MATERIAL AVAILABILITY, SUBSTITUTIONS FOR PLANT MATERIAL LISTED ABOVE MAY BE USED. ANY ALTERNATES OR SUBSTITUTIONS MUST BE ON THE ADVX LOW WATER USE PLANT LIST.
  - SUB-SPECIES OR HYBRIDS OF PLANT MATERIAL LISTED ABOVE MAY BE USED AS ALTERNATES/SUBSTITUTIONS.
  - ADDITIONAL PLANT MATERIAL MAY BE ADDED TO THE LIST BELOW DUE TO UTILITY COMPANY OR H.G.A. REQUESTS AND/OR PLANTING RESTRICTIONS WITHIN UTILITY EASEMENTS.
  - NO TREES TO BE LOCATED WITHIN THE P.U.E. OR ANY OTHER NOTED EASEMENTS.
  - ALL PLANT MATERIAL IN THE R.O.K. SHALL BE ON THE ADVX LOW WATER USE PLANT LIST.

## DISTRICT AT THE QUARTER OVERALL LANDSCAPE MASTER PLAN

15501 N. DIAL BLVD. SCOTTSDALE, AZ 85260  
AUGUST 30, 2016

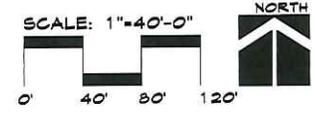




PEDESTRIAN CIRCULATION LEGEND	
	6' WIDE PUBLIC PEDESTRIAN CIRCULATION
	6' WIDE PUBLIC PEDESTRIAN CIRCULATION
	48" WIDE PUBLIC PEDESTRIAN CIRCULATION
	PRIVATE PEDESTRIAN CIRCULATION
	A.D.A. ACCESSIBLE RAMP
	INTERIOR/EXTERIOR TRANSITION
VEHICULAR CIRCULATION LEGEND	
	EXISTING ROADWAYS
	PROPOSED VEHICULAR ACCESS
	EMERGENCY ACCESS ONLY

# DISTRICT AT THE QUARTER PEDESTRIAN & VEHICULAR CIRCULATION PLAN

15501 N. DIAL BLVD. SCOTTSDALE, AZ 85260  
AUGUST 30, 2016





**SITE DATA**

GROSS SITE AREA:	448,295 SQ.FT
NET SITE AREA:	384,727 SQ.FT
ZONING:	I-1
PROPOSED ZONING:	PUD

**COMMON OPEN SPACE**

GROSS SITE AREA:	448,299 SQ.FT
PHASE I SITE AREA:	271,899.80 SQ.FT
OPEN SPACE FACTOR:	0.10
PHASE I REQUIRED OPEN SPACE:	27,189.98 SQ.FT
PHASE II SITE AREA:	176,700.49 SQ.FT
OPEN SPACE FACTOR:	0.10
PHASE II REQUIRED OPEN SPACE:	17,670.05 SQ.FT

**PHASE I - COMMON OPEN SPACE LEGEND**

	COMMON AREA OPEN SPACE - 28,187 SQ.FT
	OTHER OPEN SPACE AREA - 6,642 SQ.FT
	STREET FRONTAGE OPEN SPACE - 22,423 SQ.FT
	PARKING LOT LANDSCAPING - 2,589 SQ.FT

**PHASE II - COMMON OPEN SPACE LEGEND**

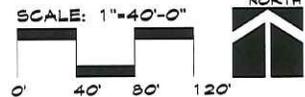
	COMMON AREA OPEN SPACE - 17,987 SQ.FT
	OTHER OPEN SPACE AREA - 10,299 SQ.FT
	STREET FRONTAGE OPEN SPACE - 7,844 SQ.FT
	NO PARKING LOT AREA IN PHASE II

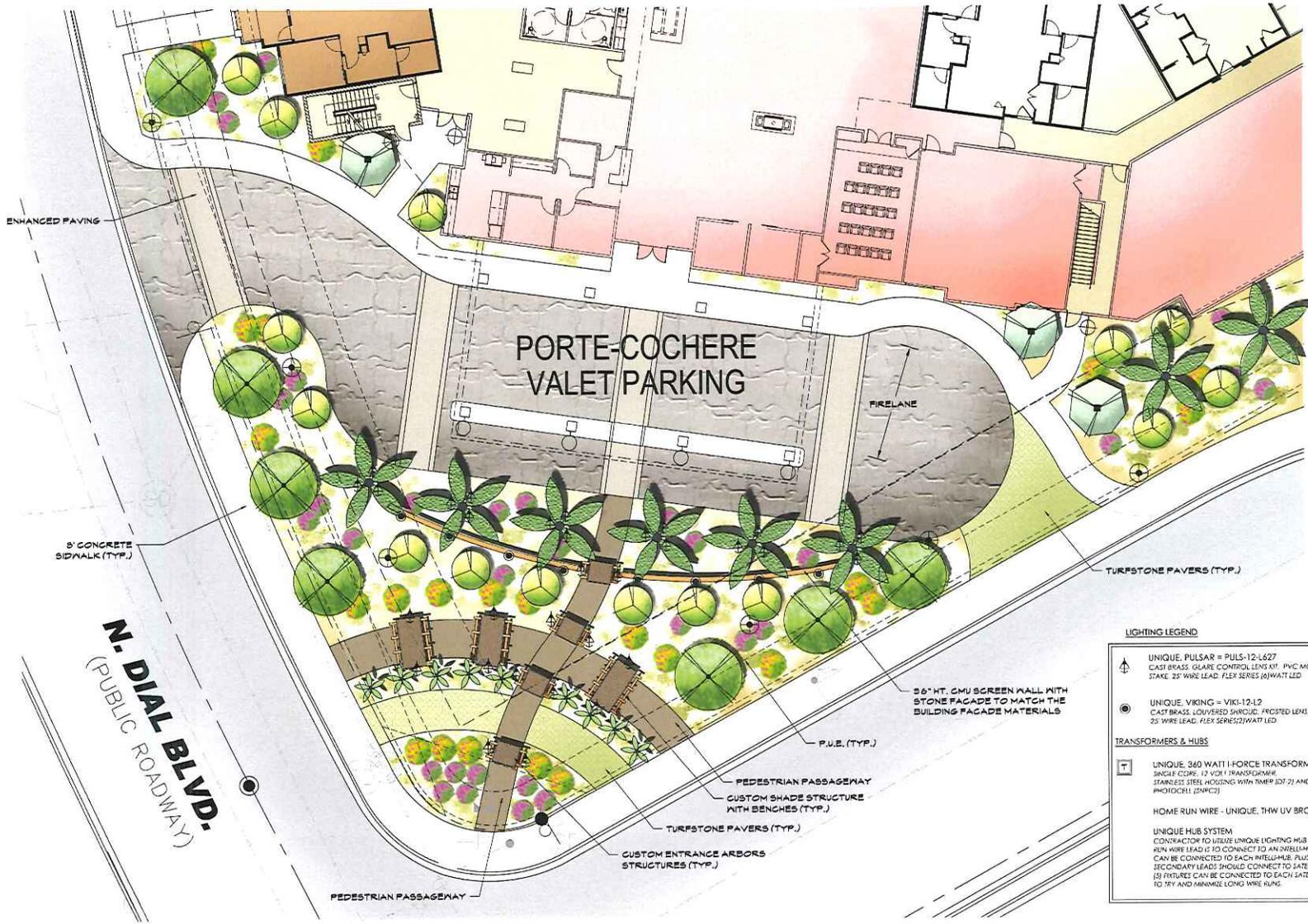
**OVERALL TOTAL OPEN SPACE FOR BOTH PHASES**

<b>COMMON OPEN SPACE PROVIDED:</b>	- 46,114 SQ.FT (10.29% OF GROSS SQ.FT) (NOT INCLUDING OTHER OPEN SPACE AREA, STREET FRONTAGE AREA OR PARKING LOT LANDSCAPING AREA).
<b>TOTAL OPEN SPACE AREA PROVIDED:</b>	- 96,402 SQ.FT (21.90% OF GROSS SQ.FT) (INCLUDES ALL OPEN SPACE AREAS, STREET FRONTAGE & PARKING LANDSCAPE AREAS).

**DISTRICT AT THE QUARTER  
COMMON OPEN SPACE PLAN**

15501 N. DIAL BLVD. SCOTTSDALE, AZ 85260  
AUGUST 30, 2016





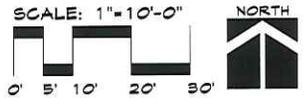
**LIGHTING LEGEND**

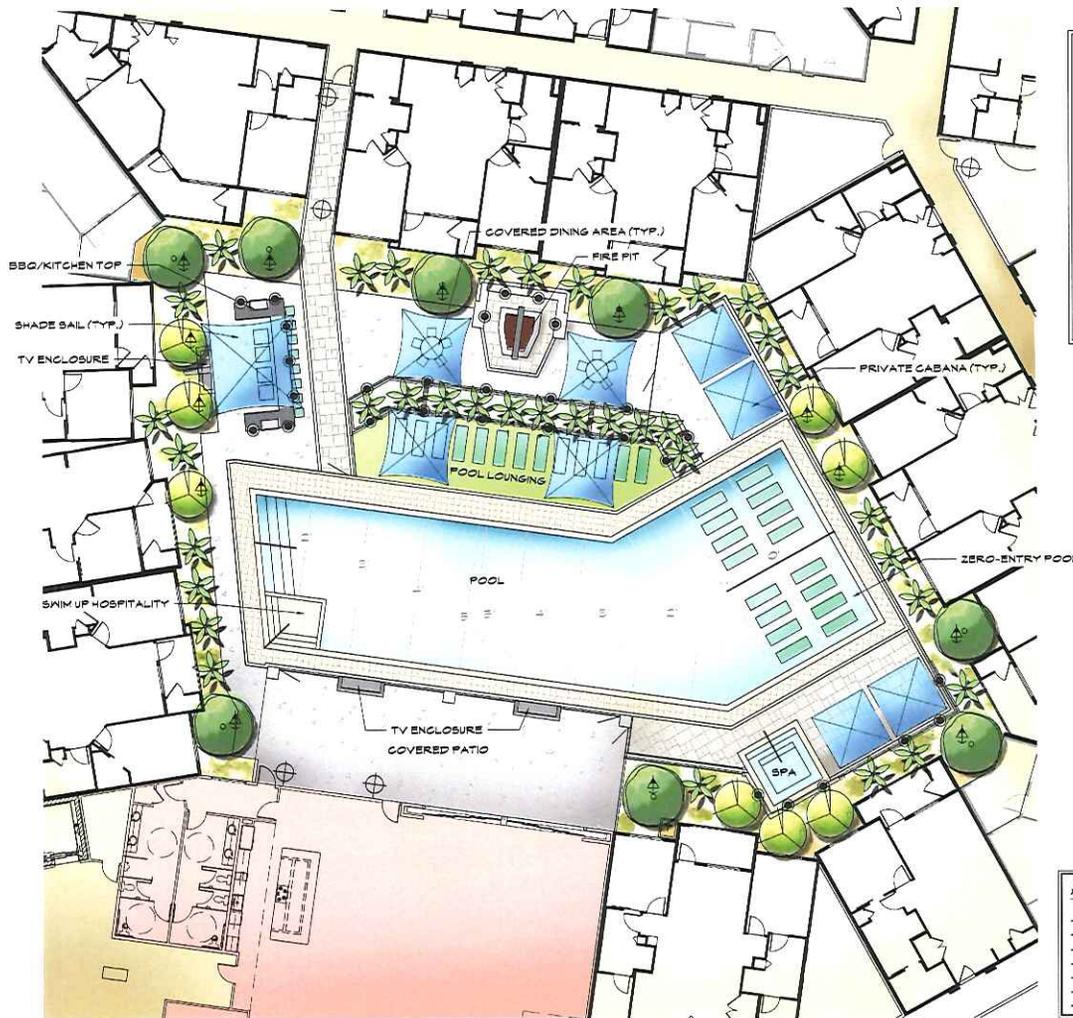
⬆	UNIQUE PULSAR = PULS-12-L627 CAST BRASS GLARE CONTROL LENS KIT, PVC MOUNTING STAKE, 25' WIRE LEAD, FLEX SERIES (8)WATT LED	QTY. (32)
⊙	UNIQUE VIBING = VIBI-12-L2 CAST BRASS LOUVERED SHROUD, FROSTED LENS, 25' WIRE LEAD, FLEX SERIES (2)WATT LED	QTY. (8)
<b>TRANSFORMERS &amp; HUBS</b>		
T	UNIQUE 340 WATT I-FORCE TRANSFORMER (340IFSS) - QTY. 1 SINGLE CORE, 12 VOLT TRANSFORMER, STAINLESS STEEL HOUSING WITH TMR (37-2) AND SNAP IN PHOTOCELL (340IC)	
HOME RUN WIRE - UNIQUE, THW UV WIRE		
UNIQUE HUB SYSTEM CONTRACTOR TO UTILIZE UNIQUE LIGHTING HUB SYSTEM. EACH HOME RUN WIRE LEAD IS TO CONNECT TO AN INTELLIHUB. A MAX. OF (3) STRUCTURES CAN BE CONNECTED TO EACH INTELLIHUB. PLUS (2) SECONDARY LEADS. SECONDARY LEADS SHOULD CONNECT TO SATELLITE HUBS. A MAX. OF (3) FIXTURES CAN BE CONNECTED TO EACH SATELLITE HUB. CONTRACTOR TO TRY AND MINIMIZE LONG WIRE RUNS.		

CORNER - ENTRY - PEDESTRIAN PASSAGEWAY - PARKING

# DISTRICT AT THE QUARTER CORNER ENLARGEMENT

15501 N. DIAL BLVD. SCOTTSDALE, AZ 85260  
AUGUST 4, 2016





**LIGHTING LEGEND**

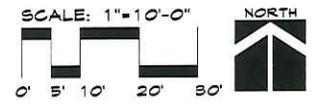
	UNIQUE, PULSAR = PULS-12-L627 CAST BRASS, GLARE CONTROL LENS KIT, PVC MOUNTING STAKE, 25' WIRE LEAD, FLEX SERIES (6)WATT LED	QTY. (13)
	UNIQUE, VIKING = VIKI-12-L2 CAST BRASS, LOUVERED SHROUD, FROSTED LENS, 25' WIRE LEAD, FLEX SERIES (2)WATT LED	QTY. (29)
<b>TRANSFORMERS &amp; HUBS</b>		
	UNIQUE, 360 WATT I-FORCE TRANSFORMER (360IFSS) - QTY. 1 SINGLE CORE, 12 VOLT TRANSFORMER STAINLESS STEEL HOUSING WITH THER (DT 3) AND SNAP IN PHOTOCELL (SNPC2)	
HOME RUN WIRE - UNIQUE, THW UV BROWN WIRE		
UNIQUE HUB SYSTEM CONTRACTOR TO UTILIZE UNIQUE LIGHTING HUB SYSTEM. EACH HOME RUN WIRE LEAD IS TO CONNECT TO AN INTELLI HUB. A MAX. OF (3)FIGURES CAN BE CONNECTED TO EACH INTELLI HUB, PLUS (2) SECONDARY LEADS. SECONDARY LEADS SHOULD CONNECT TO SATELLITE-HUBS. A MAX. OF (5) FIGURES CAN BE CONNECTED TO EACH SATELLITE-HUB. CONTRACTOR TO TRY AND MINIMIZE LONG WIRE RUNS.		

