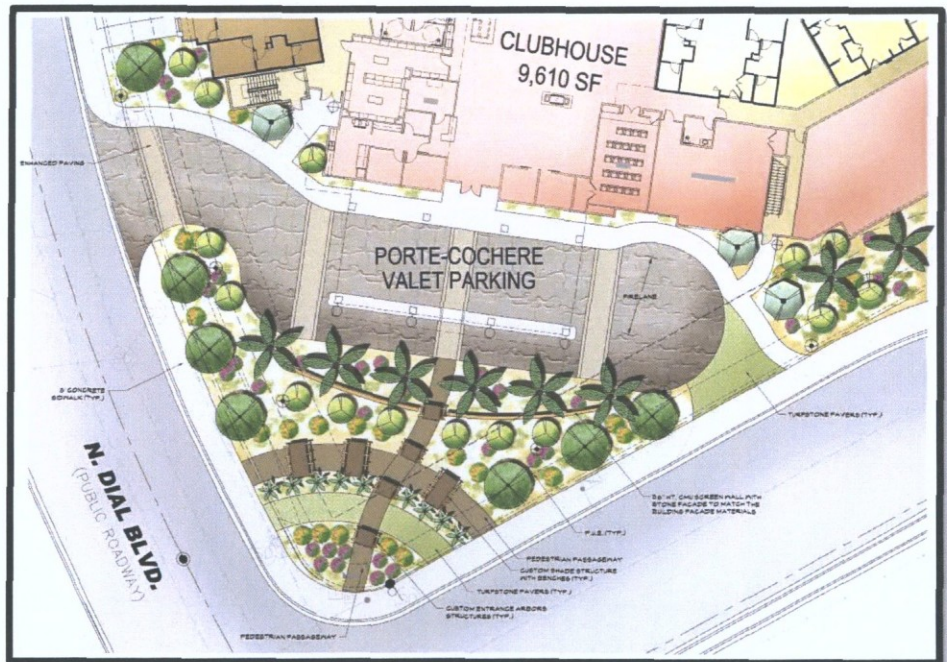


**Application
Narrative
Cash Transmittal
Pre-Application
Pre-App Narrative
Pre-App Cash Transmittal
Development Standards**

District at the Quarter

Planned Unit Development & Non-Major General Plan Amendment Narrative

Northeast corner of Greenway-Hayden Loop & 73rd Street



Cases: 3-GP-2016 & 8-ZN-2016

1st Submittal: March 30, 2016

2nd Submittal: June 14, 2016

3rd Submittal: August 5, 2016

4th Submittal: September 2, 2016

3-GP-2016 & 8-ZN-2016
9/28/16

Project Narrative/Development Plan

Non-Major General Plan Amendment

&

Rezoning

for

District at the Quarter

Case #'s:

3-GP-2016 & 8-ZN-2016

Location: 15501 North 73rd Street

Request for a Non-Major General Plan Amendment from Airpark Mixed Use (AMU) and a small portion of Employment (EMP) to Airpark Mixed Use- Residential (AMU-R)

- and -

Rezoning from Industrial Park (I-1) to Planned Unit Development District (PUD)

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TABLE OF CONTENTS

I.	Purpose of Request	4
II.	Kaplan Management Company	4
III.	District at the Quarter (The Proposal)	5
	a) Existing Conditions & Context	6
	b) Site Plan	6
	c) Retail/Commercial (Non-Residential) Component.....	7
	d) Building Architecture	8
	e) Landscape Theme	8
IV.	The Development Plan (DP)	8
	Average Building Setbacks Deviation	11
V.	Non-Major General Plan Determination.....	16
VI.	2001 General Plan/ Greater Airpark Character Area Plan	19
	2001 General Plan.....	19
	Greater Airpark Character Area Plan	28
	a) Land Use.....	35
	b) Neighborhood & Housing	39
	c) Community Mobility	41
	d) Economic Vitality.....	42
	e) Environmental Planning.....	43
	f) Character & Design.....	44
VII.	PUD Criteria	47
VIII.	Conclusion	49
Exhibits		50
	1) Dimensioned Plan Boundaries	
	2) Aerial	
	3) Context Aerial	
	4) Site Plan	
	5) Architectural Plan Set	
	• Elevations with Colors/Materials	
	• Floor Plans/Roof Plans (Floors 1-4, Building 1)	
	• Floor Plans/Roof Plans (Floors 1-4, Building 2)	
	• Parking Garages Plans (Building 1 & 2)	
	6) Landscape Plan Set	
	• Overall Landscape Master Plan	
	• Pedestrian/Vehicular Circulation Plan	
	• Open Space Plan	
	• Corner Landmark Intersection Enlargement	
	• Courtyard Enlargements (Courtyards 1-5)	

Exhibits Continued...

- 7) Phasing Plan**
- 8) Building Cross Sections, Height Diagrams, and Height Exception Area**
- 9) Average Building Setbacks**
- 10) Amended Standards Legislative Draft**
- 11) Right-of-Way**
- 12) Arizona Republic Article and Colliers International Report**
- 13) 2001 General Plan Land Use Existing & Proposed**
- 14) Greater Airpark Character Area Plan (GACAP) Land Use – Existing & Proposed**
- 15) Airpark Signature Corridors**
- 16) Airpark Future Transit Connections**
- 17) Airpark Pedestrian & Bicycle Connectivity**
- 18) Airpark Noise Contours**
- 19) Airpark Development Types**

I. **Purpose of Request:**

This request is for a Non-Major General Plan Amendment (GPA) from Airpark Mixed Use (AMU) and a small portion of Employment (EMP) to Airpark Mixed Use-Residential (AMU-R) and a rezoning from Industrial Park (I-1) to Planned Unit Development (PUD) zoning district on 10.29+/- gross acre and 8.83+/- net acre site located at 15501 North 73rd Street (a.k.a. Dial Boulevard) (the "Site"). The applicant intends to create a small mixed-use luxury residential multi-family community of approximately 622 units. (See below and also: *Dimensioned Plan Boundaries Exhibit # 1 & Aerial Exhibit # 2*)

AERIAL



II. **Kaplan Management Company**

The applicant, Kaplan Management Company ("Kaplan") was formed in 1978 and is based out of Houston, Texas. Kaplan is the diversified operator, owner, and developer of high quality multi-family properties in major metropolitan areas across the United States. Kaplan is active in emerging high growth submarkets, redeveloping existing multi-family housing, and repurposing commercial developments into residential communities, and holds 36 years of successful

management experience in the dynamic and evolving multi-family industry. Kaplan would be honored to bring their experience and success to this particular location in Scottsdale.

III. District at the Quarter (The Proposal)

The District at the Quarter residential community will provide an exciting mixed-use project featuring upscale residences including ground level live/work units and retail/commercial (i.e. non-residential) components that blends and transitions with the retail/commercial businesses strategically located along with employment entities within the Scottsdale Airpark, which is the second largest employment center in the state of Arizona. The proposed development would further many of the goals set forth in the Greater Airpark Character Area Plan ("GACAP") by creating a dynamic walkable community based on live, work, and play principles.

The Site is a transitional piece that defines the boundary between employment and retail/commercial uses within Scottsdale Airpark. This important and strategic location is bounded by two (2) Signature Corridors which also forms a Landmark Intersection as defined within the GACAP. As such, this Site will provide an appropriate level of mixed-use and residential live, work, and play community that will transition, activate and define the shopping/retail activities to the west/southwest, employment sector to the east, and formalize/enhance the Signature Corridors (i.e. Greenway-Hayden Loop and 73rd Street). The transition and addition of this mixed-use proposal at this location will help to continue enhancing the desirability of this location and the surrounding context. The retail/commercial (i.e. non-residential) and residential mix proposed will provide an ideal fit for this location and beyond. The natural progression of uses and development along Scottsdale Road, 73rd Street, and throughout the Airpark will continue draw people and activity to this location from surrounding areas, which in turn will build upon the placemaking and character that is envisioned at this Landmark Intersection.

The Airpark as a whole creates a unique opportunity to blend interrelated land uses and promote the live, work, and play concept, which is memorialized numerous times within the GACAP. Additionally, the nearby retail and surrounding employment core provides regional appeal for future owners of the District at the Quarter. The ease of accessibility, proximity to businesses, shopping, entertainment, recreation, and airport makes this development a compliment to its surroundings.

CONTEXT AERIAL



a) Existing Conditions & Context

The Site constitutes 10.29 gross acres and bordered by Greenway-Hayden Loop on the west and 73rd Street (a.k.a. Dial Boulevard) to the south. Existing businesses (I-1) border the Site to the north, east, and south. The west side is bordered by existing commercial businesses (C-3) and a recently completed multi-family development (PUD). (See above and also: *Context Aerial # 3*)

The redevelopment parcels contain an office building currently occupied by “International Cruise & Excursions” (“ICE”), a travel and vacation service company. The Site contains covered and uncovered parking surrounding the main building structure. The streets, sidewalks, and utilities surrounding the Site have all previously been built, as such this redevelopment project can be “plugged in” to this location with minimal disturbances to the area.

b) Site Plan

The design for this Site features an enhanced Landmark Intersection including an enhanced public open space, pedestrian passageway, and rest area inviting public activity and interaction provided by shaded rest areas, enhanced paving, a variety of meandering pathways, and landscaping. The Landmark Intersection of the Site is activated with ground level uses anchored by an approximately 7,855 sq. ft. clubhouse that provides services and amenities such as:

cochere/valet on-site drop-off dry cleaning service, business center, restaurant, office space, fitness center, barista station, etc. The Signature Corridor of Greenway-Hayden Loop will also feature an approximately 5,354 sq. ft. fitness center located on the ground level facing Greenway-Hayden Loop along with seven (7) live/work units located on both Greenway-Hayden Loop and 73rd Street.

A main vehicular access drive bisects the Site and allows for two (2) vehicular entry connections located at the mid-block of 73rd Street and Greenway-Hayden Loop. Additionally, this vehicular access drive will serve as access to the internal parking garages and provide for fire lane emergency access. The secondary fire lane/enhanced setback along the perimeter of the Site will be gated and serve dual purposes (i.e. building setback, vehicular pavers, landscaping, turfstone, dog walk/wash areas, walking area, and fire safety access). The site design consists of two (2) (i.e. 2 phases) 4-story luxury residential buildings that enclose a total of five (5) interior courtyards, featuring various amenities, open space, and landscaping. These outdoor activity areas provide for both active and passive recreation, relaxation, and an abundance of open space for the future residents. The main building structures are navigated through a series of pedestrian passages that lead throughout the Site and ultimately to the outer pedestrian sidewalk network and the surrounding area. The north and east boundaries of the Site are nicely buffered by a 27'-40' building setback, vehicular pavers, landscaping, turfstone, dog walk/wash areas, walking area, and fire safety access from the proposed units. (See *Site Plan Exhibit #4*)

c) Retail/Commercial (Non-Residential) Component

The retail/commercial (i.e. non-residential) component of this mixed-use development is strategically located at the Landmark Intersection of the Site in the form of a highly amenitized clubhouse (7,855 sq. ft.), restaurant (7,035 sq. ft.), office space (1,781 sq. ft.), and fitness center (5,354 sq. ft.) that will continue along Greenway-Hayden Loop with seven (7) live/work units proposed as well along both Greenway-Hayden Loop and 73rd Street. This project provides design and uses that cater to the pedestrian at the ground level particularly at the Landmark Intersection, which supports pedestrian interest and walking as well as enhancing the streetscape. These uses will be also open to the public that will generate additional interest and vibrancy.

The amount of retail/commercial (i.e. non-residential) proposed is adequate and supported within the context of the existing overall retail/commercial in the surrounding area. There is already a sufficient amount of retail/commercial in this area, and proposed in the future, that an increase in residential density is important towards balancing and supporting these uses as well as live, work, and play lifestyle concept. It is worth noting that there is currently 116,000 sq. ft. of retail and 250,000 sq. ft. of office space within the Scottsdale Airpark North; which this development would be a part of. Within walking distance of the Site, there is: 437,000 sq. ft. at Kierland Commons, 400,000 sq. ft. at Scottsdale Quarter, 700,000 at Scottsdale Promenade, and 100,000 at Zocallo. Thus, if one considers a modest 5% vacancy rate, which is typical, then that would equate to nearly 100,000 sq. ft. of vacant retail space, which at this time includes neighboring Crescent's ground floor space being vacant. To that end, the amount of retail/commercial proposed with this development (i.e. 22,025 sq. ft.), we believe, is more than adequate to properly serve the future

residents of this development and to provide a proper balance with the existing and future population, employment, and retail base within this area.

d) Building Architecture

The building architecture features varied massing, varying roof parapets, architectural features, stoops, materials and façade detailing found in many contemporary luxury multi-family developments found in Scottsdale and the Southwest region. The building massing includes a series of towers and recessed facades and patios. The massing and detailing emphasize the promotion of pedestrian activity including a walkable street level building design lined with lighting, trees, and shade structures such as: porte-cochere, awnings, and roof overhangs. The color scheme is a blend of shades of brown, white, and tan with some complementary accent colors found within the Sonoran Desert color palette and consistent and appropriate with the surrounding building context. (See *Architectural Plan Set Exhibit #5*)

Maximum building height is 48' with all rooftop mechanical equipment and screening, stairwell bulkheads, as well as rooftop deck enclosure walls and fall protection railing that exceed that building height, limited to under 30% total of the roof area for each building. (See *Building Cross Sections, Height Diagrams, and Height Exception Area Exhibit #8*)

e) Landscape Theme

The project's landscape theme will reduce overall water intake include by utilizing native trees, shrubs, accent plants, groundcover, and minimal turf. The two (2) Signature Corridors (i.e. Greenway-Hayden Loop and 73rd Street) abutting the Site has sidewalks lined with trees and shrubs providing a pleasant/cool environment to walk. The main vehicular access drive and open space areas will also contain a variety of native shrubs and landscaping providing for a friendly, enjoyable, useable, and shaded environment for residents to traverse and use the property. As such, the plan includes a total of five (5) internal courtyards which all feature a variety of landscaping and amenities such as a swimming pool, fountains, fire pit, putting green, televisions, barbeques, and shaded seating and dining areas which are all connected by a system of winding walkways. (See *Landscape Plan Set Exhibit # 6*)

IV. The Development Plan (DP)

Pursuant to the City's Zoning Ordinance for PUD rezoning requests the following four (4) items to be addressed within the required Development Plan ("DP"). They are:

- (1) The design contained in the DP is compatible with development in the area.

The Site sits on the northeast corner of Green Hayden Loop and 73rd Street. The proposal is for a planned, medium density, mixed-use apartment project with an appropriate/sustainable level of supporting retail/commercial (non-residential) situated at this Landmark Intersection. The clubhouse and adjoining fitness center as well the option of live/work units will also help to

continue to build upon the live, work, and play concept in this area by providing for amenities and recreation, business needs, etc. at the pedestrian environment subsequently anchored by an enhanced Landmark Intersection with seating, special paving, and landscaping. The enhanced pedestrian Landmark Intersection and the services proposed at this corner (i.e. clubhouse with porte-cochere/valet restaurant, office space, fitness center, etc.) will further promote the lively street scene expected at this Landmark Intersection and along the two (2) Signature Corridors (i.e. Greenway-Hayden and 73rd Street), which will continue/enhance the activity in this area and create a synergetic node.

The proposed project is compatible with and contributes to its surrounding uses, which has evolved towards a more active, dynamic, and vibrant corner. The proposed development by Kaplan, as part of this Landmark Intersection, and together with the existing mixed-use corners adjacent, creates the desired effect envisioned for this Landmark Intersection and along these two (2) Signature Corridors, which are: transitioning development away from Scottsdale Road, attracting new development into the airpark, connecting pedestrians without the use of cars, internalizing/structured parking, creating an identity/gateway, and increasing the critical mass of people to support the businesses in the area on a daily/nightly basis. There are two (2) adjacent recent mixed-use developments that help to complete two (2) of the corners of this Landmark Intersection which provide for residential living within a mixed-use environment while being within a different context/lifestyle of living. These developments are concentrated at the corners and help to complete the Landmark Intersection. Crescent Scottsdale Quarter is a 5-story apartment complex over supporting retail uses within the successful Scottsdale Quarter. The other is immediately to the west across 73rd Street called Liv North Scottsdale, which is approximately the same height of 48'. The proposed development by Kaplan and these two (2) other projects are responding to the demand for housing to support the surrounding retail/commercial/employment uses. The proposed development plan of approximately 60.4 du/ac would be of comparable intensity by providing a density option roughly within the middle of Liv North and Crescent (i.e. if we focus only on the ground area devoted to the residential use). With that being said, the District at the Quarter complies in overall height, setbacks, high quality architecture, and pedestrian/vehicular connectivity to seamlessly blend within the area, but also a unique residential development option.

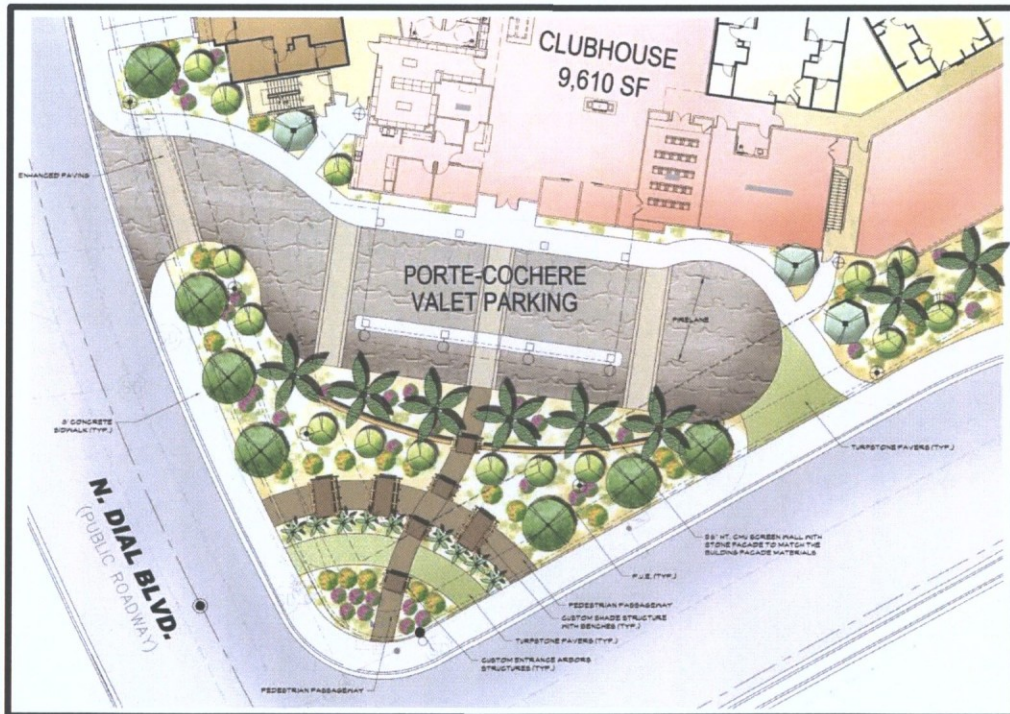
The project construction will be built in two (2) phases. (See *Phasing Plan Exhibit # 7*) The anticipated timing of construction for Phase One is expected to begin in 2017 with Phase Two anticipated to begin approximately 18-22 months later or approximately by early 2019. Phase One of construction will include the majority of non-residential floor area uses and provide for the enhanced public pedestrian area associated at the Landmark Intersection. (See next page and also: *Corner Landmark Intersection Enlargement within the Landscape Plan Set Exhibit # 6*) Phase Two will complement the first and continue the dynamic vision of the project, as well as supporting the development standards of the PUD zoning. With the public amenities and non-residential floor area uses being completed in the Phase One, it ensures that the intent of the PUD, General Plan, and Character Area Plan will be met and provide a substantial public benefit along with implementing the components of a lively mixed-use area and identifiable node.

There are five (5) total usable open space areas integrated into the DP that continue the theme of the surrounding area, which include the main swimming pool. The swimming pool is the largest open space area being approximately 12,000 sq. ft. and the smallest internal usable open space being approximately 2,600 sq. ft. It should also be noted that a dog walk/wash areas will be provided abutting the perimeter of the property (i.e. Phase Two). Finally, the entire Site is connected by winding walkways which link up to public sidewalks, bike paths, and ultimately to the surrounding area.

- (2) The DP is environmentally responsive, incorporates green building principles, contributes to the city's design guidelines and design objectives, and that any deviations from the design guidelines must be justified by compensating benefits of the DP.

The proposed development is environmentally responsive and also provides exceptional public benefits in many ways by including an enhanced common open space and public pedestrian area at the Landmark Intersection (see below) featuring seated and shaded seating, varied pathway connections, landscaping, pedestrian scaled design, and pedestrian amenities encouraged in the Scottsdale Design Guidelines.

LANDMARK INTERSECTION: PEDESTRIAN REST AREA AND PASSAGEWAY



The five (5) usable common open space areas are strategically located throughout the Site to provide areas for the residents to recreate and contemplate. The open space at the Landmark Intersection includes approximately 4,000 sq. ft. of public usable common. The Site also has a total of 46,114 sq. ft. of total common open space area (excluding street frontage area or

parking lot landscaping) or 10.3% and 96,402 sq. ft. total landscape space provided (including all open space, street frontage, and parking landscape areas) or 21.5% for the Site. It should be noted that both phases of development will exceed the required 10% open space. Again, these open space areas provide areas of contemplation, recreation (passive/active), cooling, and visual interest both on-site and off-site. The landscape palette is in keeping with a Sonoran Desert theme and water conservation measures by strategically locating low water use trees, shrubs, groundcovers, etc. to create a lush appearance that cools the environment both internally and externally (i.e. along the street frontages). Turf is strategically located on-site within a few of the areas designated for internal/activities in order to optimize comfort and use. Finally, the entire Site is interconnected by walkways which link up to public sidewalks and bike paths encouraging multimodal options.

The architectural character is a contemporary design which will utilize the most current building materials to provide for an energy efficient development. The building masses are broken up into two (2) main buildings. The south building which faces Greenway-Hayden Loop is varied in its setback an average of 38' while maintaining the required minimum 34' setback for residential and 28' setback for retail/commercial. Due to 73rd Street curving the west façade of this building bows and creates challenges with building design and as such the average setback is 38' while maintaining the required minimum 25' setback. The architectural elevations are broken up vertically by varying the roof lines, alternating between flat parapets and flat roof overhangs, awnings, porte-cochere, fitness center bump out, and live/work units. These architectural treatments help vary the roof lines vertically. Within the flat parapet areas of the building the walls of the building stepback horizontally creating wide recessed areas that provide space for awnings, patios, and visual massing reliefs. This horizontal relief occurs approximately every 100 lineal feet with massing changes, the bump out of the fitness center, live/work units, porte-cochere, and the natural curvature of the streets. The proposed design provides a strong base with a material change from stucco, limestone, glass, steel awnings, ground floor fitness center, live/work units and stoops located in strategic locations to "ground" the design. The midsection and top utilized the same materials with the top units accentuated by patio areas as well as bulkhead areas protruding towards the top of the buildings to provide access to the roof decks for those units along with alternating flat parapets and roof overhangs. The overarching intent is to create a compatible development design for the area while also being a unique, high quality, and visually appealing for one to want to live and play while being in close proximity to work, entertainment, restaurants, etc. thus reducing traffic and pollution while also maintaining long term economic success.

➤ **Average Building Setbacks Deviation**

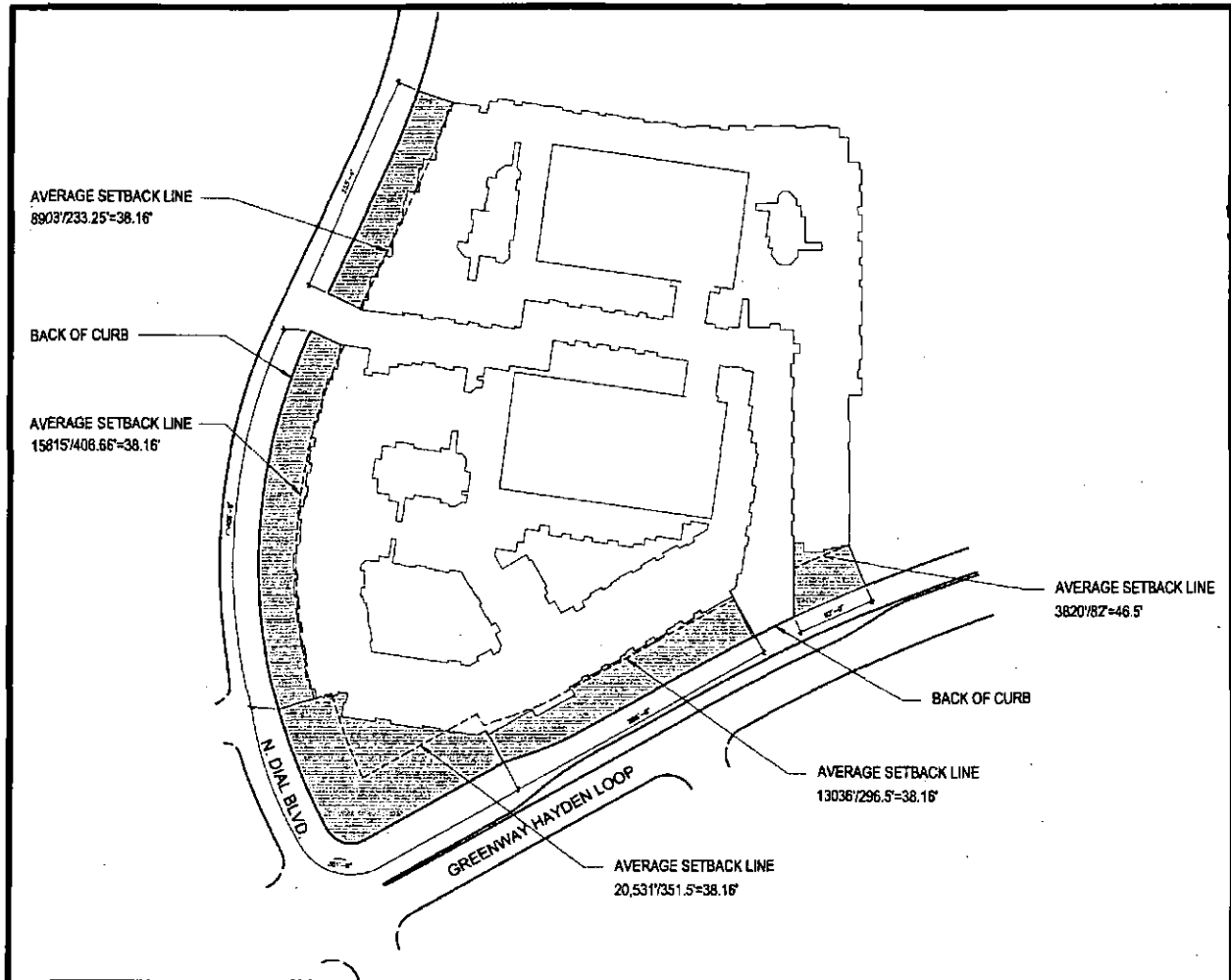
In order to provide the contemporary/clean lined architectural character as well as to design around the two (2) curved streets an average building setback deviation is being requested for both Greenway-Hayden and 73rd Street. The Site's constraints and the ability to push the buildings closer to the street frontages is hinder, but where possible every effort has been made to push the buildings towards these streets or provided for alternative means of "connecting" the development to the streets, such as: stoops, walkways, and live/work units in

order to activate the street frontages as much as possible. With that being said the south building which faces Greenway-Hayden Loop is varied in its setback and provides for the ground floor fitness center and live/work units with an average of 38' (i.e. City of Scottsdale Zoning Ordinance requires 40' average for residential and 32' average for retail/commercial) while maintaining the required minimum 34' setback for residential and 28' setback for retail/commercial. With regards to 73rd Street and its significant curve the building massing bows and creates challenges as to how close the building can get to the street. As such, the building/site design creates an average setback of 38' (i.e. City of Scottsdale Zoning Ordinance requires 30') while maintaining the required minimum 25' setback. However, stoops, walkways, and live/work units are being provided to help connect the development to the street frontage. In addition, the average setbacks deviations proposed will allow for enhanced landscaping as well as being compatible with the abutting developments to the east, west and south. (See next page and also: *Average Building Setbacks Exhibit # 9*)

In addition, the following five (5) design reasons that contribute to supporting the average building setback exception proposed along Greenway-Hayden Loop and 73rd Street. (See *Amended Standards Legislative Draft Exhibit #10*)

1. The architecture provides relief and recesses that creates impression of "movement" along the street frontages while not being directly abutting the street frontages.
2. Incorporation of ground plain interest/activity for the pedestrian such as: The corner enhancement and clubhouse, fitness center, porte-cochere, and main/secondary entries along the street frontages as well as the addition of stoops, walkways, and live/work units help connect the development to the streets.
3. Pop-outs such as: awnings, balconies, fitness center, etc. provide visual relief and interest along the street frontages and help push the buildings forward.
4. The curvature of the streets forces the buildings back providing more landscaping and areas of respite creating a more pleasant/shaded walkable environment, which is not out of character with the abutting developments to the east, west, and south.
5. Projections are used to emphasize important architectural elements and discoveries for the pedestrian and street frontages, such as: entrances, bays, stoops, porte-cochere, fitness center building projection, and verandas that draw people in and around the property.

AVERAGE BUILDING SETBACKS



- (3) The DP will not significantly increase solar shading of adjacent land in comparison with a development that could be developed under the existing zoning district.

Comparable heights, uses and generous setbacks created by the abutting streets and internal circular access drive aisle do not significantly increase solar shading of adjacent land in comparison with a development that could be developed under the existing I-1 zoning district. Moreover, the I-1 zoning district allows up to 52' in building height, which will be comparable to the proposed development.

- (4) The DP promotes connectivity between adjacent and abutting parcels, and provides open spaces that are visible at the public right-of-way and useful to the development.

The proposed development provides for an enhanced Landmark Intersection featuring usable and common open space, with public shaded seating and landscaping features that exemplify

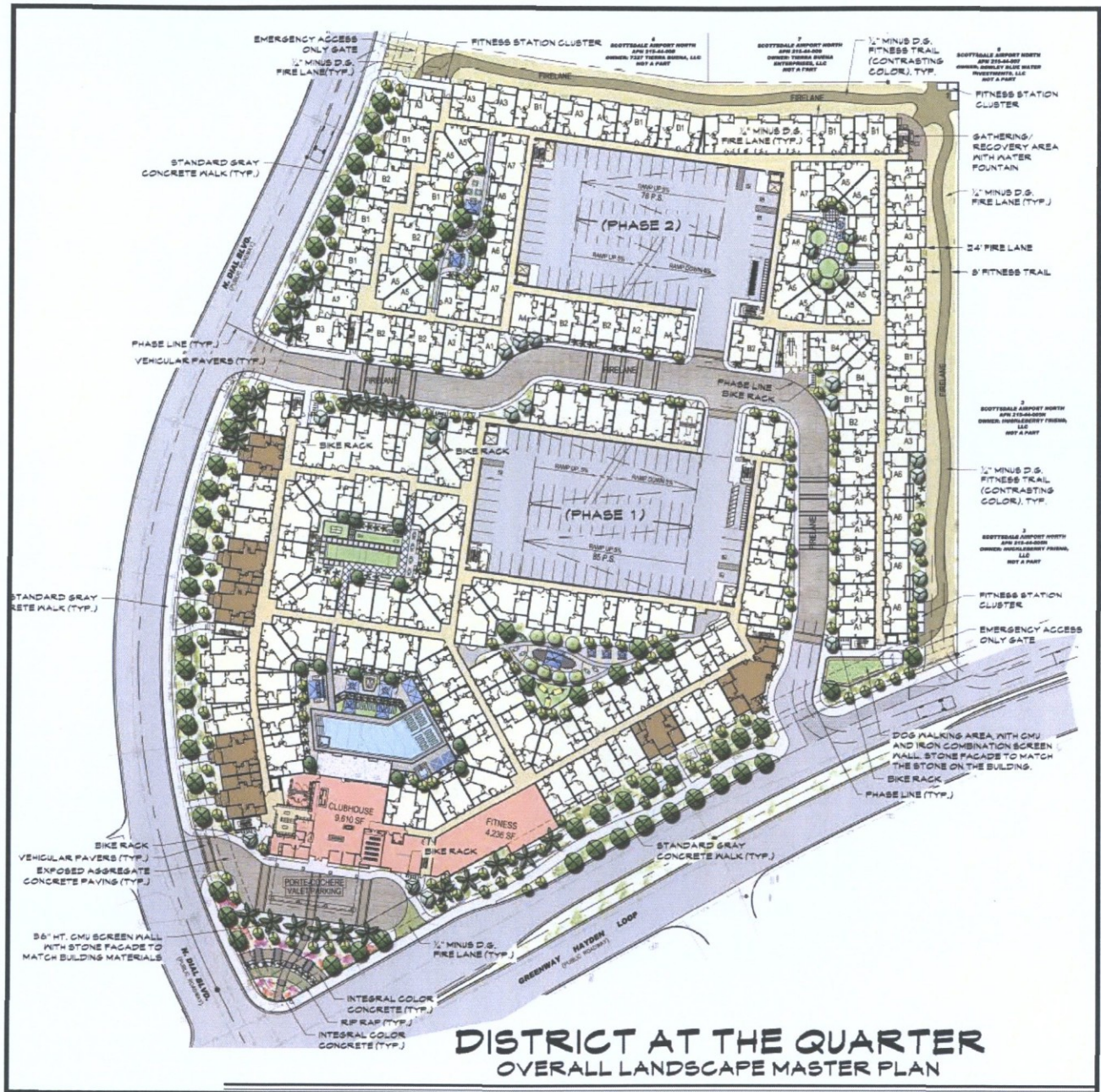
the promotion of connectivity between abutting parcels. The enhanced open space street frontage landscaping along both major roadways and corner bordering this Site creates an inviting path and enhanced enjoyable streetscape for all modes of transportation. Furthermore, with the addition of individual unit roof decks along 73rd Street and Greenway-Hayden Loop will help enhance visibility and activity (i.e. “eyes on the street”) along these major pathways and nearby uses currently located along this Signature Corridor as well as in the future along 73rd Street as it becomes more multimodal and development occurs further north. Thus, these open space areas and enhanced connections provide areas of contemplation, recreation (passive/active), cooling, visual interest and connections to the surrounding area. (See below and also: *Pedestrian and Vehicular Circulation within the Landscape Plan Set Exhibit # 6*)

PEDESTRIAN & VEHICULAR CIRCULATION



The landscape palette is in keeping with a Sonoran Desert theme with strategically locating trees, shrubs, groundcovers, etc. to create a lush appearance that cools the environment both internally and externally (i.e. along the streets). The entire Site is connected by internal walkways which link up to public sidewalks and bike paths encouraging multimodal options. Vehicular access is centrally located to provide ease of access to 73rd Street and to circulate around the building to Greenway-Hayden Loop for resident access and fire access. (See below and also: Overall Master Landscape Plan within the Landscape Plan Set Exhibit # 6)

MASTER LANDSCAPE PLAN



V. Non-Major General Plan Determination

The requested GPA is in conformance with a Non-Major General Plan Amendment, based upon the criteria set forth in the General Plan. A detailed explanation of how this proposal is consistent with the 2001 General Plan and the Greater Airpark Character Area Plan is provided below.

1. *Change in Land Use Category*

A change in land use category on the land use plan that changes the land use character from one type to another as delineated in the land use category table (page 20 of the General Plan).

Response: The current 2001 General Plan designation is Employment with a Regional Use District Overlay, and a requested GPA land use change to the Mixed Use category with a Regional Use District Overlay. Both of these land use categories are located within Group E, and therefore do not constitute a Major GPA.

2a. *Area of Criteria Change*

A change in the land use designation that includes the following gross acreages:

*Planning Zones A1, A2, B: 10 acres or more.

*Planning Zones C1, C2, C3, D, E1, and E3: 15 acres or more.

Response: The designated Planning Zone for the Site is Zone B.

Excerpt from the 2001 General Plan:

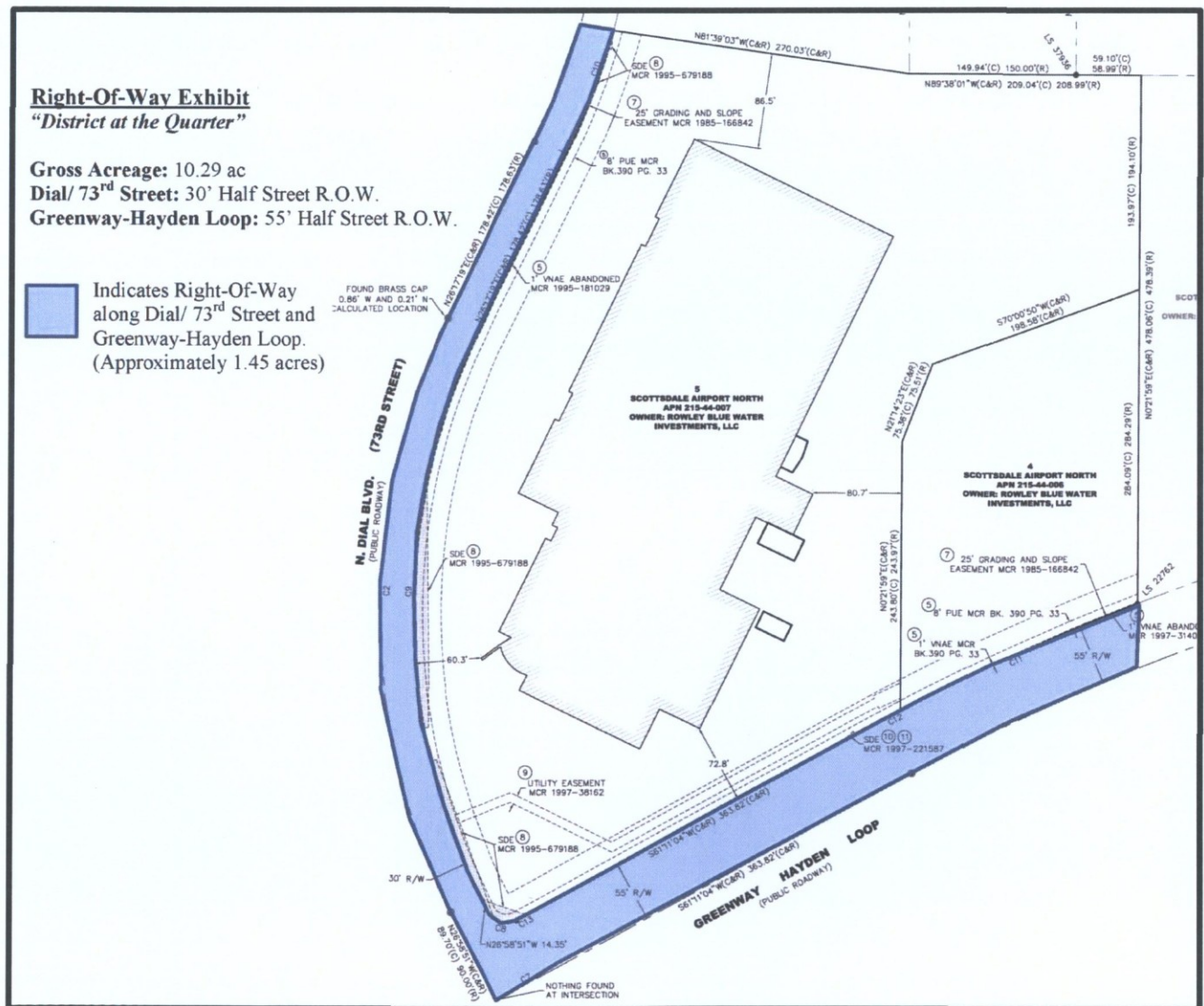
“Zone B includes the Scottsdale Airpark, the fastest growing and one of the three largest employment centers in the metropolitan area”.

This project in keeping with the mission and values of the General Plan and community, and in addition the overage being minimal creates a certain exception for a Non-Major GPA within item #2 of the “area of change criteria.” The Site totals approximately 8.84 net and 10.29 gross acres. The excessive amount of public right-of-way along Greenway-Hayden Loop (55’ right-of-way, half street) and 73rd Street (30’ right-of-way, half street) of approximately 1.45 combined acres creates a situation where the Site “technically” exceeds the 10 acre gross acreage by a nominal amount (.30 acres) without creating an increase in infrastructure needs. (See next page and also: *Right-of-Way Exhibit # 11*)

Moreover, in keeping with this area being a “large employment center” additional housing strategically located to support the continued growth of this employment center is critical. Thus, this proposal is optimally located to tap into the network of the existing employment, infrastructure, transportation system and amenities closeby.