<b>AMENITIES</b>	
-	POOL WITH ZERO-ENTRY
-	FIRE PIT
-	(5) TV ENCLOSURES
-	BBQ/KITCHEN TOP
-	SEATING/LOUNGING AREAS
-	DINING AREAS
-	SPA

**COURTYARD 1 - POOL ENTERTAINMENT**

**DISTRICT AT THE QUARTER  
COURTYARD ENLARGEMENT 1**

1550 1 N. DIAL BLVD. SCOTTSDALE, AZ 85260  
AUGUST 4, 2016





COURTYARD 2 - OUTDOOR KITCHEN - CULINARY DEMONSTRATION

**LIGHTING LEGEND**

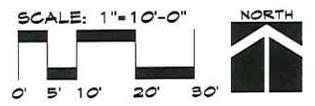
▲	UNIQUE, PULSAR = PULS-12-L627 CAST BRASS, GLARE CONTROL LENS KIT, PVC MOUNTING STAKE, 25' WIRE LEAD, FLEX SERIES (60WATT LED)	QTY. (29)
●	UNIQUE, VIKING = VIKI-12-L2 CAST BRASS, LOUVERED SHROUD, FROSTED LENS, 25' WIRE LEAD, FLEX SERIES (21WATT LED)	QTY. (11)
<b>TRANSFORMERS &amp; HUBS</b>		
⊞	UNIQUE, 360 WATT I-FORCE TRANSFORMER (360FSS) - QTY. 1 SINGLE CORE, 12 VOLT TRANSFORMER, STAINLESS STEEL HOUSING WITH TIMER (D1-2) AND SNAP IN PHOTOCELL (SNPC2)	
HOME RUN WIRE - UNIQUE, THW UV BROWN WIRE		
UNIQUE HUB SYSTEM CONTRACTOR TO UTILIZE UNIQUE LIGHTING HUB SYSTEM. EACH HOME RUN WIRE LEAD IS TO CONNECT TO AN INTELLI HUB. A MAX. OF 15 FIXTURES CAN BE CONNECTED TO EACH INTELLI HUB, PLUS (2) SECONDARY LEADS. SECONDARY LEADS SHOULD CONNECT TO SATELLITE HUBS. A MAX. OF (5) FIXTURES CAN BE CONNECTED TO EACH SATELLITE HUB. CONTRACTOR TO TRY AND MINIMIZE LONG WIRE RUNS.		

- AMENITIES**
- OUTDOOR KITCHEN/CULINARY DEMONSTRATION AREA
  - SEATING/LOUNGING AREAS
  - DINING AREAS
  - CITRUS TREES
  - HAMMOCK CLUSTER

# DISTRICT AT THE QUARTER

## COURTYARD ENLARGEMENT 2

15501 N. DIAL BLVD. SCOTTSDALE, AZ 85260  
AUGUST 4, 2016





COURTYARD 3 - GAMING AREA/ACTIVE RECREATION

**LIGHTING LEGEND**

- ▲ UNIQUE, PULSAR = PULS-12-L627 QTY. (10)  
CAST BRASS, CLEAR CONTROL LENS KIT, PVC MOUNTING  
STAKE, 25' WIRE LEAD, FLEX SERIES (6/WATT LED)
  - UNIQUE, WIKING = WIKI-12-L12 QTY. (18)  
CAST BRASS, COLOURED SHROUD, FROSTED LENS,  
25' WIRE LEAD, FLEX SERIES (2/WATT LED)
- TRANSFORMERS & HUBS**
- ⌚ UNIQUE, 360 WATT I-FORCE TRANSFORMER (340IFSS) - QTY. 1  
SINGLE CORE, 12 VOLT TRANSFORMER,  
STAINLESS STEEL HOUSING WITH DIMER (D1-2) AND SNAP IN  
PHOTOCELL (SNHC2)
  - HOME RUN WIRE - UNIQUE, THW UV BROWN WIRE
- UNIQUE HUB SYSTEM.  
CONTRACTOR TO UTILIZE UNIQUE LIGHTING HUB SYSTEM. EACH HOME RUN  
WIRE LEAD IS TO CONNECT TO AN INTELLHUB. A MAX. OF (1) FIXTURES CAN BE  
CONNECTED TO EACH INTELLHUB. (MAX. OF (2) SECONDARY LEADS.  
SECONDARY LEADS SHOULD CONNECT TO SATELLITE-HUBS. A MAX. OF  
(5) FIXTURES CAN BE CONNECTED TO EACH SATELLITE-HUB. CONTRACTOR  
TO TRY AND MINIMIZE LONG WIRE RUNS.

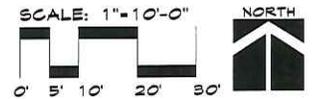
**AMENITIES**

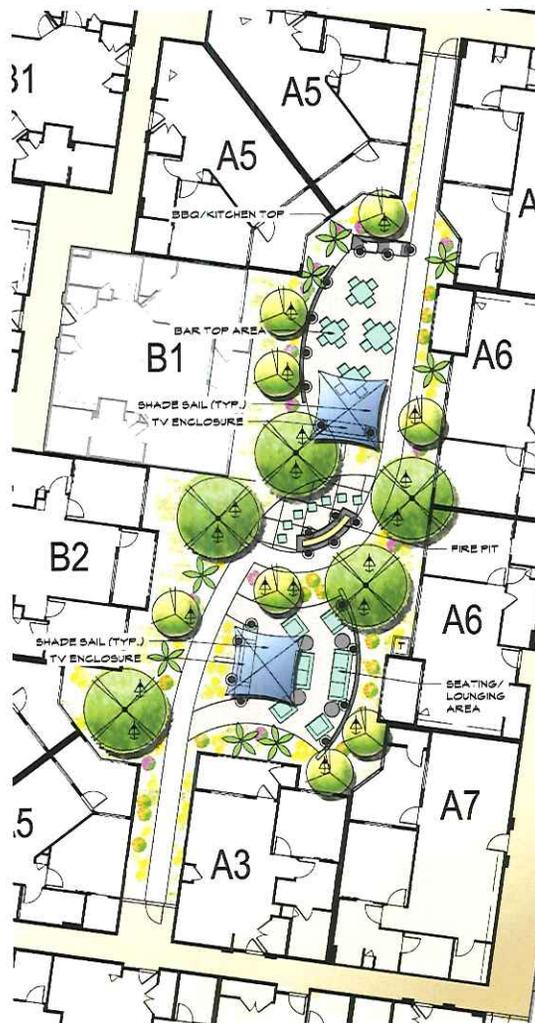
- BOCCIE BALL COURT
- BAG TOSS COURT
- PING PONG TABLE AREA
- (2) TV ENCLOSURES
- BBQ/KITCHEN TOP
- SEATING/DINING AREAS

**DISTRICT AT THE QUARTER  
COURTYARD ENLARGEMENT 3**

15501 N. DIAL BLVD. SCOTTSDALE, AZ 85260

AUGUST 4, 2016





COURTYARD 4 - OUTDOOR LIVINGROOM

**LIGHTING LEGEND**

UNIQUE, PULSAR = PULS-12-L6Z7  
CAST BRASS, GLARE CONTROL LENS KIT, PVC MOUNTING STAKE, 25' WIRE LEAD, FLEX SERIES (6)WATT LED QTY. (19)

UNIQUE, VIKING = VIKI-12-L2  
CAST BRASS, LOUVERED SHROUD, FROSTED LENS, 25' WIRE LEAD, FLEX SERIES (2)WATT LED QTY. (22)

**TRANSFORMERS & HUBS**

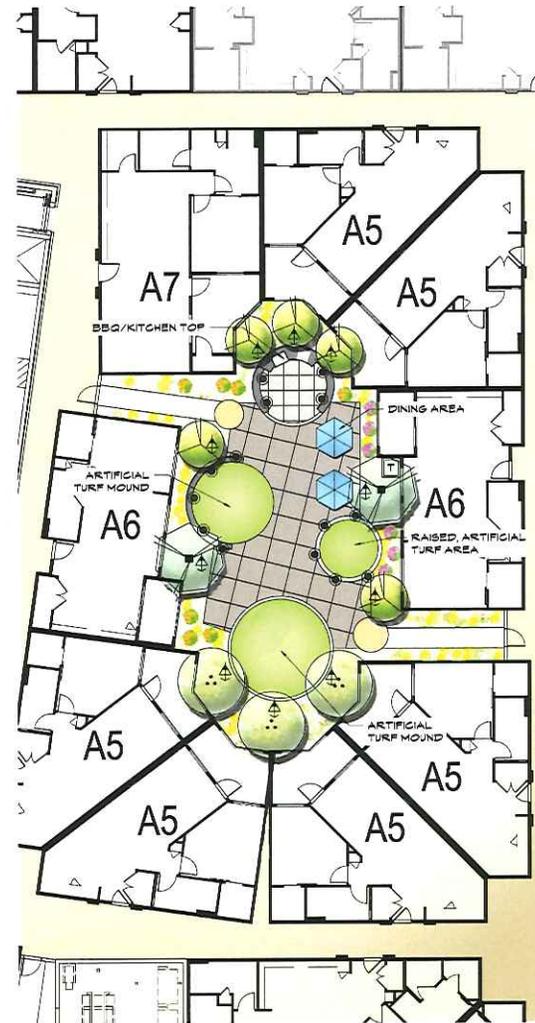
UNIQUE, 360 WATT I-FORCE TRANSFORMER (360IFSS) - QTY. 1  
SINGLE CORE, 12 VOLT TRANSFORMER  
STAINLESS STEEL HOUSING WITH TIMER (DT-2) AND SNAP IN PHOTOCELL (5NFC2)

HOME RUN WIRE - UNIQUE, THW UV BROWN WIRE

UNIQUE HUB SYSTEM  
CONTRACTOR TO UTILIZE UNIQUE LIGHTING HUB SYSTEM. EACH HOME RUN WIRE LEAD IS TO CONNECT TO AN INTELLI-HUB. A MAX. OF (5) FIXTURES CAN BE CONNECTED TO EACH INTELLI-HUB. PLUS (2) SECONDARY LEADS. SECONDARY LEADS SHOULD CONNECT TO SATELLITE-HUBS. A MAX. OF (5) FIXTURES CAN BE CONNECTED TO EACH SATELLITE-HUB. CONTRACTOR TO TRY AND MINIMIZE LONG WIRE RUNS.

**AMENITIES**

- FIRE PIT
- (2) TV ENCLOSURES
- BBQ/KITCHEN TOP
- SEATING/LOUNGING AREAS
- BAR TOP AREA



COURTYARD 5 - PASSIVE RECREATION

**LIGHTING LEGEND**

UNIQUE, PULSAR = PULS-12-L6Z7  
CAST BRASS, GLARE CONTROL LENS KIT, PVC MOUNTING STAKE, 25' WIRE LEAD, FLEX SERIES (6)WATT LED QTY. (10)

UNIQUE, VIKING = VIKI-12-L2  
CAST BRASS, LOUVERED SHROUD, FROSTED LENS, 25' WIRE LEAD, FLEX SERIES (2)WATT LED QTY. (14)

**TRANSFORMERS & HUBS**

UNIQUE, 360 WATT I-FORCE TRANSFORMER (360IFSS) - QTY. 1  
SINGLE CORE, 12 VOLT TRANSFORMER  
STAINLESS STEEL HOUSING WITH TIMER (DT-2) AND SNAP IN PHOTOCELL (5NFC2)

HOME RUN WIRE - UNIQUE, THW UV BROWN WIRE

UNIQUE HUB SYSTEM  
CONTRACTOR TO UTILIZE UNIQUE LIGHTING HUB SYSTEM. EACH HOME RUN WIRE LEAD IS TO CONNECT TO AN INTELLI-HUB. A MAX. OF (5) FIXTURES CAN BE CONNECTED TO EACH INTELLI-HUB. PLUS (2) SECONDARY LEADS. SECONDARY LEADS SHOULD CONNECT TO SATELLITE-HUBS. A MAX. OF (5) FIXTURES CAN BE CONNECTED TO EACH SATELLITE-HUB. CONTRACTOR TO TRY AND MINIMIZE LONG WIRE RUNS.

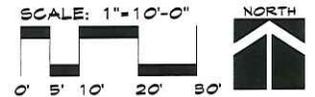
**AMENITIES**

- BBQ/KITCHEN TOP
- SEATING/LOUNGING AREAS
- DINING AREAS
- ARTIFICIAL TURF MOUNDS

# DISTRICT AT THE QUARTER

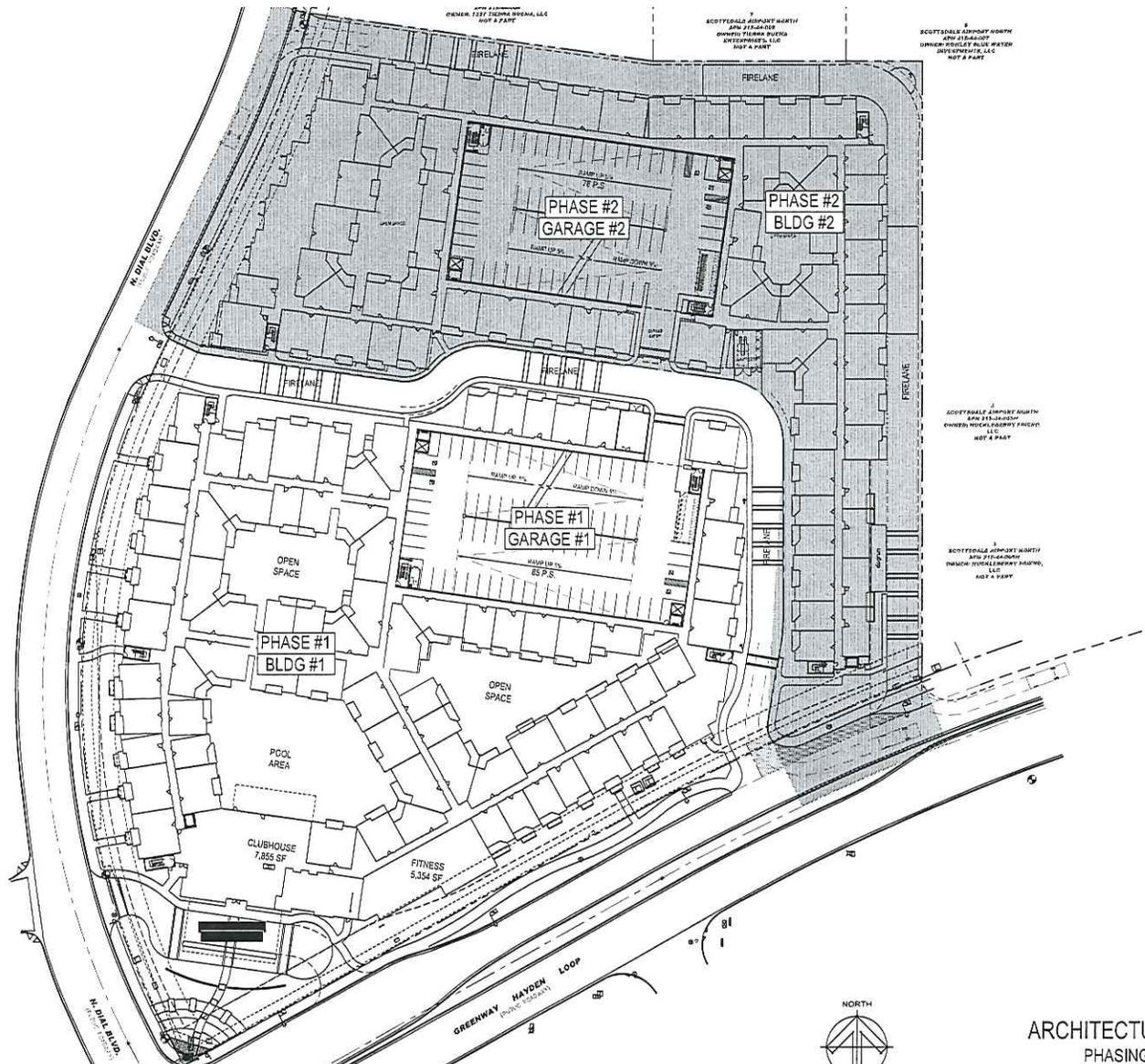
## COURTYARD ENLARGEMENTS 4 & 5

1550 1 N. DIAL BLVD. SCOTTSDALE, AZ 85260  
AUGUST 4, 2016



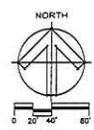
# Exhibit 7

## Phasing Plan



# DISTRICT AT THE QUARTER

## KAPLAN ACQUISITIONS, LLC



ARCHITECTURAL PHASING PLAN **A1.3**  
1" = 40'-0"

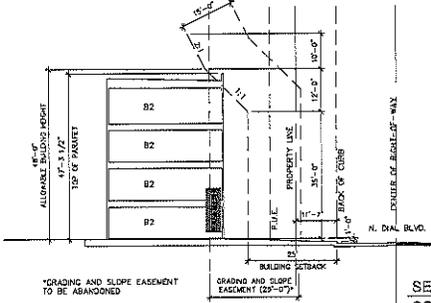
8-03-16

© BILTFORM ARCHITECTURE GROUP, INC.

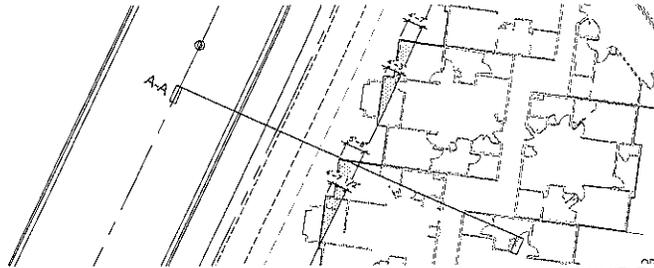
**3-GP-16/8-ZN-16**  
**08/05/16**

# Exhibit 8

## Building Cross Sections, Height Diagrams, and Height Exemption Area

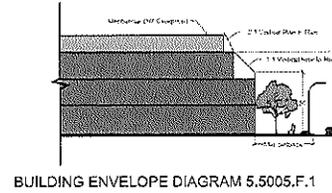


SECTION A-A  
SCALE: 1/16" = 1'-0"

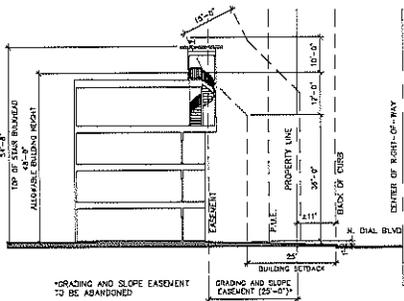


PHASE 2 - DIAL BOULEVARD

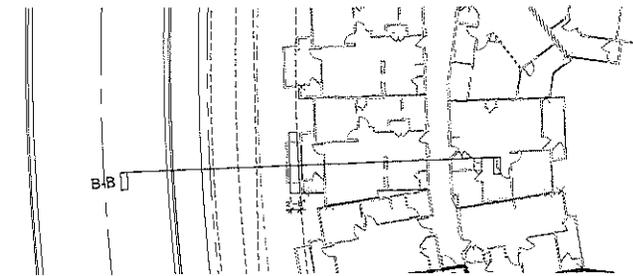
MAXIMUM ENCROACHMENT INTO EASEMENT  
SCALE: 1/16" = 1'-0"



BUILDING ENVELOPE DIAGRAM 5.5005.F.1

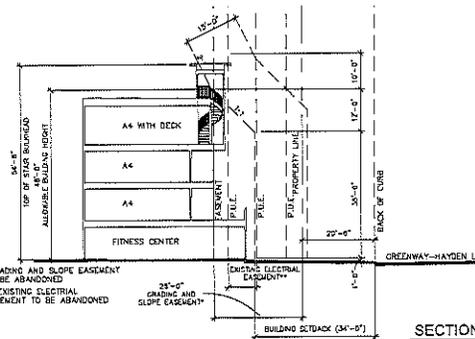


SECTION B-B  
SCALE: 1/16" = 1'-0"

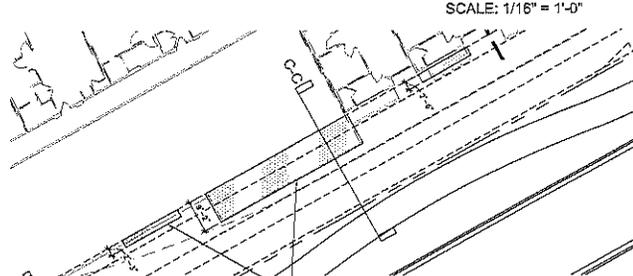


PHASE 1 - DIAL BOULEVARD

MAXIMUM ENCROACHMENT INTO EASEMENT  
SCALE: 1/16" = 1'-0"

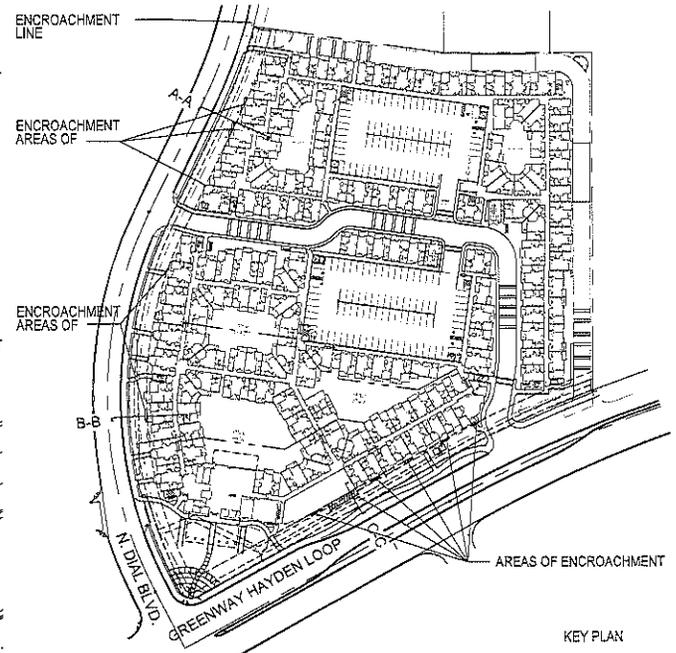


SECTION C-C  
SCALE: 1/16" = 1'-0"



PHASE 1 - GREENWAY-HADEN LOOP

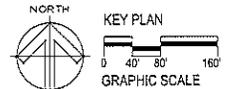
MAXIMUM ENCROACHMENT INTO EASEMENT  
SCALE: 1/16" = 1'-0"



KEY PLAN

THE AVERAGE BUILDING HEIGHT IS +1529.34' (NAVD88)

THE AVERAGE CURB HEIGHT	1481.06'
PLUS 1'-0"	+ 1.00'
OVERALL AVERAGE CURB HEIGHT	1482.06'
ZONING ORDINANCE SECTION 3.103	
OVERALL AVERAGE CURB HEIGHT	1482.06'
MAXIMUM BUILDING HEIGHT	+ 48.00'
MAXIMUM ELEVATION	1530.06'



AREAS OF ENCROACHMENT

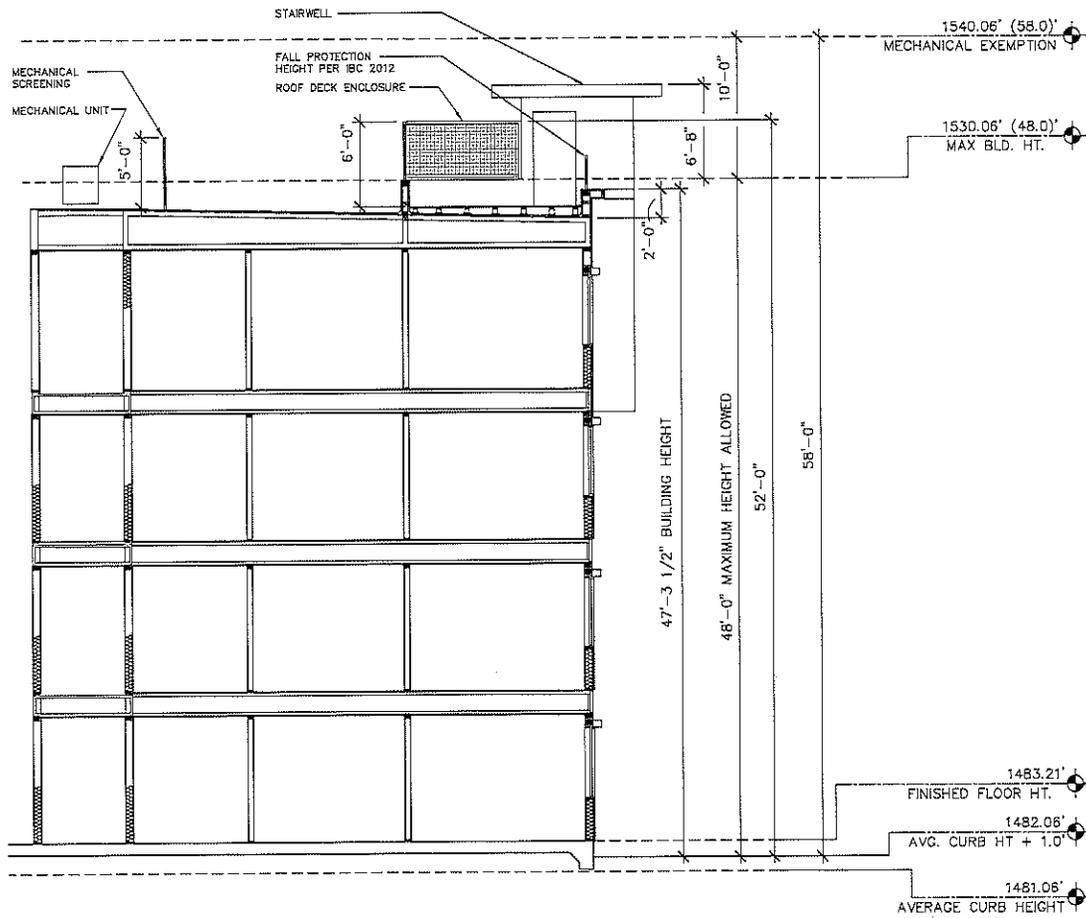
CROSS SECTIONS

A3.1

8-31-16

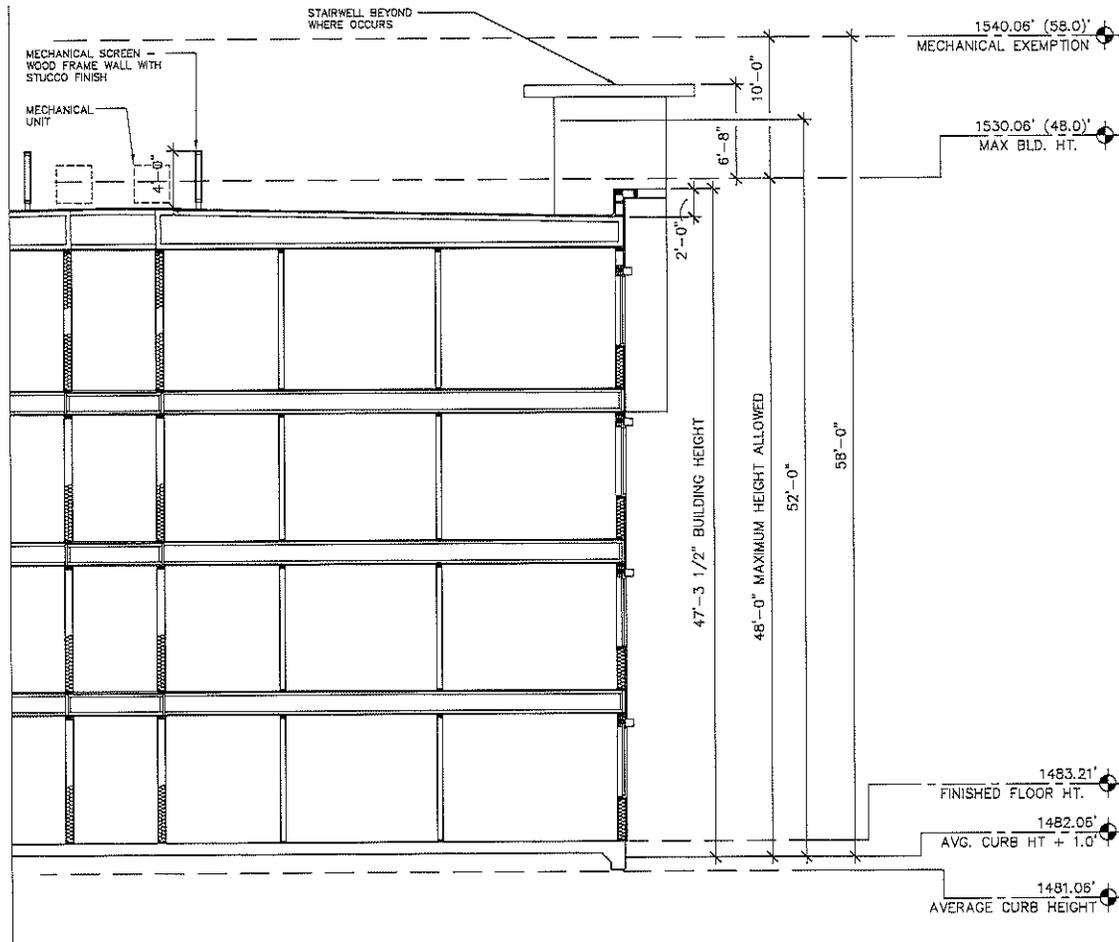
# DISTRICT AT THE QUARTER

## KAPLAN ACQUISITIONS, LLC



AVERAGE BUILDING HEIGHT DIAGRAM | **A3.3**

**DISTRICT AT THE QUARTER**  
KAPLAN ACQUISITIONS, LLC



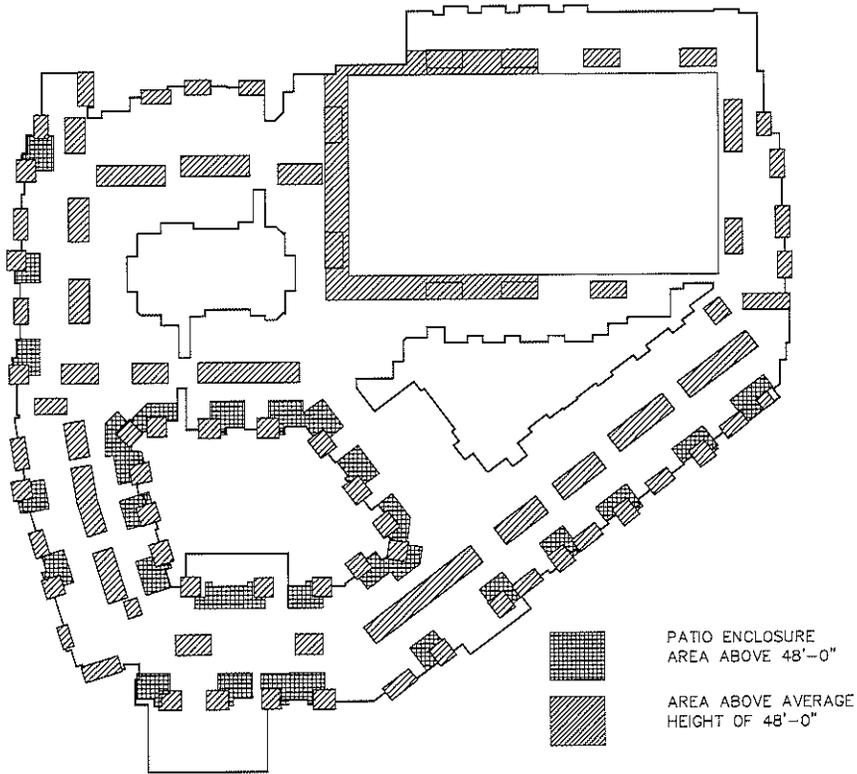
DISTRICT AT THE QUARTER  
KAPLAN ACQUISITIONS, LLC