It is worth noting, there are 55,000 jobs in the Airpark and currently only two apartment projects; Crescent (i.e. 275 units) and LIV (i.e. 240 units); with Sunrise Commons (i.e. 281 units) under construction for a total of 796 residential units within the Airpark to service approximately 55,000 jobs. It is projected that the Airpark will add 1,200 jobs annually, which means without more residential (i.e. in this area apartments) people will continue to leave the area daily to live somewhere else. Thus, the proposed number of residential units (i.e. 622 units), and thensome, is well supported, needed, and optimal located to build upon what has already been built at this intersection and supported by the GACAP. (See *Arizona Republic* article; "Scottsdale Airpark booming with urban life, 55,000 jobs" dated March 25, 2016, and "The Greater Scottsdale Airpark 2030 Report" by Colliers International dated December, 2015, Exhibit # 12)

RIGHT-OF-WAY



The proposed mixed-use project uses and design elements including the Landmark Intersection area of this project closely align with and reinforce the goals of Scottsdale's Mission, among them:

- "Promoting the livability of the community and enhancing and protecting neighborhoods and ensuring and sustaining the quality of life for all residents and visitors."
- "Enhancing and protecting neighborhoods"
- "Ensuring and sustain the quality of life for all residents and visitors."

This request provides a mix of uses, passive and active open spaces, and public benefits that are desired to enhance, protect, and sustain quality of life for the immediate neighborhood. The development provides many of the elements reflective of the goals and policies of the Scottsdale Design Guidelines, GACAP, and General Plan that speak to the intent of creating livable communities and a high quality of life desired by citizens.

Furthermore, the General Plan continues within the mission values of the land use element that contribute to Scottsdale's identity and provide a balance of uses adequate for the live, work, and play theme. This proposal makes meaningful contributions to these land use element values:

- "Land uses should provide for an unsurpassed quality of life for both its citizens and visitors."
- "Land uses should contribute to the unique identity that is Scottsdale."
- "Land uses should work in concert with transportation systems in order to promote choice and reduce negative impacts upon the lifestyle of citizens and the quality of the environment."
- "Land uses should provide opportunities for the design of uses to fit and respect the character, scale and quality of uses that exist in the community."

The uses and public benefits of this lively development furthers the goals and policies listed above by creating a live, work, and play development that enhances the public realm surrounding this area providing improvements to the Signature Corridors and Landmark Intersection, and activating the corner with ground level uses and architecture. These elements collectively meet the future intent of the area as identified in the Scottsdale Design Guidelines, GACAP, and General Plan. Therefore in keeping with the mission values of the General Plan this request will not need rise to the level of a major amendment.

3. Character Area Criteria

Character areas have been added to the city's planning process in order to recognize and maintain the unique physical, visual and functional conditions that occur in distinct areas across the community. The city recognizes that these form a context that is important to the lifestyle, economic well-being and long term viability of the community. These areas

are identified by a number of parameters including but not limited to building scale, open space types and patterns, age of development and topographic setting.

If a proposal to change the land use category has not been clearly demonstrated by the applicant to comply with the guidelines and standards embodied within an approved character area plan it will be considered a major amendment.

Response: Based on the justification and written responses below regarding the GACAP, the proposed GPA would not constitute a major amendment. The current employment designation will be enhanced with a portion of mixed-use. This location as indicated in GACAP is intended to provide a future Signature Corridor, Landmark Intersection, and is located within the shopping and activity center area. This indicates the appropriateness of the proposed mixed-use residential development in this strategic location, current trends that are important to the Airpark lifestyle, economic well-being, and long term viability.

4. Water/ Wastewater Infrastructure Criteria

If a proposal to change the planned land use category results in the premature increase in the size of a master planned water transmission or sewer collection facility, it will qualify as a major amendment.

Response: The proposed change in land use does not result in a premature increase in the water and sewer plan, therefore it will not constitute a major amendment. At this time the development team has no plans to change or upgrade any water or sewer infrastructure.

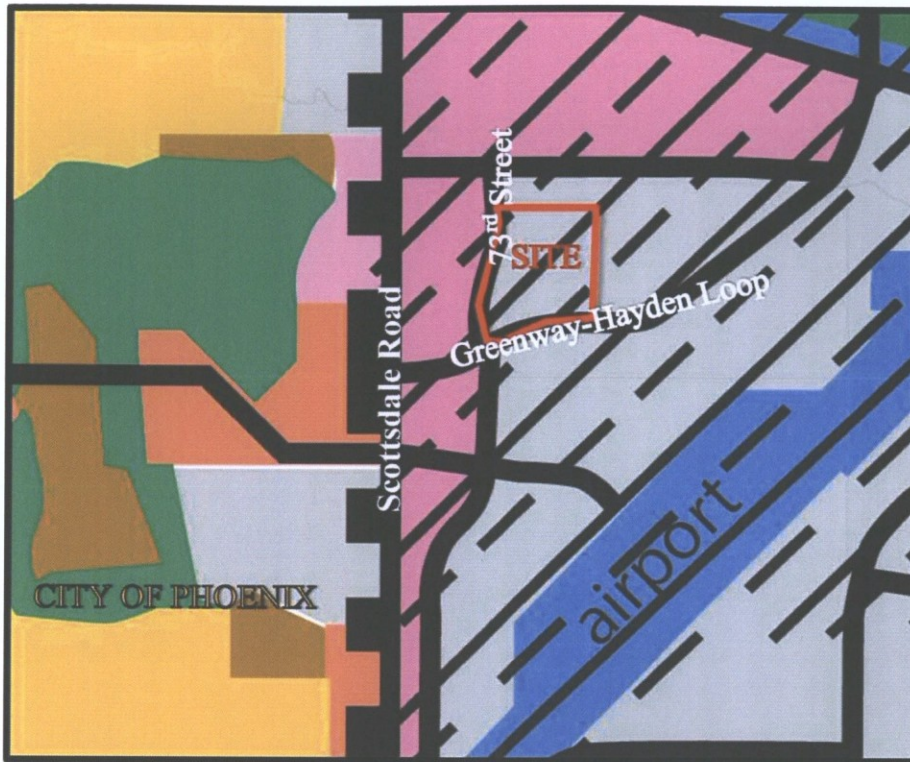
VI. 2001 General Plan/Greater Airpark Character Area Plan (GACAP)

2001 General Plan

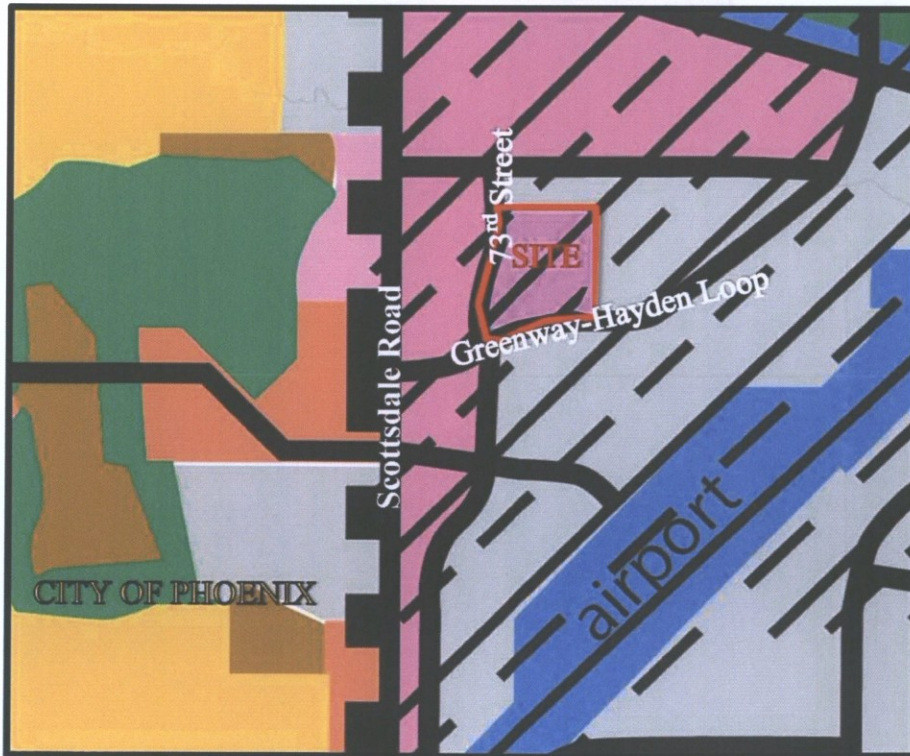
The current 2001 General Plan designation is Employment with a Regional Use District Overlay, and a requested GPA land use change to the Mixed Use category with a Regional Use District Overlay. (See next page and also: *2001 General Plan Land Use Existing & Proposed Exhibit # 13*)

The General Plan sets forward collective goals and approaches of the community with the intent to integrate Guiding Principles into the planning process and provide as a framework for proposed development and the built environment. The goals are, however, not intended to be stationary or inflexible, which the General Plan clearly recognizes with this statement: "The General Plan is designed to be a broad, flexible document that changes as the community needs, conditions and direction change." With this statement in mind, this proposed non-major general plan amendment not only meets but exceeds the goals and approaches established in the General Plan. In addition, it is important to understand that the current General Plan is 15 years old and the evolving lifestyles and economic development trends of today must meet the needs of a growing and changing city and area.

2001 GENERAL PLAN LAND USE EXISTING/ PROPOSED



2001 General Plan Existing Land Use



2001 General Plan Proposed Land Use

General Plan Elements:

Land Use:

Goal 3, Bullet Point 2: Encourage the location of more intense mixed-use centers and regional employment cores along regional networks while incorporating appropriate transitions to adjoining land uses.

Response: The location of this mixed use project is appropriately located along a major regional network at the convergence of two (2) Signature Corridors that form a Landmark Intersection and along with adjacent mixed-uses form a “gateway” to the Scottsdale Airpark from surrounding areas. The Landmark Intersection is the appropriate designation for the proposed intensity which provides pedestrian scaled uses and services supported by a balance of supporting uses. In addition, the location will support and enhance both the existing and proposed residential and non-residential uses in this location and transition between the employment center and surrounding retail. Appropriate transitions have been incorporated by way of design solutions, building construction, setbacks, noise disclosures, and avigation easements. This development is replacing the old sidewalk surrounding the site and creating a more walkable and enjoyable streetscape that helps to complete the streetscapes by creating a street scene similar to the adjacent LIV development.

Goal 4: Maintain a balance of land uses that support a high quality of life, a diverse mixture of housing and leisure opportunities and the economic base needed to secure resources to support the community.

Response: This mixed use development proposes appropriately balanced uses that are consistent with the needs and character of the surrounding developments and area uses and encourage a high quality lifestyle with many leisure opportunities and resources that support the surrounding community. The residential and non-residential uses at this location will support the quality of life and lifestyle desired in the area by providing residential units with some additional non-residential uses to help the balance of uses that support the surrounding community.

- Allow for a diversity of residential uses and supporting services that provide for the needs of the community.

Response: This proposed development allows for a unique residential housing opportunity with supporting services and amenities within the area. The residential and non-residential uses proposed at this location provides for a balance and an appropriate level of unique mixed-use development that complement and complete the surrounding area providing for the needs of community and affords for an exceptional lifestyle.

- Ensure the highest level of services and public amenities are provided to the citizens of Scottsdale at the lowest costs in terms of property taxes and travel distances.

Response: High quality and desirable services and amenities are included within this development. Furthermore, the location of the development and uses within walking distance of this development provide residents and nearby citizens the highest level of services with minimal travel. The clubhouse, restaurant, and fitness center of the live/work element included in this proposal provide for a balance of uses and amenities that promote a healthy lifestyle by encouraging walking and reduced travel. There is a balance of non-residential and commercial uses included that support the development and attract surrounding pedestrians to this Landmark Intersection location which is the intent of a walkable mixed use development.

- Support jobs/housing balance by integrating housing, employment, and supporting infrastructure in mixed-use centers located at appropriate locations.

Response: This development will support the jobs and housing balance by providing an appropriate balance of commercial and non-residential uses and services along with the residential population base to support new and existing employment and services. The designated Landmark Intersection is the ideal location for live, work, and play based mixed-use development that activates the surrounding area with walkable streetscapes and connections while utilizing existing infrastructure. In addition the option for ground floor walk-up live/work units are proposed along both Signature Corridors further integrating the live/work concept and activating the streetscape, similar to LIV.

- Provide a variety of housing types and densities and innovative development patterns and building methods that will result in greater housing affordability.

Response: This development provides a variety of residential housing options through various floor plans including ground floor live/work units. Additionally, the density proposed within this development is comparable and falls within the middle of a residential density option within the immediate area (i.e. LIV and Crescent).

- Maintain a citywide balance of land uses that support changes in community vision/dynamics (established by future community visioning processes) over time.

Response: The proposed development provides housing opportunities that support the future of the community and Scottsdale Airpark by enhancing the employment center's appeal to employers while also providing an appropriate and transitional mixed-use development in this particular location. The development provides a balance of uses to the area and non-residential services, notably, at the pedestrian scale of the Landmark Intersection which is the intent of the community's future vision and goals of the Landmark Intersection.

Goal 7, Bullet Point 5: Incorporate open space, mobility, and drainage networks while protecting the area's character and natural systems.

Response: Approximately, 10.5% of the Site has been reserved as useable open space. Private and public, passive and active forms of open space are found throughout the site plan. The Site also provides a total landscape space (including all open space, street frontage, and parking landscape areas) of 20% for the Site. It should be noted that both phases of development will exceed the required 10% open space. Again, these open space areas provide areas of contemplation, recreation (passive/active), cooling, and visual interest both on-site and off-site. Mobility and connections through the site and surrounding area are enhanced with meandering pathways, landscaping, enhanced streetscapes, and ground level uses. Native landscaping, open spaces, and drainage have been incorporated in an environmentally sensitive manner with consideration of the character in this area.

Goal 8, Bullet Point 2: Develop and reinforce links (i.e. trails, paths, open space, transit, and streets) within and between residential, retail, employment, recreational and other public land uses.

Response: Paths, connections, and open spaces have been carefully connected throughout and surrounding the site to provide for comfortable connections and alternative modes of transportation to surrounding land uses.

Goal 9: Provide a broad variety of land uses that create synergy within mixed-use neighborhoods.

- Incorporate a diverse range of residential and non-residential uses and densities within mixed-use neighborhoods.

Response: The proposed development adds a diverse range of residential and non-residential uses alike, with a different density option/density for residents within a larger mixed-use neighborhood. This development adds a complimentary balance of uses of non-residential uses at a location designated for mixed-use development with amenities and uses appropriately situated at the ground level of the Landmark Intersection.

- Promote residential uses that support the scale and function of retail, commercial and employment uses within these neighborhoods, including the use of mixed-use structures (retail or office on lower level and residential uses on upper levels).

Response: The residential/commercial uses proposed will be supportive of the existing retail, commercial, and employment uses within these neighborhoods and complete the mixed-use nature of this Landmark Intersection as envisioned. In addition, non-residential floor area uses and ground level live/work units are proposed along the Signature Corridors.

- Encourage compact mixed-use, pedestrian oriented development patterns, at urban densities, that limit the demand for parking and unnecessary automobile trips, and support alternative modes of mobility.

Response: This project utilizes existing infrastructure already in place and takes advantage of this prime location for such a development because of the surrounding employment, retail, and services. With a broad variety of employment and uses within walking distance, this development encourages and taps into non-residential uses on site and within walking distance of this location and enhances the streetscapes, which will encourage walkability and alternative modes of transportation which limits automobile trips.

Growth Areas Element

Goal 3, Bullet Point 2: Provide open spaces in designated growth areas that encourage public gathering, enhance aesthetics, preserve viewsheds, and serve as buffers between uses of significantly differing function and intensity.

Response: Open spaces have been strategically laid out within the development. The Landmark intersection and pedestrian area of this development includes amenities and open spaces that encourage public gathering and activity. The open space areas along the street frontages and enhanced design aesthetics will help to preserve viewsheds that also serve as buffers from inside and outside the development and provide for a public benefit for the area. This development strengthens the design and character of the Greater Airpark and provides enhanced streetscapes that promote a safe, comfortable, and aesthetically pleasing pedestrian environment.

Housing Element

Goal 3, Bullet Point 1: Encourage and establish appropriate incentives for development of aesthetically pleasing housing that will accommodate a variety of income levels and socioeconomic needs.

Response: The proposed development will be aesthetically pleasing and will allow for another housing opportunity within this immediate area.

Goal 3, Bullet Point 6: Consider incentives that encourage the development of diverse housing types, including smaller, more affordable units.

Response: A range of floor plans have been included with the development that provides various levels of living options (i.e. 1 and 2 bedrooms with roof access for some units and ground level live/work units) as well as amenities (i.e. concierge service).

Goal 4, Bullet Point 1: Encourage a variety of housing densities throughout Scottsdale, with mixed-uses in areas of major employment and transit hubs, to offer greater live/work choices to a broader economic range of households.

Response: This development provides a different level option in housing density while also providing a mix of uses located one of the state's largest employment centers and further enhancing the live/work philosophy.

Goal 4, Bullet Point 5: Support the use of future innovations in technology and telecommunications as a way to remain flexible to changing demographics, community profiles and quality of life choices, including opportunities like home based businesses, telecommuting, on-line shopping, etc.

Response: This development supports the use of future innovations in technology that provide opportunities for "work from home" based businesses of the present and future while providing for a high quality of life. The development will utilize state of the art Wi-Fi technology and contains on-site amenities and services within the business center that support future technology workers and can help to eliminate automobile use. The on-site business center has a conference center with meeting space and can assist in business needs such as printing all located within the development. In addition, live/work units are proposed along both Signature Corridors allowing for opportunities for home based businesses and lifestyle choices.

Goal 4, Bullet Point 6: Consider a variety of strategies to increase housing intensity and diversity in appropriate locations, such as around commercial areas, near transit centers or major employment.

Response: Located between a heavily commercial area and one of the largest employment centers in the state, at a Landmark Intersection, and with existing transit, this site is ideal for an increase in housing intensity.

Goal 4, Bullet Point 7: Explore opportunities for new or redeveloped housing to serve the employment base.

Response: The redevelopment of this site with existing infrastructure is an ideal opportunity for housing to serve the existing and future employment base in the area.

Goal 4, Bullet Point 9: Work to adjust the housing mix based on changing demographics and economics of the city.

Response: The changing economics and demographics of the city and particularly the future of the Scottsdale Airpark are evolving towards a live, work, and play philosophy which allows for "work from home" options and the Site is in an ideal location to capitalize on that philosophy.

Community Mobility Element

Goal 8 Emphasize live, work, and play land use relationships to optimize the use of citywide systems and reduce the strain on regional and local/neighborhood systems.

Response: This mixed-use development will contribute to the existing live, work, and play theme seen in adjacent developments in this location, and particularly at the GACAP designated Landmark Intersection. The mixed-use nature and design of the proposal provides for walkability and encourages alternative modes of transportation to reduce automobile trips and ultimately the strain on regional and local/neighborhood systems.

Goal 8, Bullet Point 3: Encourage, where appropriate, mixed use developments that physically incorporate residential, shopping and work environments within one area or project and place strong emphasis on connectivity with non-motorized access (pedestrian oriented development).

Response: This project provides an appropriate mixed-use development located at the GACAP designated Landmark Intersection and various commercial/retail services and business related opportunities. Furthermore, the development is designed with the intent to reduce automobile trips and encourage pedestrian oriented development by including ground level uses, live/work units, and services and enhancing the streetscapes to become walkable, comfortable, and aesthetically pleasing.

Goal 11: Provide opportunities for building “community” through neighborhood mobility.

- Encourage the sensitive integration of live, work and play land uses and their physical links within and between neighborhoods to emphasize sense of place.
- Provide a high level of service for pedestrians through facilities that are separated and protected from vehicle travel (e.g., placing landscaping between curbs and sidewalks).
- Emphasize strong pedestrian orientation (e.g. shaded safe paths, links to civic spaces) to foster a strong sense of community.

Response: The design for this development provides the opportunity to build community through neighborhood mobility by placing an amenitized and public open space at the intersection and uses and enhanced streetscapes that encourage activity and friendly neighborhood interaction. Design consideration includes separated sidewalks, shade, seating, bicycle parking, and safe pathways that foster a strong sense of community.

General Plan – Six Guiding Principles:

The following request is for a non-major GPA to the land use category and map contained in the Land Use Element of the City of Scottsdale 2001 General Plan. Six guiding principles indicate the appropriateness of a land use change to the General Plan. These six Guiding Principles are:

1. Value Scottsdale's Unique Lifestyle & Character.
2. Support Economic Vitality.
3. Enhance Neighborhoods.
4. Preserve meaningful Open Space.
5. Seek Sustainability.
6. Advance Transportation.

Responses:

1. Value Scottsdale's Unique Lifestyle & Character:

Response: Scottsdale is world renowned for a tourism and resort lifestyle. The live, work, and play theme extends the concept of the unique Scottsdale lifestyle by offering an opportunity to utilize the surrounding amenities, services, entertainment, recreation, relaxation, business, and travel within walking distance to a place of residence. With the close proximity to the TPC Golf Course, Scottsdale Airpark, and Scottsdale Quarter among many other businesses this proposed development offers a modern, active, and exciting lifestyle rooted in a location that provides access to some of the best amenities the Scottsdale lifestyle has to offer. From professionals to families, this location provides the location, lifestyle, and services that appeal to everyone.

2. Support Economic Vitality:

Response: This proposed development will generate sales tax revenue from new tenants, property taxes, and indirect revenues spent within the area at nearby businesses, which generates a substantial new economic benefit to the city and Airpark. The current employment base would not generate as great of economic benefit as a new community of residents who will generally spend a good portion of their income within the Airpark and Scottsdale. Moreover, increased residents within an area provides for greater opportunities for new employers to locate to the Airpark, with close proximity to housing, retail, entertainment/ recreation.

3. Enhance Neighborhoods:

Response: The proposed project will best utilize the Site, which is located at a designated future Signature Corridor. This project will enhance pedestrian activity and connectivity of various areas, as well as bring to the area an active residential community and support services. The economic spin-off related to the new community provides revenue to

surrounding property owners businesses through shopping, dining, airport travel, and nearby recreation and entertainment.

4. Preserve Meaningful Open Space:

Response: This project will dedicate a total of approximately 10.5% of useable open space. The most meaningful being the public benefit of an enhanced Landmark Intersection pedestrian rest area and passageway. This space provides areas for public rest, interaction, and connections. The Signature Corridors of this site include landscaped street frontage open space that creates a comfortable walking experience and enjoyable streetscape. In addition, five (5) internal interconnected courtyards throughout the development will contain substantial amenities. In addition, the Site will have beautiful natural landscaping that enhances open space and showcases the beauty of the surrounding lush Sonoran desert landscape palette.

5. Seek Sustainability:

Response: Many features of District at the Quarter will indicate the commitment to sustainability. The density and design of this dense and urban multi-family development uses less land area and encourages a more mobile lifestyle other than via an automobile (e.g. walking, biking, and transit). Other design considerations include native landscaping which will reduce water use.

6. Advance Transportation:

Response: A mix of uses and nearby services encourages alternative modes of transportation through walking and bicycling. Furthermore, the development of an exciting and active street scene in conjunction with improved landscaping will activate the intersection and both roadways connecting to nearby destinations while providing pedestrian amenities such as shade, seating, and bicycle parking.

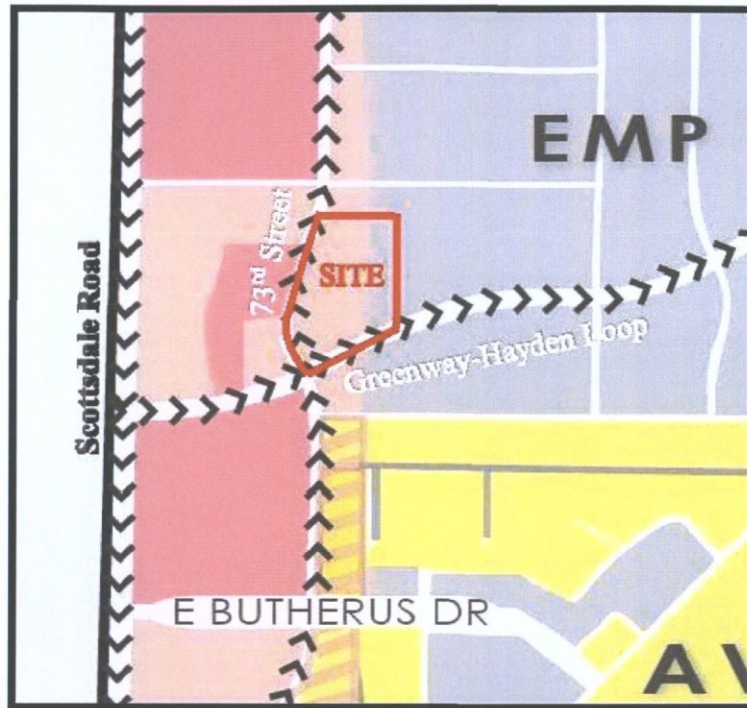
Greater Airport Character Area Plan (GACAP)

The GACAP was adopted in October 2010 (last amended: December 2014) by Scottsdale's City Council. The purpose of the GACAP is to establish "the vision for the Greater Scottsdale Airport and provide the basis for Greater Airport decision-making over a twenty-year timeframe." To achieve this, the City established a series of goals and policies to provide a framework for future development. The GACAP is divided into eight chapters each with its own focus and vision: Land Use, Neighborhood & Housing, Aviation, Community Mobility, Economic Vitality, Environmental Planning, Character & Design, and Public Service Facilities. The following paragraphs (i.e. responses) will highlight how this application meets the goals and policies of the GACAP.

The request for a Non-Major GPA on the Site from AMU and a small portion of EMP to AMU-R will allow for a Multi-Family Residential land use on this Site that is partially designated for mixed-use.

A small portion of the Employment land use boundary for the Airpark runs north and south and happens to fall on the eastern edge of the redevelopment parcel. This entire Site is owned and developed as one parcel and should be developed under the same plan, as such, a minimal portion of the Employment land use would be changed to accommodate the proposed development and pushed slightly east towards a more appropriate location for employment. (See below and also: GACAP Land Use – Existing & Proposed, Exhibit # 14)

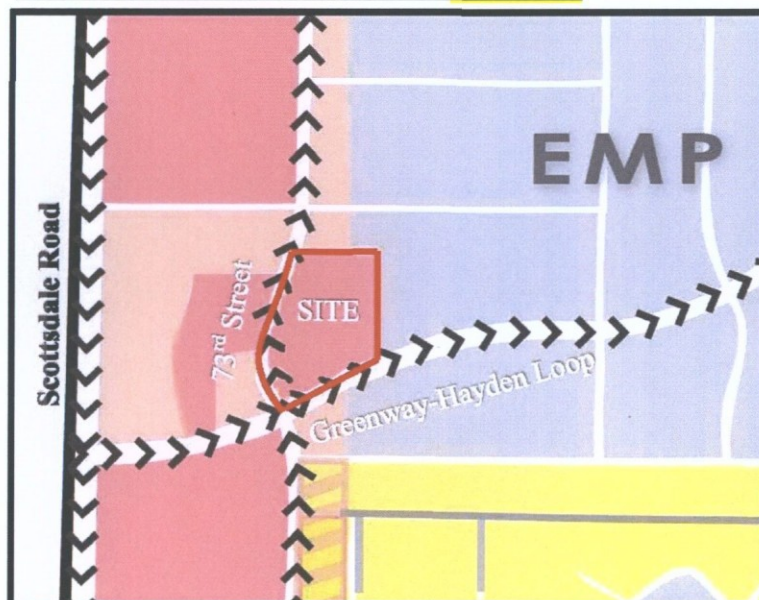
SCOTTSDALE AIRPARK LAND USE – EXISTING



LEGEND

- Land Use Plan:
- Airpark Mixed Use-Residential (AMU-R)
 - Airpark Mixed Use (AMU)
 - Regional Tourism (RT)
 - Employment (EMP)
 - Aviation (AV)
 - Open Space (OS)
 - AV or AMU
 - Greater Airpark Boundary
 - Signature Corridor
 - Powerline Corridor
 - Central Arizona Project

SCOTTSDALE AIRPARK LAND USE – PROPOSED

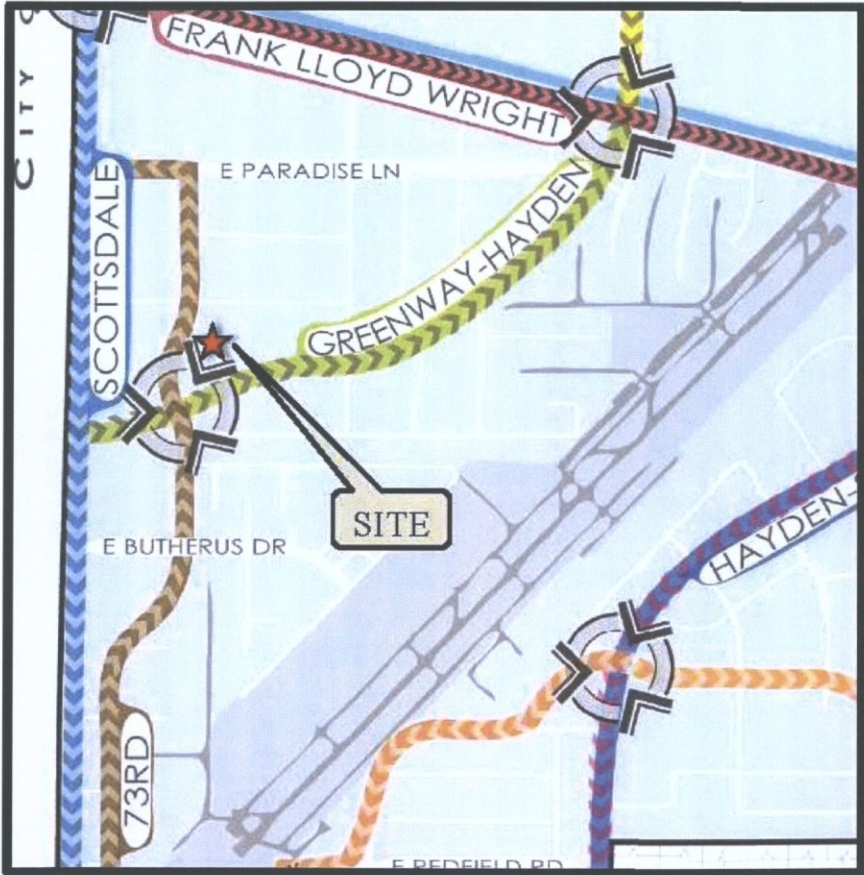


LEGEND

- Land Use Plan:
- Airpark Mixed Use-Residential (AMU-R)
 - Airpark Mixed Use (AMU)
 - Regional Tourism (RT)
 - Employment (EMP)
 - Aviation (AV)
 - Open Space (OS)
 - AV or AMU
 - Greater Airpark Boundary
 - Signature Corridor
 - Powerline Corridor
 - Central Arizona Project

As designated in the GACAP Signature Corridors Map, the Site is located along 73rd Street and Greenway-Hayden Loop, both of which are designated as Signature Corridors of the Greater Airpark. (See below and also: *Airpark Signature Corridors Exhibit # 15*) The Greater Airpark Transit Connections Map designates the Site within the “shopping/ housing” activity center which also contains a potentially future transit route on Greenway-Hayden Loop. The Pedestrian and Bicycle Connectivity shows both major roadways bordering the Site as future pedestrian and bicycle corridors, further illustrating the concept that this Site should be utilized for more residential and mixed-use development that will support the planned pedestrian activity. (See next pages and also: *Airpark Future Transit Connections Exhibit # 16 & Airpark Pedestrian & Bicycle Connectivity Exhibit #17*)

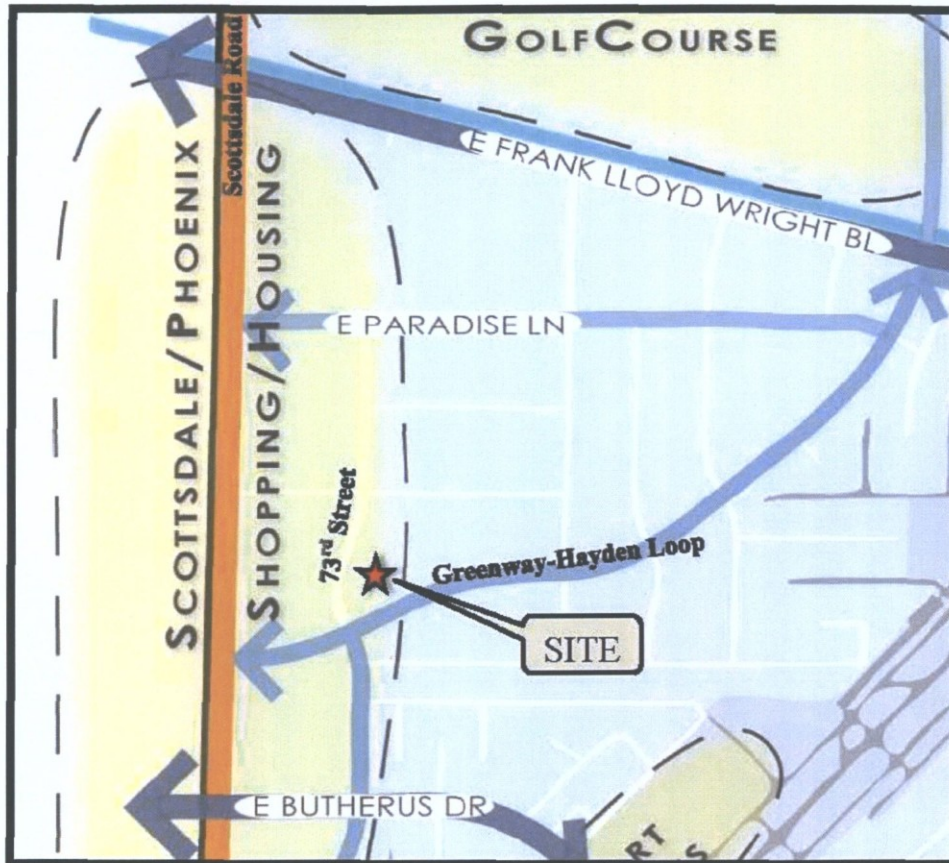
SCOTTSDALE AIRPARK SIGNATURE CORRIDORS



LEGEND

- Signature Corridors
- Greater Airpark Boundary
- Powerline Corridor
- Central Arizona Project
- Signature Corridors
- Landmark Intersections

SCOTTSDALE AIRPARK FUTURE TRANSIT CONNECTIONS

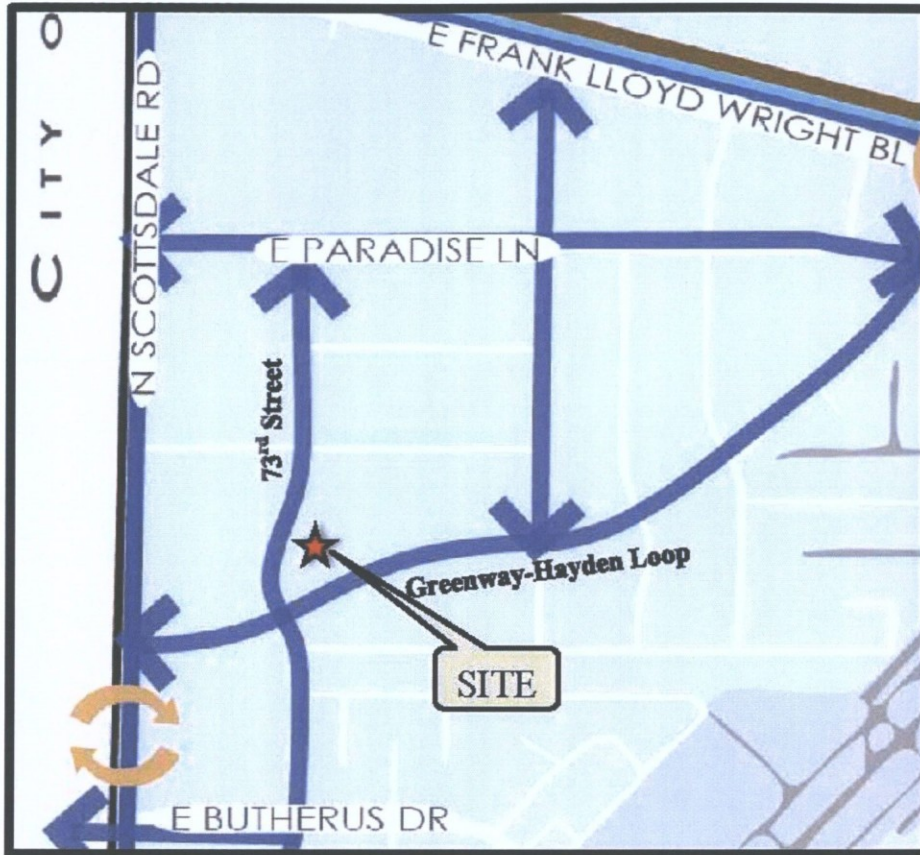


LEGEND

Conceptual Transit Connections

- Greater Airpark Boundary
- Powerline Corridor
- Central Arizona Project
- Existing Transit Route
- Potential Future Transit Route
- Future High Capacity Transit Corridor
- Activity Centers
- Park and Ride

SCOTTSDALE AIRPARK PEDESTRIAN & BICYCLE CONNECTIVITY



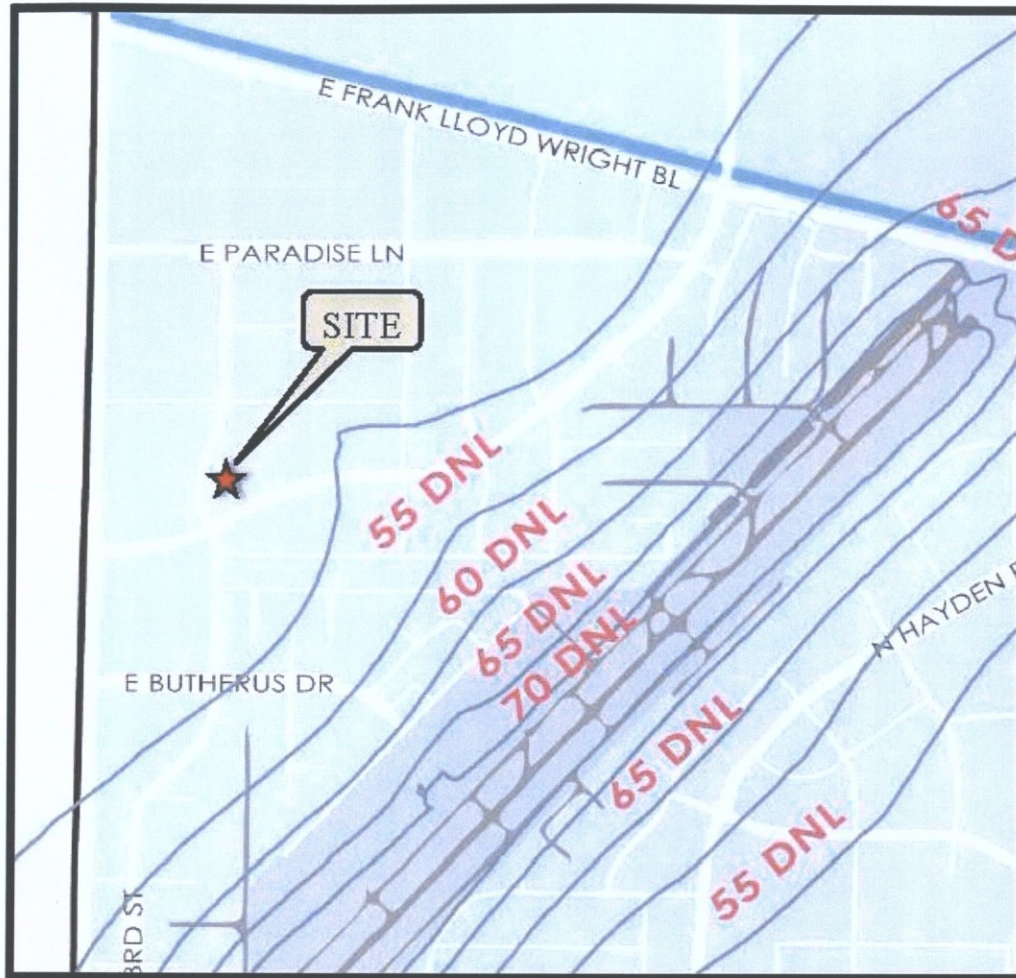
LEGEND

Conceptual Greater Airpark Pedestrian/Bicycle Connectivity

- Greater Airpark Boundary
- Powerline Corridor
- Central Arizona Project Aqueduct
- Pedestrian/ Bicycle Corridors
- Trails
- Important Pedestrian Linkage

Additionally, the proposed redevelopment Site is located outside of the 55 DNL line, which is appropriate for residential land uses. (See next page and also: *Airpark Noise Contours Exhibit # 18*) New construction will feature appropriate noise attenuation through the use of upgraded building materials and insulation, disclosure to residents and an avigation easement in conformance with the City’s requirements. Flight paths for both helicopter and fixed wing planes do not cross over this property, rather they route to the north, northeast, south and southwest. This Site is clear of most flight path tracks as compared to other properties located within the airpark.