MECHANICAL SCREEN DIAGRAM

A3.4

1/4" = 1'-0"

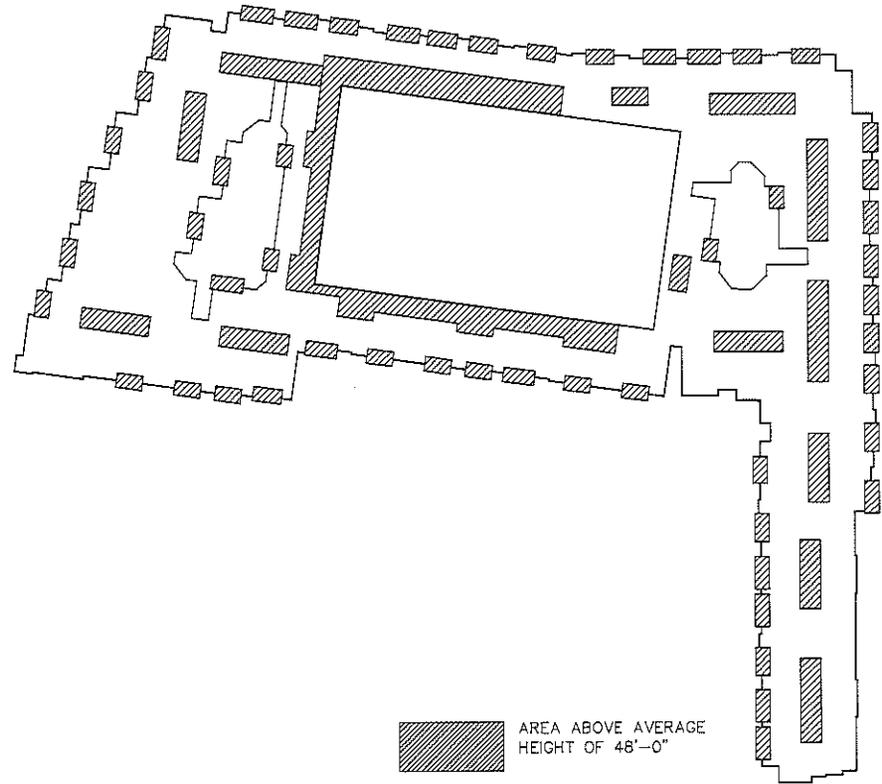
8-31-16



 PATIO ENCLOSURE  
AREA ABOVE 48'-0"  
 AREA ABOVE AVERAGE  
HEIGHT OF 48'-0"

30,957 SQ.FT. / 133,897 TOTAL  
ROOF AREA = 23%

ROOF AREA CALCULATION  
30% MAXIMUM PER 5.5005.D



 AREA ABOVE AVERAGE  
HEIGHT OF 48'-0"

22,003 SQ.FT. / 83,656 TOTAL ROOF  
AREA = 27.5%

ROOF AREA CALCULATION  
30% MAXIMUM PER 5.5005.D



# DISTRICT AT THE QUARTER

## KAPLAN ACQUISITIONS, LLC

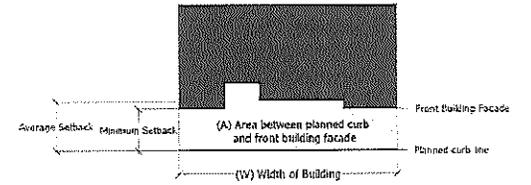
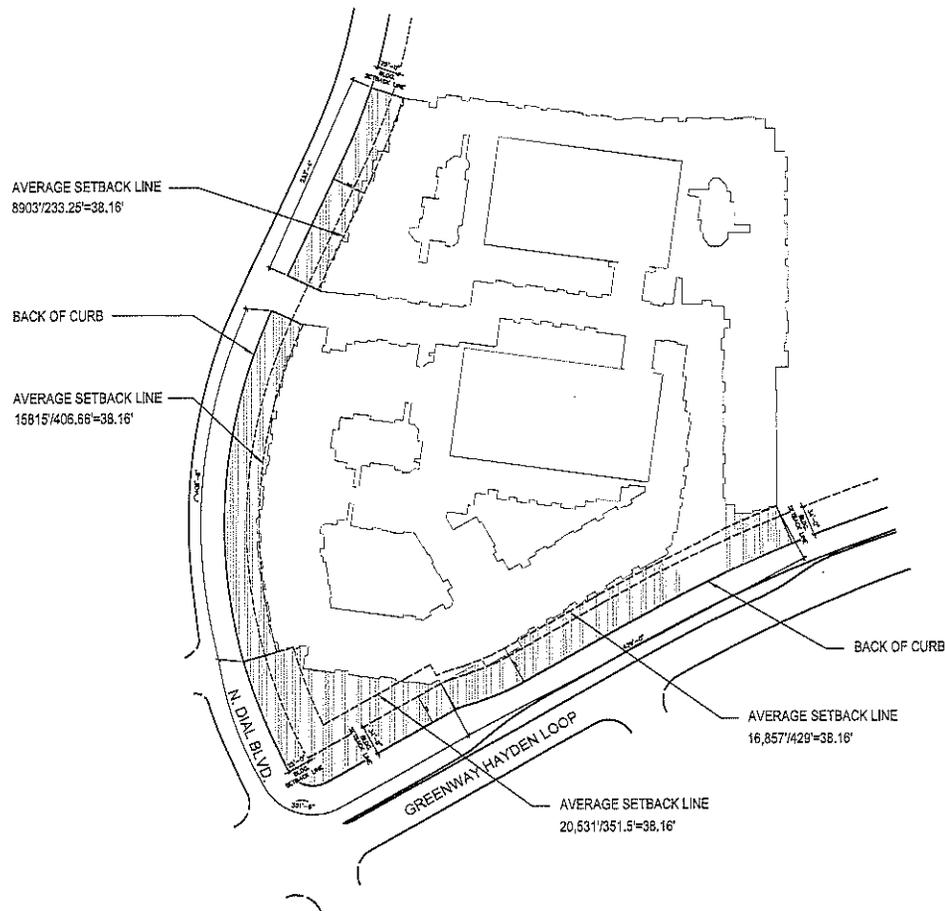
ROOF AREA  
NOT TO SCALE:

# A3.5

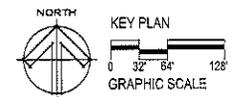
8-31-16

# Exhibit 9

## Average Building Setbacks



AVERAGE SETBACK DIAGRAM  
THIS DIAGRAM ILLUSTRATES SECTION 5.5005.E.2.



AVERAGE SETBACK

**A3.2**

8-24-16

**DISTRICT AT THE QUARTER**  
**KAPLAN ACQUISITIONS, LLC**

# Exhibit 10

## Amended Standards Legislative Draft

**\*\*\*AMENDED STANDARDS LEGISLATIVE DRAFT\*\*\***

Section 5.5000. Planned Development Unit (PUD).

...

Section 5.5005. Development Standards.

...

F. Building envelope.

1. Starting at a point thirty-six (36) feet above the minimum building setback line, the building envelope inclined stepback plane shall slope upwards at 1:1 (ratio of the vertical rise to the horizontal run) to a point measured forty-eight (48) feet above the minimum building setback line. Thereafter the building envelope inclined stepback plane shall slope upwards at 2:1 (ratio of the vertical rise to the horizontal run) on all sides of a property adjacent to public and private streets.

- a. **Encroachments to the stepback provision along Greenway-Hayden Loop and 73<sup>rd</sup> Street (a.k.a. Dial Blvd.) shall be allowed pursuant to the typical building cross-sections as noted on the following page. (See Section A-A, Section B-B, and Section C-C)**

**\*\*\*AMENDED STANDARDS LEGISLATIVE DRAFT\*\*\***

Section 5.5000. Planned Development Unit (PUD).

...

Section 5.5005. Development Standards.

...

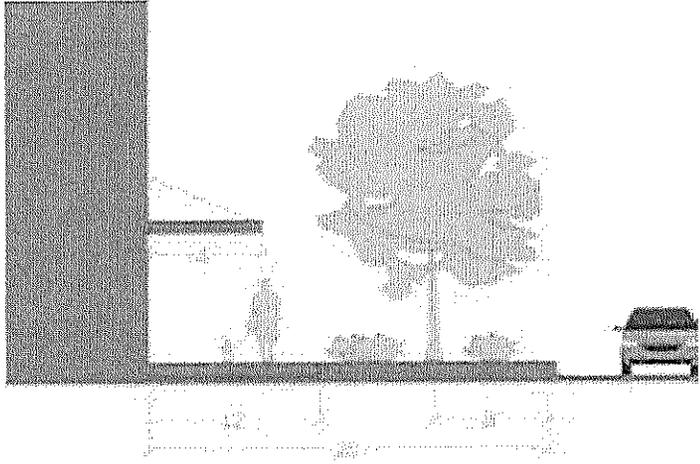
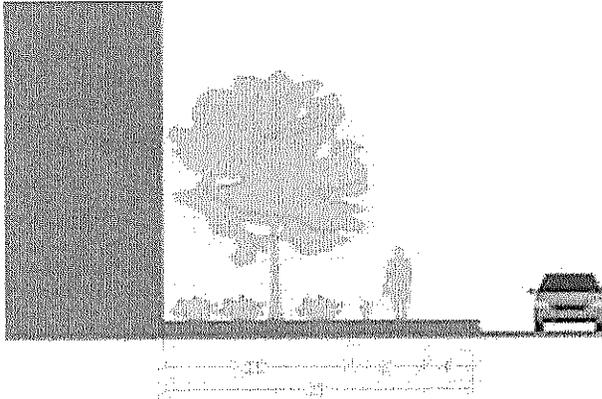
E. *Building setbacks.*

1. *Minimum setback.* Buildings adjacent to public and private streets shall be setback from the back of the planned curb line, including the planned curb line for bus bays and turn lanes, in accordance with Section 5.5005.E. Table A. The planned curb line, including the planned location for bus bays and turn lanes, shall be as described in the Transportation Master Plan and the Design Standards & Policies Manual.

<b>TABLE A</b>			
<b>BUILDING SETBACK FROM BACK OF THE PLANNED CURB ALONG STREETS</b>			
<b>Street Classification Type and Use</b>	<b>Minimum Setback</b>	<b>Average Setback</b>	<b>Setback Diagram - This Illustrates Setbacks, Excluding Average Setback. Dimensions are for Illustrative Purposes.</b>
Major and Minor Arterial - Residential on First Floor Adjacent to the Street  (Greenway-Hayden Loop)	34 feet	40-feet 38 Feet	

**TABLE A**

**BUILDING SETBACK FROM BACK OF THE PLANNED CURB ALONG STREETS**

Street Classification Type and Use	Minimum Setback	Average Setback	Setback Diagram - This Illustrates Setbacks, Excluding Average Setback. Dimensions are for Illustrative Purposes.
Major and Minor Arterial - Retail and Commercial on First Floor Adjacent to the Street  (Greenway-Hayden Loop)	28 feet	<del>32 feet</del> 38 Feet	
Major and Minor Collector, Local Streets, and Unclassified Streets (Including Private Streets but Excluding Alleys) - Residential on First Floor Adjacent to the Street  (73 <sup>rd</sup> Street/Dial Boulevard)	25 feet	<del>30 feet</del> 38 feet	

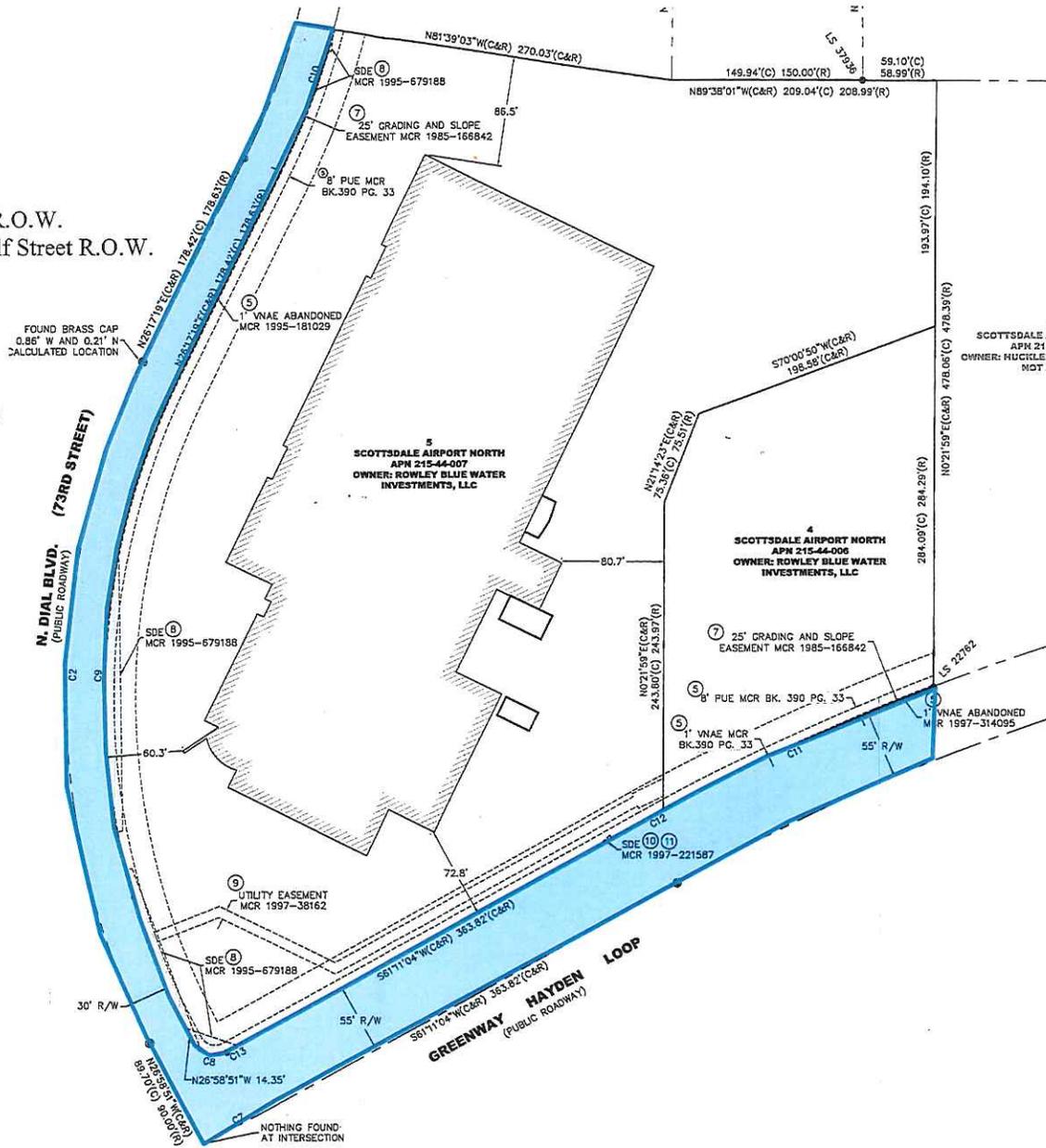
# Exhibit 11

## Right-of-Way

**Right-Of-Way Exhibit**  
*"District at the Quarter"*

Gross Acreage: 10.29 ac  
 Dial/ 73<sup>rd</sup> Street: 30' Half Street R.O.W.  
 Greenway-Hayden Loop: 55' Half Street R.O.W.

 Indicates Right-Of-Way along Dial/ 73<sup>rd</sup> Street and Greenway-Hayden Loop. (Approximately 1.45 acres)



# Exhibit 12

## Arizona Republic Article and Colliers International Report

# Scottsdale Airpark booming with urban life, 55,000 jobs

[Parker Leavitt](#), The Republic | [azcentral.com](#) 2:53 p.m. MST March 25, 2016



*A small jet comes in for a landing at Scottsdale Airport on March 10, 2015. The airport is one of the busiest corporate jet facilities in the state. (Photo: Tom Tingle/The Arizona Republic)*

A record number of businesses and employees have moved into Scottsdale's bustling Airpark corridor, which is emerging as a one of the Phoenix area's major hubs for urban growth, according to a new report from Colliers International.

More than 55,000 employees work for about 3,000 companies within the Scottsdale Airpark area, which covers about 5 square miles in north Scottsdale and Phoenix's Kierland neighborhood. The Airpark added nearly 1,200 jobs over the past year and more than 7,000 jobs since the height of the recession in December 2009, according to Colliers.

The previous record for jobs in the Scottsdale Airpark area was set in 2014 at 54,100, the report said. There were also 2,950 companies in the area last year.

The annual report, compiled by Jim Keeley, founding partner of Colliers' Scottsdale office, also shows significant improvement in vacancy rates, land sales and property values over the past

several years. Colliers is among the real-estate firms that broker deals in the Airpark and other parts of the Valley.

Although Airpark office space was selling for about \$50 to \$70 per square foot in 2011, tenants are paying closer to \$150 or even \$200 per square foot now, Keeley said. He expects rates to continue climbing over the next two or three years toward the prerecession peak of around \$270 to \$300.

Land sales totaled \$94.8 million for about 89 acres in 2015, up from \$46.7 million for about 38 acres the previous year, according to Colliers.

Investment from GoDaddy founder Bob Parsons and global investment firm Vanguard helped the Airpark build on that momentum in 2015, Keeley said.

**His Colliers report detailed several key announcements for the area last year, including:**

- Parsons opened the world's largest Harley-Davidson dealership in November at Hayden Road and Northsight Boulevard. The dealership features a 95-seat chapel for weddings and a movie theater.
- Vanguard added 600 employees while expanding to fill a 150,000-square-foot building near Pima Road and Raintree Drive.
- American Girl opened its only Arizona retail store, which includes a 25-seat bistro, at Scottsdale Quarter in August. The store is one of only 20 in the U.S.

## **Urban living arrives, transit needed**

Residential towers have begun to spring up among the Airpark's corporate offices and shopping centers as young professionals pursue a more urban lifestyle within walking distance of their workplaces.

Crescent Communities recently opened 275 high-end apartments at Scottsdale Quarter, and another 220 luxury condos are under construction across Scottsdale Road at Optima Kierland.



*This rendering shows Optima Kierland, a 12-story luxury condominium development going up at Scottsdale Road and Kierland Boulevard. (Photo: Optima Kierland)*

Liv North Scottsdale built 240 apartments just north of Scottsdale Quarter in 2014, and a 282-unit complex called Sunrise Commons is planned to replace a former car dealership on Hayden Road.

"I think society is into enjoying urban living," Keeley said. "The Millennials, that next generation of working people, they have a different mind-set."

With the arrival of more "transit-oriented development" at Scottsdale Quarter and Kierland Commons comes the need for more public-transit options, Keeley said. A city proposal to add trolley service in the Airpark would help, but Keeley would like to see light rail come up through Scottsdale from the south.

"There's a definite need for improvement," Keeley said. "I think if you look around the country at other employment bases that are evolving, you need that type of alternative."

A new transportation plan up for City Council consideration this year includes three potential rail routes through Scottsdale, including two that would end near Scottsdale Fashion Square mall. The plan also includes a new bus route with non-stop service between the Airpark and Fashion Square.

If approved by the council, Airpark trolley service could start in April 2018, and the bus service could start in October 2019, according to city officials.

## **A cornucopia of industries**

Although some airport commerce parks trend more toward heavy industry and warehousing, the area surrounding Scottsdale Airport features a higher concentration of corporate office space, including several regional and national headquarters.

JDA Software announced plans last year to move its headquarters to two floors of a new six-story office building opening at Scottsdale Quarter. Taser International maintains its main offices 2 miles to the northeast, and New Zealand-based Orion Health picked the Scottsdale Airpark in 2014 for its North American headquarters and about 500 jobs.

In all, there are 130 business categories represented within the Airpark, ranging from aerospace to hospitality, according to Colliers.

The area ranks among the largest employment centers in the Phoenix area and is uniquely located to draw employees from the West Valley, Phoenix, Scottsdale and the Southeast Valley, Scottsdale Economic Development Director Danielle Casey said.

"The top thing that everybody has been talking about in the last several years is that talent is the biggest issue," Casey said. "That's one of the things going for us in the Airpark. It can pull from pretty much all nodes, which is very advantageous when you are looking at a dispersed talent pool."

## **Airport grows busier**

Along with the growth has come a spike in air traffic at Scottsdale Airport, which in 2015 surged to No. 21 among the nation's busiest general-aviation airports. When you account for major airlines, which don't have a presence in Scottsdale, and military operations, the airport ranks No. 76 nationally, according to the Federal Aviation Administration.

U.S. Customs and Border Patrol maintains a daily presence at Scottsdale Airport, which boosts its appeal to international travelers and businesses, Casey said.

The FAA reported about 77,300 general-aviation takeoffs and landings at Scottsdale Airport last year, up from about 74,700 flights in 2014 and 69,800 in 2010. General-aviation traffic reflects private flights to and from other airports.

The airport reported more than 157,000 takeoffs and landings overall in 2015.



# THE GREATER SCOTTSDALE AIRPARK 2030 REPORT



**Jim Keeley, SIOR, CCIM**  
Founding Partner  
Scottsdale Office

### LOCATION

The "Greater Scottsdale Airpark" Boundaries:

NORTH Loop 101 Pima Freeway

SOUTH Thunderbird Rd.

WEST 64th St.

EAST 90th St. - between Raintree & the CAP Canal; 96th St. - between the CAP Canal & Bell Rd.

### ZONING

Commercial Office; Light Industrial; Hotel; Commercial Retail; Aviation; Multifamily

### SIZE

±3,300 Acres

±34,193,616 SF of Buildings

### CURRENTLY DEVELOPED

±3,100 Acres

### NUMBER OF COMPANIES

±3,025

### BUSINESS CATEGORIES

±130 (i.e., accountants, auto, publishing, aerospace/defense, light manufacturing, retail, high tech, healthcare & biotech, business services, hospitality, retail, etc.)

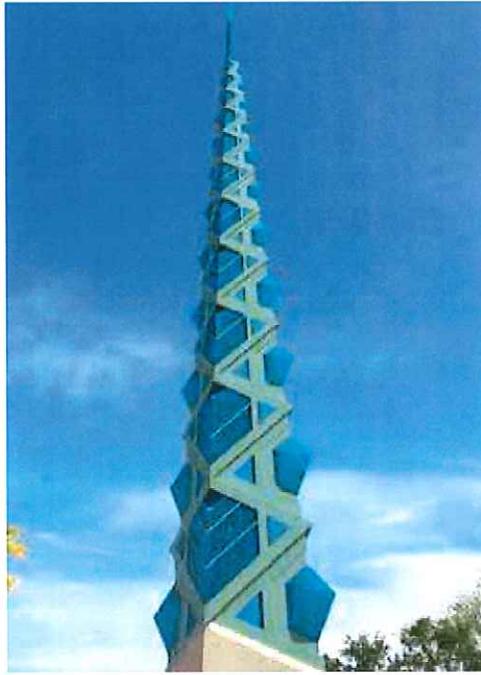
### NUMBERS OF EMPLOYEES

±55,260

### NUMBERS OF BUILDINGS - 1,156

(including hotels, auto dealerships, all retail)

Building Usage	Square Feet	Percentage
Office	12,886,844	38%
Industrial/Flex	9,241,921	27%
Hangar	837,772	2%
Retail	8,331,727	24%
Hotels	2,895,352	8%
Total	34,193,616	

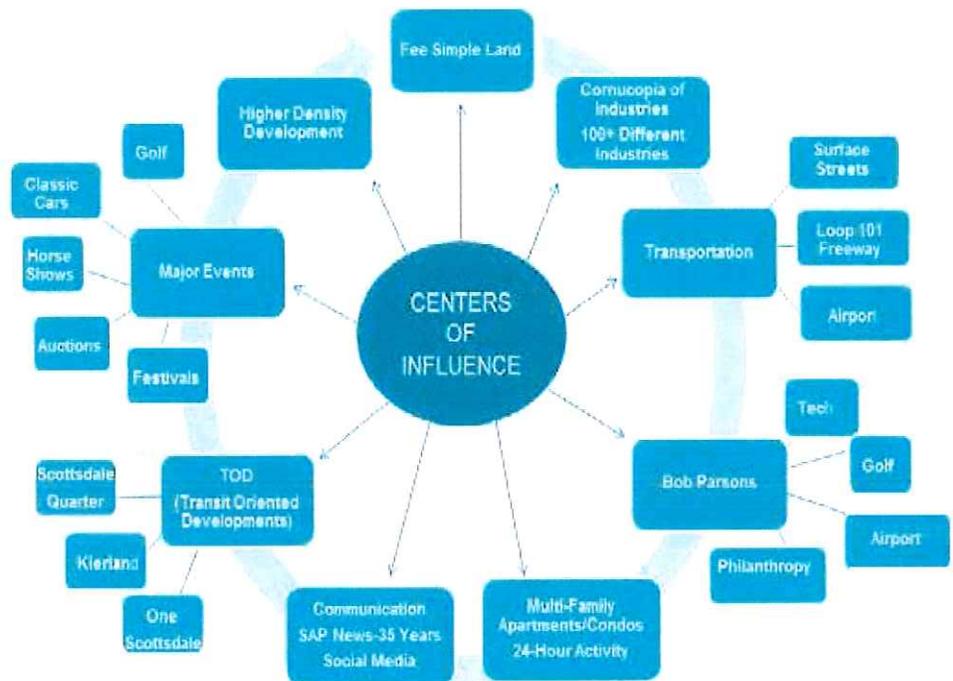


## Spire

Standing 125 feet tall on the southeast corner of Frank Lloyd Wright Boulevard and Scottsdale Road, the Spire has become one of Scottsdale's most recognizable shapes since it was built in 2004. Designed by Frank Lloyd Wright originally for the state capitol building, the Spire instead stands on his namesake road in Scottsdale. This was considered tall in 1994.

**Jim Keeley Prediction** - "By 2024, it is reasonable to expect having 10 buildings taller than the Spire."

## The Cogs in the Wheel of the Greater Scottsdale Airpark



January, 2016 - Past, Present & Future to 2030

Jim Keeley, Founding Partner Scottsdale Office, 14080 N. Northsight Blvd., Scottsdale, AZ  
480 596 9000 Jim.Keeley@colliers.com

## How Do The Centers of Influence Impact the Greater Scottsdale Airpark?

**FEE SIMPLE LAND:** The Spirit of the Wild West predicated that Rugged Individualism helped build Arizona and the ability to own land. Fee Simple has been a big catalyst for the Greater Scottsdale Airpark. As an employment base, we are in close proximity to State of Arizona land and SRP Indian Community land which allows development on leased land. With Fee Simple land diminishing, it is likely that more State Leased land will become part of the Greater Scottsdale Airpark Employment Base.

**CORNUCOPIA OF 130+ INDUSTRIES:** From accountants to millennials, auto, publishing, aerospace/defense, light manufacturing, retail, high tech, healthcare & biotech, business services, hospitality, retail, etc.

**TRANSPORTATION:** Loop 101 Freeway: Prior to the opening in of the Loop 101 freeway in 2000, the Greater Scottsdale Airpark had already established itself as an employment base with 15,000,000 square feet of buildings and 31,000 employees. Since then it has grown to 34,000,000 square feet of buildings and 54,100 employees.

**BOB PARSONS:** The Leader and the epitome of Rugged Individualism.

- › Go Daddy Founder
- › World's largest Harley Davidson dealership
- › Parson's Extreme Golf "PXG"
- › YAM Properties
- › Philanthropy
- › Sneaky Big Studios - Scottsdale (Arizona's largest production studio)
- › Big Yam, The Parsons Agency

### **MULTI FAMILY:**

- › North Scottsdale Apartments: 240 units
- › Crescent Communities: 275 units
- › Plaza Loft at Kierland Commons: 54 units
- › Optima Kierland Center: 220 units (under construction)
- › Sunrise Commons: 282 unites (planned)
- › The View at Cascade: 187 units

**COMMUNICATION:** This year will mark the 35th anniversary of the Scottsdale Airpark News, which will continue to cover all the local news and provide a place for the cornucopia of businesses in The Airpark to share their stories. Social media will also continue to play a major role in all of our communications.

**TRANSIT ORIENTED DEVELOPMENT (TOD):** Driven by our change in culture and Millennials, the Scottsdale Quarter and Kierland Commons are the best current examples of TOD. These two centers bring together ambience, shopping, offices, restaurants, entertainment, condos and apartments.

### **MAJOR EVENTS:**

- › Barrett Jackson Collector Car Auction
- › Waste Management Phoenix Open Golf Tournament
- › Arabian Horse Show
- › International Motorcycle Show
- › Westworld Events
- › International Sportsman's Expo

**HIGHER DENSITY:** The Scottsdale Quarter, a 1,200,000 SF mall, replaced the former Dial/Henke 200,000 SF facility on 28 acres. That MXD set the new standard and several more will follow in the same fashion.