SCOTTSDALE AIRPARK NOISE CONTOURS



GACAP Definition (emphasis added):

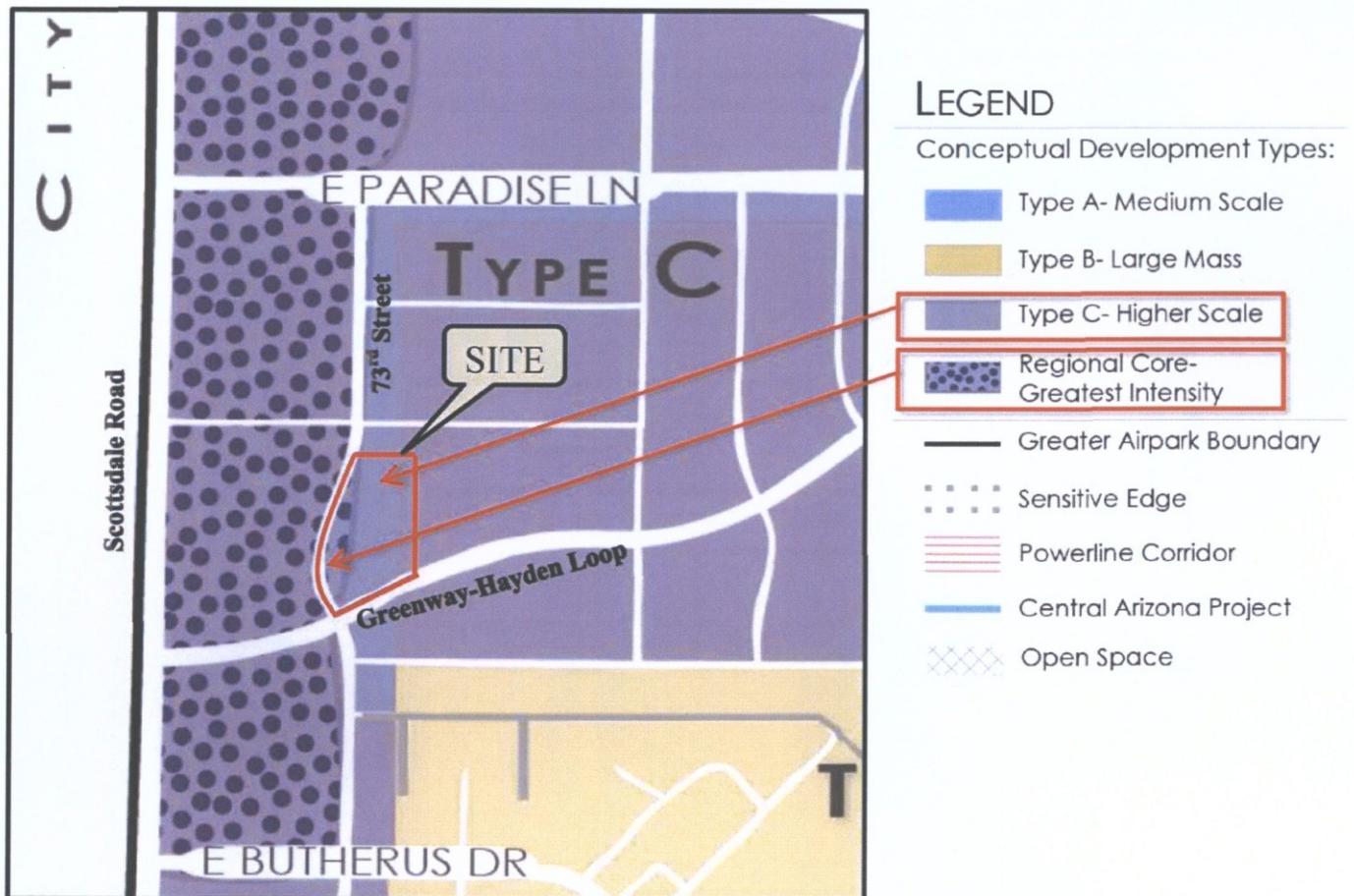
Airpark Mixed Use Residential areas are appropriate for the greatest variety of Residential (AMU-R) land uses in the Greater Airpark. Appropriate uses may include a combination of personal and business services, employment, office, institutional, cultural amenities, retail, hotel, and higher density residential. Developments in AMU-R areas should be pedestrian-oriented, have access to multiple modes of transportation, and should be located outside of the Airport's 55 DNL contour. Residential and other sensitive uses should be a lesser component of development and include adequate sound attenuation. Design of residential uses in the area south of the Central Arizona Project Aqueduct should support businesses and tourism uses, such as time-shares, multi-family rental units, and corporate housing.

The Conceptual Development Type Map designates the majority of the property as “Type C-Higher Scale” and a small portion also as “Regional Core” and they are defined in the GACAP as follows (emphasis added):

***Type C** development represents medium to higher scale development which supports pedestrian activity in the Greater Airpark. Type C is encouraged in areas with access to multiple modes of transportation, served by regional transportation networks (i.e. freeways or transit corridors), and where the scale will complement the area’s character. Type C development is appropriate in areas next to both Types C1 and B development. Type C is not recommended immediately adjacent to the Scottsdale airport.*

*The **Regional Core** designation denotes areas appropriate for the greatest development intensity in the Greater Airpark to support regional land uses. Regional core is only appropriate in Type C areas that are or will be served by high-capacity transit or a freeway. Regional Core areas should not be located adjacent to single-family residential areas or within Type A or B development areas (emphasis added). (See below and also: Airpark Development Types Exhibit # 19)*

DEVELOPMENT TYPES



Airpark Development Types Exhibit

a) Land Use

Policy LU 1.1 Maintain and expand the diversity of land uses in the Greater Airpark.

Response: The proposal to add a multifamily land use adds an appropriate residential component in a prime location of the Scottsdale Airpark. Characteristics of successful mixed-use developments include a range of land uses and promote the “live, work, and play” philosophy. The proposed development accomplishes a range of goals including helping partially to enhance the regional core, integrating high quality, vibrant architecture and site planning to the area, and creating pedestrian synergy that will complement the surrounding context.

Policy LU 1.2 Support a mix of land uses within the Greater Airpark that promote a sense of community and economic efficiency, such as clustering similar/ supportive uses and incorporating residential intended for the area’s workforce, where appropriate.

Response: The Airpark is predominately an employment center. Integrating the proposed multifamily land use will provide a residential rental housing opportunity for residents of Scottsdale in an employment and service core area of the City. The location of the property not only provides an opportunity for housing of workers in and adjacent to the employment core, but also residents seeking connectivity to the retail, restaurants, and businesses established in the nearby developments that will enhance their sustainability. District at the Quarter will promote an integrated mix of uses within the broader area’s existing uses contributing towards the live, work, play goals identified in the GACAP.

Policy LU 1.3 Promote development intensities supportive of existing and future market needs.

Response: The proposal promotes development intensities consistent with the present area context and appropriately responds to the needs of future development intensity planning for the Landmark Intersection and the Site is within the Airpark Type C development and Regional Core designations.

Policy LU 4.5 Greater visual variety and architectural interest should be considered in the design of the Greater Airpark’s tallest buildings (Regional Core), particularly at the pedestrian level.

Response: District at the Quarter intends to create a unique luxury 4-story residential rental community with Southwestern contemporary elevations complementary to the character of the adjacent Scottsdale Airpark. The site design and architecture will further cater to the pedestrian scale particularly with the incorporation of the ground level commercial component and public pedestrian area of the project.

Policy LU 4.7 *Encourage greater visual variety between employment/commercial land uses and residential neighborhoods, and avoid continuous building shapes and mass adjacent to residential neighborhoods.*

Response: The unique design demonstrates rich character and architectural visual variety that create a distinct and appropriate transition between the proposed development and adjacent employment land uses. The building and site planning design provides pedestrian scale massing along the Signature Corridor and a hierarchy of masses and sensitivity to pedestrian scale.

Goal LU 5 Encourage Greater Airpark development flexibility.

Policy LU 5.1 *Update and provide greater flexibility in development regulations to achieve the goals of the Greater Airpark Character Area Plan and encourage revitalization in the area.*

Response: The AMU-R zoning will provide the flexibility in development standards to accommodate residential and mixed land uses. This will further promote the type of mixed-use development and character that revitalizes the area and creates synergy that defines land uses, lifestyle and activities, and furthers the economic goals of the Scottsdale Airpark while providing enhancements and amenities for the public benefit.

Policy LU 5.2 *Greater Airpark public amenities and benefits should be provided by the private sector when development bonuses, such as increased floor area, greater intensity, greater height, development standard flexibility, and/or street abandonment are considered. Potential public amenities and benefits may include, but are not limited to:*

- *New/expanded usable open space areas;*
- *Linkages to planned or existing trails and/or paths;*
- *Transit and/or other mobility enhancements, including bicycle and pedestrian amenities;*
- *Workforce housing (where appropriate);*
- *Infrastructure improvements;*
- *Regional tourism facilities or accommodations;*
- *Green building standards, such as LEED certification;*
- *Gateway and/or Landmark Intersection enhancements;*
- *Integrated daycare facilities;*
- *Public/shared/park-and-ride parking facilities;*
- *Net-zero/significant energy efficiency and/or on-site alternative energy generation for multiple properties; and/or*
- *Area stormwater infrastructure.*

Response: This development provides many public benefits most notably providing an enhanced Landmark Intersection for public use that serves as a “gateway” to the Airpark. This enhanced Landmark Intersection space features shade, seating, various meandering pathways, public open space, landscaping, and ground level uses and services which produce a public benefit in the form of placemaking for this intersection and area. In addition, the development provides various passive and active open space areas, and enhanced Signature Corridor streetscapes with various interesting connections through the site and to surrounding areas. This development provides public benefits and amenities that promote walkability and alternative modes of transportation through improved streetscapes, pedestrian scaled services and uses, bicycle parking, and ground level live/work units that help activate the streetscapes and intersection providing visually interesting and comfortable public spaces and an exceptional walkable community.

Goal 6 Promote the Greater Airpark as a mixed-use economic and aviation-based employment center that is complementary to Downtown Scottsdale, the city’s premier cultural, civic, and residential mixed-use core.

Response: The mixed-use, non-residential floor area uses, amenities, and residential population base support the economic activity and boost revenue of this major employment center while strengthening the synergy and achieving the future goals of the Scottsdale Airpark. The mixed-use development proposed is ideal for this particular intersection due to the many attributes identified with the GACAP for this intersection (i.e. Landmark Intersection, transit, bike, etc.) of the airpark.

Policy LU 6.4 Enforce and modify development standards and building codes to enhance compatibility of residential uses with aviation and employment, and buffer existing industrial and aviation uses from new and existing residential development.

Response: The proposed development buffers primarily lower intensity employment and boutique industrial which includes predominately closed buildings (i.e. Piano Gallery) to the north and east, making the transition of the proposed high density residential apartment development use more easily compatible with the proposed mixed-use project. In addition to design solutions such as building construction, and setbacks, and other measures (see below) are being taken to ensure the compatibility of uses. Examples of Policy LU 6.4 illustrate how the residential use is compatible in the area:

- Sound and attenuation measures will be included in the development.
- A maximum height of 48 feet and 58 feet for rooftop mechanical stairwell and patio enclosure elements, not exceeding 30% of the roof are, is included.
- Notification will be given of proximity to Airport.
- Tenants will be required to sign noise disclosures and aviation easements.
- The proposed development will be in compliance for lighting standards set forth by the Federal Aviation Administration and Scottsdale Design Standards and Policies Manual.

Policy LU 6.5 *In accordance with the Airport's Part 150 Noise Compatibility Study, aviation easements and fair disclosure statements are required for all new and redevelopment projects in the Greater Airpark.*

Response: Aviation easements and fair disclosure statements will be required for new tenants.

Goal 7 *Develop an interconnected network of Signature Corridors to support the Greater Airpark as a place for meeting, creating, shopping, learning, as well as working.*

Response: This development creates and helps interconnect the network of two (2) Signature Corridors which will be enhanced by the mixed-use nature of pedestrian features and ground level uses located at the Landmark Intersection, surrounding spaces, and supporting commercial/retail and employment services.

Policy LU 7.1 *Encourage growth along corridors with the greatest potential for activity, new development, revitalization, tourist attractions, and enhanced multi-modal connections.*

Response: The proposed mixed-use development encourages growth, activity, and revitalization at the intersection of two (2) Signature Corridors along 73rd Street and Greenway-Hayden Loop. This proposal provides a vibrant and active residential community at this integral location within the Airpark, which is positioned within walking distance of shopping and restaurants along Scottsdale Road and within walking proximity of employment land uses further promoting pedestrian oriented activity.

Policy LU 7.2 *Promote a greater mix of uses along identified Signature Corridors, which complement and are compatible with each respective land use designation.*

Response: Expanding on the statement above, this project will integrate multi-family residential and mixed uses at the intersection of two (2) Signature Corridors, further broadening the mix of uses within the area. Walk up style live/work units are proposed along the Signature Corridor that support the mixed-use component as well as employment for this area.

Policy LU 7.3 *Encourage and incentivize revitalization along Signature Corridors, particularly south of the Central Arizona Project Aqueduct.*

Response: The proposed redevelopment encourages and incentivizes revitalization along both the Signature Corridors of 73rd Street and Greenway-Hayden loop which is located south of the Central Arizona Project Aqueduct.

Policy LU 7.4 *Integrate entertainment, education uses, cultural uses, and tourist attractions along Signature Corridors.*

Response: The redevelopment of the Signature Corridors bordering the site combined with the non-residential floor area uses support pedestrian interest, activity, and connections to the nearby

educational, cultural, and tourism based uses of the surrounding area. This project will serve to enhance the proximity of the residential population base that supports the surrounding entertainment, tourism, and education based uses. The enhanced Landmark Intersection and the Greenway-Hayden Loop Signature Corridor provide active ground level uses in the form of a clubhouse and fitness center that encourages community interaction and contemplation.

Goal LU 8 *Create an interconnected network of meaningful open spaces within the Greater Airpark.*

Response: This development as the “gateway” to the Airpark enhances and encourages the pedestrian based activity and connections including utilizing alternative modes of transportation that create a meaningful network of connections to open spaces within the development and surrounding area. The enhanced open space and pedestrian scale and amenities of the Landmark Intersection will serve as an inviting gateway.

Policy LU 8.3 *Promote public/private partnerships in the design of development plans that provide functional urban open spaces, such as plazas and pocket parks, within and between projects.*

Response: A large number of functional open spaces, amenities, and open space has been included in this high quality design and development, most notably, the enhanced Landmark Intersection with public open space and pedestrian elements that will engage the public.

b) Neighborhood & Housing

Goal NH 2 *Create complete neighborhoods within the Greater Airpark, through the development of urban dwelling types and mixed- use developments, while being respectful of the Greater Airpark as an aviation-based employment center.*

Response: This project offers various dwelling types not found in surrounding housing options and in a location ideal for a mixed use development. This mixed use project creates and compliments the surrounding developments to create a complete neighborhood and respect and compliment the employment area of the Airpark to the east. This project furthers the intent of a mixed-use development in this location while providing for neighborhood commercial/ retail services and live/work units complimentary to the Airpark and the LIV development across 73rd Street.

Policy NH 2.1 *Encourage developments, in Airpark Mixed Use Future Land Use Areas (AMU and AMU-R), to provide support services for current and future Greater Airpark residents, such as local markets, drugstores, and other essential services.*

Response: The subject Site is located within an Airpark Mixed Use Future Land Use Area and as proposed would provide a mixed-use development featuring a variety of commercial and non-residential uses and floor area such as: retail/commercial clubhouse component with restaurant, office, and fitness center along with seven (7) live/work units.

Policy NH 2.3 *Incorporate gathering spaces and recreational opportunities into the design of mixed-use development to support a high quality of life for Greater Airpark residents.*

Policy NH 2.4 *Promote opportunities for parks, open space, and trail connections within new mixed-use development and as a part of the redevelopment of existing property.*

Response: This centrally located project with its enhanced pedestrian corner has the ability to draw visitors from across the Valley. Living in this area has the unique benefits of those who fly regularly from Scottsdale Airport, attend events at nearby Westworld, or hike the McDowell Mountains and nearby trails. The development provides a mix of uses and design elements that cater to the pedestrian and providing urban character and gathering spaces through the use of building design, connectivity, landscaping, open spaces, hardscape and lighting. The proposed site plan calls for an enhanced public pedestrian area and amenities, a total of five (5) courtyards, a clubhouse, and private areas providing high quality and active and passive open space and recreation for residents and the public to connect and converse on a daily basis. The development also provides a network of sidewalks that tie to outer perimeter sidewalks and into the greater pedestrian network. The walkable nature of this development along with the public Landmark Intersection and Signature Corridors provide connectivity and economic vitality that supports the continued growth within the area and the future connections.

Policy NH 3.1 *Encourage thoughtful and creative residential development that enhances, supports, and is sensitive to Airport operations and the Greater Airpark's identity as an employment center.*

Response: As previously discussed, the proposed uses will support the work, live, play philosophy emphasized throughout the GACAP. Special measures will be taken to create buildings that are sensitive and compatible to the nearby Airport operations.

Policy NH 3.2 *Incorporate residential into Airpark Mixed Use- Residential Future Land Use Areas to reduce traffic congestion, improve air quality, and provide opportunities for workforce housing where:*

- *Dwellings units will not be directly adjacent to industrial uses that could be in conflict with residential uses;*
- *Dwellings will not lie within the 55 day-night average noise level (DNL) or higher areas established by the FAA; and*
- *Multi-modal transportation options will be incorporated into residential design.*

Response: The proposed mixed-use multi-family community will add residential to an area ideally suited for residential development. The proposed dwelling units have a buffer from existing industrial uses, by way of an internal roadway, setbacks, and landscaping. Residences will be well aware of Airpark and employment uses within the surrounding area. The proposed dwelling units are also located well outside the 55 DNL line (see *Airpark Noise Contours Exhibit # 4*) and

appropriate sound attenuation, building materials, and insulation will be utilized. Multi-modal transportation options will be available to residents including, but not limited to walking, bicycling, driving and public transit.

- **Policy NH 3.3** *Support the integration of workforce housing and a diversity of dwelling types within the Greater Airpark.*
- **Policy NH 3.3.1** *Encourage residential development for a variety of income groups, in appropriate Greater Airpark locations, and in surrounding areas.*
- **Policy NH 3.3.2** *Develop a variety of urban dwelling types, including condominiums, apartments, townhomes, lofts, time-shares, patio homes, and work-live and/or live/work units.*
- **Policy NH 3.3.3** *Encourage live/work units in Airpark Mixed Use-Residential and Regional Tourism Future Land Use Areas in the Greater Airpark.*

Response: The proposed development will provide a unique retail/commercial and residential mixed-use experience for the business owners of Scottsdale. As noted above and throughout the GACAP, the work/live concept is promoted and supported in AMU-R areas. The proposed GPA will create a development that facilitates an important transition of uses and provides opportunity for live/work units.

c) Community Mobility

Policy CM 4.2 *Improve pedestrian and bicycle connections from adjacent neighborhoods to Greater Airpark destinations.*

Response: The Signature Corridors adjacent to the property include 73rd Street and Greenway-Hayden Loop. These streets provide a framework for pedestrian and bicycle connections within the immediate area. Furthermore, the development plan will create a walk-friendly environment for its residents with internal pedestrian connections between multi-family buildings, on site amenities, and adjoining land uses.

Goal CM 6 Enhance pedestrian and bicyclist access and activity for Greater Airpark residents, visitors, and employees.

Policy CM 6.5 *Design corridors that accommodate and attract pedestrians and bicyclists, particularly in Airpark Mixed Use Future Land Use Areas and along Signature Corridors.*

Policy CM 6.6 *Design safe, comfortable, and aesthetically-pleasing Greater Airpark pedestrian and bicyclist facilities through the incorporation of universally accessible designs, coordinated street lighting, visually-interesting landscape treatments, shading, bicycle lanes, and public art integrated into facility design.*

Goal CM 7 *Promote sustainable transportation options that meet the needs of the current and future Greater Airpark community.*

Goal CM 7.1 *Incorporate site design features that promote more access to those walking, cycling, or taking public transit, such as ground-floor retail and parking located in the rear of buildings, particularly along Signature Corridors and within Airpark Mixed Use Future Land Use Areas.*

Policy CM 7.2 *Promote more sustainable modes of passenger transportation, such as alternative fuel vehicles, walking, biking, and/or other future technologies.*

Response: The Greater Airpark designates this property as "Shopping/Housing" along a future transit connection route, two (2) Signature Corridors, and a Landmark Intersection. (See *Airpark Signature Corridors Exhibit # 15*, *Airpark Future Transit Connections Exhibit # 16*, and *Airpark Pedestrian & Bicycle Connectivity Exhibit #17*) In response, the site plan has been designed in a manner that pays particular attention to pedestrians and bicyclists along the perimeter of the Site, by providing safe and aesthetically pleasing streetscapes while utilizing existing bike lanes.

The mixed use nature of District at the Quarter promotes pedestrian mobility and activity with ground floor retail/commercial, active non-residential floor area uses, enhanced streetscapes with pedestrian amenities, shade and landscaping, and a distinctive public gathering space at the intersection of the two (2) Signature Corridors to form a Landmark Intersection. These elements together complete an enhanced streetscape that is pleasing, interesting, safe, and comfortable while providing for pedestrian access that easily transitions into the surrounding area.

Integration of housing within the greater area and a mixed use development at this Airpark location increases the efficiency of movement by people by locating them close to employment and retail services and minimizing vehicular trips creating a sustainable walkable community. The design and location of this development promotes walkability and alternative modes of transportation including biking infrastructure and easy access to nearby transit. Furthermore, parking is located towards the back and internal to the development, in order to activate the streets with pedestrian amenities, public space, and ground floor mixed-uses.

d) Economic Vitality

Policy EV 1.1 *Develop and implement long-term economic development strategies that maintain and enhance city revenue streams in order to balance the area's revenue generation with the cost of services and ensure financial stability now and in the future.*

Response: The proposed work, live, play land use concept achieves this policy on several levels. The construction of new multi-family development will generate significant building permit fees and revenue for the City of Scottsdale as well as increased sales tax revenue for the City. Secondly, the integration of approximately 622 residential units in the Scottsdale Airpark area will

increase retail sales and sales tax revenue for the City. Lastly, the proposed residential community will provide on-site jobs for management and maintenance.

The integration of new development will enhance the City's revenue streams and provide financial benefits to Scottsdale businesses and Airpark. Residents and business owners require services, goods, food, and entertainment and draw from local businesses and resources. By creating a walkable environment that already has abundant retail and service opportunities nearby, a land use balance will be created which will strengthen the City's long-term economic stability. The Scottsdale Airpark as an economic engine generates hundreds of millions of dollars per year, and billions of dollars of spin-off financial benefits. The inclusion of this mixed use development will increase the money spent by residents and workers living, working, and playing within the Airpark. This project provides the activity and subsequent revenue to ensure long term financial stability and precedent for surrounding investment.

Policy EV 2.5 *Aggressively market the Greater Scottsdale Airpark as an ideal destination to work, live, and play.*

Response: The residential and commercial/retail components proposed under this application complements the existing mixed-use character of the Scottsdale Airpark of which is particularly found at and planned for at this intersection. The Airpark as a whole creates a unique opportunity to foster interrelated land uses and promote the live, work, and play concept, which is memorialized numerous times in the GACAP. The surrounding retail development, airport, and nearby employment core provide regional appeal for future residents. This application is driven by the Site's surrounding land uses and strong market demand for multi-family residential in this area. Business and housing trends indicate a fundamental shift away from traditional homeownership and business operations as a result of the growing tech generation (i.e. millennials) and their desire for true work/live experience an alternative mixed-use, high quality (i.e. design/tech oriented/services/amenities) development options fit their lifestyle.

e) Environmental Planning

Policy EP1.3 *Promote landscape design and irrigation methods that contribute to water and energy conservation.*

Response: The landscaping for the proposed development will be in accordance with the city approved landscape palette and blends with surrounding developments in a cohesive and coherent manner. With a judicious use of water as a design element, there has been an intentional preference to surround the project with native plants that show off the natural Sonoran beauty. The project will proudly showcase the combination of natural shapes, textures, and materials from this region and incorporate shade structures.

Policy EP 4.8 *Building design should respect and enhance the Sonoran Desert context of the Greater Airpark using building orientation, landscape buffers, colors, textures, material and lighting.*

Policy EP 5.4 *Encourage landscape improvements that limit the amount of turf area and make optimal use of indigenous adapted desert plants.*

Response: To further elaborate on the statements above, the buildings have been designed in a manner to respond to the Sonoran Desert climate through the use of shading, landscaping, recessed windows, articulation, material selection, textures, paint colors, scale and massing in balance with the surrounding community. The development proposal promotes a rich desert landscape palette in a contemporary theme that celebrates the unique character and quality of life of the Sonoran Desert while providing an attractive context appropriate setting for the buildings.

f) Character & Design

Policy CD 1.1 *Promote innovative, high quality design using specific design criteria associated with each Future Land Use Area in the Greater Airpark:*

Airpark Mixed Use Future Land Use Areas (AMU & AMU-R)

The character of these areas is pedestrian-oriented, urban, and human-scale and features a variety of open spaces, gathering areas, and multi-modal transportation options. Multi-modal transportation should include bicycle and transit access connected to a pedestrian network to encourage social contact and interaction among the community. Design elements should be oriented toward people, such as the provision of shelter and shade for the pedestrian, active land uses at the ground floor/ street level, and a variety of building forms and facade articulation to visually shorten long distances. A variety of textures and natural materials is encouraged to provide visual interest and richness, particularly at the pedestrian level. Design of this Future Land Use Area should be based on a small city block layout with mid-block connections to promote greater walkability. The public realm may be activated through building and site design, orientation toward the street, high-activity uses on the street level, and the integration of public art.

Response: The proposed development achieves this policy in the following ways:

- Mixed-use live, work, play philosophy enhanced with proposed combination of uses.
- Building design includes context appropriate massing, architecture and materials including pedestrian scale design at floor level.
- Special attention given to pedestrian linkages both internal to the Site and along the perimeter to emphasize connectivity.
- Compatibility with surrounding context.
- Site and building design focused on Sonoran Desert climate through the use of shading, recessed windows, articulation, material selection, textures, paint colors, scale and massing.

- Open space maximized; development provides abundant open space with approximately 10.3% (excluding street frontage area or parking lot landscaping) useable open space (i.e. 10% required) with approximately 20% (including all open space, street frontage, and parking landscape areas). This also includes dog walk/wash areas too.

In summation, there will be many gathering areas where people can come together in an environmentally friendly passive and/or active setting.

Policy CD 1.2 *Lighting should be designed to minimize glare, conserve energy, and accent the respective Future Land Use Area character.*

Response: The on-site lighting will be designed in a manner to minimize glare and conserve energy while respecting and remaining consistent with the surrounding land uses. One of the lighting goals will be to provide appropriate low-level pedestrian scale lighting (bollard and foot lighting) for pedestrians walking at night. The lighting will be integrated with the abundant desert landscaping proposed with this development.

Goal CD 2 *Create vibrant Signature Corridors in the Greater Airpark to provide a distinct identity and design theme in the area.*

Response: The Signature Corridors bordering this site are designed to create a comfortable, safe, and aesthetically pleasing streetscape with passive open spaces along the street frontage and anchored by an enhanced Landmark Intersection pedestrian area with public pedestrian amenities and open space. Various design measures are included such as shade, seating, bicycle parking, landscaping, and ground level live/work units along both Signature Corridors.

Policy CD 2.1.3 *Greenway-Hayden Signature Corridor (emphasis added)*

The Greenway-Hayden Signature Corridor should serve as the "Main Street" of the Greater Airpark, north of the runway, and should reference multi-modal connections from other cities to major area destinations, such as the Tournament Players Club Golf Course and Scottsdale Sports Complex. The public realm should animate the street during all seasons. Design elements could include large window displays, banners, integrated signage, passive cooling elements, covered patios, shade, public art, bicycle/pedestrian connections, and gathering areas.

Response: The proposed development will highlight Greenway-Hayden Loop as a Signature Corridor and provide design elements that cater to the pedestrian through the use of building character, design, signage, walkway connectivity, landscape separated sidewalks, hardscape and lighting. This corridor connection orients pedestrian towards the Landmark Intersection pedestrian areas and open space as well as the clubhouse and fitness center located at and designed for the ground level containing large glass windows and activity. This corridor streetscape is designed to cater to the pedestrian serving as an inviting connection to the surrounding areas and public spaces and amenities that provide shelter, shade, and a variety of

building articulation to visually shorten longer building expanses. Furthermore, the Signature Corridor streetscape will contain walkways and stoop elements for optional ground level live/work units from the sidewalk that further build upon community interaction. (See *Corner Landmark Intersection Enlargement within the Landscape Plan Set Exhibit # 6 & Airpark Signature Corridors Exhibit # 15*)

Policy CD 2.1.7 73rd Signature Corridor (emphasis added)

The 73rd Signature Corridor is a major pedestrian corridor in the Greater Airpark. Design elements along this street should include aviation themes and human-scale building orientation. Shade is an important element and may be incorporated through the built environment, as well as natural shading. Hardscape elements could include bollards, foot lighting, and sophisticated fencing that secure taxi lanes from pedestrian activity. Low-lying vegetation, such as vines and shrubs, are important landscape features that should soften the continuous building mass characteristics of land uses on the east side of the corridor.

Response: The proposed development will emphasize 73rd Street as a Signature Corridor and provide design elements that are pedestrian oriented through the use of building and pedestrian scaled design, shaded rest areas, signage, landscape separated walkways, live/work stoops, hardscape, and lighting that enhance the streetscape. This Signature Corridor provides a safe, comfortable, and aesthetically pleasing streetscape connection that encourages walking and orients pedestrians towards ground level uses, public spaces, and the nearby surrounding areas. A variety of building articulation visually shortens long building expanses. Additionally, the completion of this development and walkable streetscape will improve and encourage activity and mobility further along this north to south corridor as well as drawing pedestrians in and through the development. The walkways and stoop elements at the street level will reinforce and help to complete the street scene similar to the adjacent LIV development. The completion will promote pedestrian access to shopping, retail, and businesses to the south, and current and future destinations to the north such as the ultimate redevelopment of the Cracker-Jax site.

Policy CD 2.2 Signature Corridor streetscapes should provide continuity among adjacent uses through a comprehensive landscape design, including decorative paving, street furniture, public art, and integrated infrastructure improvements.

Response: The Signature Corridors included in this development are designed to create an enhanced streetscape theme featuring passive open spaces, shade, and decorative paving that provide continuity to the adjacent and surrounding uses and create a safe and comfortable experience.

Policy CD 2.3 In designated Signature Corridors, encourage pedestrian- and transit-oriented development, with parking and automobile access in the rear of the development, and short access paths to transit.

Response: This development is designed to create a pedestrian oriented environment at this location of two (2) Signature Corridors featuring streetscapes, automobile parking located internally (i.e. garages), and short and comfortable pathways to nearby transit.

Policy CD 2.4 *Incorporate multi-modal access along Signature Corridors.*

Response: With the addition of enhanced streetscapes and their ultimate connectivity to transit and surrounding area that promote walking, bicycle parking has also been included in the front of the development.

Policy CD 2.5 *Frame designated Landmark Intersections with prominent buildings, community landmarks, high quality architecture, and enhanced streetscape treatments to enhance Greater Airpark identity and assist with wayfinding.*

Response: The Landmark Intersection will be a prominent and memorable location framed by the ground level uses high quality architecture and enhancement of the Landmark Intersection public open space features and amenities that assist in wayfinding/placemaking and contribute to the identity of the immediate intersection and area.

Policy CD 2.6 *Where Signature Corridors intersect, and particularly at designated Landmark Intersections, incorporate distinct, landmark architecture, which incorporates elements of the intersecting design themes.*

Response: The proposal includes an enhanced and distinctive public pedestrian area at the Landmark Intersection that includes decorative paving, street furniture, shade, seating, and pedestrian level uses and distinctive architecture incorporating elements of the intersecting design themes. The Landmark Intersection will be a safe, comfortable, and distinctive which will be a memorable and pleasing addition to the area.

VII. PUD Criteria

Section 5.5003 of the Zoning Ordinance states that the development proposals shall comply with the following criteria:

A. PUD Zoning District Approval Criteria,

1. *As part of the approval or modified approval of an application for a PUD district, the Planning Commission shall recommend and the City Council shall find that the following criteria have been met:*
 - A. *The proposed development promotes revitalization, the goals, policies and guidelines of the General Plan, Area Plans and Design Guidelines.*

Response: The proposed development accomplishes a range of goals including the public benefit of the revitalization of two (2) Signature Corridors streetscapes and Landmark Intersection/ "gateway", integrating high quality, vibrant architecture and site planning to the area, and creating pedestrian synergy that will complement the surrounding area. The proposed development meets and furthers the goals and policies of the General Plan and GACAP as discussed in this narrative. For example, the development of enhanced common open spaces of the Landmark Intersection and streetscapes of the Site.

B. The proposed development's uses, densities, or development standards would not otherwise be permitted by the property's existing zoning.

Response: The proposed development would not be permitted under the existing I-1 zoning designation.

C. The proposed development will be compatible with adjacent land uses and promotes the stability and integrity of abutting or adjacent residential neighborhoods.

Response: The proposed development is compatible with adjacent land uses and maintains the integrity of the Airpark employment core by providing a balance between residential and employment uses. Adjacent multi-family residential to the project is compatible and of similar character.

D. That there is adequate infrastructure and City services to serve the development.

Response: There are adequate infrastructure and City services to serve the development.

E. That the proposal meets the following location criteria:

i. The proposed development is not located within any areas zoned environmentally sensitive lands ordinance (ESL) nor within the boundaries of the Downtown Plan.

Response: The property is not located within the ESL area or within the boundaries of the Downtown Plan.

ii. The proposed development fronts onto a major or minor arterial and/or major collector street as designated in the City's transportation master plan.

Response: The proposed development fronts Greenway-Hayden Loop, a minor arterial and 73rd Street a minor collector.

VIII. Conclusion

In summary, the applicant is seeking a Minor GPA and rezoning on an approximately 10.29+/- gross acre and 8.83+/- net acre site located at 15501 North 73rd Street to create a unique luxury multi-family residential and mixed use community with approximately 622 units and 22,025 sq. ft. commercial/ retail (i.e. non-residential) use to enhance upon the work, live, and play environment encouraged within this area of the Greater Airpark Character Area Plan.

As mentioned above, workers, millennials, and professionals alike desire a work/live lifestyle option that is different than a traditional workplace and household environment. One that affords them a flexible schedule live, work, and play concept like the cutting edge experience proposed for District at the Quarter; a development that promotes a mix of land uses, walkability/bike riding, reduced trip generation, environmental responsibility, amenities, and professional synergy.

The proposed mix of uses will enhance the local and regional economic base. The property is surrounded by a variety of employment, recreation, entertainment, office, instructional/education and service related business in the Scottsdale Airpark and there is a strong demand for this unique work/live concept.

This is a very unique and exciting project that will not only be a success, but will exemplify the vision that the city of Scottsdale hopes to achieve in the area.

EXHIBITS

Exhibit 1

Dimensioned Plan Boundaries

District at the Quarter – Proposed Boundary Dimensions

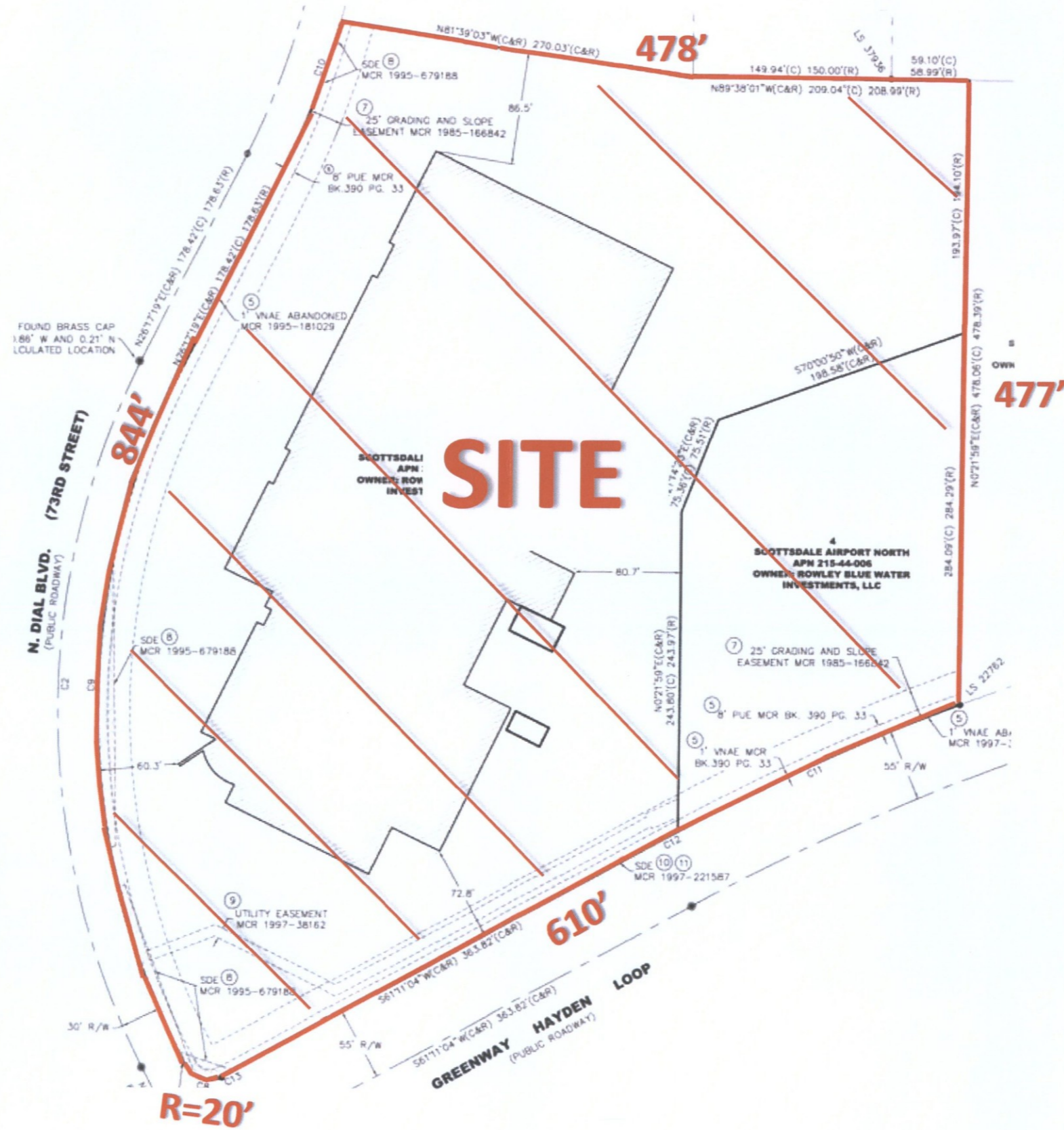


Exhibit 2

Aerial

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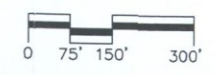


DISTRICT AT THE QUARTER 15510 N. 73RD STREET SCOTTSDALE, MARICOPA COUNTY, ARIZONA	
AERIAL EXHIBIT	
SHEET EX-1 1 OF 1	NO. DATE REVISION

512.669-5560
BIG RED DOG
 ENGINEERING | CONSULTING
 2001 E. 5TH STREET SCOTTSDALE, ARIZONA 85251
 ARIZONA REGISTRATION NO. 19744
 ENGINEERING REG. NO. 79702
 ARCHITECTURE REG. NO. 19744

Exhibit 3

Context Aerial



CONTEXT
SITE PLAN **A1.2**
GRAPHIC SCALE

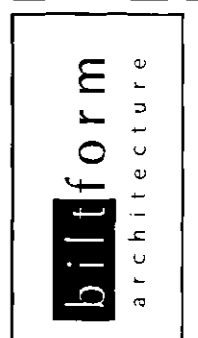
8-03-16

biliform
architecture

DISTRICT AT THE QUARTER
KAPLAN ACQUISITIONS, LLC

Exhibit 4

Site Plan



biltform architecture group, inc.
11450 north central express (loop 1)
phoenix, arizona 85029
Phone 602.285.9200 Fax 602.285.9220

PRELIMINARY
NOT FOR
CONSTRUCTION
EXPIRES 9/30/19

DISTRICT AT THE QUARTER
Apartment Homes
GREENWAY/HAYDEN LOOP AND DIAL BOULEVARD SCOTTSDALE, ARIZONA
KAPLAN ACQUISITIONS, LLC
7150 E. CAMELBACK ROAD, SUITE 444 SCOTTSDALE, AZ 85251
PHONE: 480-477-8119

90%
PROGRESS SET
REVISIONS:
JOB NO: 16-024
SCALE: 1"=40'-0"
SHEET NO:
A1.1

PROJECT DATA: PHASE 1

OVERALL SITE DATA:

SITE AREA: 45.12 NET ACRES
16.23 GROSS ACRES

ZONING: EXISTING: I-1
PROPOSED: PUD
APN NO'S: 215-44-006 / 215-44-007
NOT A PART

PROPOSED USE: MULTI-FAMILY

MAXIMUM DENSITY ALLOWED: N/A

DENSITY: 332 D.U./6.23 AC = 53.3 D.U. PER ACERS

MAXIMUM BUILDING HEIGHT: 48 FEET/58 FEET TO MECH.

PROPOSED BUILDING HEIGHT: 48 FEET(4 STORIES)/58 FEET TO MECH.