### **NEWS IN 2015:**

- › Regus: Opens in Raintree Corporate Center to complement the other Airpark office at Kierland. Originally just Executive Suites, they've now expanded to offer "Shared Office Space" and "Co-Working" environments that creates culture and community. "We Work" is another concept now open in 8 major cities and making its way to future employment bases.
- › Harley-Davidson of Scottsdale: Bob Parsons opened the world's largest Harley Davidson Dealership in November which includes a 95-seat chapel and multi-purpose room for weddings and events, a coffee bar, movie theater and many more unique amenities.
- › American Girl: The Mattel-owned brand opened at the Scottsdale Quarter in August to a frenzied crowd. It is the 20th US store to open and includes a 25-seat bistro. The American Girl trend is fueled in part by Millennials who are now parents and loved American Girl growing up.
- › Vanguard: Added 600 employees, taking over the entire 150,000 SF building at 15111 N. Pima Road.
- › Persil: A Henkel detergent brand goes from Europe to Scottsdale .
- › Economic Impact Study: Conducted by Lee McPheters, Director of the JPMorgan Chase Economic Outlook Center at ASU, in April, 2015, determined that aviation activity at the Scottsdale Airport, generated \$536,000,000 in total economic benefits for the region last year.

LAND SALES				
YEAR	# OF LOT SALES	SALES VOLUME	TOTAL ACRES	AVG PRICE/SF
2015	12	\$94,821,765	88.85	\$24.50
2014	13	\$46,716,714	37.57	\$24.00
2013	6	\$26,558,420	25.36	\$24.04
2012	4	\$10,880,978	15.36	\$16.25
2011	3	\$4,803,456	8.21	\$13.43
2010	5	\$6,209,156	9.73	\$14.64
2009	0	N/A	N/A	N/A
2008	3	\$7,250,000	4.45	\$37.40
2007	16	\$55,896,182	51.14	\$25.09

THE GREATER SCOTTSDALE AIRPARK GROWTH - JUNE 1981 - DECEMBER 2014				
DATE	SQUARE FEET	VACANCY	NUMBER OF EMPLOYEES	NUMBER OF BUSINESSES
Jun. 1981	1,580,611	13.9%	±3,320	268
Jun. 1982	1,720,210	10.1%	±3,680	289
Jun. 1983	1,900,188	6.1%	±4,038	316
Jan. 1984	2,288,439	10.9%	±4,504	383
Jun. 1984	2,587,000	10.5%	±5,320	460
Jan. 1985	2,683,394	11.8%	±5,345	480
Jul. 1985	3,206,670	17.3%	±5,500	517
Jan. 1986	3,673,589	18.7%	±5,861	534
Jan. 1987	4,419,226	19.4%	±8,661	733
Aug. 1987	4,811,007	19.0%	±8,912	841
Aug. 1988	4,881,979	16.0%	±9,802	957
Aug. 1989	5,234,493	17.2%	±10,355	930
Aug. 1990	5,396,211	14.3%	±10,120	943
Aug. 1991	5,868,194	14.2%	±10,654	1,082
Aug. 1992	6,141,078	12.6%	±11,345	1,076
Nov. 1993	6,146,866	6.7%	±13,402	1,237
Sep. 1994	6,569,987	3.5%	±13,846	1,257
Sep. 1995	6,932,228	2.3%	±14,132	1,332
Dec. 1996	8,126,066	3.1%	±17,275	1,477
Dec. 1997	9,697,203	3.2%	±19,837	1,624
Dec. 1998	11,085,954	4.1%	±25,891	1,708
Dec. 1999	14,626,300	6.0%	±30,344	1,807
Dec. 2000	15,657,920	7.6%	±31,992	1,868
Dec. 2001	16,282,920	9.8%	±33,015	1,907
Dec. 2002	17,356,120	12.9%	±41,265	2,198
Dec. 2003	18,562,463	11.9%	±44,815	2,288
Dec. 2004	20,553,975	11.5%	±47,218	2,397
Dec. 2005	21,403,975	9.9%	±48,634	2,481
Dec. 2006	22,350,975	9.7%	±50,450	2,554
Dec. 2007	28,454,277	12.9%	±52,500	2,774
Dec. 2008	28,852,256	15.1%	±52,000	2,800
Dec. 2009	28,942,406	29.0%	±48,000	2,576
Dec. 2010	28,962,889	28.8%	±48,500	2,580
Dec. 2011	28,971,900*	23.0%	±49,200	2,601
Dec. 2012	33,928,220*	18.5%	±52,000	2,848
Dec. 2013	33,986,890*	16%	±53,400	2,900
Dec. 2014	34,006,890*	13%	54,100	2,950
Dec. 2015	34,193,616*	11.6%	55,260	3,025

\*Using the newest technology (CoStar) we have updated the list of all buildings, including auto dealerships, Costco, Walmart, etc., that we had not included in the past.

## Major Companies

Accolade Healthcare  
 Alliance Defense Fund  
 APL Container Transportation Services  
 Apogee Physicians  
 AutoNation Ford  
 Best Western  
 Chase Bank  
 Choice Hotels International, Inc.  
 Colliers International  
 Corporate Jets, Inc.  
 Costco  
 Cox Cable of Scottsdale  
 Dillon Precision Industries  
 Discount Tire Company, Inc.  
 Early Warning Services  
 Earnhardt Cadillac  
 Fairmont Scottsdale Princess  
 Farmers Insurance  
 Fender Musical Instruments Corporation  
 First Fidelity Bank  
 Flow Dynamics, Inc.  
 GE Franchised Finance Corporation  
 General Mills  
 Go AZ Motorcycles  
 Go Daddy Software  
 Go Video  
 GovNet, Inc.  
 GPS Insight  
 GTCO CalComp Input Technologies Division  
 Harley-Davidson of Scottsdale  
 Henkel Corporate Headquarters  
 Home Depot  
 HomeSmart  
 Hypercom Corporation  
 IKON  
 Ingram Micro  
 Interface, Inc.  
 International Cruise & Excursions (SEARS)  
 IO Capital Princess, LLC  
 JDA Software  
 Knowledge Net  
 Kohl's Department Store  
 KYOCERA Solar  
 Lowes Home Improvement Warehouse  
 Markel West  
 Mayo Clinic  
 Mercer Advisors, Inc.  
 Meritage Corporation  
 Metris Company  
 MidFirst Bank  
 Mobility Electronics  
 Morgan Stanley Smith Earney, LLC  
 Nautilus Insurance Co.  
 North Central University  
 Orion Health  
 Pegasus Solutions  
 Peter Piper Corporate Headquarters  
 Phase 2 Solutions  
 Prudential Insurance  
 Pulte Homes  
 Rental Service Corp.  
 Redirect Health  
 Right Honda and Toyota  
 Russo & Steele  
 Schumacher European, Ltd. (Mercedes-Benz)  
 Scottsdale Association of Realtors  
 Scottsdale Prep Academy  
 SonicAir  
 South Hills Design Corporation  
 State Farm Insurance  
 Target  
 TASER International  
 Tesla Motors  
 The Tech Group / Tech Poulson  
 Trivita  
 Universal Technology Institute (UTI)  
 U.S. Postal Service  
 U-Haul  
 Unison Health Care  
 Van Chevrolet  
 VanGuard  
 Wal-Mart Supercenter  
 Westin Kierland Resort  
 World at Work

BUILDING SALES					
TYPE	# OF SALES	SALES VOLUME	TOTAL SF	AVG PRICE/SF	HIGH/LOW
<b>2015</b>					
Office	24	\$178,174,381	919,060	\$194	\$254/\$98
Industrial	23	\$48,994,677	395,149	\$124	\$192/\$77
Retail	13	\$97,120,600	448,244	\$217	\$767/\$132
<b>Total</b>	<b>60</b>	<b>\$324,289,658</b>	<b>1,762,453</b>		
<b>2014</b>					
Office	30	\$69,800,000	555,965	\$139	\$217/\$79
Industrial	39	\$60,000,000	668,488	\$114	\$257/\$52
Retail	6	\$39,744,000	208,927	\$190	\$414/\$15,
<b>Total</b>	<b>75</b>	<b>\$169,544,000</b>	<b>1,433,380</b>		
<b>2013</b>					
Office	22	\$179,000,800	1,098,164	\$163	\$312/\$97
Industrial	25	\$24,194,800	9,000	\$101	\$149/\$58
Retail	2	\$4,200,000	239,552	\$250	N/A
<b>Total</b>	<b>49</b>	<b>\$207,395,600</b>	<b>1,346,716</b>		
<b>2012</b>					
Office	41	\$204,454,653	998,687	\$114	\$305/\$48
Industrial	28	\$50,662,667	553,022	\$92	\$303/\$25
Retail	7	\$94,522,237	637,434	\$148	\$350/\$101
<b>Total</b>	<b>76</b>	<b>\$349,639,557</b>	<b>2,189,143</b>		
<b>2011</b>					
Office	29	\$27,185,499	261,400	\$104	\$188/\$54
Industrial	38	\$85,246,048	835,745	\$102	\$185/\$58
Retail	11	\$68,809,575	537,574	\$128	\$248/\$85
<b>Total</b>	<b>78</b>	<b>\$181,241,122</b>	<b>1,634,719</b>		
<b>2010</b>					
Office	37	\$66,533,044	608,465	\$109	\$219/\$56
Industrial	24	\$21,838,832	206,253	\$106	\$235/\$67
Retail	5	\$5,797,024	45,855	\$126	\$175/\$69
<b>TOTAL</b>	<b>66</b>	<b>94,168,900</b>	<b>860,573</b>		
<b>2009</b>					
Office	16*	\$37,917,000	308,000	\$123	\$285/\$75
Industrial	3	\$8,197,000	65,027	\$126	\$196/\$80
Retail	0	N/A	N/A	N/A	N/A
<b>TOTAL</b>	<b>19</b>	<b>\$46,114,000</b>	<b>373,027</b>		
*69% of the office sales in 2009 were for office condos smaller than 8,600 SF					
<b>2008</b>					
Office	17	\$23,739,375	92,360	\$257.03	\$348/\$157
Industrial	14	\$23,551,319	144,345	\$163.16	\$324/\$118
Retail	5	\$140,955,000	430,106	\$327.72	\$552/\$167
<b>TOTAL</b>	<b>36</b>	<b>\$188,245,694</b>	<b>666,811</b>		



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Jim Keeley, SIOR, CCIM, founded Classic Real Estate Corporation, now known as CC Partners, L.L.C., DBA Colliers International, in 1983. The firm conducts the majority of its commercial brokerage business within the Scottsdale Airpark and the Loop 101 employment base and has concluded over 5,000 transactions and \$2.7 billion of business.

A special thanks to Colliers International brokers for their input and, in particular, to Cristiana Zarr for the graphic design, Pete O'Neil for research efforts and Jamie McClellan for pulling it all together.

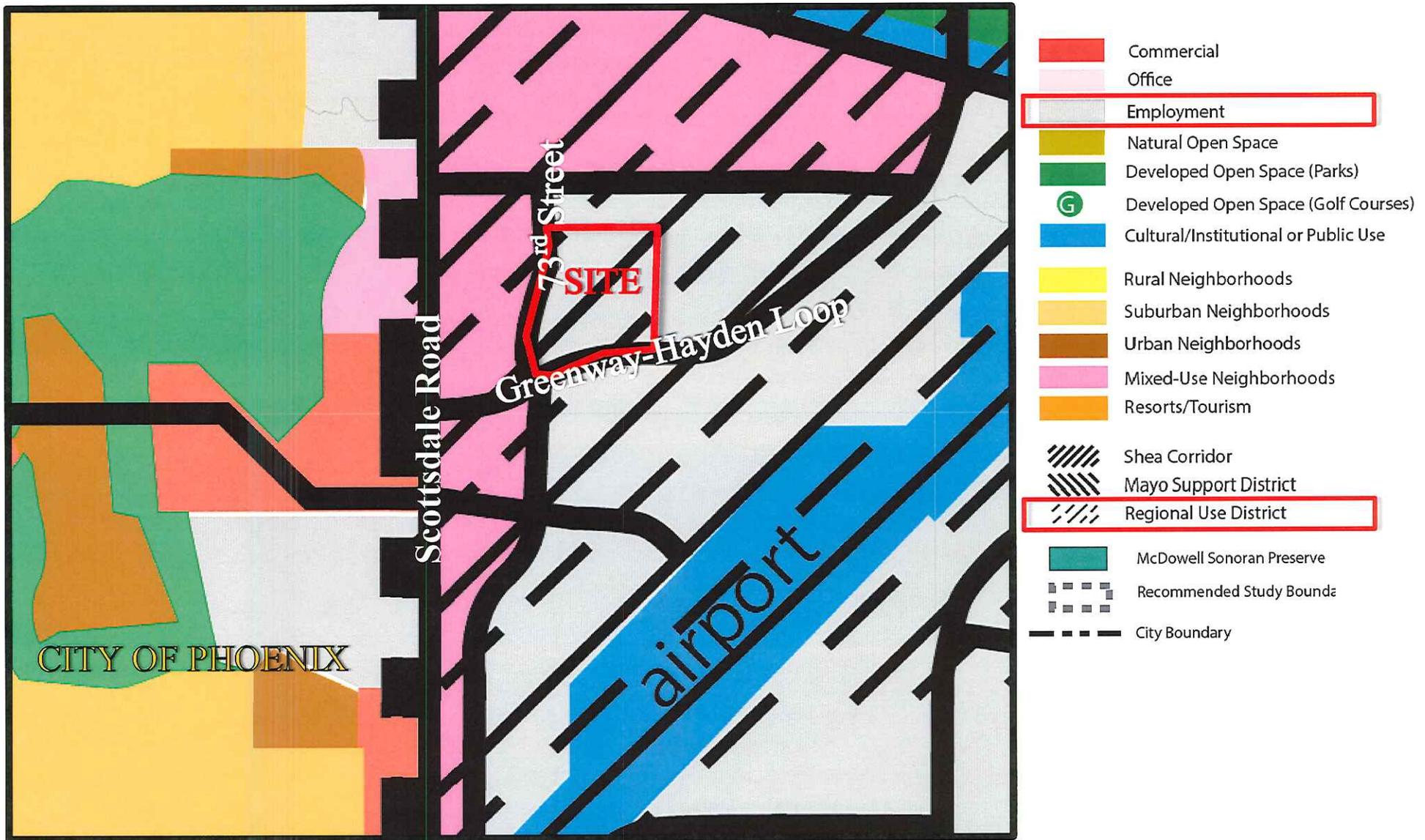
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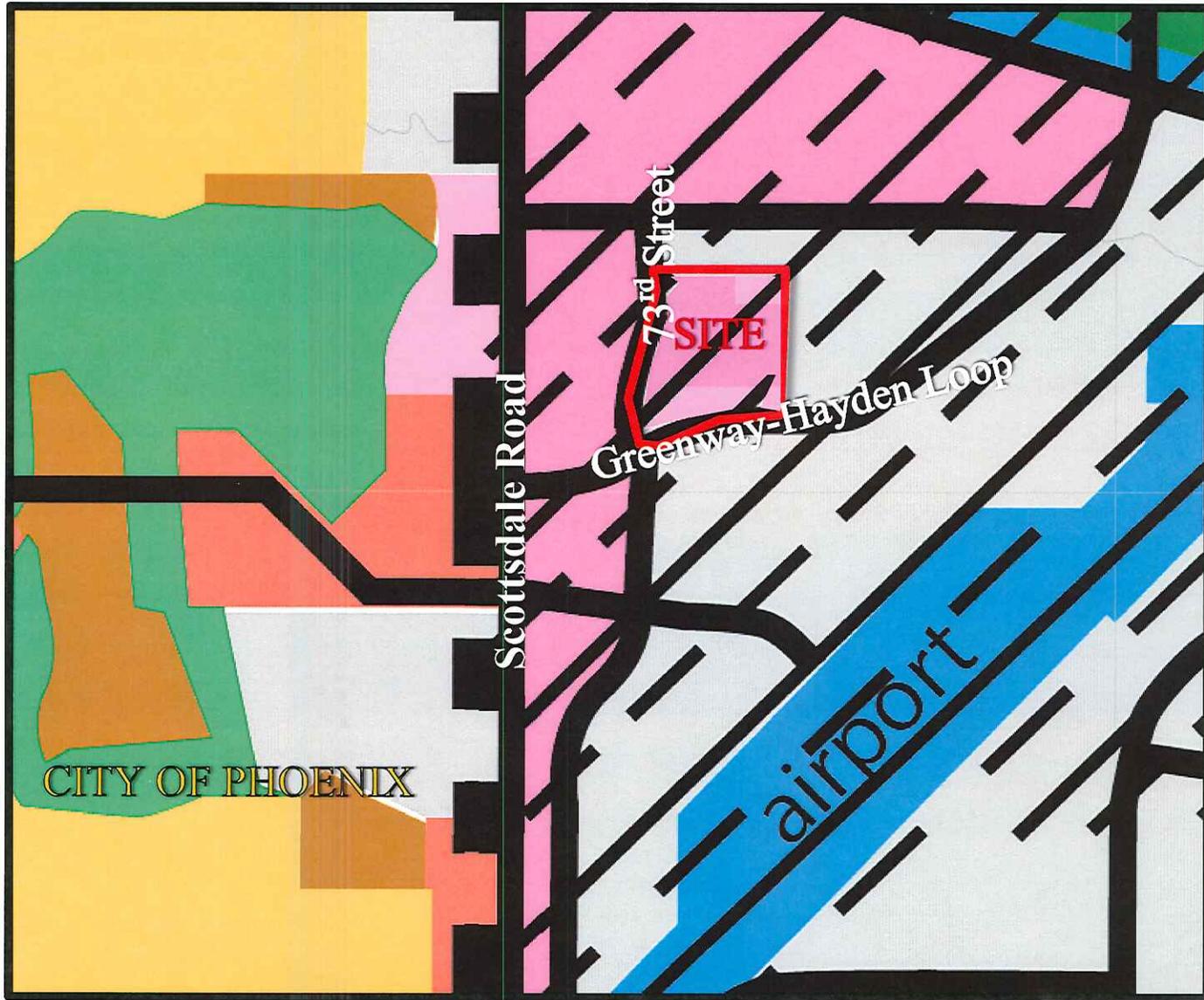
Accelerating success.