OPEN SPACE REQUIRED 10%: 6.34 ACRES = 27,160 S.F.

OPEN SPACE PROVIDED: 28,157 S.F.

TOTAL AREA: 536,604 SQ. FT.

TOTAL GROSS BUILDING AREA: 307,119 SQ. FT.

TOTAL GROSS LIVABLE AREA (RES): 252,365 SQ. FT.

TOTAL GROSS COMMERCIAL AREA - (CLUBHOUSE, RESTAURANT, OFFICE, FITNESS CLUB): 166,662 SQ. FT.

TOTAL GROSS GARAGE AREA: 166,662 SQ. FT.

(PHASE 1) UNIT MIX:

ONE BEDROOM UNITS:	192 (57.8%)
TWO BEDROOM UNITS:	140 (42.2%)
TOTAL:	332 D.U.

UNIT TYPE	AREA LIVABLE	OPEN SPACE	UNITS	GROSS AREA
UNIT A1	1 BED/1 BA	655 S.F.	101	50,305 S.F.
UNIT A2 (CLUB)	1 BED/1 BA	742 S.F.	240	178,080 S.F.
UNIT A2 (CLUB)	1 BED/1 BA	701 S.F.	101	70,801 S.F.
UNIT A2 ALT.	1 BED/1 BA	826 S.F.	245	202,370 S.F.
UNIT A3 (CLUB)	1 BED/1 BA	969 S.F.	78	75,582 S.F.
UNIT A3 (CLUB)	1 BED/1 BA	750 S.F.	95	71,250 S.F.
UNIT A3 ALT.	1 BED/1 BA	913 S.F.	244	222,732 S.F.
UNIT A4 (CLUB)	1 BED/1 BA	802 S.F.	88	70,576 S.F.
UNIT A4 ALT.	1 BED/1 BA	965 S.F.	244	235,420 S.F.
UNIT A4 (CLUB)	1 BED/1 BA	851 S.F.	76	64,676 S.F.
UNIT A4 ALT.	1 BED/1 BA	883 S.F.	76	67,028 S.F.
UNIT A4 ALT.	1 BED/1 BA	877 S.F.	76	66,852 S.F.
UNIT A5 (CLUB)	1 BED/1 BA	816 S.F.	246	200,736 S.F.
UNIT A5 ALT.	1 BED/1 BA	821 S.F.	116	95,316 S.F.
UNIT A5 ALT.	1 BED/1 BA	882 S.F.	267	237,534 S.F.
UNIT A6	1 BED/1 BA	766 S.F.	69	52,854 S.F.

TOTAL: 332 D.U. 307,119 S.F. 929 P.S.

(PHASE 1) TOTAL PARKING:

REQUIRED: TABLE 9.103
1 BEDROOM OR LESS: 182 X 1.3 = 236
2 BEDROOMS OR MORE: 140 X 1.7 = 238
TOTAL REQUIRED: 474 P.S.

RESTAURANT: 1 SPACE PER 325 S.F. 7,035/325 = 22
OFFICE/FLEX: 1 SPACE PER 325 S.F. 1,781/325 = 5
FITNESS CENTER (30%): 1 SPACE PER 325 S.F. 1,806/325 = 5
TOTAL REQUIRED: 32

PROVIDED:
GARAGE PARKING SPACE REQUIRED: 520 P.S.
GARAGE PARKING (COVERED): 525 P.S.

TOTAL PROVIDED: 525 P.S.

ACCESSIBLE PARKING REQUIRED: 4% X 525 P.S. = 21 P.S.
COVERED: 21 P.S.

BICYCLE PARKING: (1) SPACES PER 10 VEHICAL SPACES. SEC. 9.103
(486'0" X 48.8') 49 SPACES PROVIDED

PROJECT DATA: PHASE 2

OVERALL SITE DATA:

SITE AREA: 43.72 NET ACRES
14.06 GROSS ACRES

ZONING: EXISTING: I-1
PROPOSED: PUD
APN NO'S: 215-44-006 / 215-44-007
NOT A PART

PROPOSED USE: MULTI-FAMILY

MAXIMUM DENSITY ALLOWED: N/A

DENSITY: 290 D.U./4.06 AC = 71.4 D.U. PER ACERS

MAXIMUM BUILDING HEIGHT: 48 FEET/58 FEET TO MECH.

PROPOSED BUILDING HEIGHT: 48 FEET(4 STORIES)/58 FEET TO MECH.

OPEN SPACE REQUIRED 10%: 4.395 ACRES = 17,670 S.F.

OPEN SPACE PROVIDED: 17,957 S.F.

TOTAL AREA: 439,744 SQ. FT.

TOTAL GROSS BUILDING AREA: 252,365 SQ. FT.

TOTAL GROSS LIVABLE AREA (RES): 155,850 SQ. FT.

TOTAL GROSS GARAGE AREA: 155,850 SQ. FT.

(PHASE 2) UNIT MIX:

ONE BEDROOM UNITS:	187 (64%)
TWO BEDROOM UNITS:	103 (36%)
TOTAL:	290 D.U.

UNIT TYPE	AREA LIVABLE	OPEN SPACE	UNITS	GROSS AREA
UNIT A1	1 BED/1 BA	655 S.F.	101	50,305 S.F.
UNIT A2	1 BED/1 BA	701 S.F.	101	70,801 S.F.
UNIT A3	1 BED/1 BA	750 S.F.	95	71,250 S.F.
UNIT A4	1 BED/1 BA	831 S.F.	76	63,276 S.F.
UNIT A5	1 BED/1 BA	755 S.F.	112	84,560 S.F.
UNIT A6	1 BED/1 BA	766 S.F.	69	52,854 S.F.
UNIT A7	1 BED/1 BA	973 S.F.	159	154,667 S.F.
UNIT B1	2 BED/2 BA	1,060 S.F.	84	89,040 S.F.
UNIT B2	2 BED/2 BA	1,130 S.F.	75	84,750 S.F.
UNIT B3	2 BED/2 BA	1,294 S.F.	88	113,872 S.F.
UNIT B4	2 BED/2 BA	1,131 S.F.	125	141,375 S.F.

TOTAL: 290 D.U. 252,365 S.F. 870 S.F.

(PHASE 2) TOTAL PARKING:

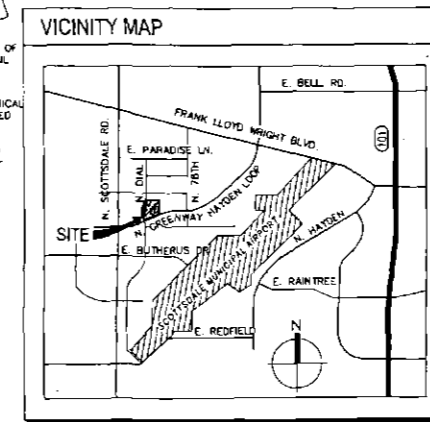
REQUIRED: TABLE 9.103
1 BEDROOM OR LESS: 187 X 1.3 = 243
2 BEDROOMS OR MORE: 103 X 1.7 = 175
TOTAL REQUIRED: 418 P.S.

PROVIDED:
GARAGE PARKING (COVERED): 477 P.S.

TOTAL PROVIDED: 477 P.S.

ACCESSIBLE PARKING REQUIRED: 4% X 477 P.S. = 19 P.S.
COVERED: 19 P.S.

BICYCLE PARKING: (1) SPACES PER 10 VEHICAL SPACES. SEC. 9.103
(393'1" X 39.3') 40 SPACES PROVIDED



CURVE TABLE

CURVE	DELTA	RADIUS	LENGTH	CHORD
C8	00°40'17"(C)	91'29"(R)	31.85'(C)	31.94'(R)
C9	05°16'10"(C&R)	570.00'(C&R)	528.84'(C&R)	507.46'(C) 511.00'(R)
C10	01°05'42"(C)	630.00'(C&R)	122.00'(C&R)	120.44'28"(C) 121.81'(R)
C11	07°15'47"(C)	1895.00'(C&R)	235.15'(C) 235.19'(R)	365'23'59"(C) 234.99'(R)
C12	00°33'01"(C)	1853.00'(C&R)	17.87'(C&R)	361'29'76"(C) 17.76'(R)
C13	02°20'47"(C)	1943.00'(C&R)	11.76'(C&R)	161'21'27"(C) 11.76'(R)

NORTH

0 20 40 60

PROJECT DATA: PHASE 1 AND 2

OVERALL SITE DATA:

SITE AREA: 88.84 NET ACRES
30.29 GROSS ACRES

ZONING: EXISTING: I-1
PROPOSED: PUD
APN NO'S: 215-44-006 / 215-44-007
NOT A PART

PROPOSED USE: MULTI-FAMILY

MAXIMUM DENSITY ALLOWED: N/A

DENSITY: 60.4 D.U./GROSS AC

MAXIMUM BUILDING HEIGHT: 48 FEET/58 FEET TO MECH.

PROPOSED BUILDING HEIGHT: 48 FEET(4 STORIES)/58 FEET TO MECH.

OPEN SPACE PROVIDED: 46,114 S.F./10.28%

TOTAL AREA: 332 D.U. 307,119 S.F.

PHASE 2 UNITS: 290 D.U. 252,365 S.F.

TOTAL UNITS: 622 D.U. 559,331 S.F.

TOTAL GROSS COMMERCIAL AREA - (CLUBHOUSE, RESTAURANT, OFFICE, FITNESS CLUB): 166,662 SQ. FT.

TOTAL GROSS GARAGE AREA: 166,662 SQ. FT.

TOTAL PARKING:
REQUIRED: 1,002 P.S.
GARAGE PARKING: 1,002 P.S.

TOTAL PROVIDED: 1,002 P.S.

SHEET NOTES:
(a) UNOBSTRUCTED VERTICAL CLEARANCE MIN. 13'-6" (ORD. 4045, 503.2.1)
(b) KEY SWITCH/EMERGENCY SENSOR REQUIRED. (ORD. 4045, 503.2.1)
(c) FIRE LANE SURFACE WALL SUPPORT 83,000 LBS QVM. (OSAPM. 2-1.802.3)
(d) NO FENCE/WALL OVER 6' IN HEIGHT.

**ARCHITECTURAL
SITE PLAN**
1" = 40'-0"

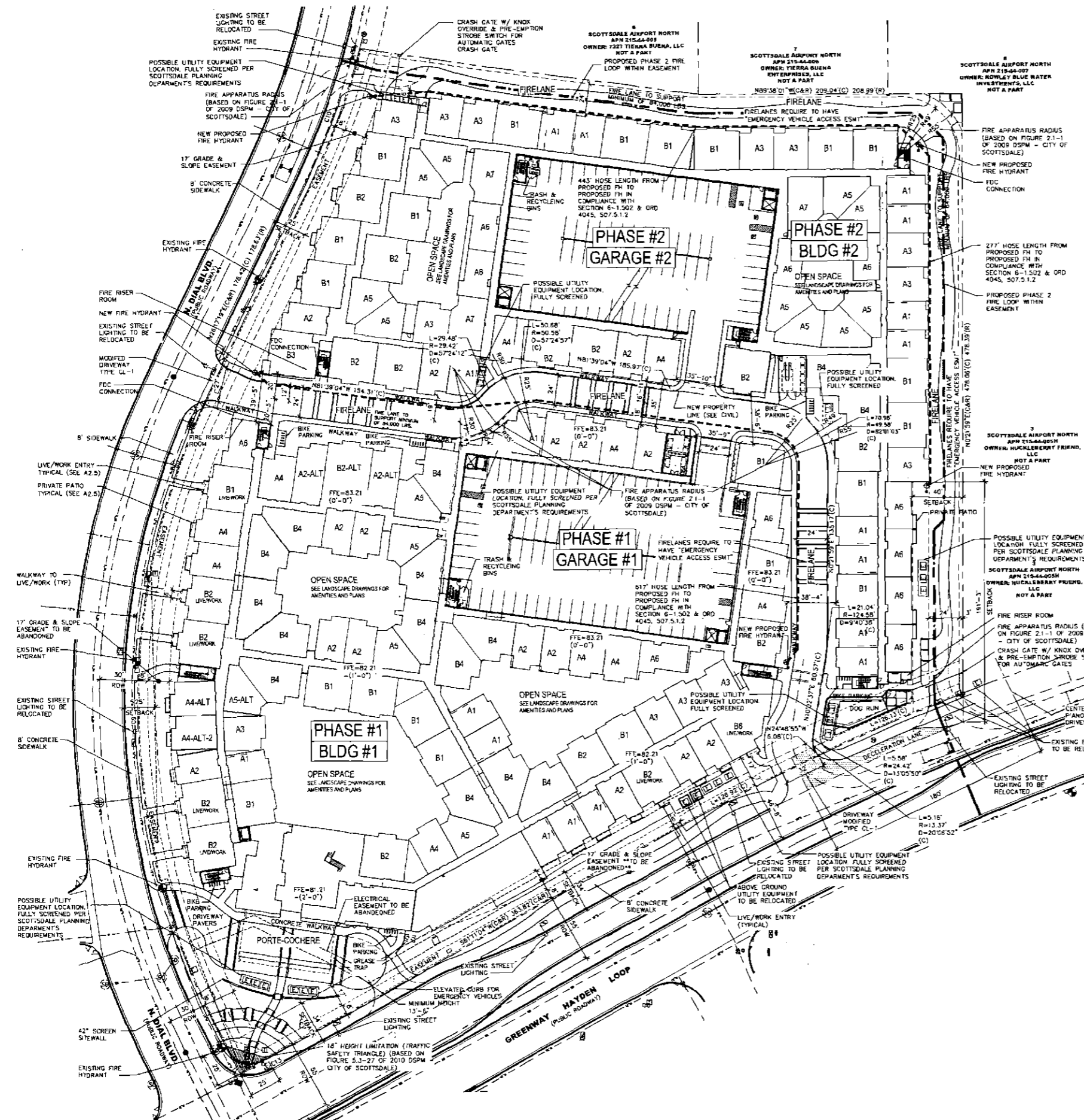


Exhibit 5

Architectural Plan Set



DISTRICT AT THE QUARTER

KAPLAN ACQUISITIONS, LLC

PERSPECTIVE
CORNER OF DIAL BLVD AND GREENWAY HAYDEN LOOP
NOT TO SCALE

A2.1
8-03-16



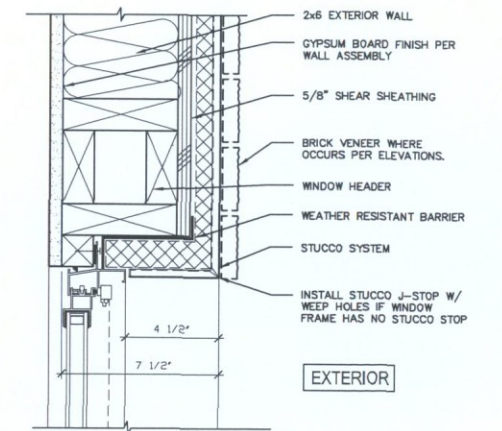
biltform
architecture

DISTRICT AT THE QUARTER

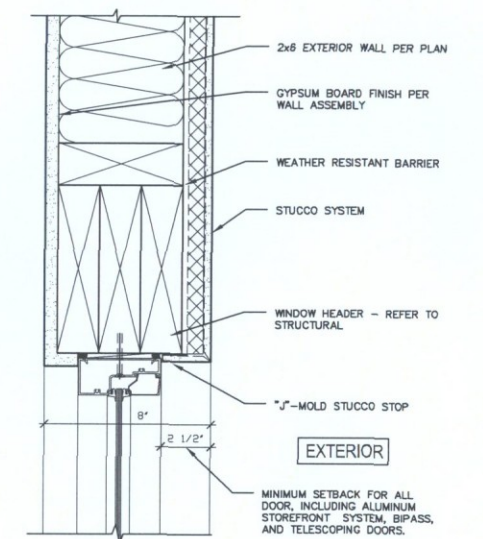
KAPLAN ACQUISITIONS, LLC

Perspective
DIAL BOULEVARD
NOT TO SCALE

A2.2
8-03-16



RECESSED WINDOW HEADER
 3"=1'-0"



RECESSED DOOR HEADER
 3"=1'-0"

NOTES
 ALL FRAME-OUTS WALL PROJECTIONS ARE TAKEN FROM CORRESPONDING ELEVATION PLANE MARKED AS +0.

TYPICAL ELEVATION WORK SHEET
 DIAL BOULEVARD
 NOT TO SCALE

A2.3

8-03-16

biltform
 architecture



DISTRICT AT THE QUARTER
 KAPLAN ACQUISITIONS, LLC



TYPICAL UNITS ON SOUTH SIDE
SOLAR SHADING - JUNE - 21ST (SOLAR NOON)



LOCATION OF ENLARGED AREAS OF SOUTH ELEVATION

DISTRICT AT THE QUARTER

KAPLAN ACQUISITIONS, LLC

NOTES
ALL FRAME-OUTS WALL PROJECTIONS ARE TAKEN FROM
CORRESPONDING ELEVATION PLANE MARKED AS +0.

TYPICAL ELEVATION WORK SHEET
DIAL BOULEVARD
NOT TO SCALE

A2.4

8-03-16





LIVE WORK UNIT PATIO ALONG GREENWAY-HAYDEN AND DIAL BLVD. WITH ACCESS TO MAIN SIDEWALK.

TYPICAL BUILDING ENTRY ALONG GREENWAY-HAYDEN AND DIAL BLVD.



LOCATION OF ENLARGED AREAS OF EAST ELEVATION

NOTES
ALL FRAME-OUTS WALL PROJECTIONS ARE TAKEN FROM CORRESPONDING ELEVATION PLANE MARKED AS +0.

TYPICAL ELEVATION WORK SHEET
DIAL BOULEVARD
NOT TO SCALE

A2.5

8-03-16

DISTRICT AT THE QUARTER
KAPLAN ACQUISITIONS, LLC

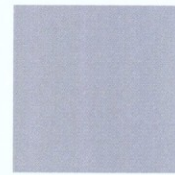
biliform
architecture

COOL DECEMBER
DUNN EDWARDS - DEW383
BODY COLOR

ALMOND
DUNN EDWARDS - DEC753
BODY COLOR

COVERED IN PLATINUM
DUNN EDWARDS - DE6367
BODY COLOR

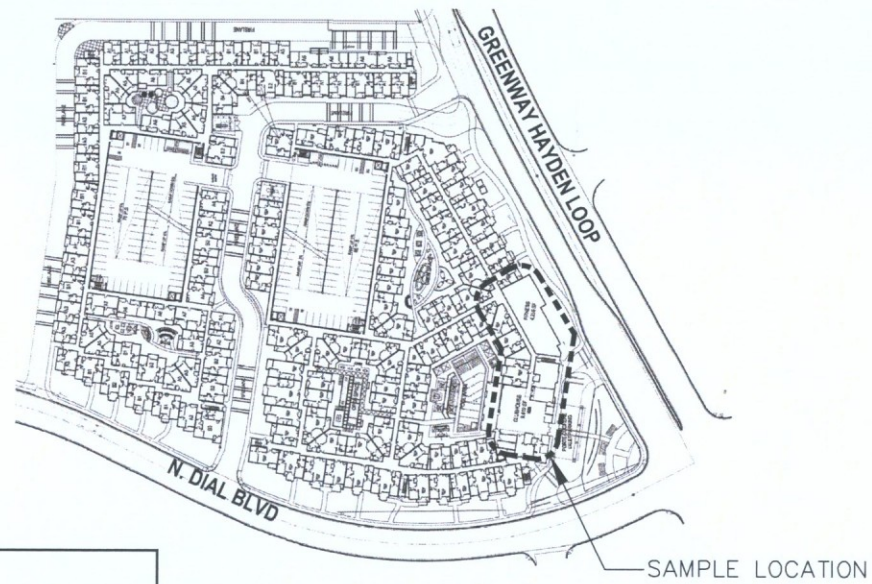
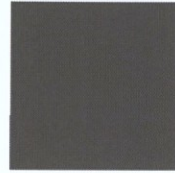
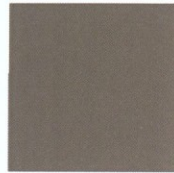
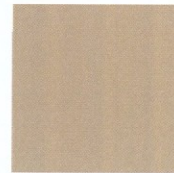
COUNTRY BEIGE
FRENCH LIMESTONE
CORONADO STONE



HICKORY
DUNN EDWARDS - DEC759
CORNICHE

COCOA POWDER
DUNN EDWARDS - DET631
RAILINGS/DOORS/CANOPIES

BRONZE
WINDOW/STOREFRONT FRAMES



HICKORY
ALMOND
HICKORY
BRONZE
COOL DECEMBER
COUNTRY BEIGE
HICKORY



HICKORY
ALMOND
COOL DECEMBER
COCOA POWDER
COOL DECEMBER
COOL DECEMBER
COVERED IN PLATINUM
ALMOND
COUNTRY BEIGE
BRONZE



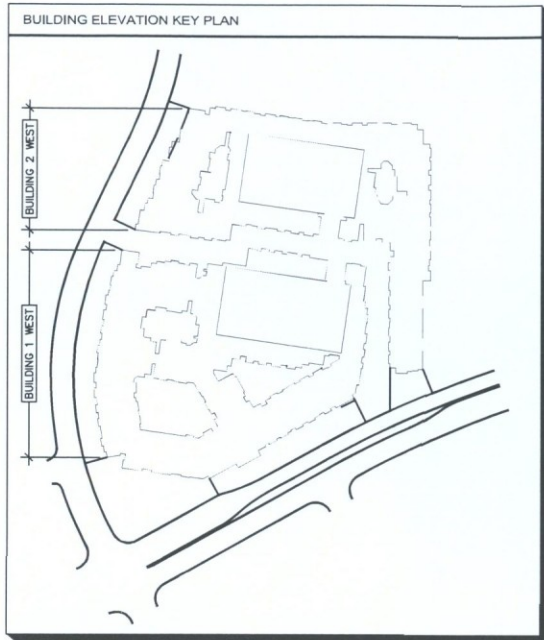
DISTRICT AT THE QUARTER

KAPLAN ACQUISITIONS, LLC

BUILDING COLOR AND MATERIALS
DIAL BOULEVARD
NOT TO SCALE

A2.6

8-03-16



- KEY NOTES**
1. STUCCO FINISH
 2. 8" DEEP STUCCO FINISH EYEBROW - SHADING DEVICE.
 3. SOLARIUM UNIT AT THE 4TH FLOOR.
 4. METAL SHADE CANOPY - SHADING DEVICE.
 5. DECORATIVE METAL RAILING +42" A.F.F.
 6. PATIO WALL WITH STUCCO FINISH
 7. STUCCO CONTROL JOINT
 8. WINDOW UNIT, RECESSED 2" MINIMUM.
 9. FRENCH DOOR.
 10. FOLDING OR TELESCOPING DOOR SYSTEM.
 11. STOREFRONT GLAZING.
 12. METAL PARAPET CAP - 42" MINIMUM, HEIGHT MEASURED FROM FINISHED ROOF DECK.
 13. METAL DOOR - PAINTED ACCENT COLOR.
 14. STAIR BULKHEAD.
 15. SLIDING GLASS PATIO DOOR.
 16. TOP OF ROOF DECK BEYOND @ +46'-0".
 17. 6" WALL POP-OUT.
 18. ROOF TOP DECK.
 19. PROJECT SIGNAGE.
 20. 4" STUCCO POP-OUT WINDOW SURROUND.
 21. EXTERIOR LIGHT
 22. LIVE / WORK ENTRY



BUILDING #2 - WEST ELEVATION - DIAL BOULEVARD



BUILDING #1 - WEST ELEVATION - DIAL BOULEVARD

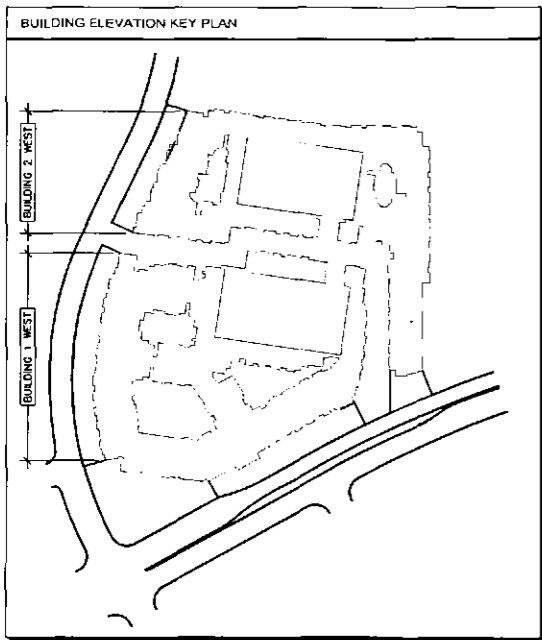


DISTRICT AT THE QUARTER
KAPLAN ACQUISITIONS, LLC

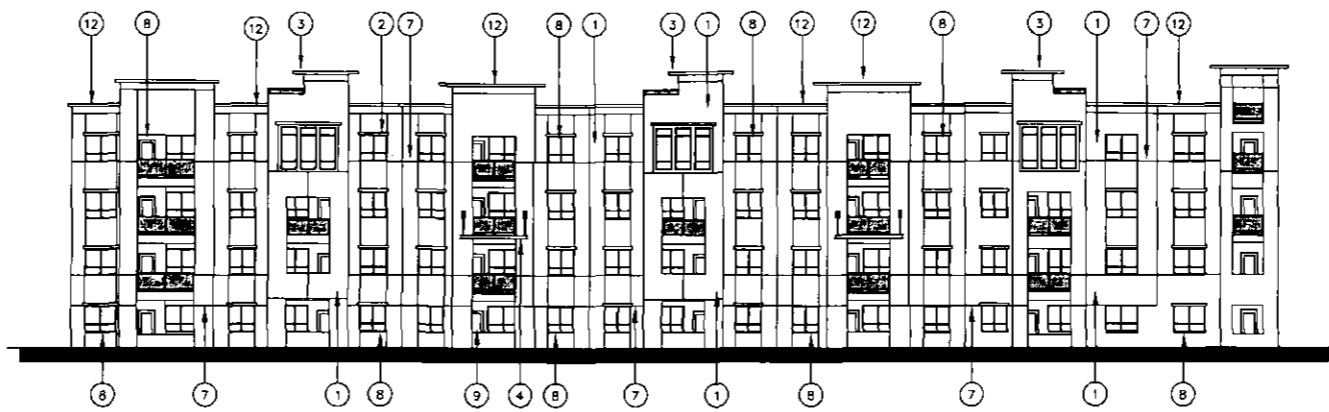


BUILDING ELEVATIONS
BUILDING #1 ELEVATIONS
1/16" = 1'-0"

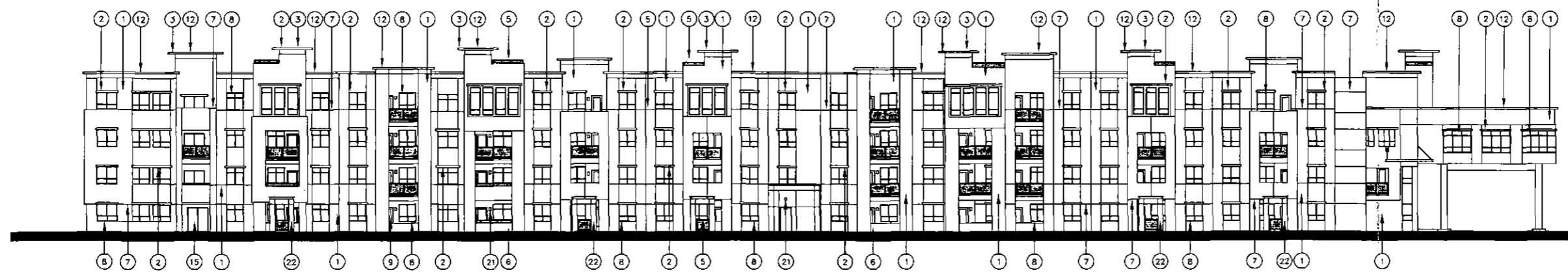
A7.1
8-03-16



- KEY NOTES**
1. STUCCO FINISH
 2. 8" DEEP STUCCO FINISH EYEBROW - SHADING DEVICE.
 3. SOLARIUM UNIT AT THE 4TH FLOOR.
 4. METAL SHADE CANOPY - SHADING DEVICE.
 5. DECORATIVE METAL RAILING +42" A.F.F.
 6. PATIO WALL WITH STUCCO FINISH
 7. STUCCO CONTROL JOINT
 8. WINDOW UNIT, RECESSED 2" MINIMUM.
 9. FRENCH DOOR.
 10. FOLDING OR TELESCOPING DOOR SYSTEM.
 11. STOREFRONT GLAZING.
 12. METAL PARAPET CAP - 42" MINIMUM, HEIGHT MEASURED FROM FINISHED ROOF DECK.
 13. METAL DOOR - PAINTED ACCENT COLOR.
 14. STAIR BULKHEAD.
 15. SLIDING GLASS PATIO DOOR.
 16. TOP OF ROOF DECK BEYOND $\odot +46'-0"$.
 17. 6" WALL POP-OUT.
 18. ROOF TOP DECK.
 19. PROJECT SIGNAGE.
 20. 4" STUCCO POP-OUT WINDOW SURROUND.
 21. EXTERIOR LIGHT
 22. LIVE / WORK ENTRY



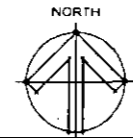
BUILDING #2 - WEST ELEVATION - DIAL BOULEVARD



BUILDING #1 - WEST ELEVATION - DIAL BOULEVARD

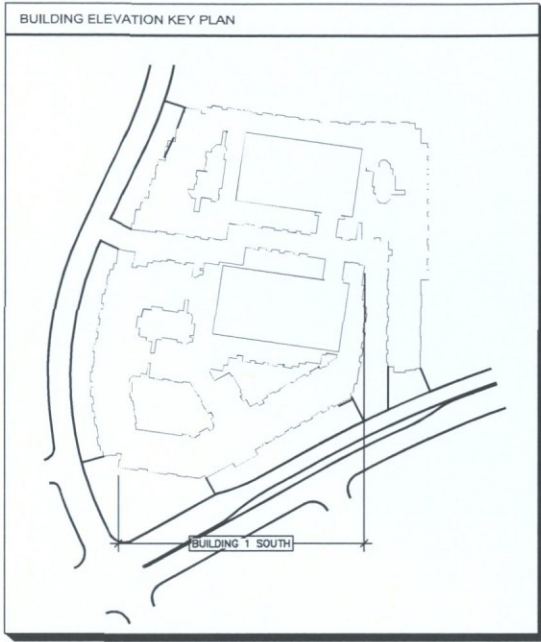


DISTRICT AT THE QUARTER
KAPLAN ACQUISITIONS, LLC



BUILDING ELEVATIONS
 BUILDING #1 ELEVATIONS
 1/16" = 1'-0"

A7.2
 8-03-16



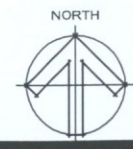
- KEY NOTES**
1. STUCCO FINISH
 2. 8" DEEP STUCCO FINISH EYEBROW - SHADING DEVICE.
 3. SOLARIUM UNIT AT THE 4TH FLOOR.
 4. METAL SHADE CANOPY - SHADING DEVICE.
 5. DECORATIVE METAL RAILING +42" A.F.F.
 6. PATIO WALL WITH STUCCO FINISH
 7. STUCCO CONTROL JOINT
 8. WINDOW UNIT, RECESSED 2" MINIMUM.
 9. FRENCH DOOR.
 10. FOLDING OR TELESCOPING DOOR SYSTEM.
 11. STOREFRONT GLAZING.
 12. METAL PARAPET CAP - 42" MINIMUM, HEIGHT MEASURED FROM FINISHED ROOF DECK.
 13. METAL DOOR - PAINTED ACCENT COLOR.
 14. STAIR BULKHEAD.
 15. SLIDING GLASS PATIO DOOR.
 16. TOP OF ROOF DECK BEYOND @ +46'-0".
 17. 6" WALL POP-OUT.
 18. ROOF TOP DECK.
 19. PROJECT SIGNAGE.
 20. 4" STUCCO POP-OUT WINDOW SURROUND.
 21. EXTERIOR LIGHT
 22. LIVE / WORK ENTRY



BUILDING #1 - SOUTH ELEVATION - GREENWAY HAYDEN LOOP

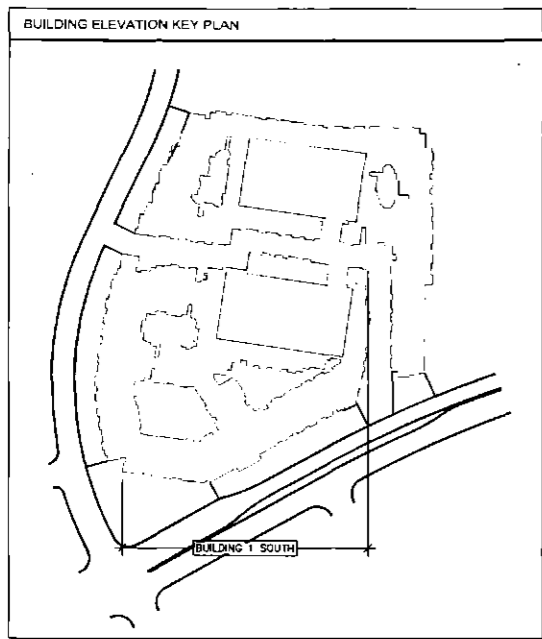


DISTRICT AT THE QUARTER
KAPLAN ACQUISITIONS, LLC



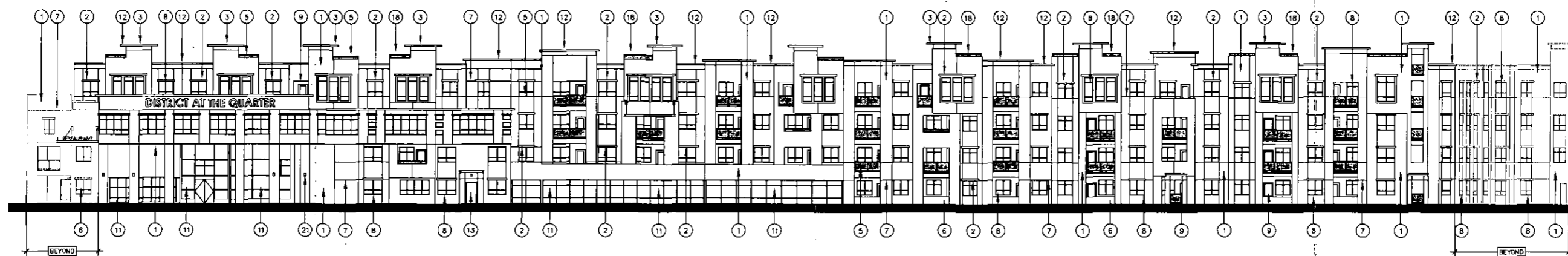
BUILDING ELEVATIONS
BUILDING #1 ELEVATIONS
1/16" = 1'-0"

A7.3
8-03-16

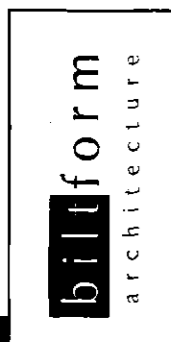


KEY NOTES

1. STUCCO FINISH
2. 8" DEEP STUCCO FINISH EYEBROW - SHADING DEVICE.
3. SOLARIUM UNIT AT THE 4TH FLOOR.
4. METAL SHADE CANOPY - SHADING DEVICE.
5. DECORATIVE METAL RAILING +42" A.F.F.
6. PATIO WALL WITH STUCCO FINISH
7. STUCCO CONTROL JOINT
8. WINDOW UNIT, RECESSED 2" MINIMUM.
9. FRENCH DOOR.
10. FOLDING OR TELESCOPING DOOR SYSTEM.
11. STOREFRONT GLAZING.
12. METAL PARAPET CAP - 42" MINIMUM, HEIGHT MEASURED FROM FINISHED ROOF DECK.
13. METAL DOOR - PAINTED ACCENT COLOR.
14. STAIR BULKHEAD.
15. SLIDING GLASS PATIO DOOR.
16. TOP OF ROOF DECK BEYOND +46'-0".
17. 6" WALL POP-OUT.
18. ROOF TOP DECK.
19. PROJECT SIGNAGE.
20. 4" STUCCO POP-OUT WINDOW SURROUND.
21. EXTERIOR LIGHT
22. LIVE / WORK ENTRY



BUILDING #1 - SOUTH ELEVATION - GREENWAY HAYDEN LOOP



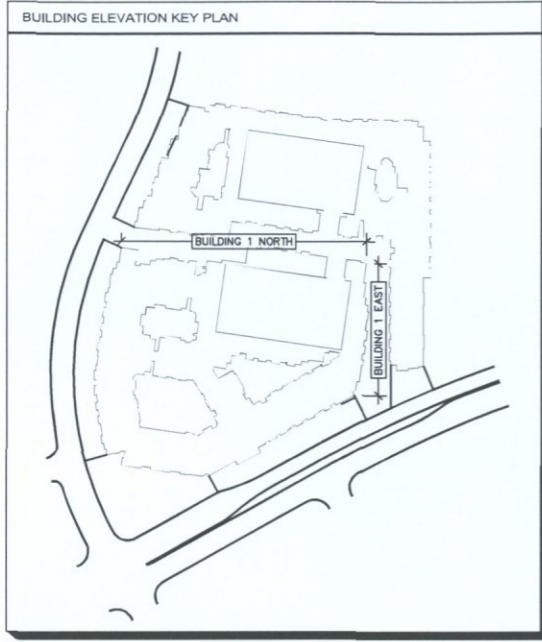
DISTRICT AT THE QUARTER
KAPLAN ACQUISITIONS, LLC



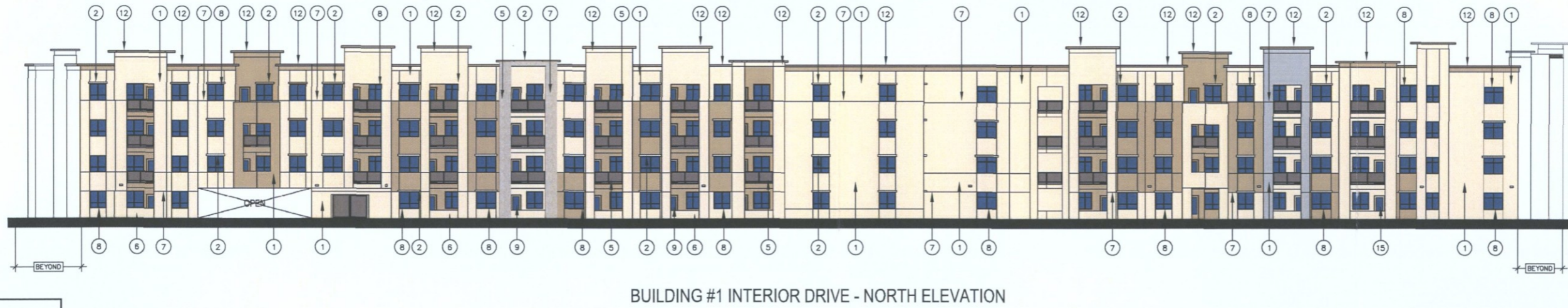
BUILDING ELEVATIONS
 BUILDING #1 ELEVATIONS

A7.4
 8-03-16

1/16" = 1'-0"



- KEY NOTES**
1. STUCCO FINISH
 2. 8" DEEP STUCCO FINISH EYEBROW - SHADING DEVICE.
 3. SOLARIUM UNIT AT THE 4TH FLOOR.
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 13. METAL DOOR - PAINTED ACCENT COLOR.
 14. STAIR BULKHEAD.
 15. SLIDING GLASS PATIO DOOR.
 16. TOP OF ROOF DECK BEYOND @ +46'-0".
 17. 6" WALL POP-OUT.
 18. ROOF TOP DECK.
 19. PROJECT SIGNAGE.
 20. 4" STUCCO POP-OUT WINDOW SURROUND.
 21. EXTERIOR LIGHT
 22. LIVE / WORK ENTRY