# Exhibit 13

## 2001 General Plan Land Use Existing & Proposed



2001 General Plan Existing Land Use



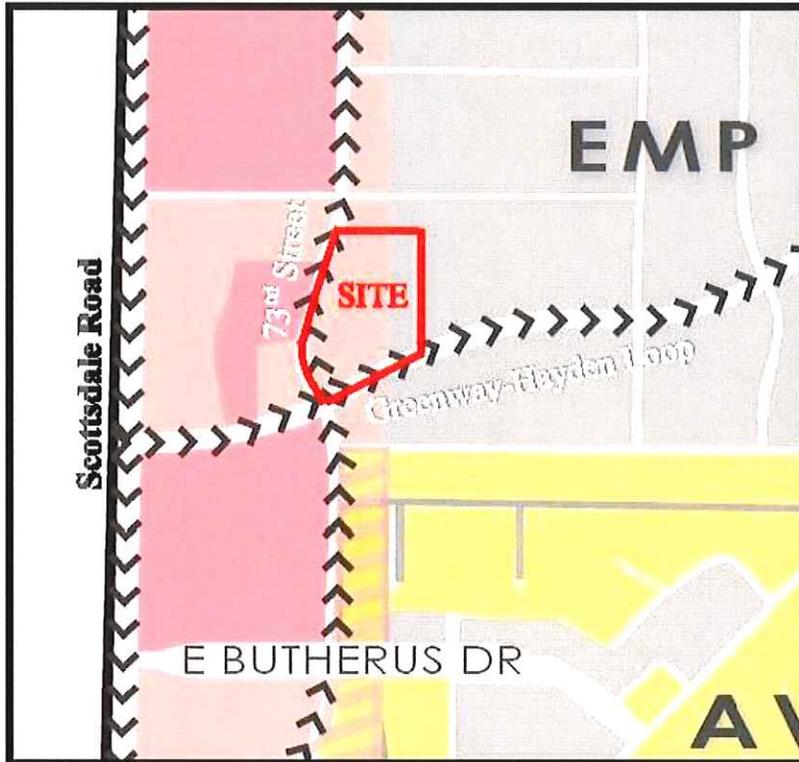
- Commercial
- Office
- Employment
- Natural Open Space
- Developed Open Space (Parks)
- G Developed Open Space (Golf Courses)
- Cultural/Institutional or Public Use
- Rural Neighborhoods
- Suburban Neighborhoods
- Urban Neighborhoods
- Mixed-Use Neighborhoods
- Resorts/Tourism
- Shea Corridor
- Mayo Support District
- Regional Use District
- McDowell Sonoran Preserve
- Recommended Study Bounds
- City Boundary

2001 General Plan Proposed Land Use

# Exhibit 14

## Greater Airpark Character Area Plan (GACAP) Land Use – Existing & Proposed

Scottsdale Airpark Land Use – Existing

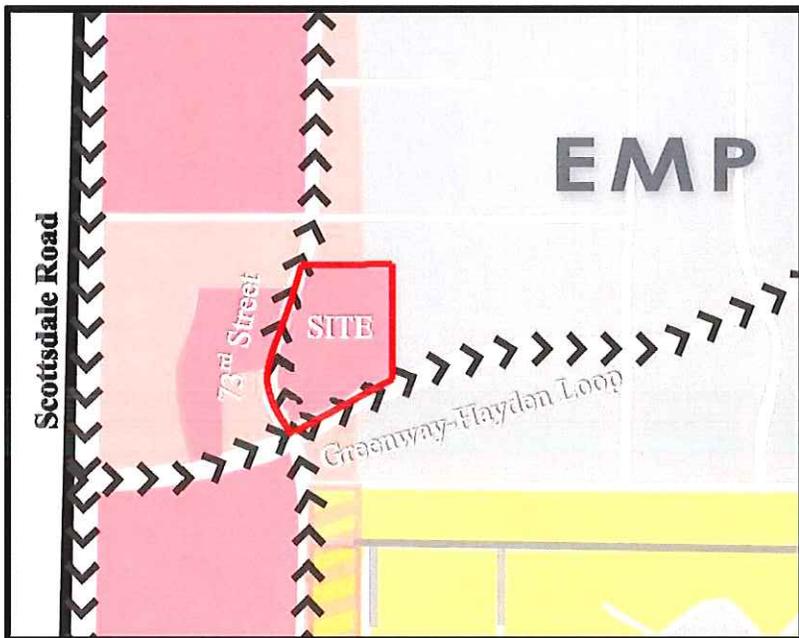


LEGEND

Land Use Plan:

- Airpark Mixed Use-Residential (AMU-R)
- Airpark Mixed Use (AMU)
- Regional Tourism (RT)
- Employment (EMP)
- Aviation (AV)
- Open Space (OS)
- AV or AMU
- Greater Airpark Boundary
- Signature Corridor
- Powerline Corridor
- Central Arizona Project

Scottsdale Airpark Land Use – Proposed



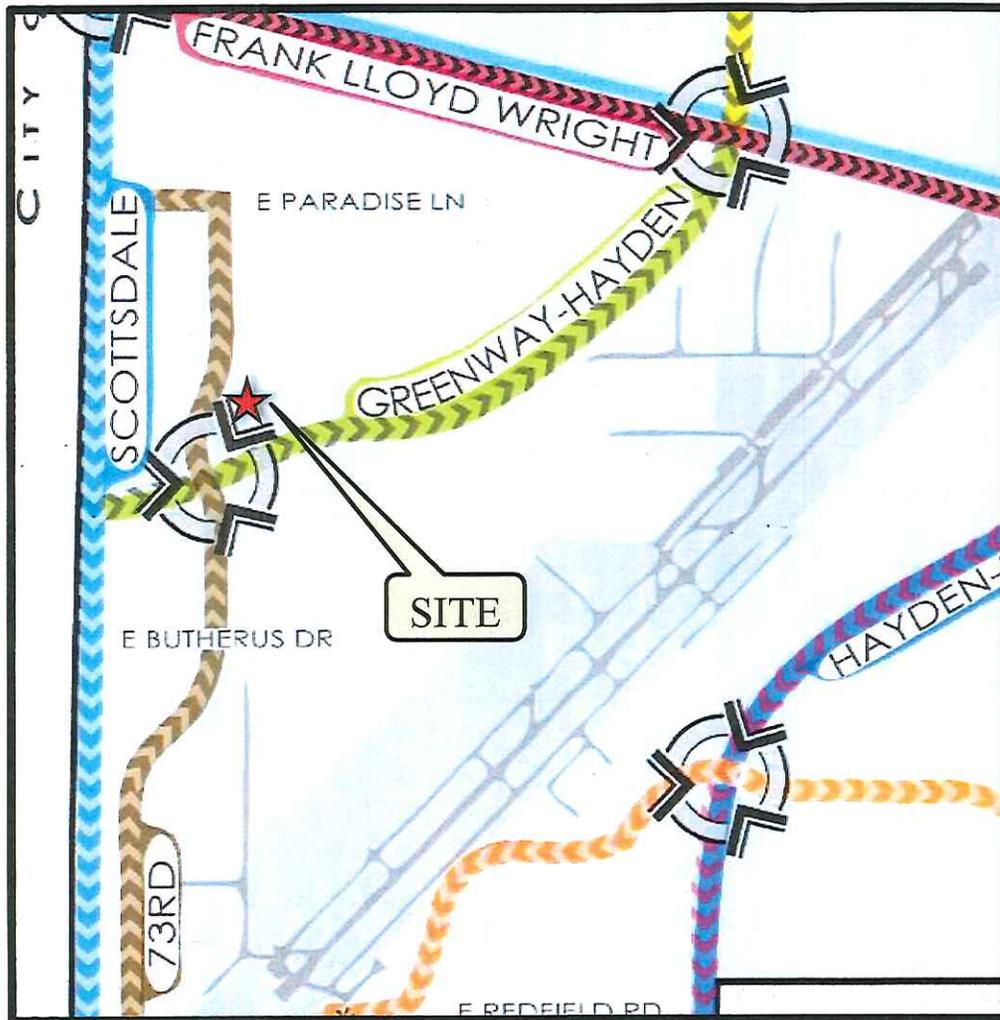
LEGEND

Land Use Plan:

- Airpark Mixed Use-Residential (AMU-R)
- Airpark Mixed Use (AMU)
- Regional Tourism (RT)
- Employment (EMP)
- Aviation (AV)
- Open Space (OS)
- AV or AMU
- Greater Airpark Boundary
- Signature Corridor
- Powerline Corridor
- Central Arizona Project