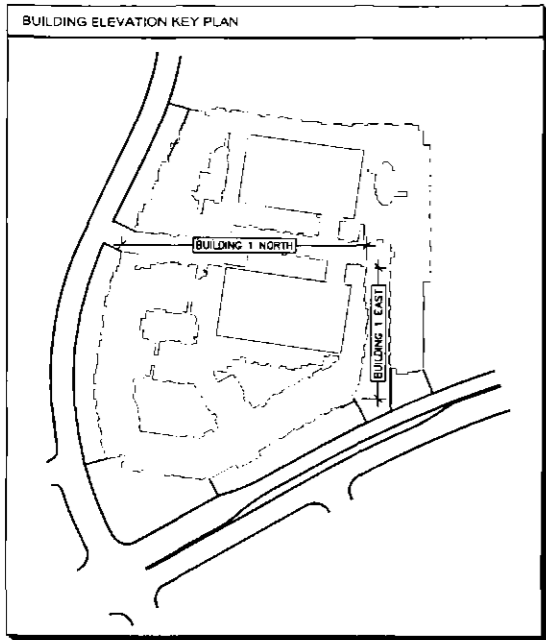


DISTRICT AT THE QUARTER
KAPLAN ACQUISITIONS, LLC

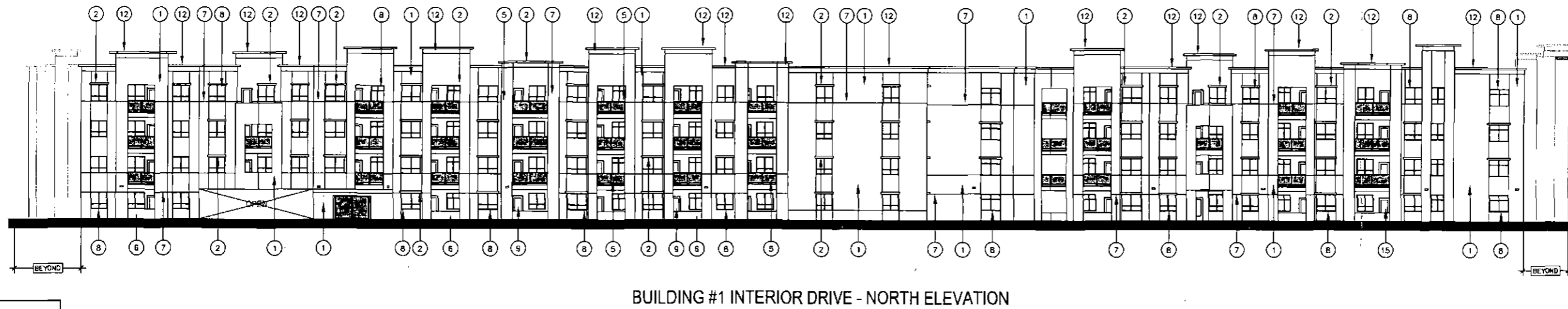
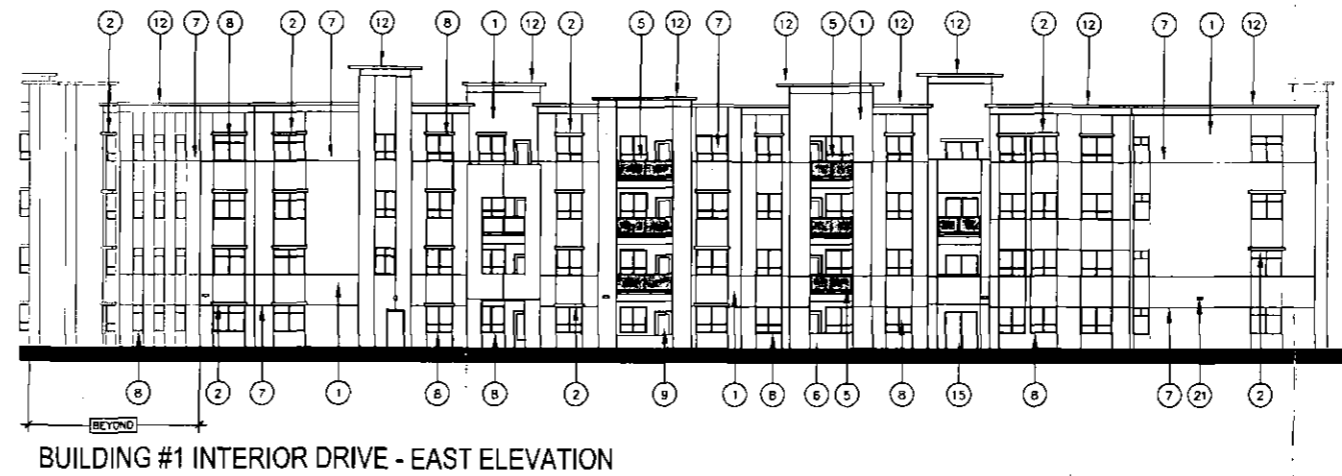


BUILDING ELEVATIONS
BUILDING #1 ELEVATIONS
 1/16" = 1'-0"

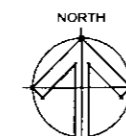
A7.5
 8-03-16



- KEY NOTES**
1. STUCCO FINISH
 2. 8" DEEP STUCCO FINISH EYEBROW - SHADING DEVICE.
 3. SOLARIUM UNIT AT THE 4TH FLOOR.
 4. METAL SHADE CANOPY - SHADING DEVICE.
 5. DECORATIVE METAL RAILING +42" A.F.F.
 6. PATIO WALL WITH STUCCO FINISH
 7. STUCCO CONTROL JOINT
 8. WINDOW UNIT, RECESSED 2" MINIMUM.
 9. FRENCH DOOR.
 10. FOLDING OR TELESCOPING DOOR SYSTEM.
 11. STOREFRONT GLAZING.
 12. METAL PARAPET CAP - 42" MINIMUM, HEIGHT MEASURED FROM FINISHED ROOF DECK.
 13. METAL DOOR - PAINTED ACCENT COLOR.
 14. STAIR BULKHEAD.
 15. SLIDING GLASS PATIO DOOR.
 16. TOP OF ROOF DECK BEYOND ϕ +46'-0".
 17. 6" WALL POP-OUT.
 18. ROOF TOP DECK.
 19. PROJECT SIGNAGE.
 20. 4" STUCCO POP-OUT WINDOW SURROUND.
 21. EXTERIOR LIGHT
 22. LIVE / WORK ENTRY

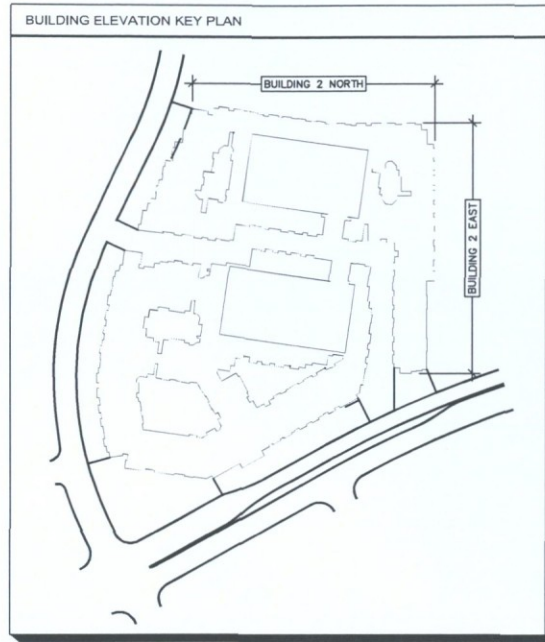


DISTRICT AT THE QUARTER
 KAPLAN ACQUISITIONS, LLC



BUILDING ELEVATIONS
 BUILDING #1 ELEVATIONS
 1/16" = 1'-0"

A7.6
 8-03-16



- KEY NOTES**
1. STUCCO FINISH
 2. 8" DEEP STUCCO FINISH EYEBROW - SHADING DEVISE.
 3. SOLARIUM UNIT AT THE 4TH FLOOR.
 4. METAL SHADE CANOPY - SHADING DEVISE.
 5. DECORATIVE METAL RAILING +42" A.F.F.
 6. PATIO WALL WITH STUCCO FINISH
 7. STUCCO CONTROL JOINT
 8. WINDOW UNIT, RECESSED 2" MINIMUM.
 9. FRENCH DOOR.
 10. FOLDING OR TELESCOPING DOOR SYSTEM.
 11. STOREFRONT GLAZING.
 12. METAL PARAPET CAP - 42" MINIMUM, HEIGHT MEASURED FROM FINISHED ROOF DECK.
 13. METAL DOOR - PAINTED ACCENT COLOR.
 14. STAIR BULKHEAD.
 15. SLIDING GLASS PATIO DOOR.
 16. TOP OF ROOF DECK BEYOND @ +46'-0".
 17. 6" WALL POP-OUT.
 18. ROOF TOP DECK.
 19. PROJECT SIGNAGE.
 20. 4" STUCCO POP-OUT WINDOW SURROUND.
 21. EXTERIOR LIGHT
 22. LIVE / WORK ENTRY



BUILDING #2 - NORTH ELEVATION



BUILDING #2 - EAST ELEVATION

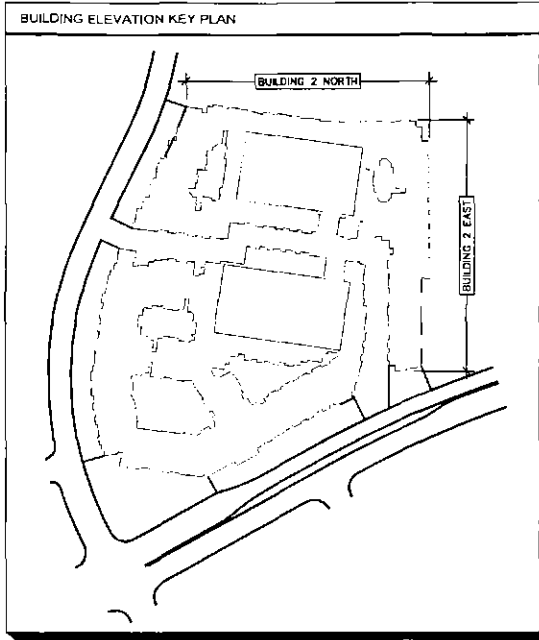


DISTRICT AT THE QUARTER
KAPLAN ACQUISITIONS, LLC

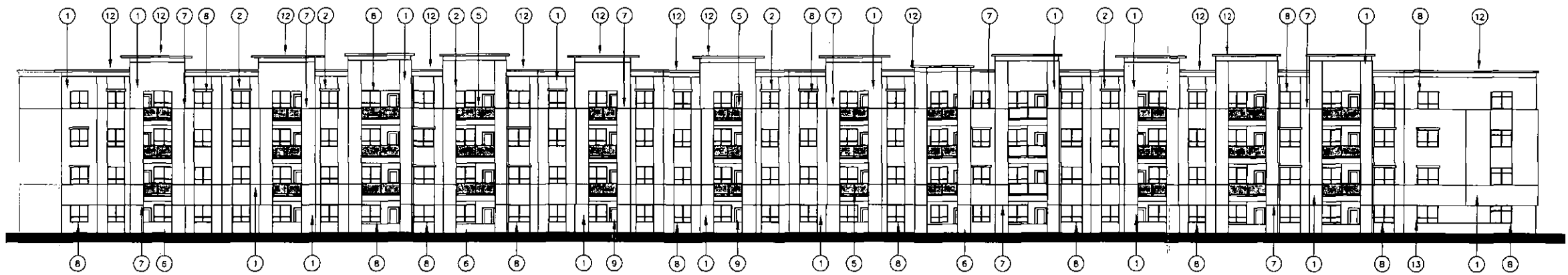


BUILDING ELEVATIONS
BUILDING #2 ELEVATIONS
1/16" = 1'-0"

A7.7
8-03-16



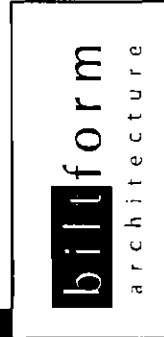
KEY NOTES	
1.	STUCCO FINISH
2.	8" DEEP STUCCO FINISH EYEBROW - SHADING DEVICE.
3.	SOLARIUM UNIT AT THE 4TH FLOOR.
4.	METAL SHADE CANOPY - SHADING DEVICE.
5.	DECORATIVE METAL RAILING +42" A.F.F.
6.	PATIO WALL WITH STUCCO FINISH
7.	STUCCO CONTROL JOINT
8.	WINDOW UNIT, RECESSED 2" MINIMUM.
9.	FRENCH DOOR.
10.	FOLDING OR TELESCOPING DOOR SYSTEM.
11.	STOREFRONT GLAZING.
12.	METAL PARAPET CAP - 42" MINIMUM, HEIGHT MEASURED FROM FINISHED ROOF DECK.
13.	METAL DOOR - PAINTED ACCENT COLOR.
14.	STAIR BULKHEAD.
15.	SLIDING GLASS PATIO DOOR.
16.	TOP OF ROOF DECK BEYOND @ +46'-0".
17.	6" WALL POP-OUT.
18.	ROOF TOP DECK.
19.	PROJECT SIGNAGE.
20.	4" STUCCO POP-OUT WINDOW SURROUND.
21.	EXTERIOR LIGHT
22.	LIVE / WORK ENTRY



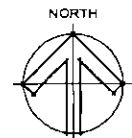
BUILDING #2 - NORTH ELEVATION



BUILDING #2 - EAST ELEVATION

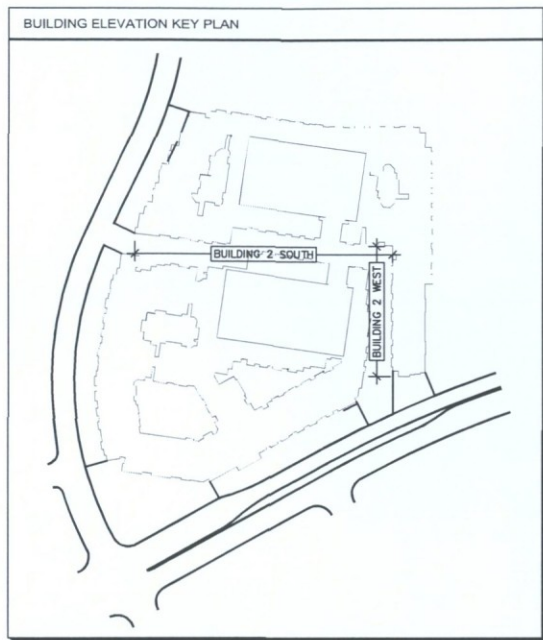


DISTRICT AT THE QUARTER
KAPLAN ACQUISITIONS, LLC



BUILDING ELEVATIONS
 BUILDING #2 ELEVATIONS
 1/16" = 1'-0"

A7.8
 8-03-16



- KEY NOTES**
1. STUCCO FINISH
 2. 8" DEEP STUCCO FINISH EYEBROW - SHADING DEVICE.
 3. SOLARIUM UNIT AT THE 4TH FLOOR.
 4. METAL SHADE CANOPY - SHADING DEVICE.
 5. DECORATIVE METAL RAILING +42" A.F.F.
 6. PATIO WALL WITH STUCCO FINISH
 7. STUCCO CONTROL JOINT
 8. WINDOW UNIT, RECESSED 2" MINIMUM.
 9. FRENCH DOOR.
 10. FOLDING OR TELESCOPING DOOR SYSTEM.
 11. STOREFRONT GLAZING.
 12. METAL PARAPET CAP - 42" MINIMUM, HEIGHT MEASURED FROM FINISHED ROOF DECK.
 13. METAL DOOR - PAINTED ACCENT COLOR.
 14. STAIR BULKHEAD.
 15. SLIDING GLASS PATIO DOOR.
 16. TOP OF ROOF DECK BEYOND \pm +46'-0".
 17. 6" WALL POP-OUT.
 18. ROOF TOP DECK.
 19. PROJECT SIGNAGE.
 20. 4" STUCCO POP-OUT WINDOW SURROUND.
 21. EXTERIOR LIGHT
 22. LIVE / WORK ENTRY



BUILDING #2 INTERIOR DRIVE - WEST ELEVATION



BUILDING #2 INTERIOR DRIVE - SOUTH ELEVATION

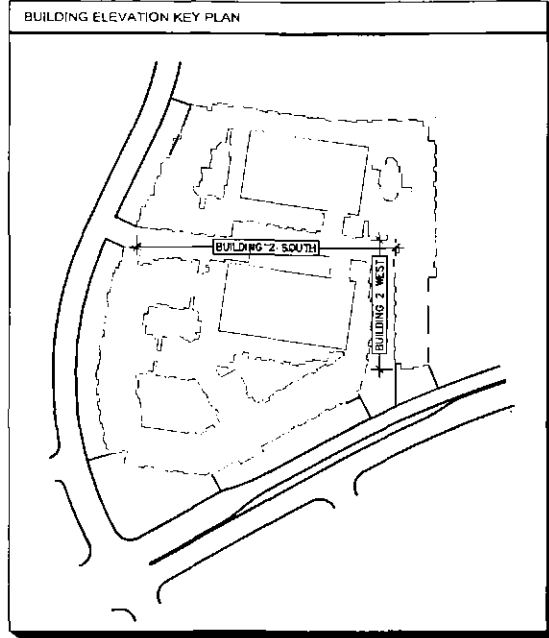


DISTRICT AT THE QUARTER
 KAPLAN ACQUISITIONS, LLC

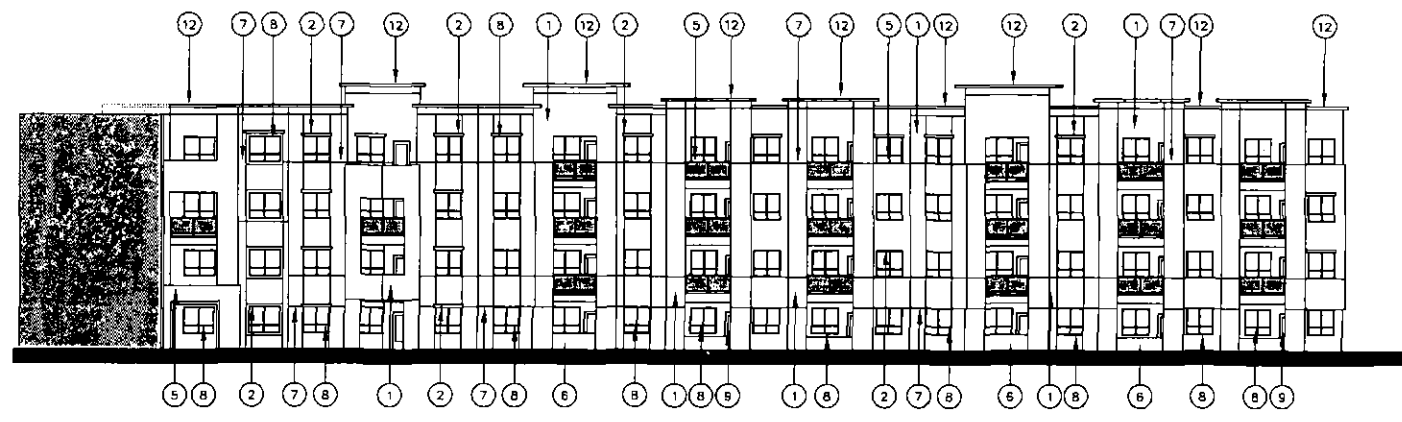


BUILDING ELEVATIONS
 BUILDING #2 ELEVATIONS
 1/16" = 1'-0"

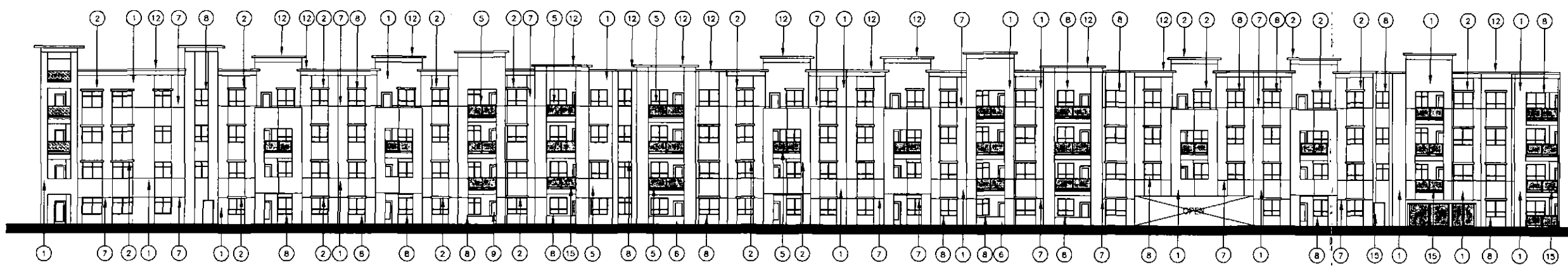
A7.9
 8-03-16



- KEY NOTES**
1. STUCCO FINISH
 2. 8" DEEP STUCCO FINISH EYEBROW - SHADING DEWSE.
 3. SOLARIUM UNIT AT THE 4TH FLOOR.
 4. METAL SHADE CANOPY - SHADING DEVICE.
 5. DECORATIVE METAL RAILING +42" A.F.F.
 6. PATIO WALL WITH STUCCO FINISH
 7. STUCCO CONTROL JOINT
 8. WINDOW UNIT, RECESSED 2" MINIMUM.
 9. FRENCH DOOR.
 10. FOLDING OR TELESCOPING DOOR SYSTEM.
 11. STOREFRONT GLAZING.
 12. METAL PARAPET CAP - 42" MINIMUM, HEIGHT MEASURED FROM FINISHED ROOF DECK.
 13. METAL DOOR - PAINTED ACCENT COLOR.
 14. STAIR BULKHEAD.
 15. SLIDING GLASS PATIO DOOR.
 16. TOP OF ROOF DECK BEYOND \pm +46'-0".
 17. 6" WALL POP-OUT.
 18. ROOF TOP DECK.
 19. PROJECT SIGNAGE.
 20. 4" STUCCO POP-OUT WINDOW SURROUND.
 21. EXTERIOR LIGHT
 22. LIVE / WORK ENTRY



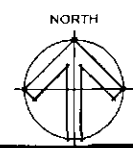
BUILDING #2 INTERIOR DRIVE - WEST ELEVATION



BUILDING #2 INTERIOR DRIVE - SOUTH ELEVATION



DISTRICT AT THE QUARTER
KAPLAN ACQUISITIONS, LLC

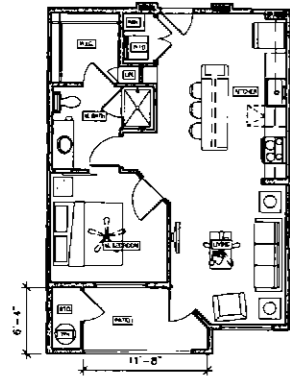


BUILDING ELEVATIONS
 BUILDING #2 ELEVATIONS

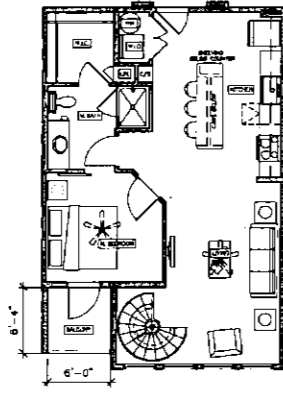
A7.10

1/16" = 1'-0"

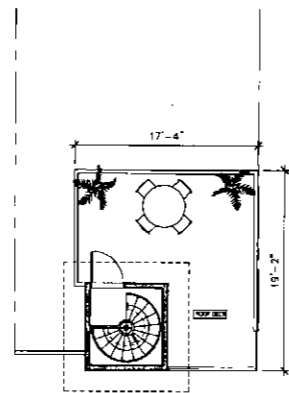
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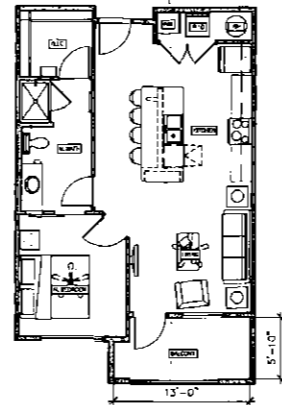
UNIT A1
FIRST - FOURTH FLOORS
GROSS FLOOR AREA: 662 SF
PATIO OR EXTERIOR PRIVATE AREA: 101 SF



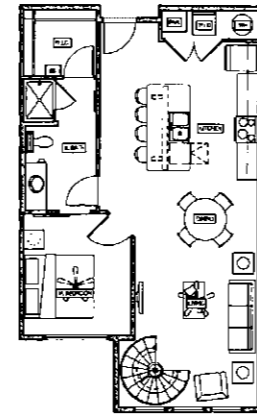
UNIT A1 ROOFDECK
FOURTH FLOOR
GROSS FLOOR AREA: 742 SF
PATIO OR EXTERIOR PRIVATE AREA: 42 SF



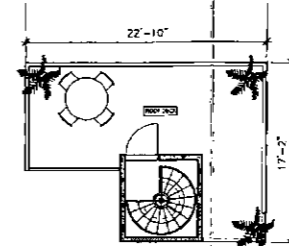
UNIT A1 ROOFDECK - 2ND FLOOR
UPPER FLOOR
PATIO OR EXTERIOR PRIVATE AREA: 240 SF



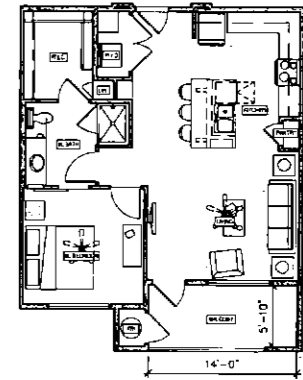
UNIT A2
FIRST - FOURTH FLOORS
GROSS FLOOR AREA: 552 SF
PATIO OR EXTERIOR PRIVATE AREA: 101 SF



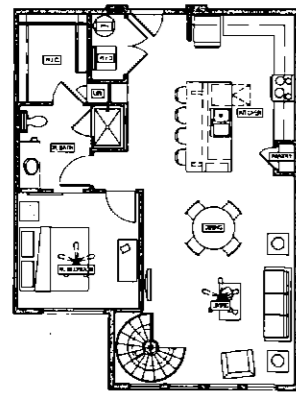
UNIT A2 ROOFDECK
FOURTH FLOOR
GROSS FLOOR AREA: 926 SF



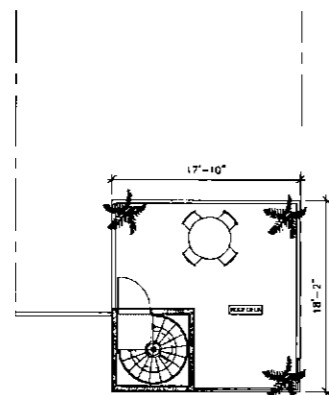
UNIT A2 ROOFDECK - 2ND FLOOR
UPPER FLOOR
PATIO OR EXTERIOR PRIVATE AREA: 245 SF



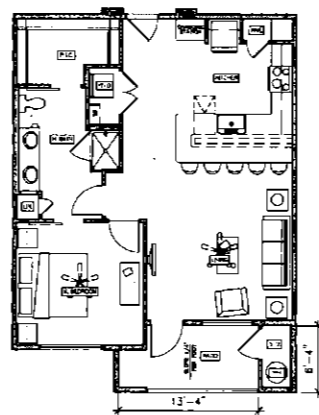
UNIT A3
FIRST - FOURTH FLOORS
GROSS FLOOR AREA: 802 SF
PATIO OR EXTERIOR PRIVATE AREA: 75 SF



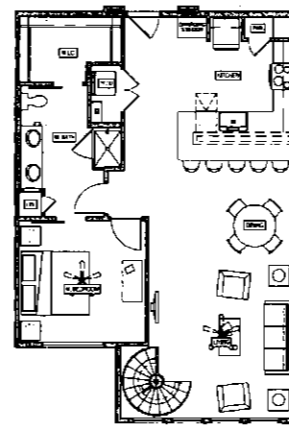
UNIT A3 ROOFDECK
FOURTH FLOOR
GROSS FLOOR AREA: 913 SF



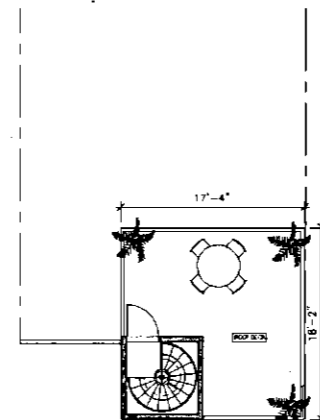
UNIT A3 ROOFDECK - 2ND FLOOR
UPPER FLOOR
PATIO OR EXTERIOR PRIVATE AREA: 244 SF



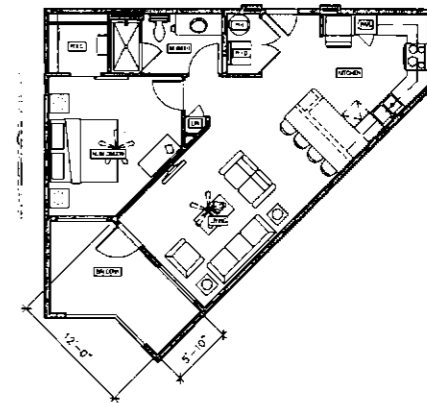
UNIT A4
FIRST - FOURTH FLOORS
GROSS FLOOR AREA: 811 SF
PATIO OR EXTERIOR PRIVATE AREA: 76 SF



UNIT A4 ROOFDECK
FOURTH FLOOR
GROSS FLOOR AREA: 985 SF



UNIT A4 ROOFDECK - 2ND FLOOR
UPPER FLOOR
PATIO OR EXTERIOR PRIVATE AREA: 244 SF



UNIT A5
FIRST - FOURTH FLOORS
GROSS FLOOR AREA: 755 SF
PATIO OR EXTERIOR PRIVATE AREA: 112 SF

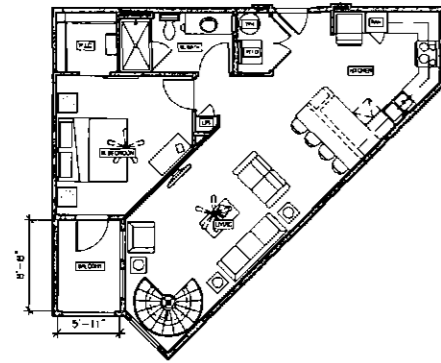


DISTRICT AT THE QUARTER

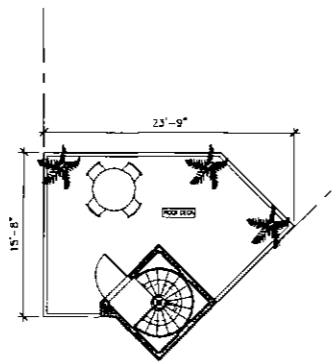
KAPLAN ACQUISITIONS, LLC

FLOOR PLAN WORK SHEET
UNIT PLANS
1/8" = 1'-0"

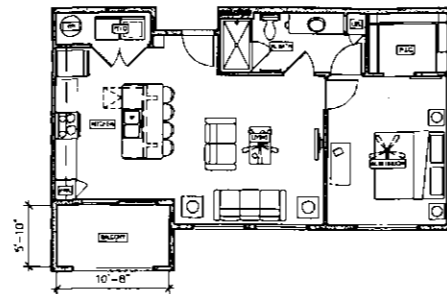
A4.1
8-03-16



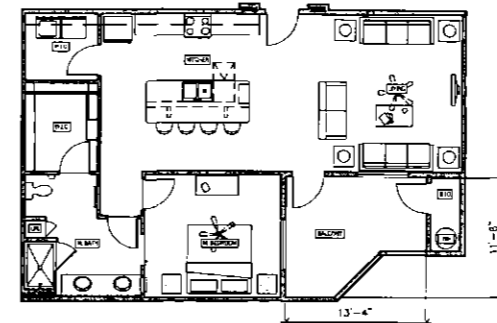
UNIT A5 ROOFDECK
FOURTH FLOOR
GROSS FLOOR AREA: 816 SF
PATIO OR EXTERIOR PRIVATE AREA: 54 SF



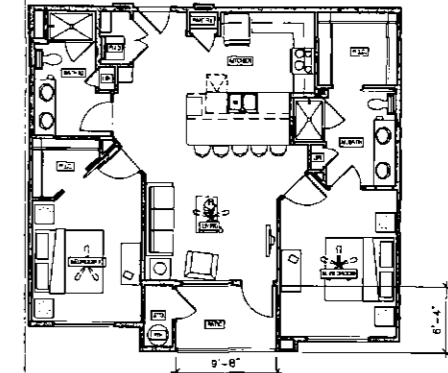
UNIT A5 ROOFDECK - 2ND FLOOR
UPPER FLOOR
PATIO OR EXTERIOR PRIVATE AREA: 245 SF



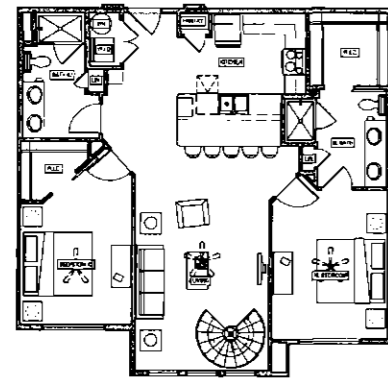
UNIT A6
FIRST - FOURTH FLOORS
GROSS FLOOR AREA: 716 SF
PATIO OR EXTERIOR PRIVATE AREA: 89 SF



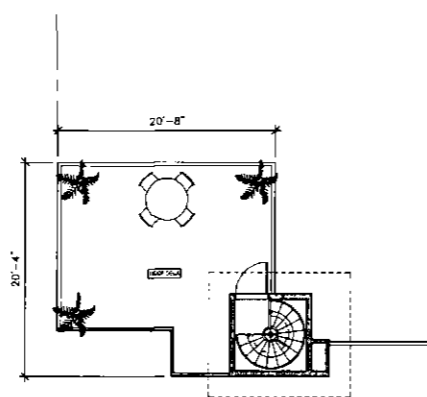
UNIT A7
FIRST - FOURTH FLOORS
GROSS FLOOR AREA: 973 SF
PATIO OR EXTERIOR PRIVATE AREA: 158 SF



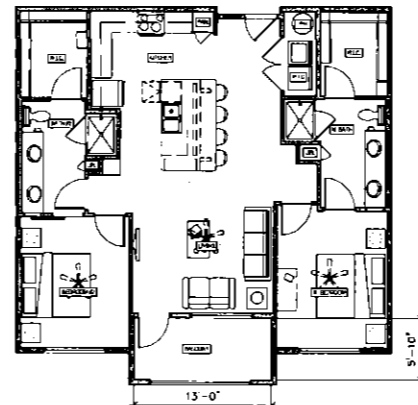
UNIT B1
FIRST - FOURTH FLOORS
GROSS FLOOR AREA: 1,063 SF
PATIO OR EXTERIOR PRIVATE AREA: 118 SF



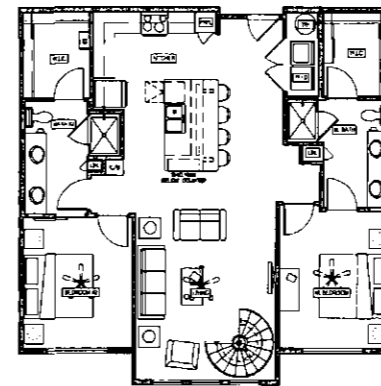
UNIT B1 ROOFDECK
FOURTH FLOOR
GROSS FLOOR AREA: 1,174 SF



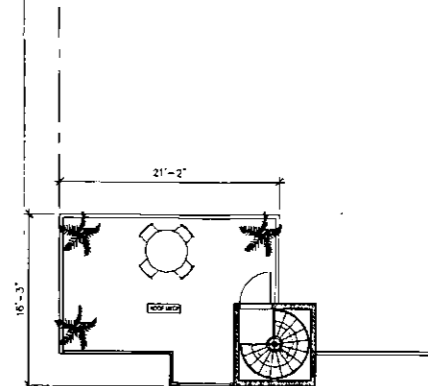
UNIT B1 ROOFDECK - 2ND FLOOR
UPPER FLOOR
PATIO OR EXTERIOR PRIVATE AREA: 331 SF



UNIT B2
FIRST - FOURTH FLOORS
GROSS FLOOR AREA: 1,130 SF
PATIO OR EXTERIOR PRIVATE AREA: 75 SF



UNIT B2 ROOFDECK
FOURTH FLOOR
GROSS FLOOR AREA: 1,215 SF



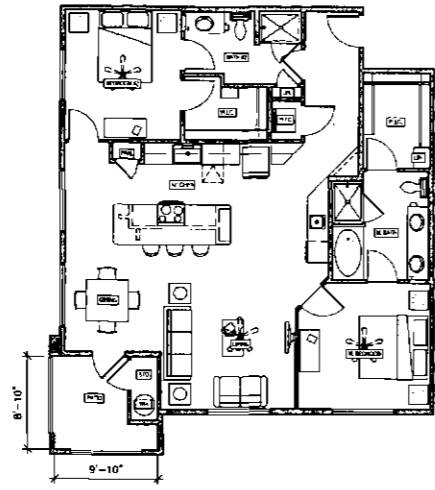
UNIT B2 ROOFDECK - 2ND FLOOR
UPPER FLOOR
PATIO OR EXTERIOR PRIVATE AREA: 253 SF



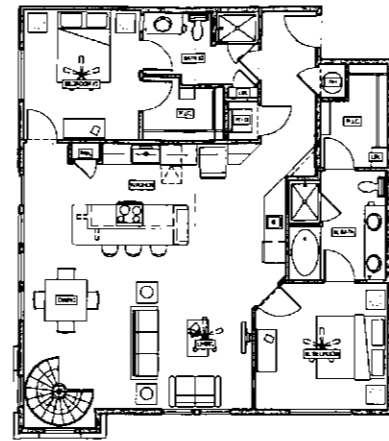
DISTRICT AT THE QUARTER
KAPLAN ACQUISITIONS, LLC

FLOOR PLAN WORK SHEET
UNIT PLANS
1/8" = 1'-0"

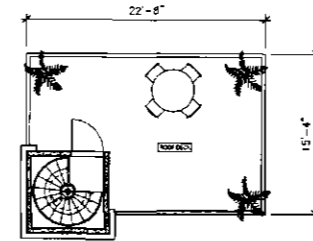
A4.2
8-03-16



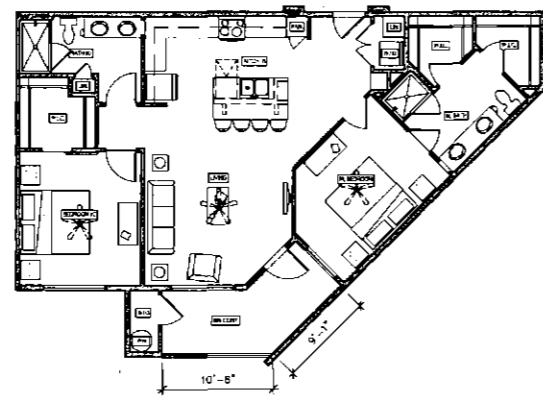
UNIT B3
 FIRST - FOURTH FLOORS
 GROSS FLOOR AREA: 1,294 SF
 PATIO OR EXTERIOR PRIVATE AREA: 88 SF



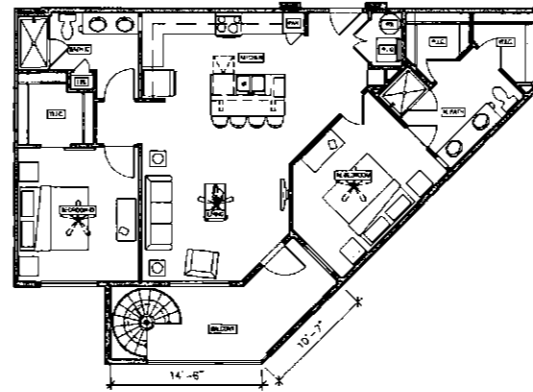
UNIT B3 ROOFDECK
 FOURTH FLOOR
 GROSS FLOOR AREA: 1,294 SF



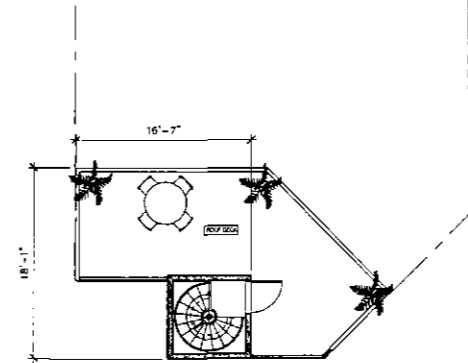
UNIT B3 ROOFDECK - 2ND FLOOR
 UPPER FLOOR
 PATIO OR EXTERIOR PRIVATE AREA: 263 SF



UNIT B4
 FIRST - FOURTH FLOORS
 GROSS FLOOR AREA: 1,131 SF
 PATIO OR EXTERIOR PRIVATE AREA: 125 SF



UNIT B4 ROOFDECK
 FOURTH FLOOR
 GROSS FLOOR AREA: 1,131 SF
 PATIO OR EXTERIOR PRIVATE AREA: 145 SF



UNIT B4 ROOFDECK - 2ND FLOOR
 UPPER FLOOR
 PATIO OR EXTERIOR PRIVATE AREA: 323 SF

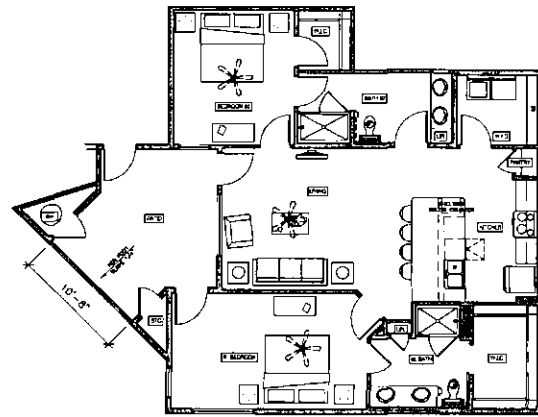


DISTRICT AT THE QUARTER
 KAPLAN ACQUISITIONS, LLC

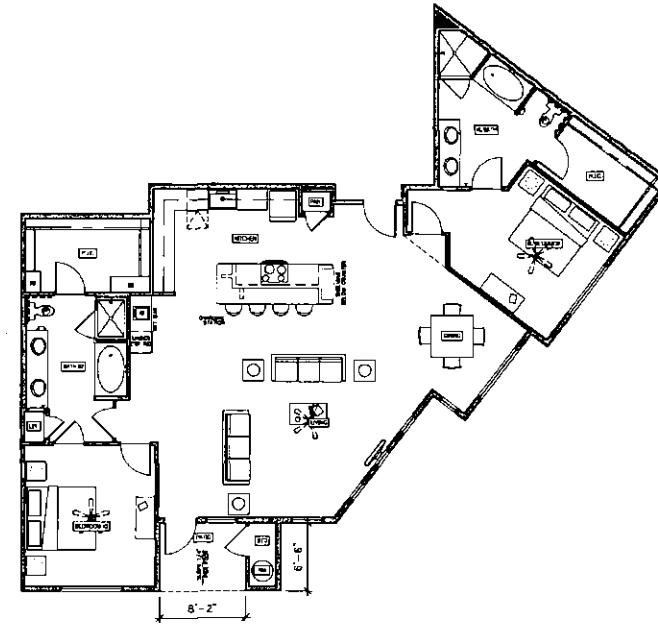
FLOOR PLAN WORK SHEET
 UNIT PLANS
 1/8" = 1'-0"

A4.3

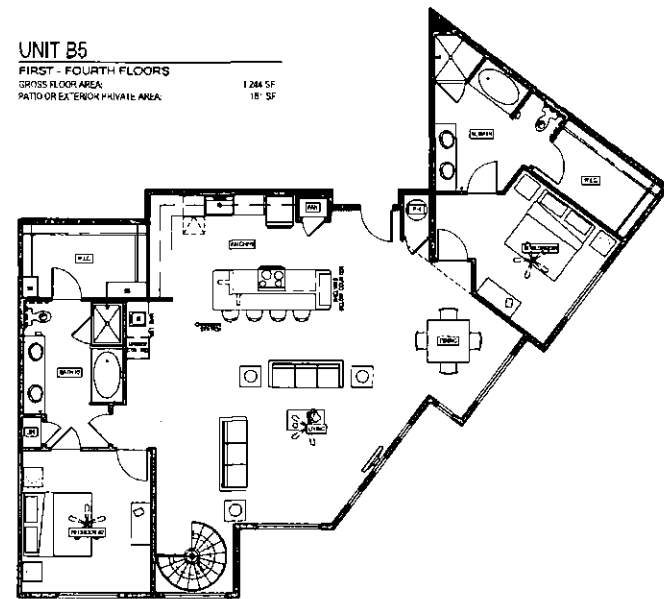
8-03-16



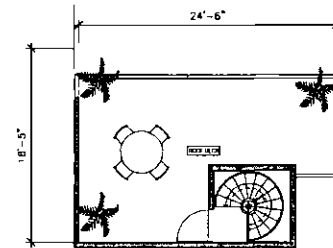
UNIT B5
 FIRST - FOURTH FLOORS
 GROSS FLOOR AREA: 1,284 SF
 PATIO OR EXTERIOR PRIVATE AREA: 18' SF



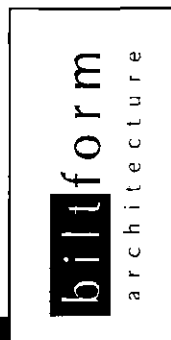
UNIT B6
 FIRST - THIRD FLOORS
 GROSS FLOOR AREA: 1,747 SF
 PATIO OR EXTERIOR PRIVATE AREA: 53 SF



UNIT B6 ROOFDECK
 FOURTH FLOOR
 GROSS FLOOR AREA: 1,947 SF
 PATIO OR EXTERIOR PRIVATE AREA: 101 SF



UNIT B6 ROOFDECK - 2ND FLOOR
 UPPER FLOOR
 GROSS FLOOR AREA: 300 SF
 PATIO OR EXTERIOR PRIVATE AREA: 300 SF

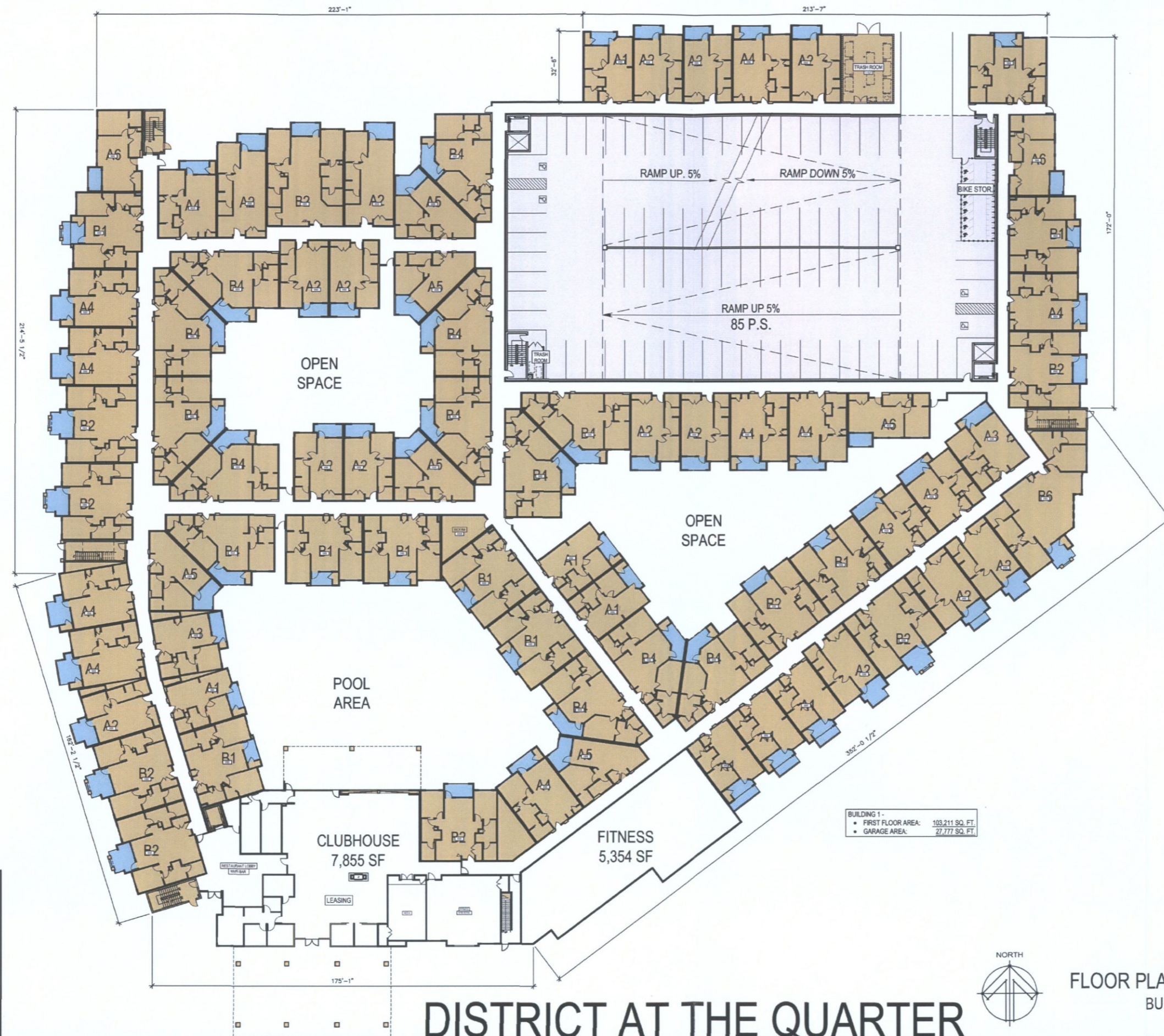


DISTRICT AT THE QUARTER
 KAPLAN ACQUISITIONS, LLC

FLOOR PLAN WORK SHEET
 UNIT PLANS
 1/8" = 1'-0"

A4.4

8-03-16



BUILDING 1 -
 • FIRST FLOOR AREA: 103,211 SQ. FT.
 • GARAGE AREA: 27,777 SQ. FT.

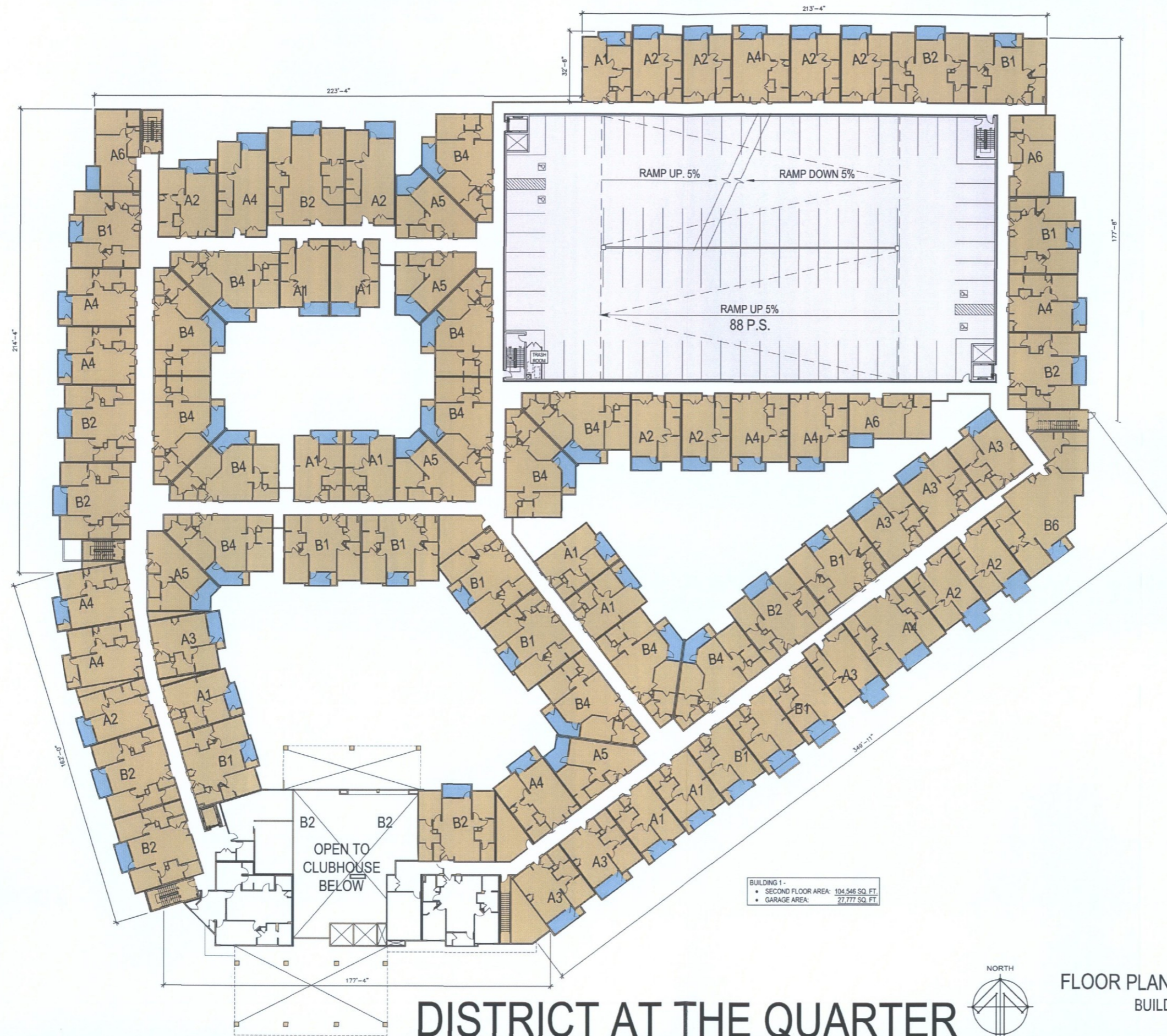


DISTRICT AT THE QUARTER
 KAPLAN ACQUISITIONS, LLC



FLOOR PLAN WORK SHEET
 BUILDING 1 - 1ST FLOOR
 3/64" = 1'-0"

A5.1.1C
 8-03-16



BUILDING 1 -
 • SECOND FLOOR AREA: 104,546 SQ. FT.
 • GARAGE AREA: 27,777 SQ. FT.



DISTRICT AT THE QUARTER
 KAPLAN ACQUISITIONS, LLC



FLOOR PLAN WORK SHEET
 BUILDING 1 - 2ND FLOOR
 3/64" = 1'-0"

A5.1.2C
 8-03-16



BUILDING 1 -
 • THIRD FLOOR AREA: 104,546 SQ. FT.
 • GARAGE AREA: 27,777 SQ. FT.

RESTAURANT
6,398 SF

OFFICE
1,781 SF



DISTRICT AT THE QUARTER

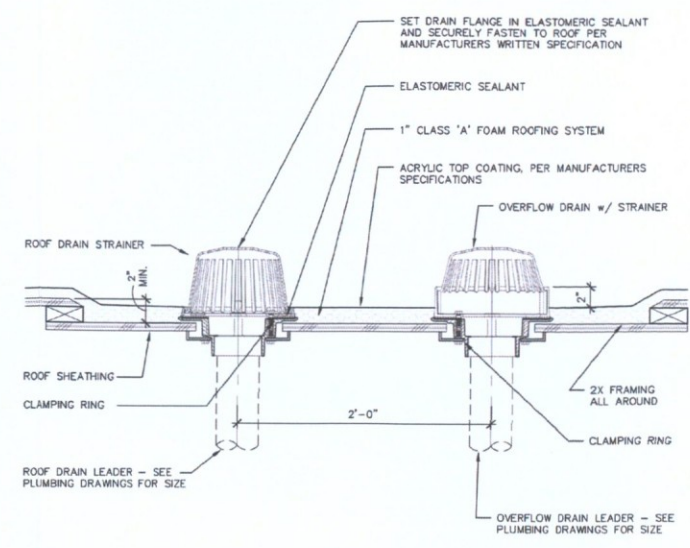
KAPLAN ACQUISITIONS, LLC



FLOOR PLAN WORK SHEET
 BUILDING 1 - 3RD FLOOR
 3/64" = 1'-0"

A5.1.3C

8-03-16

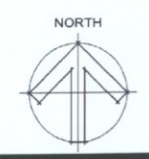


1 ROOF DRAIN AND OVERFLOW DRAIN TYPICAL
1-1/2" = 1'-0"

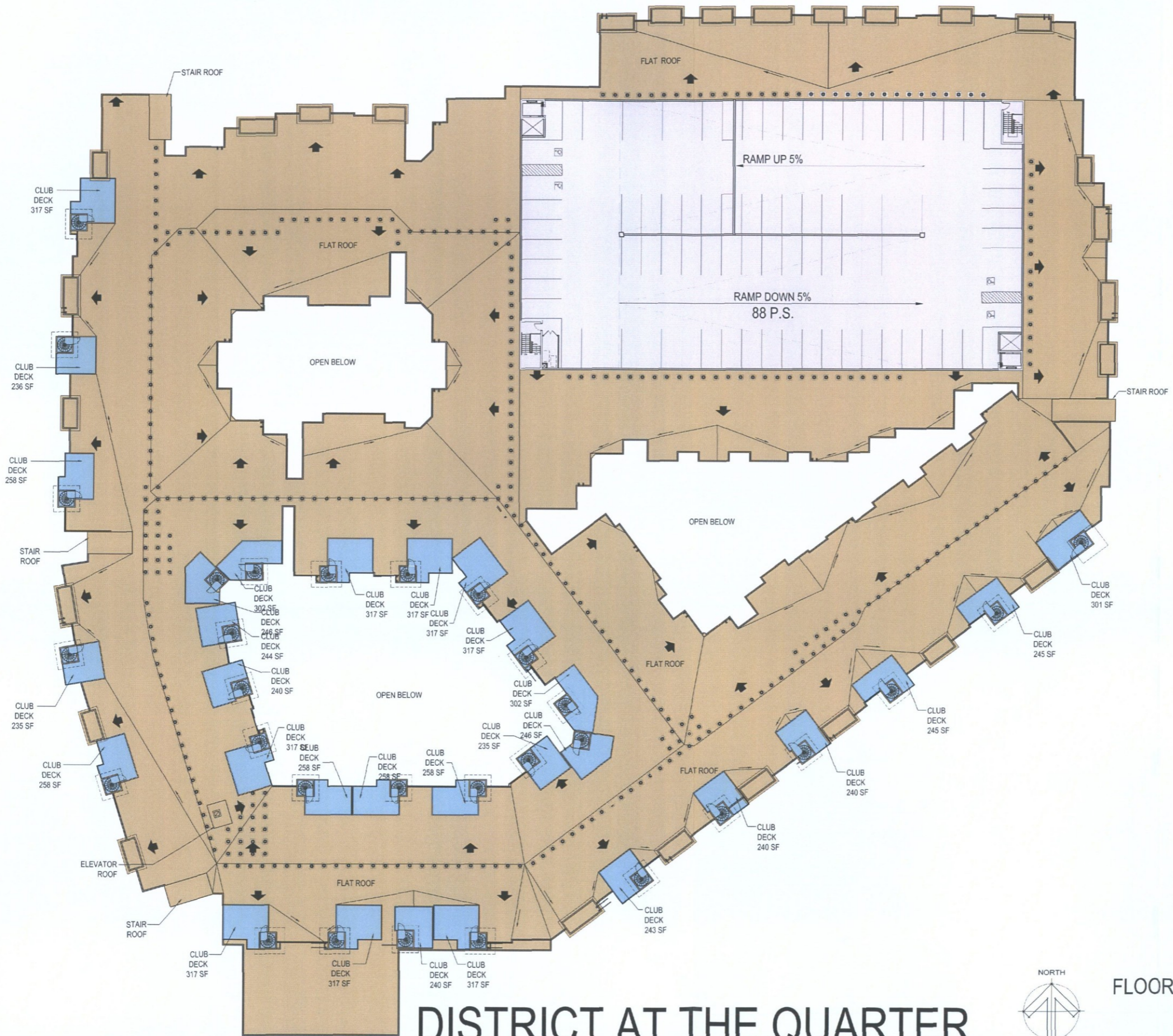
BUILDING 1 -	
• SECOND FLOOR AREA:	105,403 SQ. FT.
• GARAGE AREA:	27,777 SQ. FT.
R = ROOF TOP UNITS	TOTAL 30



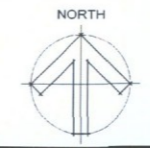
DISTRICT AT THE QUARTER
KAPLAN ACQUISITIONS, LLC



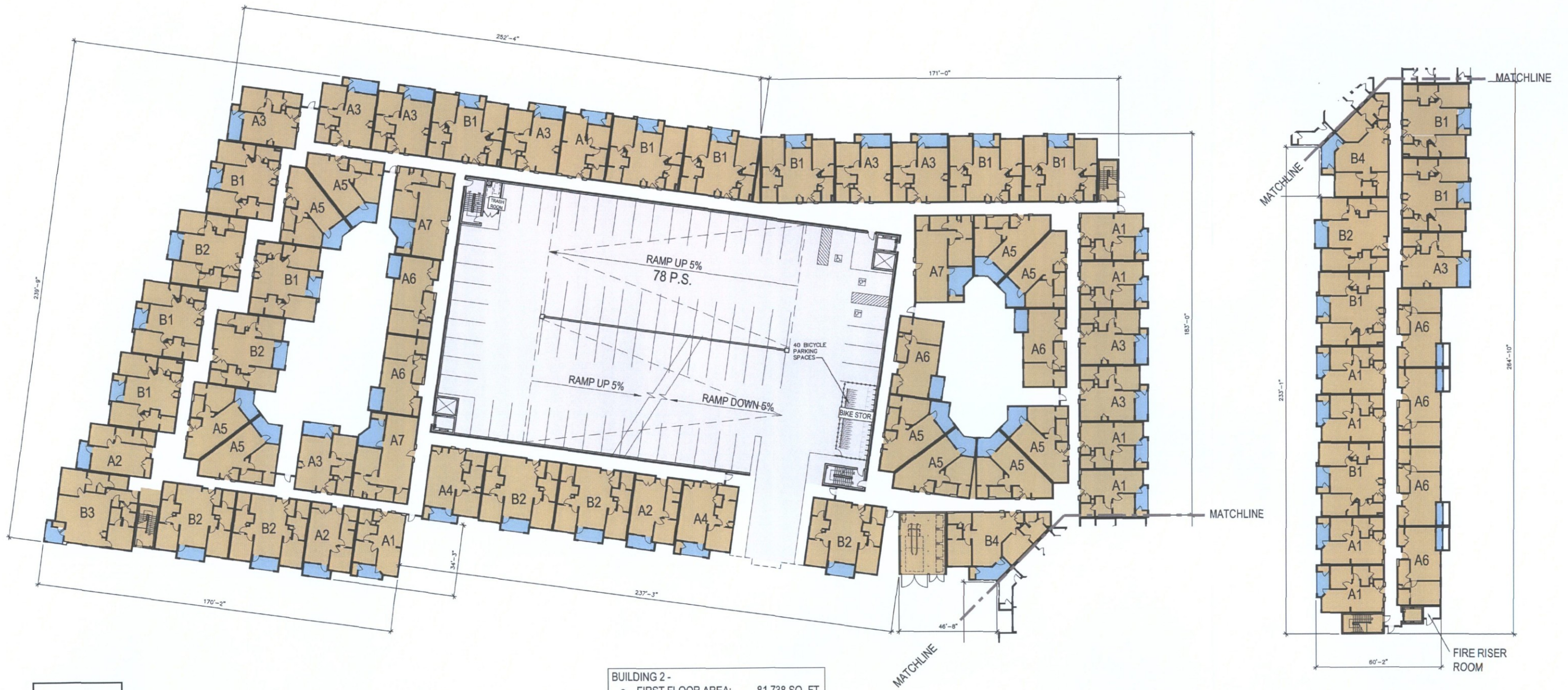
FLOOR PLAN WORK SHEET
BUILDING 1 - 4TH FLOOR
3/64" = 1'-0" **A5.1.4C**
7-21-16



DISTRICT AT THE QUARTER
KAPLAN ACQUISITIONS, LLC



FLOOR PLAN WORK SHEET
 BUILDING 1 - ROOF
 3/64" = 1'-0" **A5.1.5**
 8-03-16



BUILDING 2 -	
• FIRST FLOOR AREA:	81,738 SQ. FT.
• GARAGE AREA:	25,686 SQ. FT.



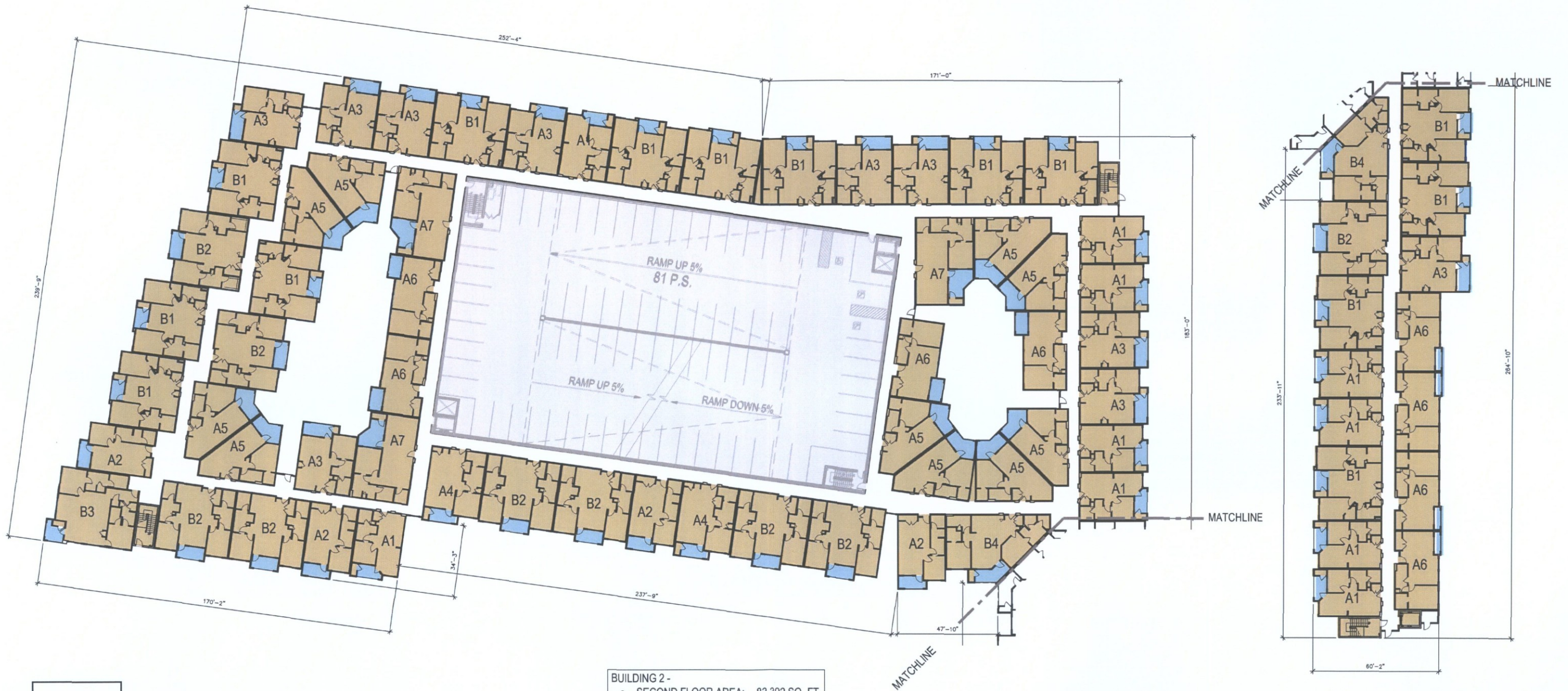
DISTRICT AT THE QUARTER

KAPLAN ACQUISITIONS, LLC



FLOOR PLAN WORK SHEET
 BUILDING 2 - 1ST FLOOR
 3/64" = 1'-0"

A5.2.1C
 8-03-16



BUILDING 2 -
 • SECOND FLOOR AREA: 83,392 SQ. FT.
 • GARAGE AREA: 25,686 SQ. FT.



DISTRICT AT THE QUARTER

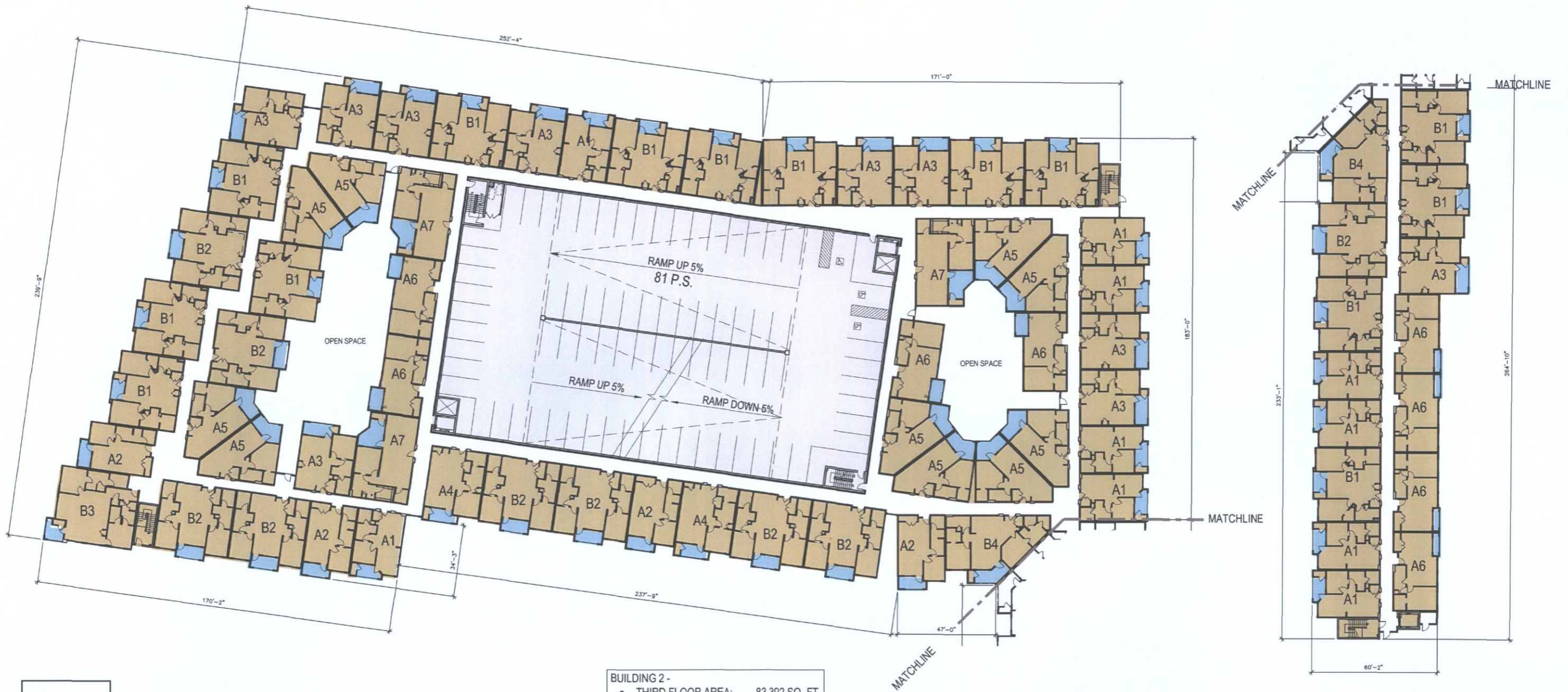
KAPLAN ACQUISITIONS, LLC



FLOOR PLAN WORK SHEET
 BUILDING 2 - 2ND FLOOR
 3/64" = 1'-0"

A5.2.2C

8-03-16



BUILDING 2 -
 • THIRD FLOOR AREA: 83,392 SQ. FT.
 • GARAGE AREA: 25,686 SQ. FT.



DISTRICT AT THE QUARTER

KAPLAN ACQUISITIONS, LLC



FLOOR PLAN WORK SHEET
 BUILDING 2 - 3RD FLOOR
 3/64" = 1'-0" **A5.2.3C**
 8-03-16



BUILDING 2 -
 • FOURTH FLOOR AREA: 83,392 SQ. FT.
 • GARAGE AREA: 25,686 SQ. FT.



DISTRICT AT THE QUARTER

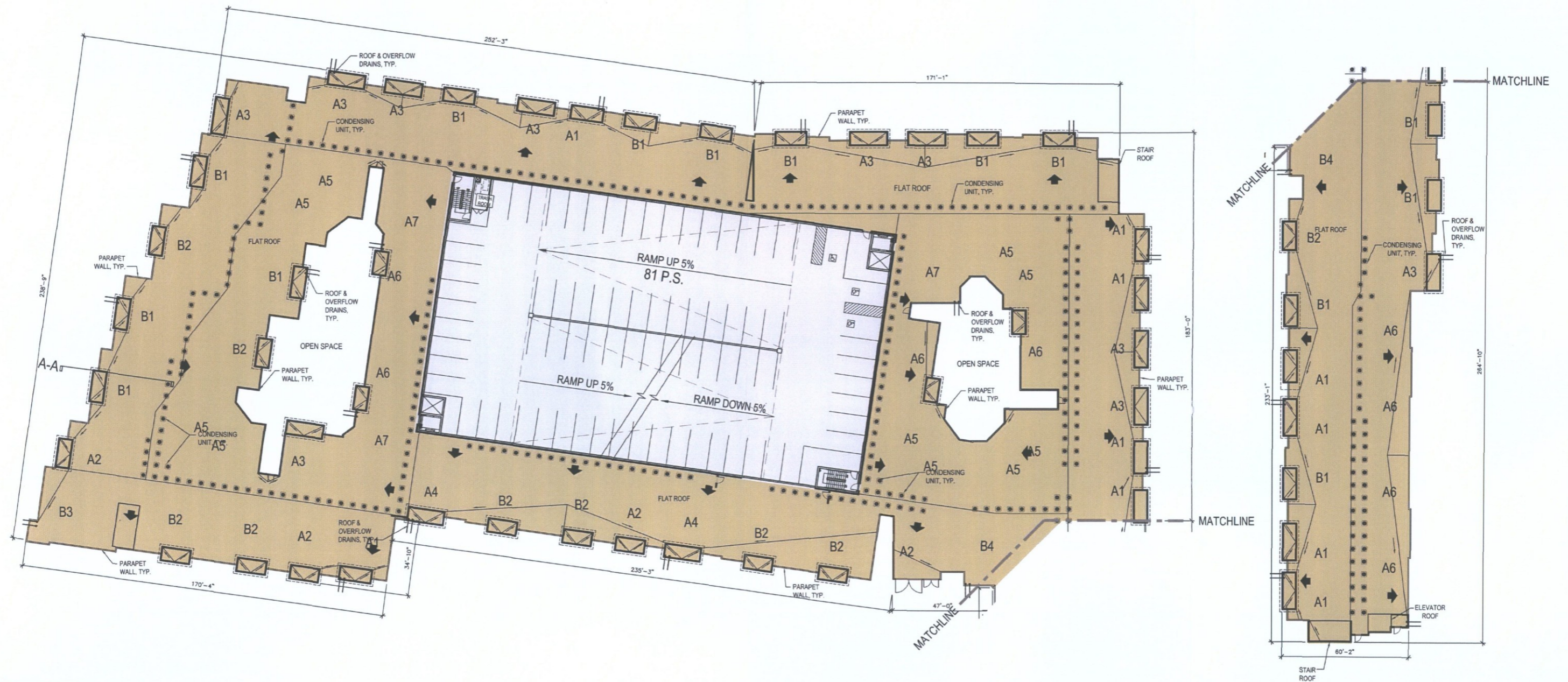
KAPLAN ACQUISITIONS, LLC



FLOOR PLAN WORK SHEET
 BUILDING 2 - 4TH FLOOR
 3/64" = 1'-0"

A5.2.4C

8-03-16



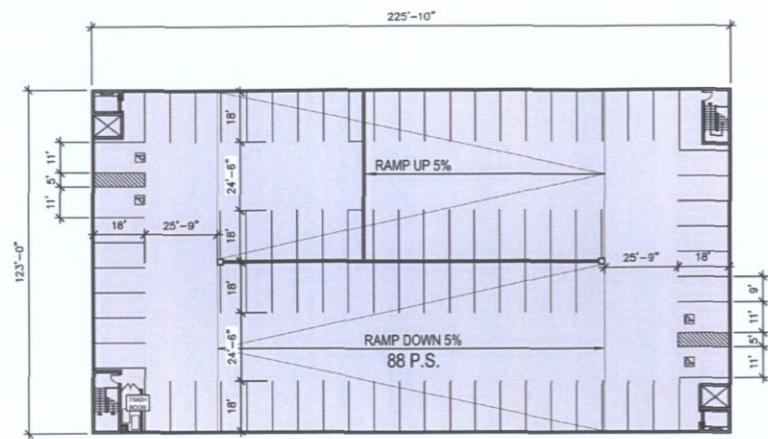
DISTRICT AT THE QUARTER
 KAPLAN ACQUISITIONS, LLC



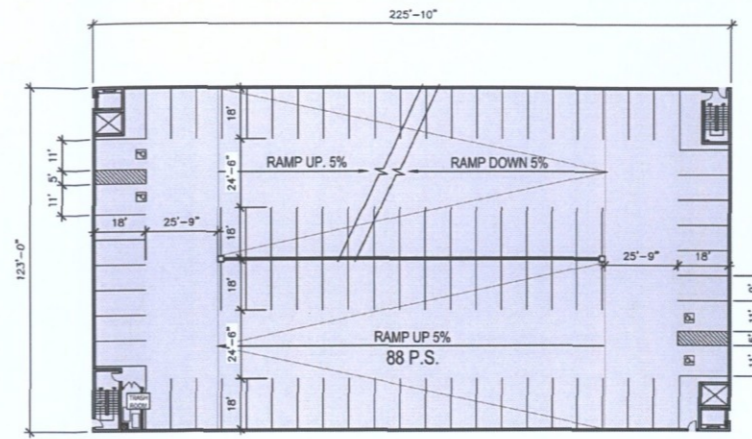
FLOOR PLAN WORK SHEET
 BUILDING 2 - ROOF
 3/64" = 1'-0"

A5.2.5C

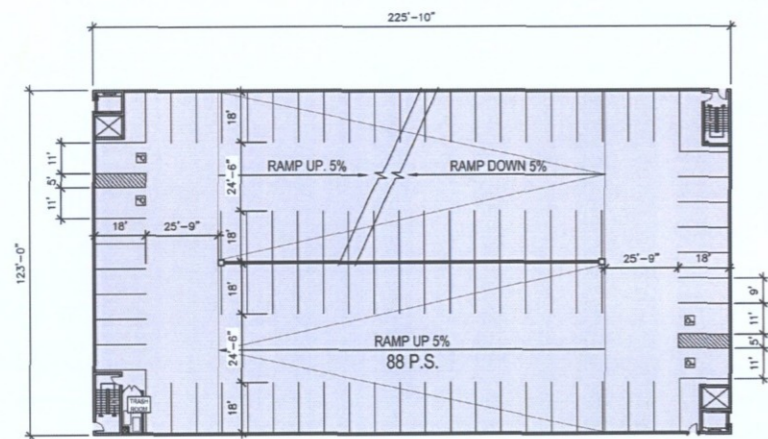
8-03-16



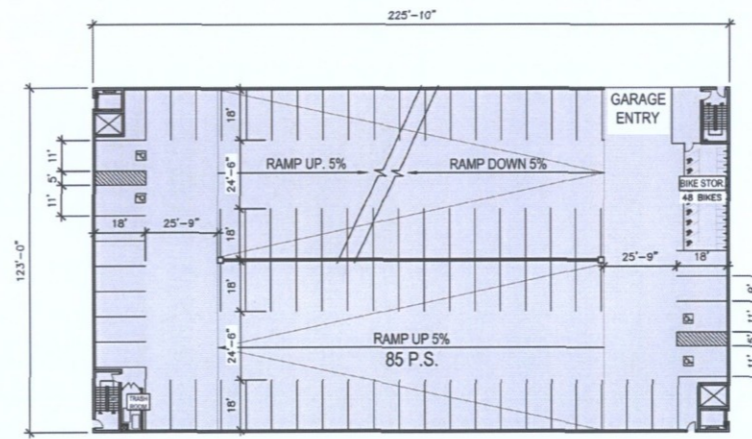
MEZZANINE LEVEL



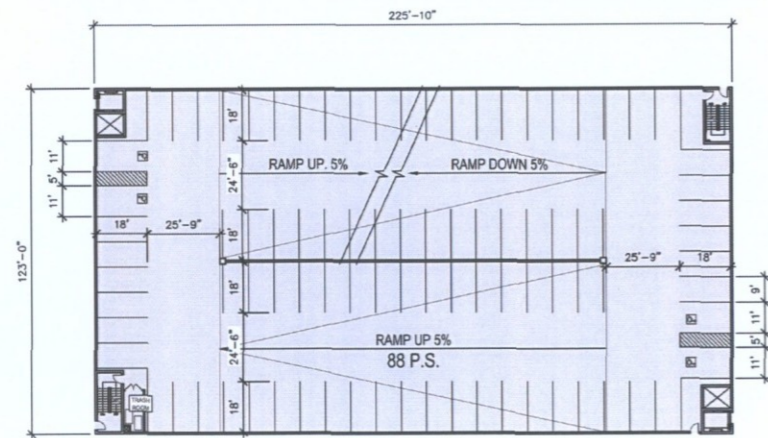
SECOND LEVEL



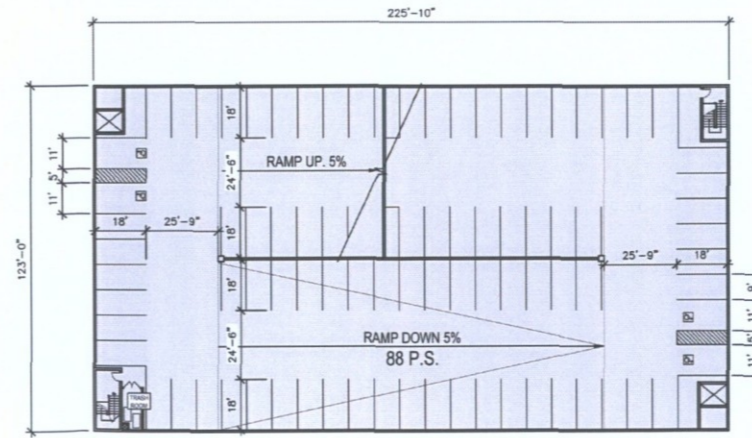
FOURTH LEVEL



GRADE LEVEL



THIRD LEVEL



LOWER LEVEL

PARKING SUMMARY - BUILDING 1	
(PHASE 1) TOTAL PARKING:	
REQUIRED, TABLE 9.103.B:	
1 BEDROOM OR LESS (1 SPACE)	192 X 1 = 192
2 BEDROOMS OR MORE (2 SPACES)	140 X 2 = 280
TOTAL REQUIRED:	472 P.S.
PROVIDED:	
GARAGE PARKING (COVERED)	525 P.S.
VISITORS PARKING (OPEN SPACES)	0 P.S.
TOTAL PROVIDED:	525 P.S.
ACCESSIBLE PARKING REQUIRED:	4% X 525 P.S. = 21 P.S.
ACCESSIBLE PARKING PROVIDED:	COVERED = 21 P.S.
BICYCLE PARKING: (1) SPACES PER 10 VEHICAL SPACES; SEC.9.103	
(472/10)=47.2	48 SPACES PROVIDED