# Exhibit 15

## Airpark Signature Corridors



## LEGEND

### Signature Corridors

— Greater Airpark Boundary

≡≡≡ Powerline Corridor

— Central Arizona Project

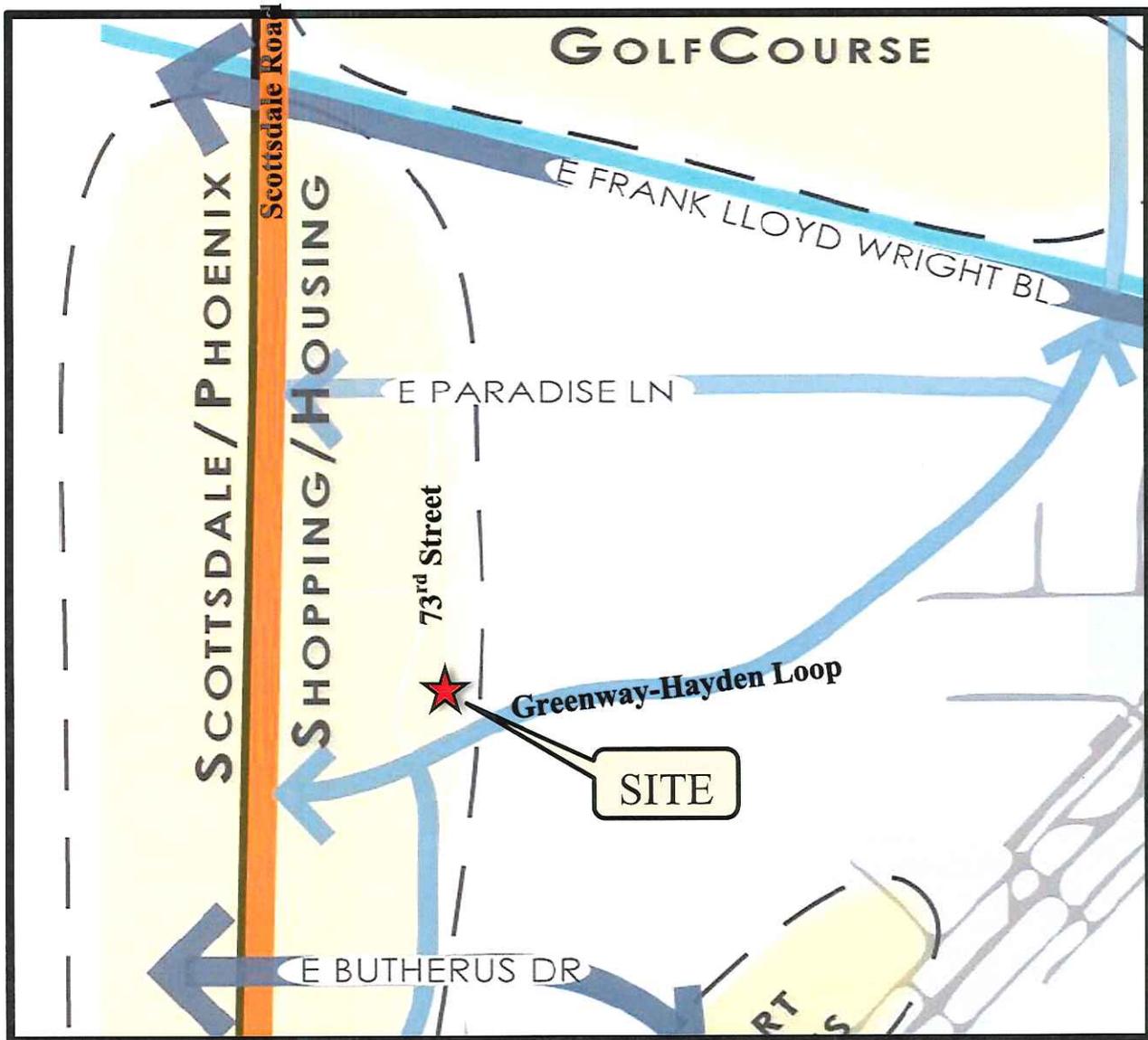
▨ Signature Corridors

⊕ Landmark Intersections

**Airpark Signature Corridors**

# Exhibit 16

## Airpark Future Transit Connections



## LEGEND

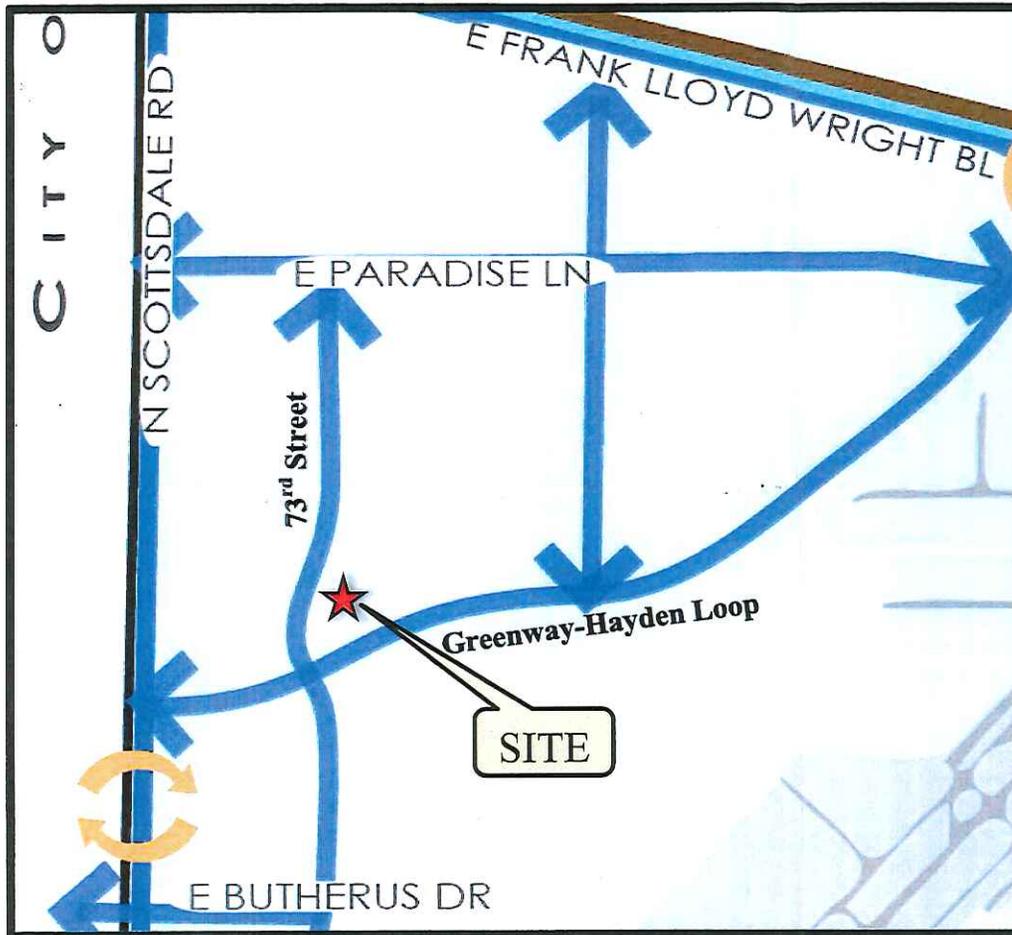
### Conceptual Transit Connections

-  Greater Airpark Boundary
-  Powerline Corridor
-  Central Arizona Project
-  Existing Transit Route
-  Potential Future Transit Route
-  Future High Capacity Transit Corridor
-  Activity Centers
-  Park and Ride

Airpark Future Transit

# Exhibit 17

## Airpark Pedestrian & Bicycle Connectivity



**Airpark Pedestrian & Bicycle Connections**

## LEGEND

Conceptual Greater Airpark  
Pedestrian/Bicycle Connectivity

— Greater Airpark Boundary

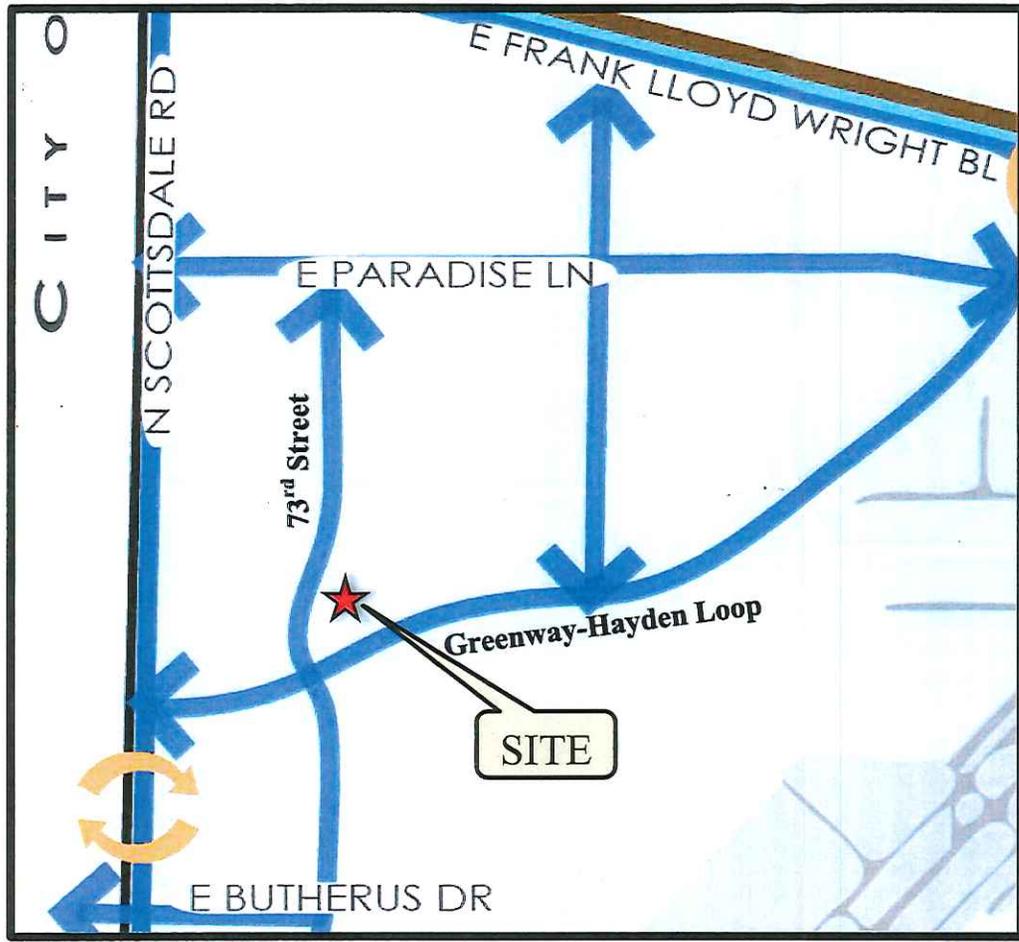
≡≡≡ Powerline Corridor

— Central Arizona Project  
Aqueduct

→ Pedestrian/ Bicycle Corridors

→ Trails

↻ Important Pedestrian Linkage



**Airpark Pedestrian & Bicycle Connections**

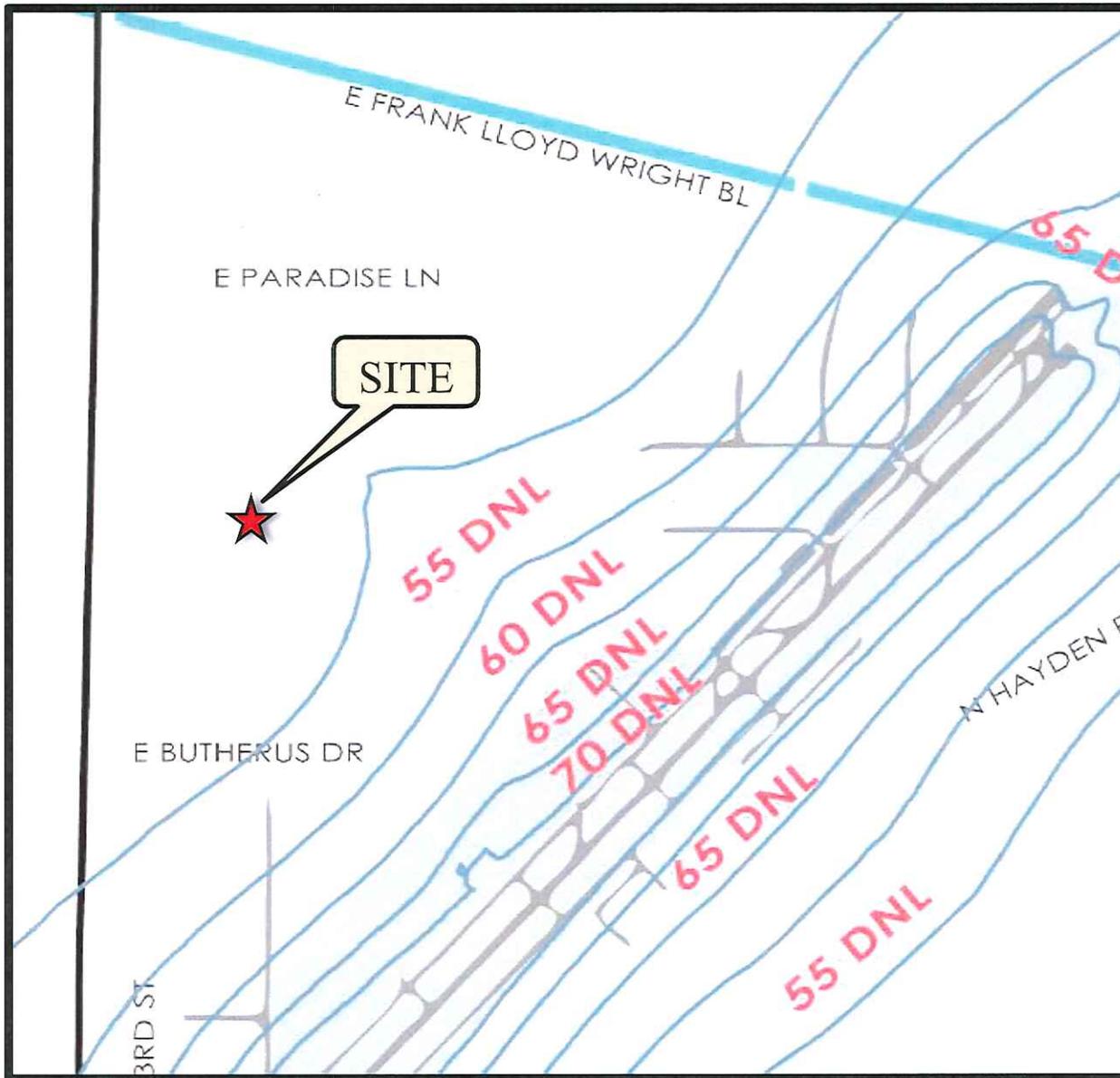
## LEGEND

Conceptual Greater Airpark  
Pedestrian/Bicycle Connectivity

-  Greater Airpark Boundary
-  Powerline Corridor
-  Central Arizona Project  
Aqueduct
-  Pedestrian/ Bicycle Corridors
-  Trails
-  Important Pedestrian Linkage

# Exhibit 18

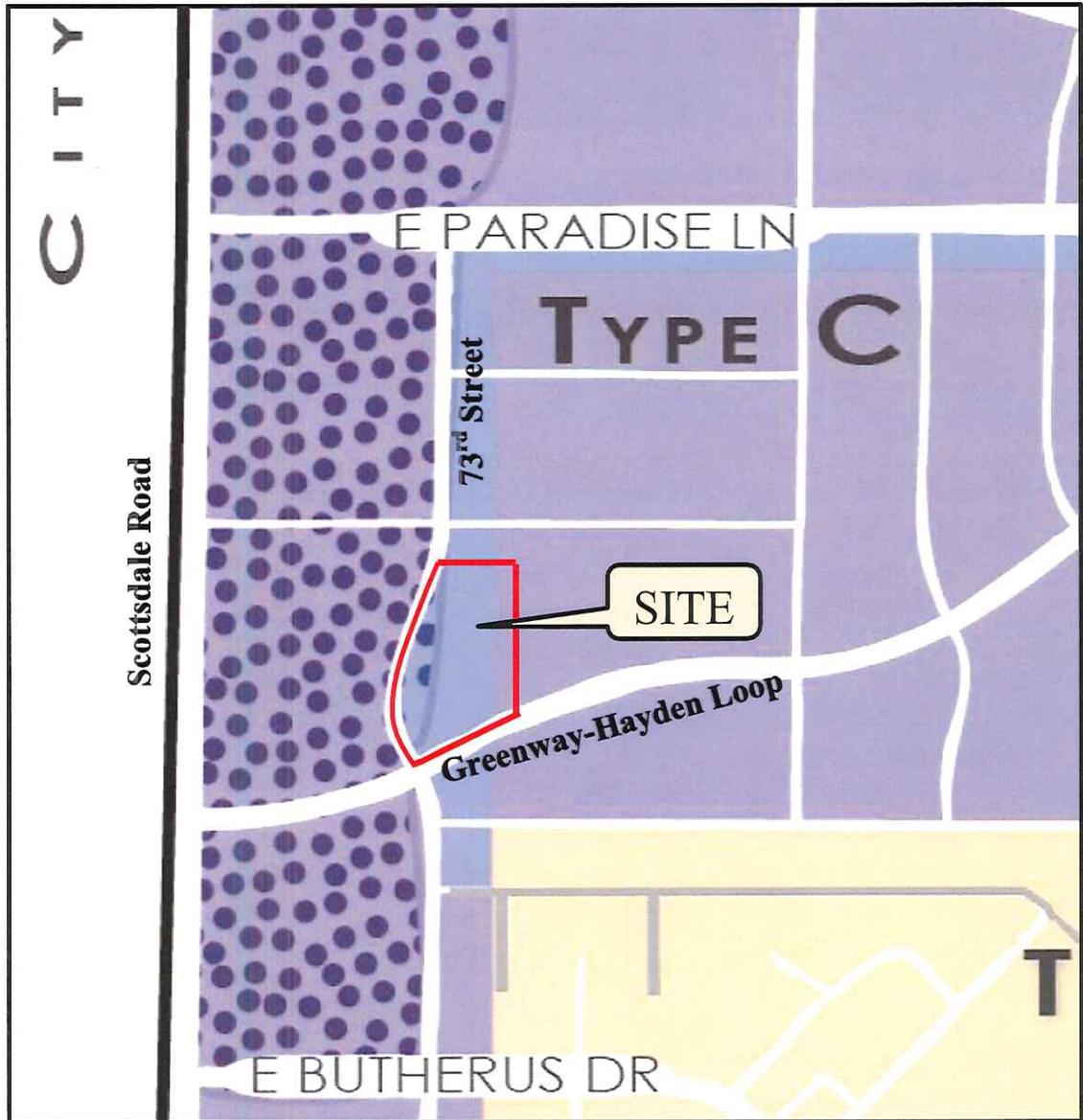
## Airpark Noise Contours



Scottsdale Airpark Noise Contours Exhibit

# Exhibit 19

## Airpark Development Types



## LEGEND

Conceptual Development Types:

 Type A- Medium Scale

 Type B- Large Mass

 Type C- Higher Scale

 Regional Core-  
Greatest Intensity

 Greater Airpark Boundary

 Sensitive Edge

 Powerline Corridor

 Central Arizona Project

 Open Space

Airpark Development Types Exhibit