DISTRICT AT THE QUARTER

KAPLAN ACQUISITIONS, LLC

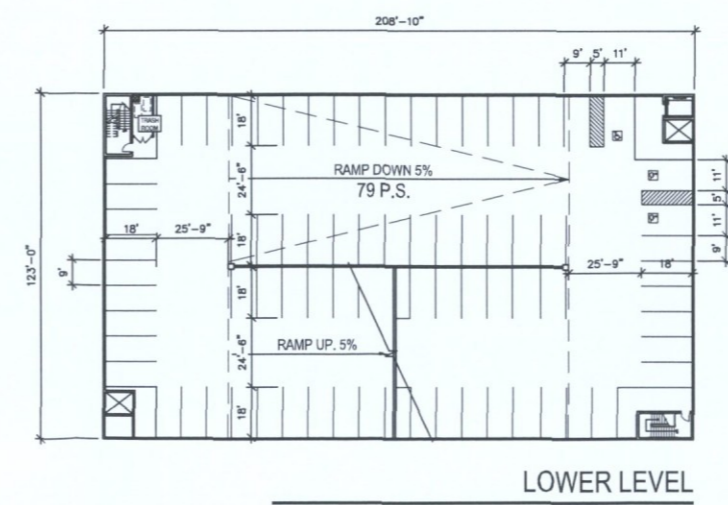
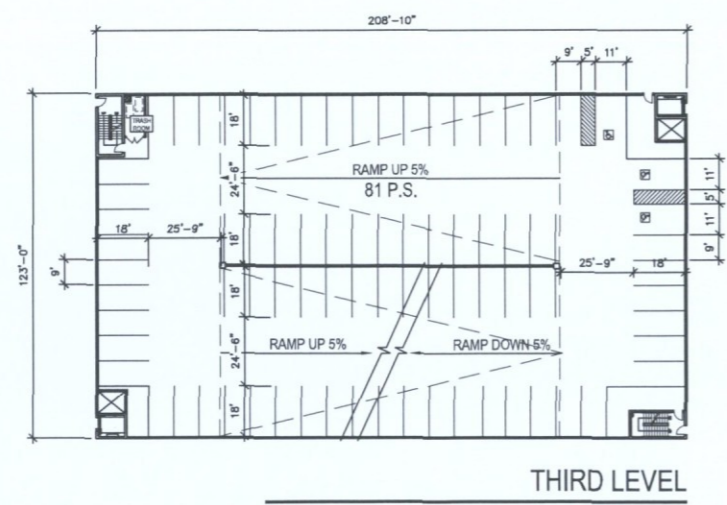
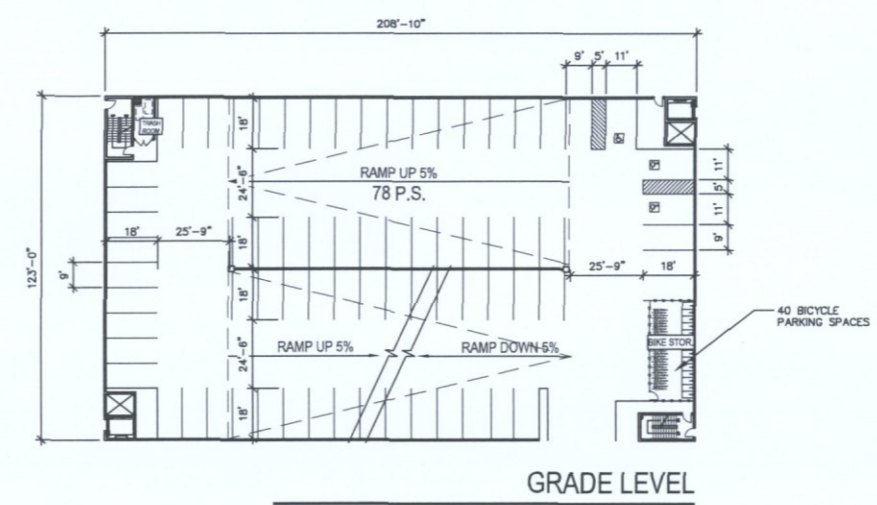
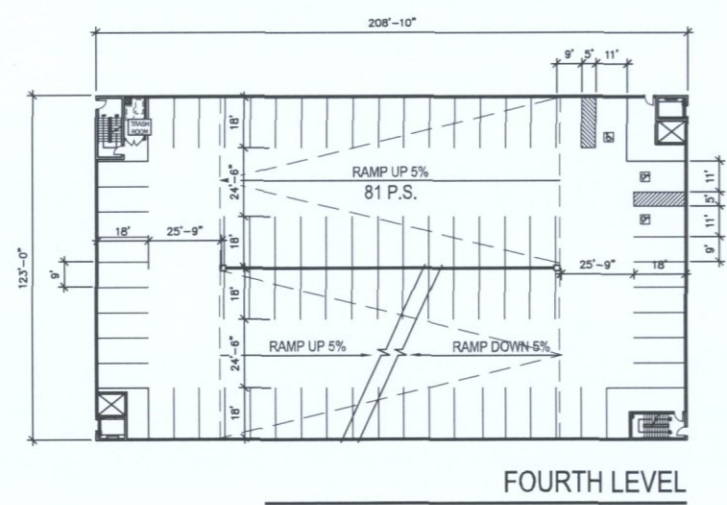
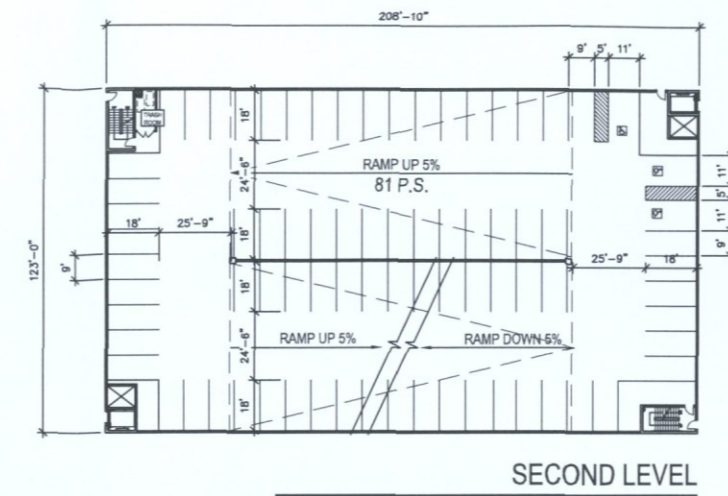
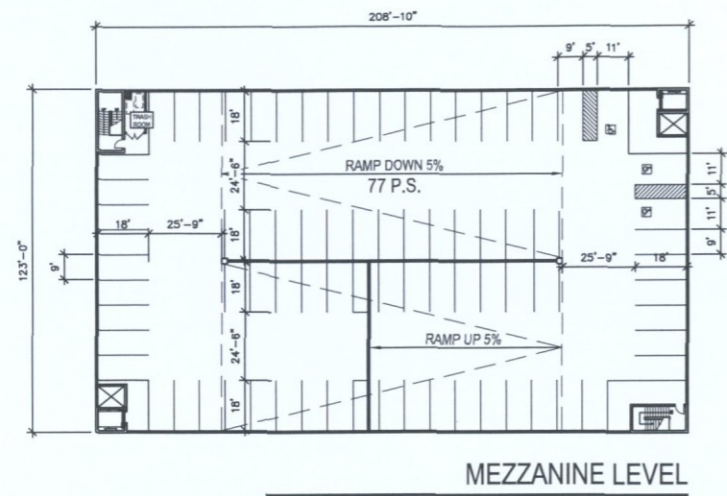
GARAGE PLAN WORK SHEET
BUILDING 1 - 1ST LEVEL

A6.1C

3/64" = 1'-0"

8-03-16

PARKING SUMMARY - BUILDING 2	
(PHASE 2) TOTAL PARKING:	
REQUIRED: TABLE 9.103.B	
1 BEDROOM OR LESS (1 SPACE)	187 X 1 = 187
2 BEDROOMS OR MORE (2 SPACES)	103 X 2 = 206
TOTAL REQUIRED:	393 P.S.
PROVIDED:	
GARAGE PARKING (COVERED)	477 P.S.
VISITORS PARKING (OPEN SPACES)	
TOTAL PROVIDED:	477 P.S.
ACCESSIBLE PARKING REQUIRED:	4% X 423 P.S. = 17 P.S.
ACCESSIBLE PARKING PROVIDED:	COVERED = 17 P.S.
BICYCLE PARKING: (1) SPACES PER 10 VEHICAL SPACES; SEC.9.103 (393/10=39.3) 40 SPACES PROVIDED	



DISTRICT AT THE QUARTER

KAPLAN ACQUISITIONS, LLC

GARAGE PLAN WORK SHEET
BUILDING 2
1" = 30'-0"

A6.2

8-03-16

Exhibit 6

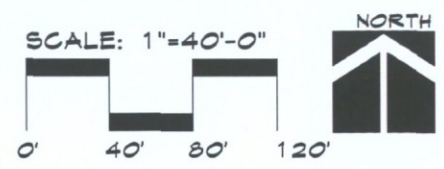
Landscape Plan Set



CONCEPTUAL PLANT PALETTE

- TREES (24" BOX MIN. CALIFERS TO MEET ANA STANDARDS.)**
- MULGA (ACACIA ANURA)
 - SIBBOO TREE (DALBERGIA SIBBOO)
 - TEXAS EBONY (EBENOPSIS EBANO)
 - PURPLE LEAF PLUM (PRUNUS CERASIFERA)
 - PALO VERDE THORNLESS HYBRID (PARKINSONIA HYBRID)
 - RED PISH PISTACHE (PISTACIA CHINENSIS RED PISH)
 - MASTIC TREE (PISTACIA LENTISCUS)
 - TREE OLEANDER (OLEANDER SP.)
 - LIVE OAK SP. 4 SSP. (QUERCUS VIRGINIANA)
 - MESQUITE SPECIES (PROSOPIS SPECIES)
 - TEXAS MOUNTAIN LAUREL (SOPHORA SECUNDFLORA)
 - CITRUS TREES (CITRUS SPECIES)
- FALM TREES**
- DATE PALM (PHOENIX DACTYLIFERA)
 - MEDITERRANEAN FAN PALM (CHAMEROPS HUMILIS)
- SHRUBS - 5 GAL. MIN.**
- BOUGAINVILLEA (BOUGAINVILLEA SPECIES)
 - DESERT BIRD OF PARADISE (CAESALPINIA SPECIES)
 - FAIRY DUSTER (CALLIANDRA SPECIES)
 - CASSIA (CASSIA SPECIES)
 - EMU BUSH (EREMOPHILA SPECIES)
 - TEXAS SAGE (LEUCOPHYLLUM SPECIES)
 - DEERGRASS (MULLENBERGIA SPECIES)
 - JOJOBA (SIMMONDSIA CHINENSIS)
 - BAJO RUELLIA (RUELLIA PENINSULARIS)
 - SALVIA (SALVIA SPECIES)
 - ARIZONA YELLOW BELLS (TECOMA SPECIES)
 - ORANGE JUBILEE (TECOMA SPECIES)
 - DWARF MYRTLE (MYRTUS COMMUNIS)
 - LITTLE JOHN BOTTLEBRUSH (CALLISTEMON SPECIES)
- ACCENT PLANTS - 5 GAL. MIN.**
- AGAVE (AGAVE SPECIES)
 - ALOE (ALOE SPECIES)
 - DESERT SMOOTH SPOONS (DASYLIRION SPECIES)
 - EUPHORBIA (EUPHORBIA SPECIES)
 - HEDGEHOG CACTUS (ECHINOCEBUS ENGELMANNII)
 - BARREL CACTUS (FEROCACTUS WILZENII)
 - GIANT HESPERALOE (HESPERALOE FUNIFERA)
 - RED YUCCA (HESPERALOE PARVIFLORA)
 - FRICKLY FEAR (OPUNTIA SPECIES)
 - LADY SLIPPER (PEDILANTHUS MACROCARPUS)
 - YUCCA (YUCCA SPECIES)
- GROUNDCOVER - 5 GAL. MIN.**
- DESERT CARPET ACACIA (ACACIA REDOLENS 'DESERT CARPET')
 - ASPARAGUS FERN (ASPARAGUS SPRENGERI)
 - EREMOPHILA GLABRA 'OUTBACK SUNRISE'
 - VERBENA (SLANDULARIA SPECIES)
 - LANTANA (LANTANA SPECIES)
 - IRENE ROSEMARY (ROSMARINUS SPECIES)
 - YELLOW DOT (NEDELIA TRILOBATA)
- DECOMPOSED GRANITE**
- 3/4" SCREENED - 2" DEPTH MIN. COLOR TO BE DETERMINED**
- ARTIFICIAL TURF**

- NOTES:**
1. DUE TO PLANT MATERIAL AVAILABILITY, SUBSTITUTIONS FOR PLANT MATERIAL LISTED ABOVE MAY BE USED. ANY ALTERNATES OR SUBSTITUTIONS MUST BE ON THE ADWR LOW WATER USE PLANT LIST.
 2. SUB-SPECIES OR HYBRIDS OF PLANT MATERIAL LISTED ABOVE MAY BE USED AS ALTERNATES/SUBSTITUTIONS.
 3. ADDITIONAL PLANT MATERIAL MAY BE ADDED TO THE LIST BELOW DUE TO UTILITY COMPANY OR H.O.A. REQUESTS AND/OR PLANTING RESTRICTIONS WITHIN UTILITY EASEMENTS.
 4. NO TREES TO BE LOCATED WITHIN THE P.U.E. OR ANY OTHER NOTED EASEMENTS.
 5. ALL PLANT MATERIAL IN THE R.O.W. SHALL BE ON THE ADWR LOW WATER USE PLANT LIST.



DISTRICT AT THE QUARTER OVERALL LANDSCAPE MASTER PLAN

1550 1 N. DIAL BLVD. SCOTTSDALE, AZ 85260
AUGUST 30, 2016





PEDESTRIAN CIRCULATION LEGEND	
	8' WIDE PUBLIC PEDESTRIAN CIRCULATION
	6' WIDE PUBLIC PEDESTRIAN CIRCULATION
	48' WIDE PUBLIC PEDESTRIAN CIRCULATION
	PRIVATE PEDESTRIAN CIRCULATION
	A.D.A. ACCESSIBLE RAMP
	INTERIOR/EXTERIOR TRANSITION
VEHICULAR CIRCULATION LEGEND	
	EXISTING ROADWAYS
	PROPOSED VEHICULAR ACCESS
	EMERGENCY ACCESS ONLY

DISTRICT AT THE QUARTER PEDESTRIAN & VEHICULAR CIRCULATION PLAN

15501 N. DIAL BLVD. SCOTTSDALE, AZ 85260
AUGUST 30, 2016





SITE DATA

GROSS SITE AREA: 448,295 SQ.FT
 NET SITE AREA: 384,121 SQ.FT
 ZONING: I-1
 PROPOSED ZONING: PUD

COMMON OPEN SPACE

GROSS SITE AREA: 448,299 SQ.FT
 PHASE I SITE AREA: 271,599.80 SQ.FT
 OPEN SPACE FACTOR: 0.10
 PHASE I REQUIRED OPEN SPACE: 27,160 SQ.FT
 PHASE II SITE AREA: 176,700.49 SQ.FT
 OPEN SPACE FACTOR: 0.10
 PHASE II REQUIRED OPEN SPACE: 17,670 SQ.FT

PHASE I - COMMON OPEN SPACE LEGEND

- COMMON AREA OPEN SPACE - 28,157 SQ.FT
- OTHER OPEN SPACE AREA - 6,692 SQ.FT
- STREET FRONTAGE OPEN SPACE - 22,925 SQ.FT
- PARKING LOT LANDSCAPING - 2,589 SQ.FT

PHASE II - COMMON OPEN SPACE LEGEND

- COMMON AREA OPEN SPACE - 17,957 SQ.FT
- OTHER OPEN SPACE AREA - 10,258 SQ.FT
- STREET FRONTAGE OPEN SPACE - 7,844 SQ.FT
- NO PARKING LOT AREA IN PHASE II

OVERALL TOTAL OPEN SPACE FOR BOTH PHASES

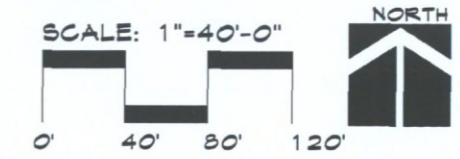
COMMON OPEN SPACE PROVIDED:
 - 46,114 SQ.FT (10.29% OF GROSS SQ.FT)
 (NOT INCLUDING OTHER OPEN SPACE AREA, STREET FRONTAGE AREA OR PARKING LOT LANDSCAPING AREA).

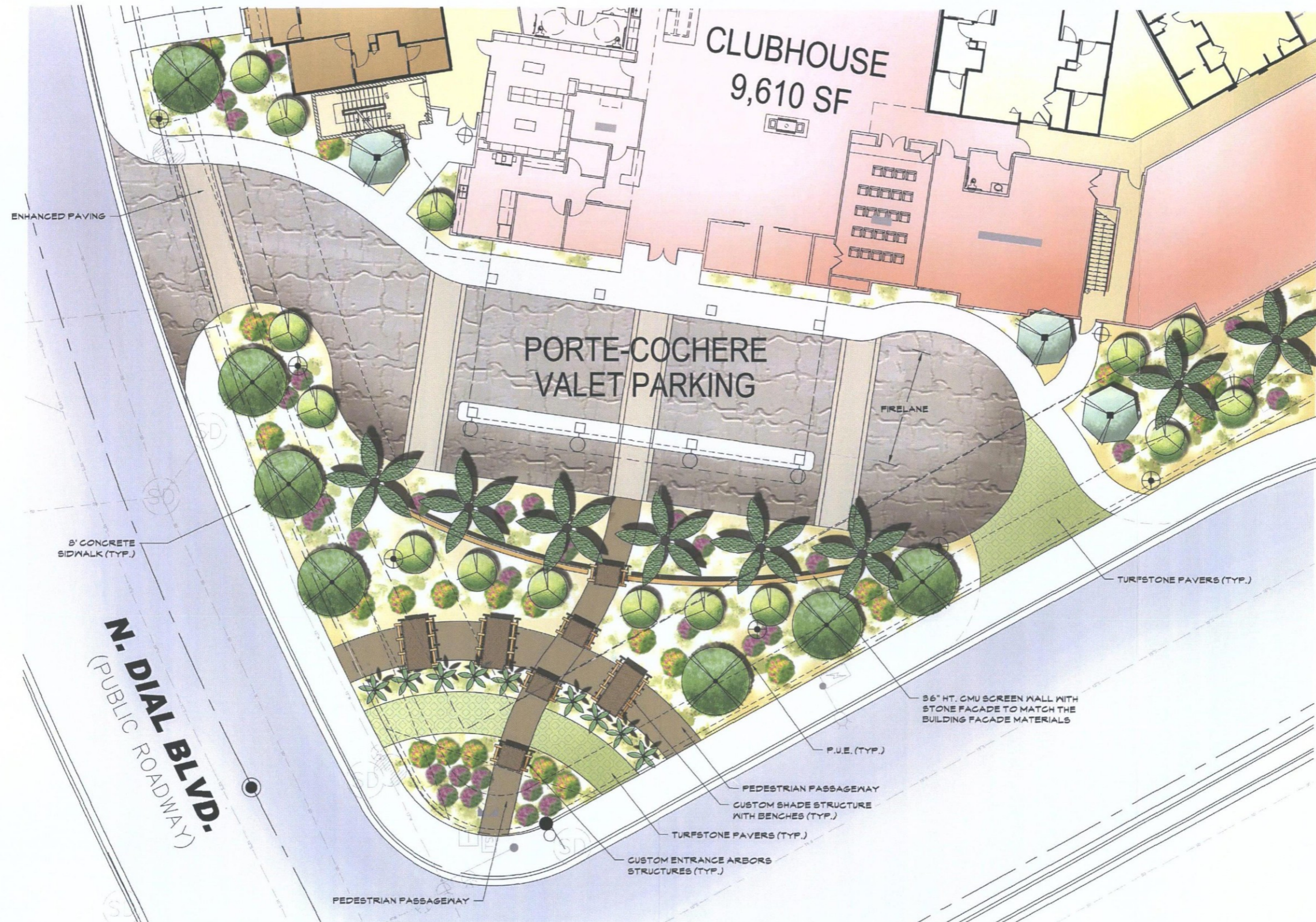
TOTAL OPEN SPACE AREA PROVIDED:
 - 96,402 SQ.FT (21.50% OF GROSS SQ.FT)
 (INCLUDES ALL OPEN SPACE AREAS, STREET FRONTAGE & PARKING LANDSCAPE AREAS).



DISTRICT AT THE QUARTER COMMON OPEN SPACE PLAN

15501 N. DIAL BLVD. SCOTTSDALE, AZ 85260
 AUGUST 30, 2016





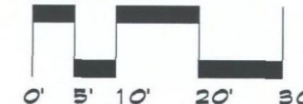
CORNER - ENTRY - PEDESTRIAN PASSAGEWAY - PARKING

DISTRICT AT THE QUARTER CORNER ENLARGEMENT

15501 N. DIAL BLVD. SCOTTSDALE, AZ 85260

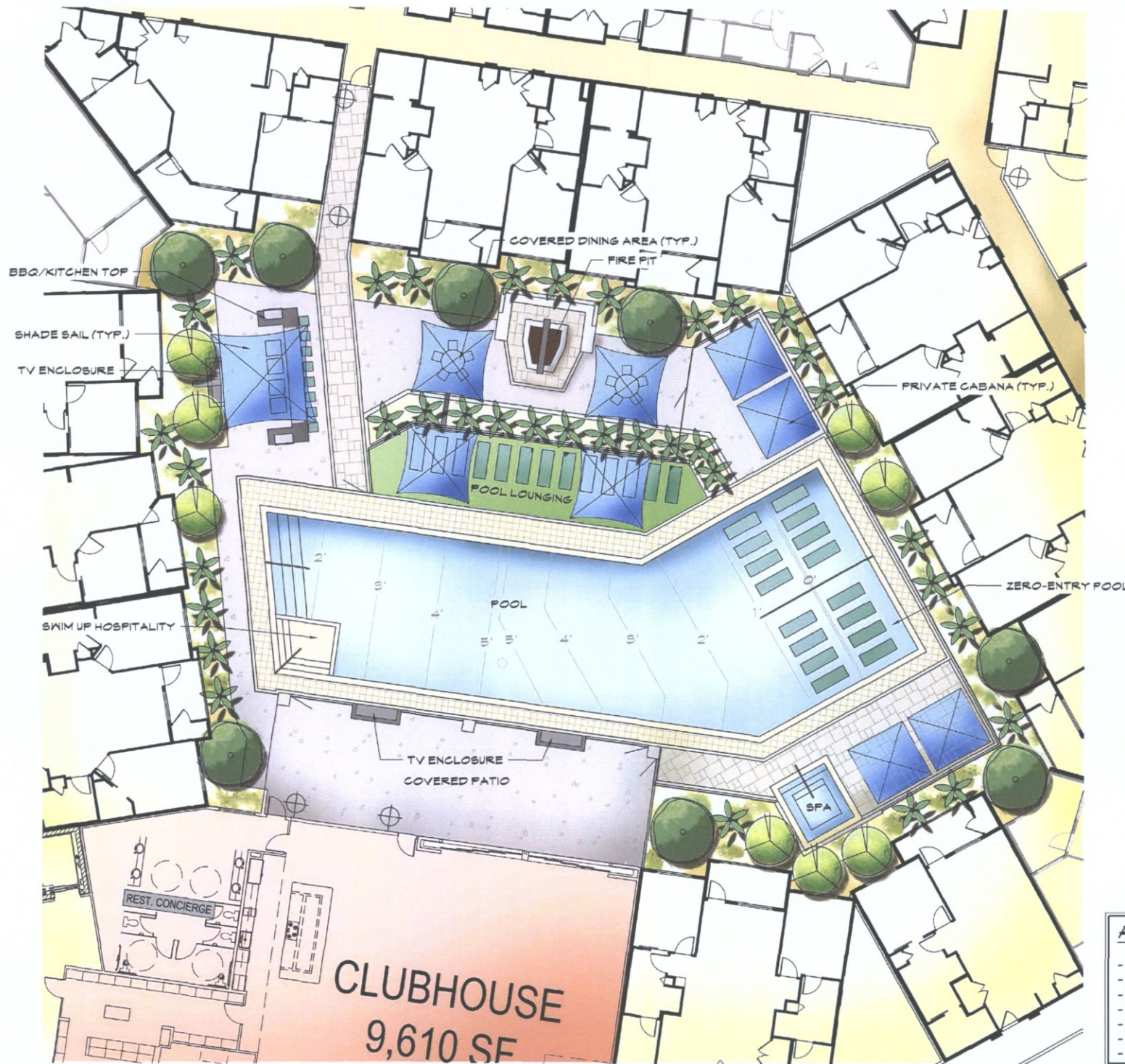
AUGUST 4, 2016

SCALE: 1"=10'-0"



NORTH



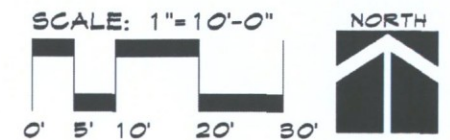


- AMENITIES**
- POOL WITH ZERO-ENTRY
 - FIRE PIT
 - (5) TV ENCLOSURES
 - BBQ/KITCHEN TOP
 - SEATING/LOUNGING AREAS
 - DINING AREAS
 - SPA

COURTYARD 1 - POOL ENTERTAINMENT

DISTRICT AT THE QUARTER
COURTYARD ENLARGEMENT 1

15501 N. DIAL BLVD. SCOTTSDALE, AZ 85260
 AUGUST 4, 2016





- AMENITIES**
- OUTDOOR KITCHEN/CULINARY DEMONSTRATION AREA
 - SEATING/LOUNGING AREAS
 - DINING AREAS
 - CITRUS TREES
 - HAMMOCK CLUSTER

COURTYARD 2 - OUTDOOR KITCHEN - CULINARY DEMONSTRATION

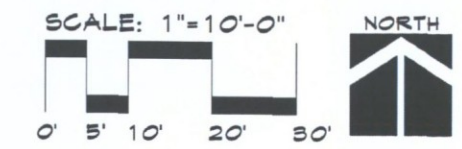


DISTRICT AT THE QUARTER

COURTYARD ENLARGEMENT 2

15501 N. DIAL BLVD. SCOTTSDALE, AZ 85260

AUGUST 4, 2016





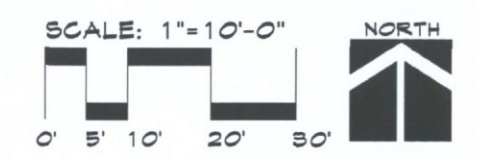
- AMENITIES**
- BOCCE BALL COURT
 - BAG TOSS COURT
 - PING PONG TABLE AREA
 - (2) TV ENCLOSURES
 - BBQ/KITCHEN TOP
 - SEATING/DINING AREAS

COURTYARD 3 - GAMING AREA/ACTIVE RECREATION



**DISTRICT AT THE QUARTER
COURTYARD ENLARGEMENT 3**

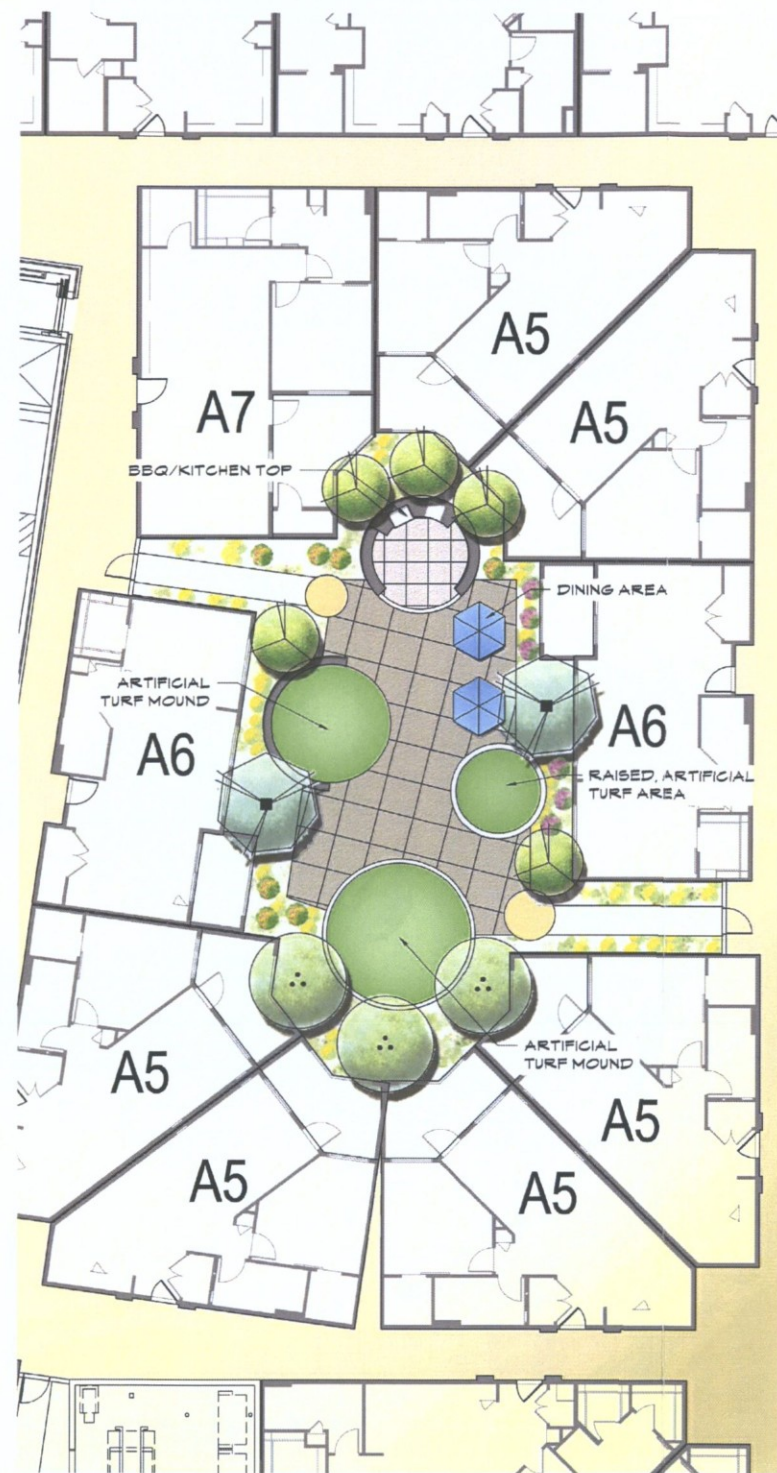
15501 N. DIAL BLVD. SCOTTSDALE, AZ 85260
AUGUST 4, 2016





COURTYARD 4 - OUTDOOR LIVINGROOM

- AMENITIES**
- FIRE PIT
 - (2) TV ENCLOSURES
 - BBQ/KITCHEN TOP
 - SEATING/LOUNGING AREAS
 - BAR TOP AREA



COURTYARD 5 - PASSIVE RECREATION

- AMENITIES**
- BBQ/KITCHEN TOP
 - SEATING/LOUNGING AREAS
 - DINING AREAS
 - ARTIFICIAL TURF MOUNDS



DISTRICT AT THE QUARTER COURTYARD ENLARGEMENTS 4 & 5

15501 N. DIAL BLVD. SCOTTSDALE, AZ 85260
AUGUST 4, 2016

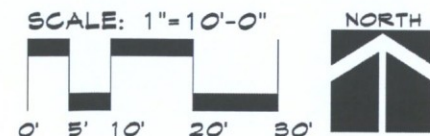
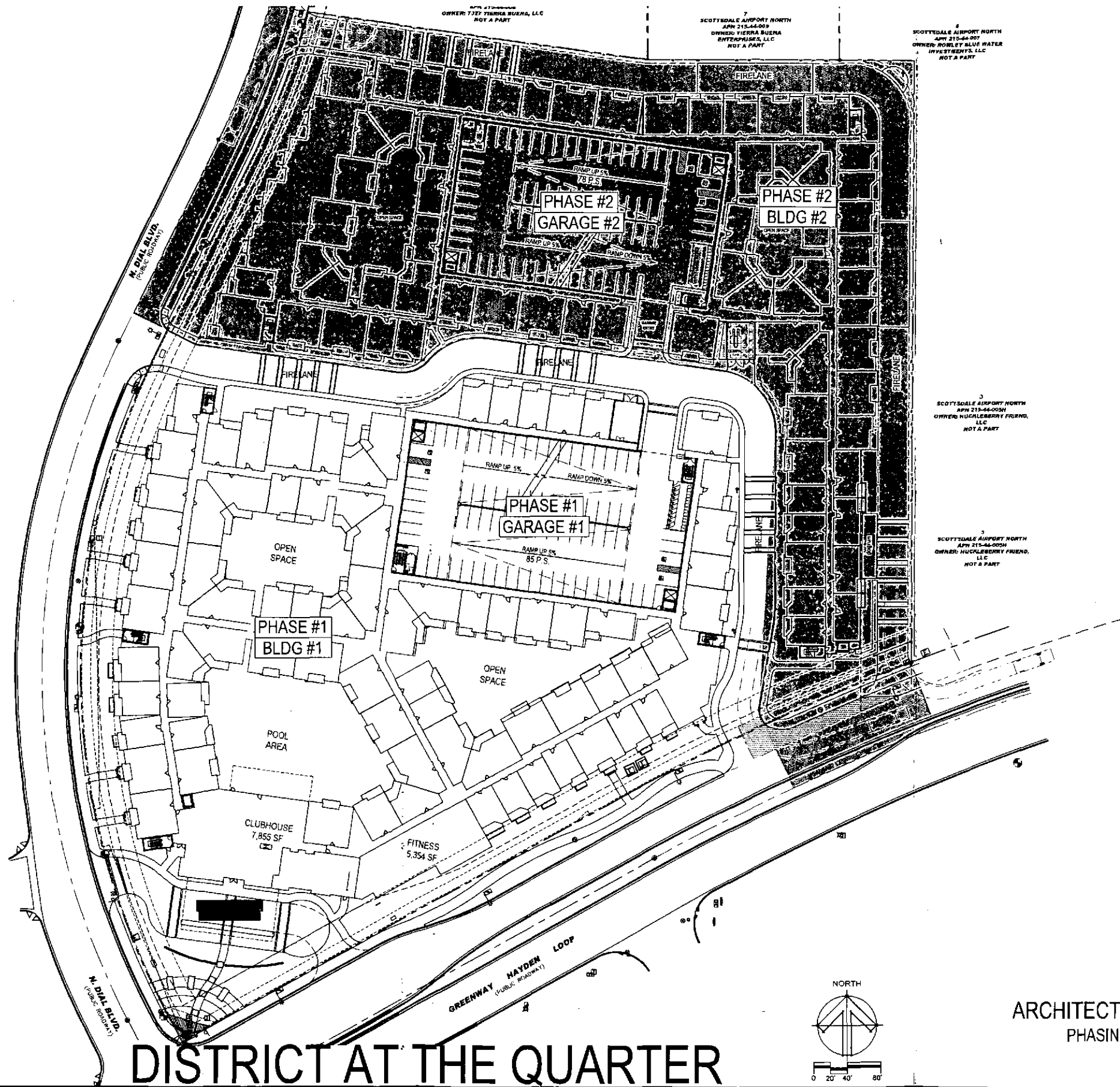


Exhibit 7

Phasing Plan



DISTRICT AT THE QUARTER
KAPLAN ACQUISITIONS, LLC

ARCHITECTURAL PHASING PLAN **A1.3**
1" = 40'-0"

8-03-16

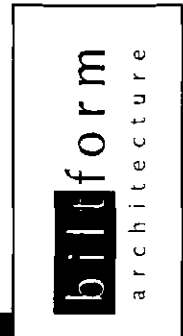
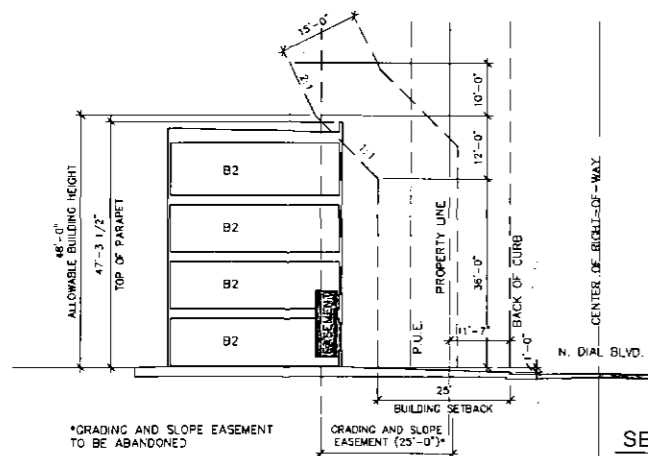
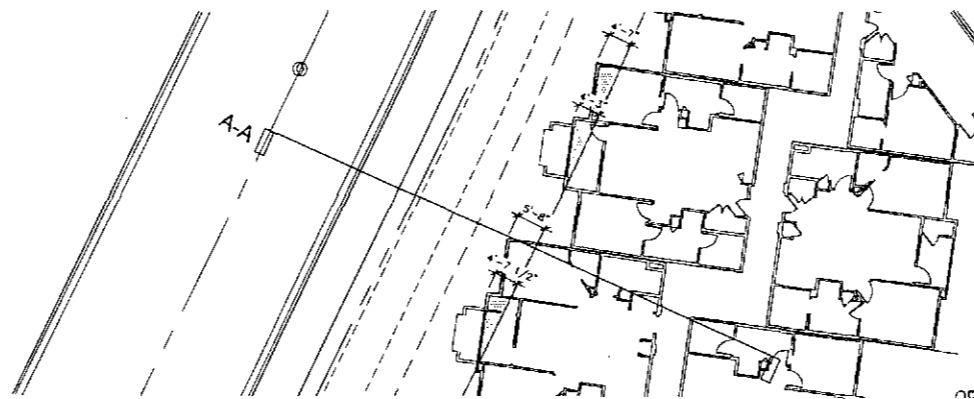


Exhibit 8

Building Cross Sections, Height Diagrams, and
Height Exemption Area

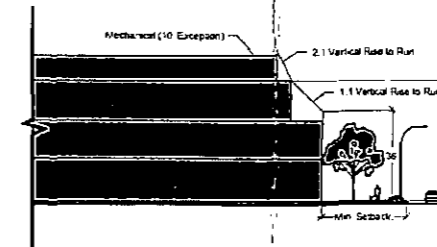


SECTION A-A
SCALE: 1/16" = 1'-0"

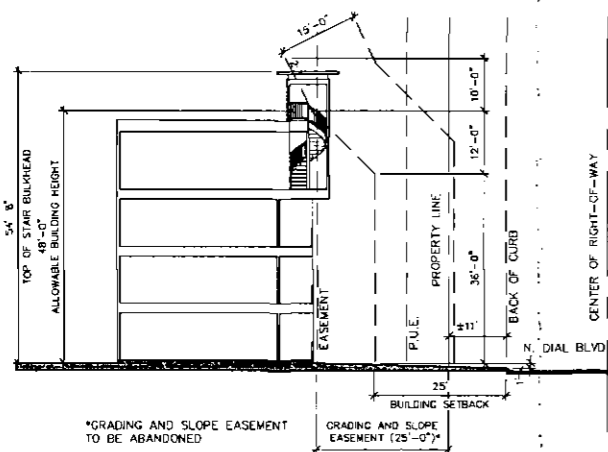


PHASE 2 - DIAL BOULEVARD

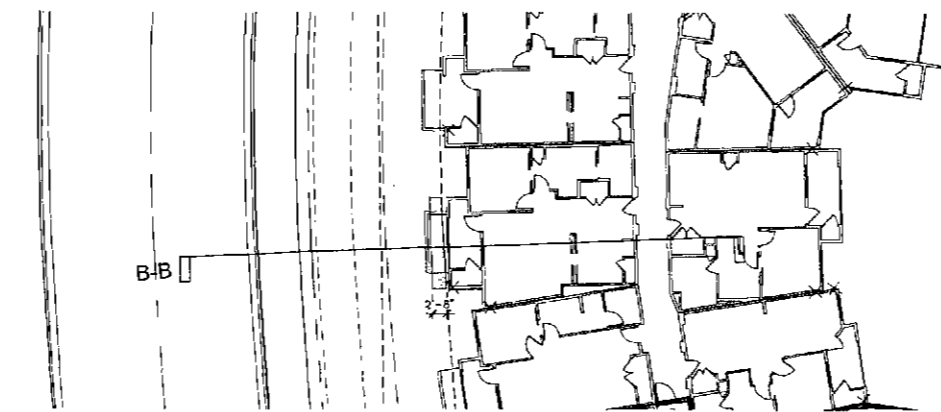
MAXIMUM ENCROACHMENT INTO EASEMENT
SCALE: 1/16" = 1'-0"



BUILDING ENVELOPE DIAGRAM 5.5005.F.1

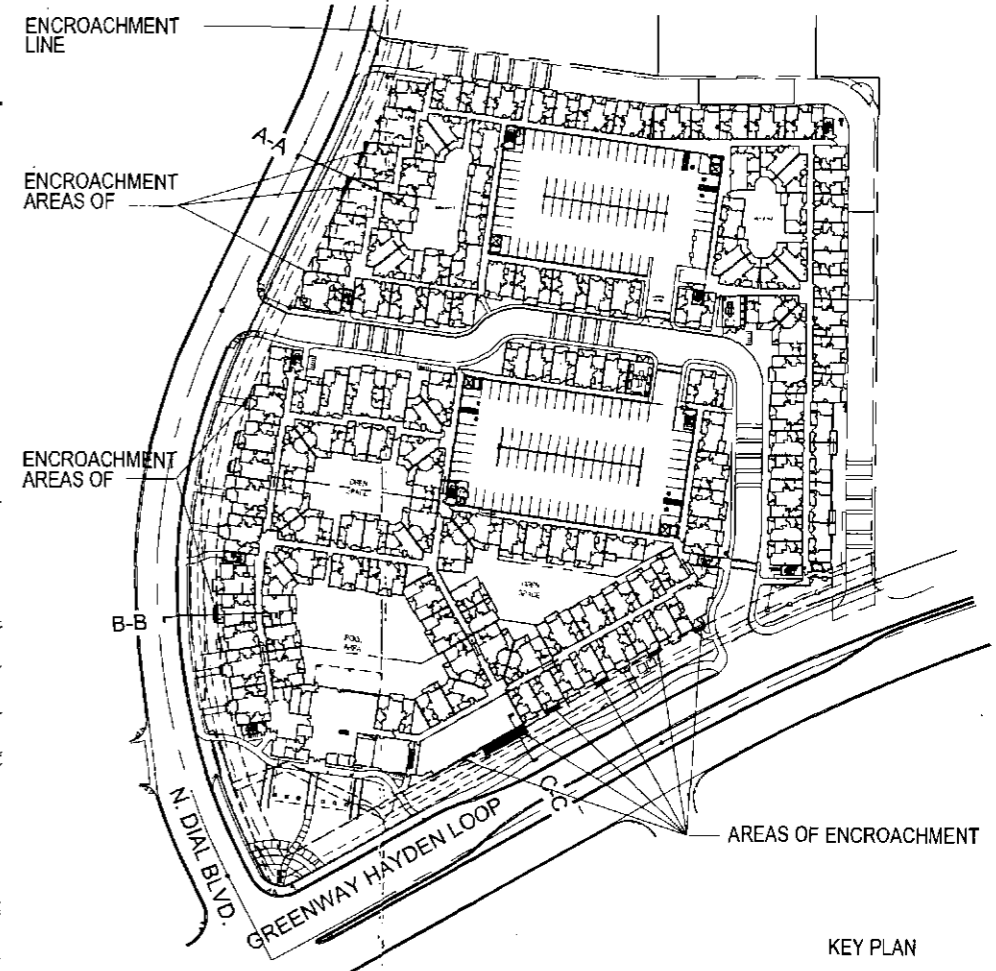


SECTION B-B
SCALE: 1/16" = 1'-0"



PHASE 1 - DIAL BOULEVARD

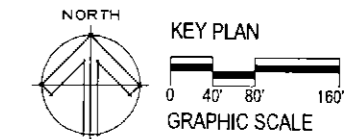
MAXIMUM ENCROACHMENT INTO EASEMENT
SCALE: 1/16" = 1'-0"



KEY PLAN

THE AVERAGE BUILDING HEIGHT IS +1529.34' (NAVD88)

THE AVERAGE CURB HEIGHT	1481.06'
PLUS 1'-0"	+ 1.00'
OVERALL AVERAGE CURB HEIGHT	1482.06'
ZONING ORDINANCE SECTION 3.102	
OVERALL AVERAGE CURB HEIGHT	1482.06'
MAXIMUM BUILDING HEIGHT	+ 48.00'
MAXIMUM ELEVATION	1530.06'



AREAS OF ENCROACHMENT

CROSS SECTIONS

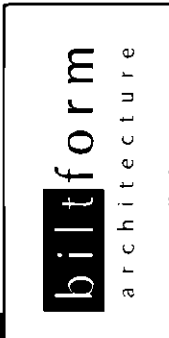
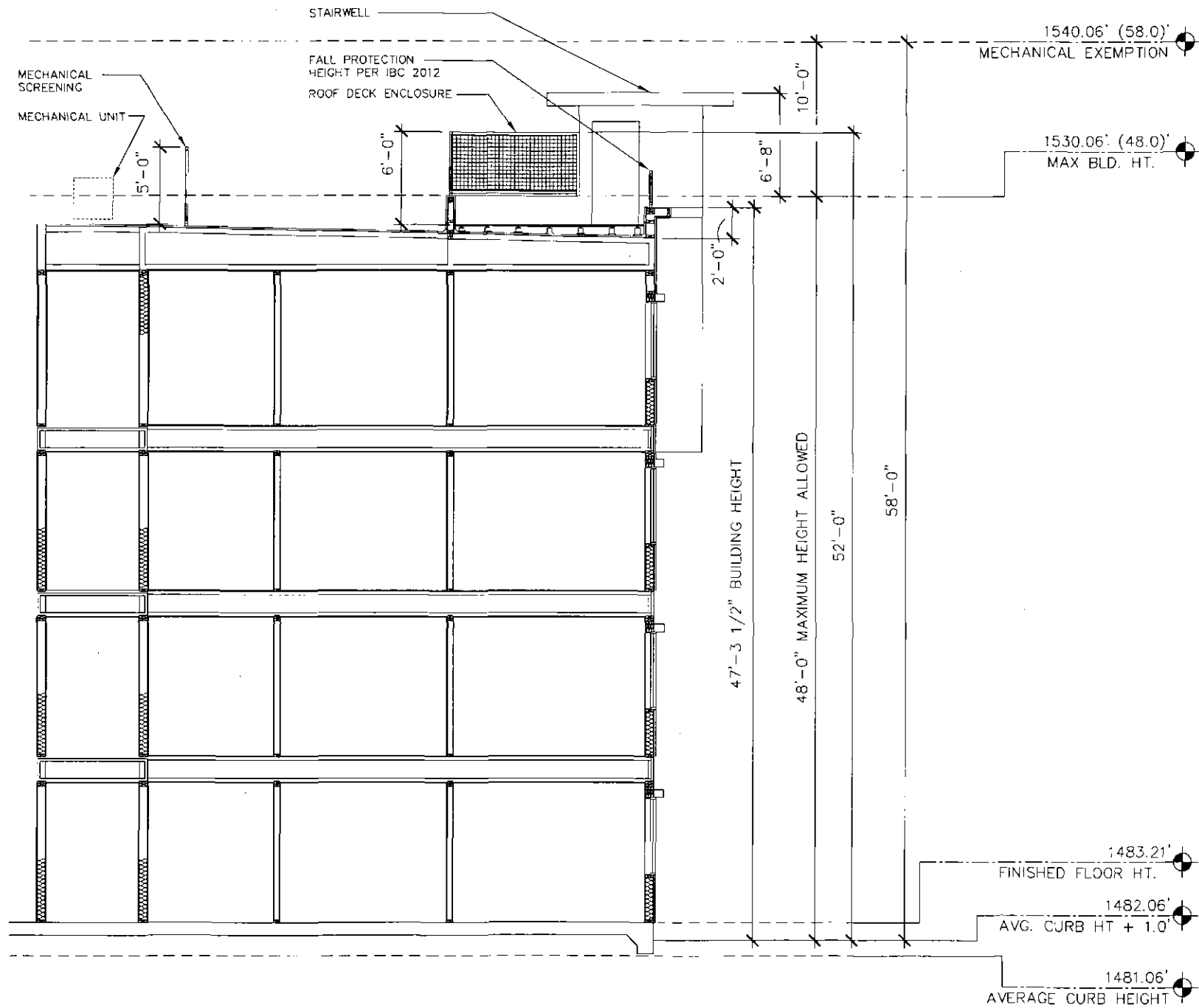
A3.1

8-31-16



DISTRICT AT THE QUARTER

KAPLAN ACQUISITIONS, LLC

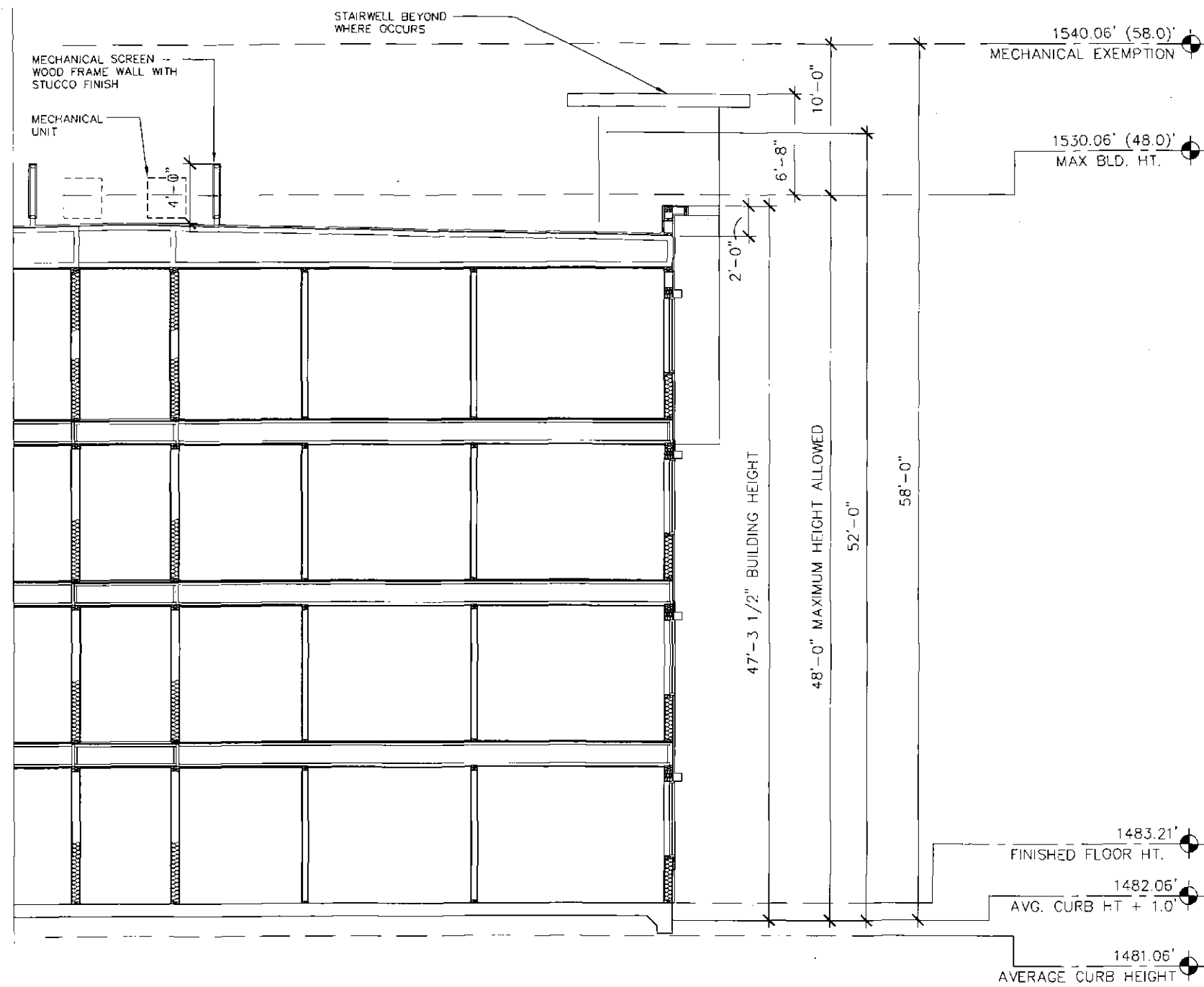


DISTRICT AT THE QUARTER
KAPLAN ACQUISITIONS, LLC

AVERAGE BUILDING HEIGHT DIAGRAM

A3.3

8-31-16



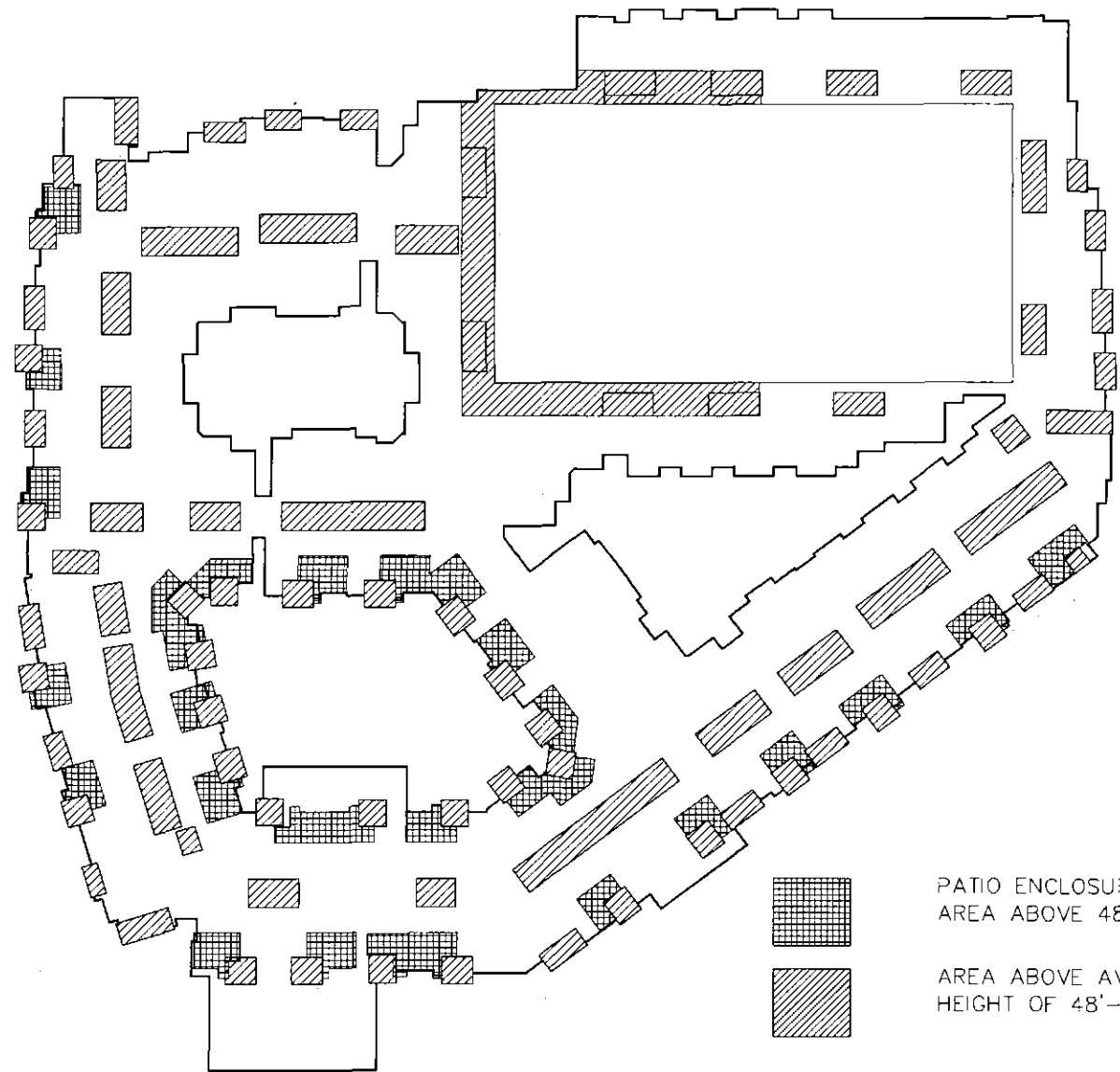
DISTRICT AT THE QUARTER
KAPLAN ACQUISITIONS, LLC



MECHANICAL SCREEN DIAGRAM

A3.4

1/4" = 1'-0"

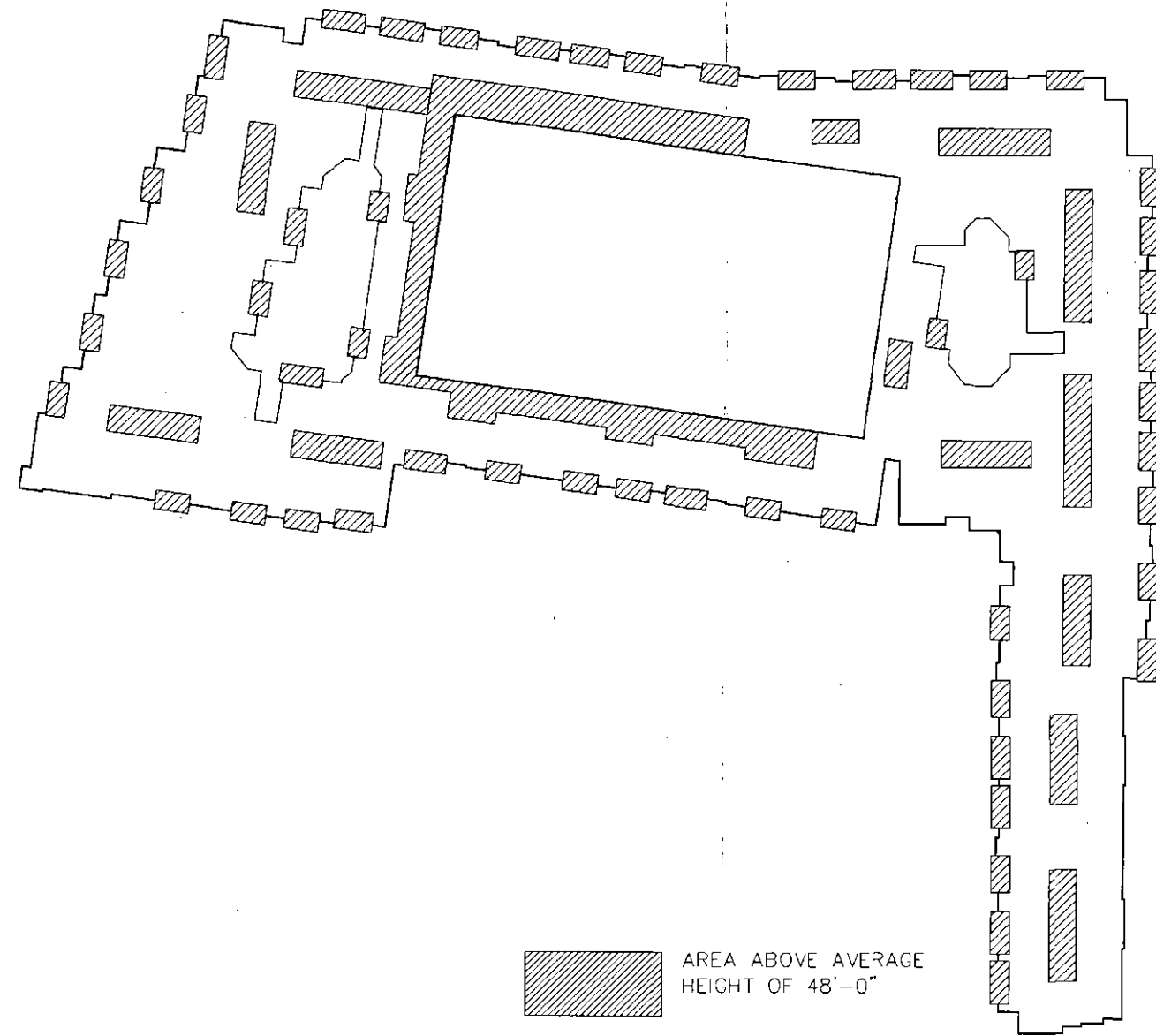
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 PATIO ENCLOSURE
 AREA ABOVE 48'-0"
 AREA ABOVE AVERAGE
 HEIGHT OF 48'-0"

ROOF AREA CALCULATION
 30% MAXIMUM PER 5.5005.D

30,957 SQ.FT. / 133,897 TOTAL
 ROOF AREA = 23%



 AREA ABOVE AVERAGE
 HEIGHT OF 48'-0"

22,003 SQ.FT. / 83,656 TOTAL ROOF
 AREA = 27.5%

ROOF AREA CALCULATION
 30% MAXIMUM PER 5.5005.D



DISTRICT AT THE QUARTER

KAPLAN ACQUISITIONS, LLC

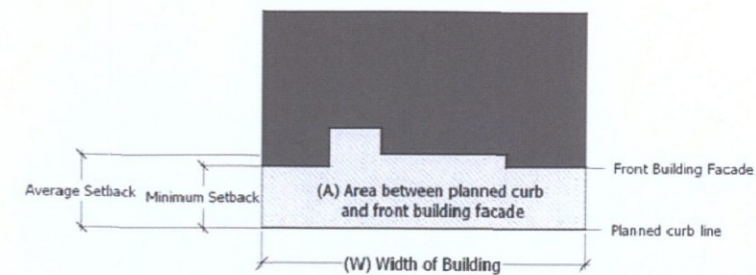
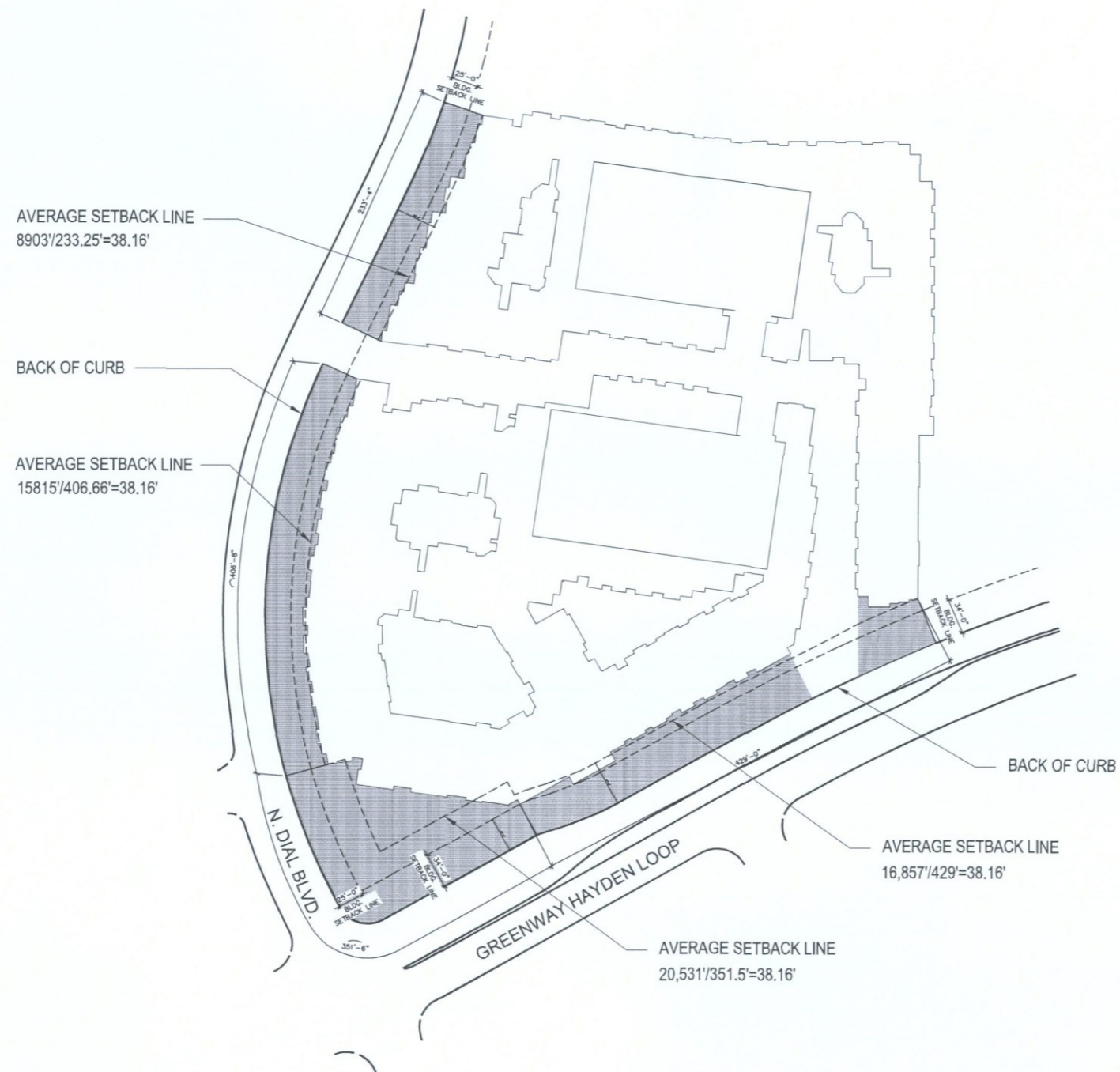
ROOF AREA
 NOT TO SCALE:

A3.5

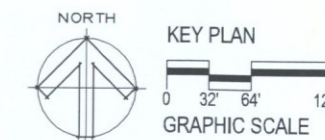
8-31-16

Exhibit 9

Average Building Setbacks



AVERAGE SETBACK DIAGRAM
THIS DIAGRAM ILLUSTRATES SECTION 5.5005.E.2.



AVERAGE SETBACK

A3.2

8-24-16

biltform
architecture

DISTRICT AT THE QUARTER
KAPLAN ACQUISITIONS, LLC

Exhibit 10

Amended Standards Legislative Draft

*****AMENDED STANDARDS LEGISLATIVE DRAFT*****

Section 5.5000. Planned Development Unit (PUD).

...

Section 5.5005. Development Standards.

...

E. Building setbacks.

1. *Minimum setback.* Buildings adjacent to public and private streets shall be setback from the back of the planned curb line, including the planned curb line for bus bays and turn lanes, in accordance with Section 5.5005.E. Table A. The planned curb line, including the planned location for bus bays and turn lanes, shall be as described in the Transportation Master Plan and the Design Standards & Policies Manual.

TABLE A			
BUILDING SETBACK FROM BACK OF THE PLANNED CURB ALONG STREETS			
Street Classification Type and Use	Minimum Setback	Average Setback	Setback Diagram - This Illustrates Setbacks, Excluding Average Setback. Dimensions are for Illustrative Purposes.
Major and Minor Arterial - Residential on First Floor Adjacent to the Street (Greenway-Hayden Loop)	34 feet	40 feet 38 Feet	

TABLE A

BUILDING SETBACK FROM BACK OF THE PLANNED CURB ALONG STREETS

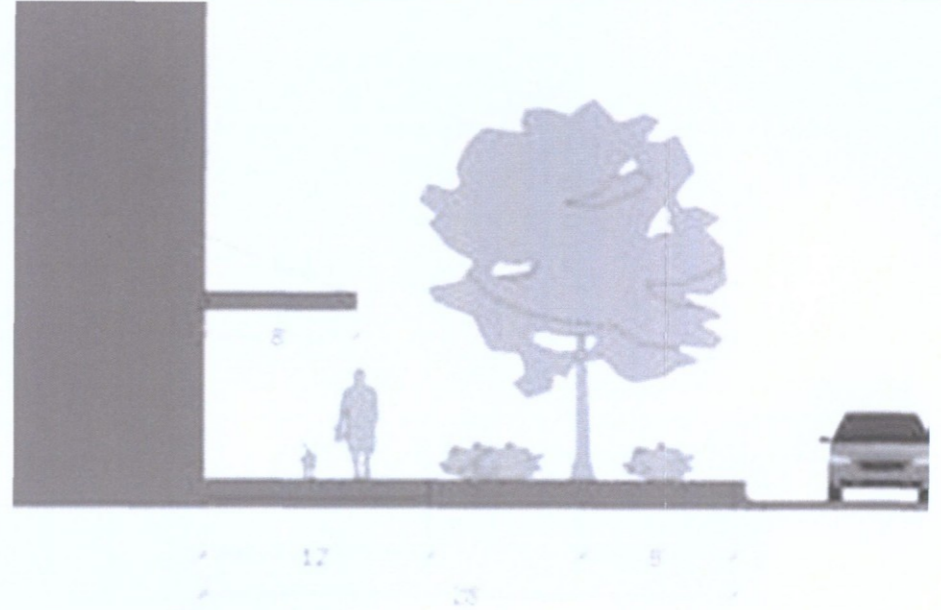
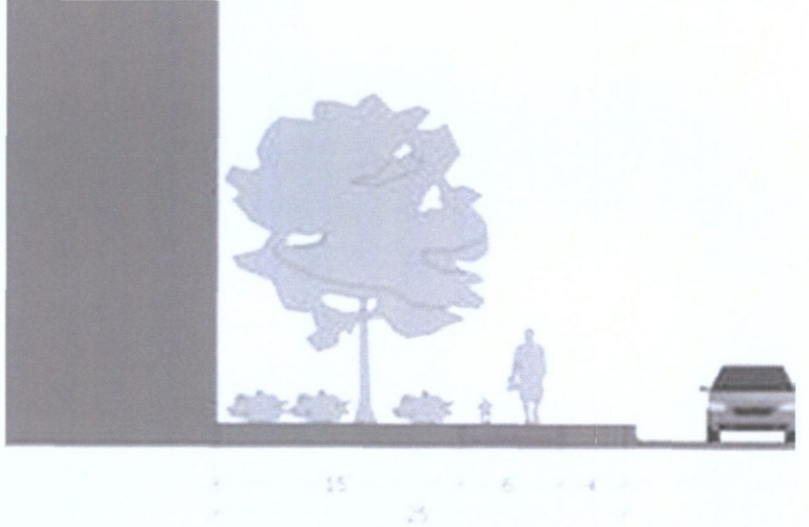
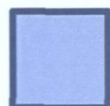
Street Classification Type and Use	Minimum Setback	Average Setback	Setback Diagram - This Illustrates Setbacks, Excluding Average Setback. Dimensions are for Illustrative Purposes.
Major and Minor Arterial - Retail and Commercial on First Floor Adjacent to the Street (Greenway-Hayden Loop)	28 feet	32 feet 38 Feet	
Major and Minor Collector, Local Streets, and Unclassified Streets (Including Private Streets but Excluding Alleys) - Residential on First Floor Adjacent to the Street (73rd Street/Dial Boulevard)	25 feet	30 feet 38 feet	

Exhibit 11

Right-of-Way

Right-Of-Way Exhibit
"District at the Quarter"

Gross Acreage: 10.29 ac
Dial/ 73rd Street: 30' Half Street R.O.W.
Greenway-Hayden Loop: 55' Half Street R.O.W.

 Indicates Right-Of-Way along Dial/ 73rd Street and Greenway-Hayden Loop. (Approximately 1.45 acres)

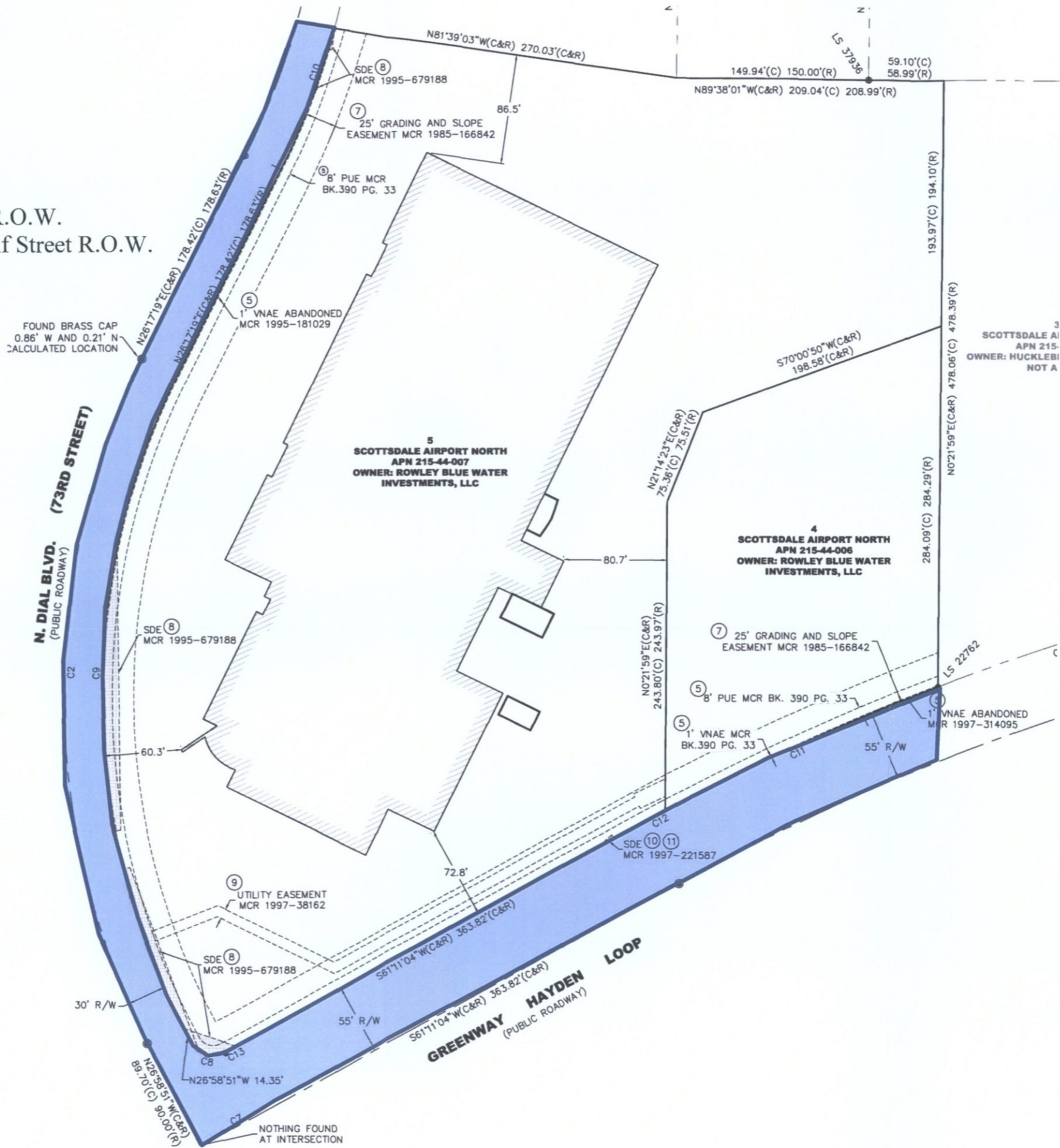


Exhibit 12

Arizona Republic Article and Colliers
International Report

Scottsdale Airpark booming with urban life, 55,000 jobs

[Parker Leavitt](#), The Republic | [azcentral.com](#) 2:53 p.m. MST March 25, 2016



A small jet comes in for a landing at Scottsdale Airport on March 10, 2015. The airport is one of the busiest corporate jet facilities in the state. (Photo: Tom Tingle/The Arizona Republic)

A record number of businesses and employees have moved into Scottsdale's bustling Airpark corridor, which is emerging as a one of the Phoenix area's major hubs for urban growth, according to a new report from Colliers International.

More than 55,000 employees work for about 3,000 companies within the Scottsdale Airpark area, which covers about 5 square miles in north Scottsdale and Phoenix's Kierland neighborhood. The Airpark added nearly 1,200 jobs over the past year and more than 7,000 jobs since the height of the recession in December 2009, according to Colliers.

The previous record for jobs in the Scottsdale Airpark area was set in 2014 at 54,100, the report said. There were also 2,950 companies in the area last year.

The annual report, compiled by Jim Keeley, founding partner of Colliers' Scottsdale office, also shows significant improvement in vacancy rates, land sales and property values over the past

several years. Colliers is among the real-estate firms that broker deals in the Airpark and other parts of the Valley.

Although Airpark office space was selling for about \$50 to \$70 per square foot in 2011, tenants are paying closer to \$150 or even \$200 per square foot now, Keeley said. He expects rates to continue climbing over the next two or three years toward the prerecession peak of around \$270 to \$300.

Land sales totaled \$94.8 million for about 89 acres in 2015, up from \$46.7 million for about 38 acres the previous year, according to Colliers.

Investment from GoDaddy founder Bob Parsons and global investment firm Vanguard helped the Airpark build on that momentum in 2015, Keeley said.

His Colliers report detailed several key announcements for the area last year, including:

- Parsons opened the world's largest Harley-Davidson dealership in November at Hayden Road and Northsight Boulevard. The dealership features a 95-seat chapel for weddings and a movie theater.
- Vanguard added 600 employees while expanding to fill a 150,000-square-foot building near Pima Road and Raintree Drive.
- American Girl opened its only Arizona retail store, which includes a 25-seat bistro, at Scottsdale Quarter in August. The store is one of only 20 in the U.S.

Urban living arrives, transit needed

Residential towers have begun to spring up among the Airpark's corporate offices and shopping centers as young professionals pursue a more urban lifestyle within walking distance of their workplaces.

Crescent Communities recently opened 275 high-end apartments at Scottsdale Quarter, and another 220 luxury condos are under construction across Scottsdale Road at Optima Kierland.



This rendering shows Optima Kierland, a 12-story luxury condominium development going up at Scottsdale Road and Kierland Boulevard. (Photo: Optima Kierland)

Liv North Scottsdale built 240 apartments just north of Scottsdale Quarter in 2014, and a 282-unit complex called Sunrise Commons is planned to replace a former car dealership on Hayden Road.

"I think society is into enjoying urban living," Keeley said. "The Millennials, that next generation of working people, they have a different mind-set."

With the arrival of more "transit-oriented development" at Scottsdale Quarter and Kierland Commons comes the need for more public-transit options, Keeley said. A city proposal to add trolley service in the Airpark would help, but Keeley would like to see light rail come up through Scottsdale from the south.

"There's a definite need for improvement," Keeley said. "I think if you look around the country at other employment bases that are evolving, you need that type of alternative."

A new transportation plan up for City Council consideration this year includes three potential rail routes through Scottsdale, including two that would end near Scottsdale Fashion Square mall. The plan also includes a new bus route with non-stop service between the Airpark and Fashion Square.

If approved by the council, Airpark trolley service could start in April 2018, and the bus service could start in October 2019, according to city officials.

A cornucopia of industries

Although some airport commerce parks trend more toward heavy industry and warehousing, the area surrounding Scottsdale Airport features a higher concentration of corporate office space, including several regional and national headquarters.

JDA Software announced plans last year to move its headquarters to two floors of a new six-story office building opening at Scottsdale Quarter. Taser International maintains its main offices 2 miles to the northeast, and New Zealand-based Orion Health picked the Scottsdale Airpark in 2014 for its North American headquarters and about 500 jobs.

In all, there are 130 business categories represented within the Airpark, ranging from aerospace to hospitality, according to Colliers.

The area ranks among the largest employment centers in the Phoenix area and is uniquely located to draw employees from the West Valley, Phoenix, Scottsdale and the Southeast Valley, Scottsdale Economic Development Director Danielle Casey said.

"The top thing that everybody has been talking about in the last several years is that talent is the biggest issue," Casey said. "That's one of the things going for us in the Airpark. It can pull from pretty much all nodes, which is very advantageous when you are looking at a dispersed talent pool."

Airport grows busier

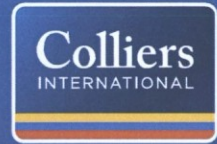
Along with the growth has come a spike in air traffic at Scottsdale Airport, which in 2015 surged to No. 21 among the nation's busiest general-aviation airports. When you account for major airlines, which don't have a presence in Scottsdale, and military operations, the airport ranks No. 76 nationally, according to the Federal Aviation Administration.

U.S. Customs and Border Patrol maintains a daily presence at Scottsdale Airport, which boosts its appeal to international travelers and businesses, Casey said.

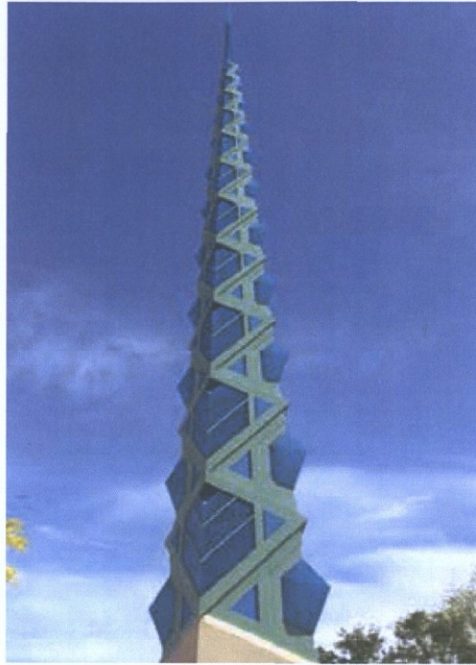
The FAA reported about 77,300 general-aviation takeoffs and landings at Scottsdale Airport last year, up from about 74,700 flights in 2014 and 69,800 in 2010. General-aviation traffic reflects private flights to and from other airports.

The airport reported more than 157,000 takeoffs and landings overall in 2015.

THE GREATER SCOTTSDALE AIRPARK 2030 REPORT



Jim Keeley, SIOR, CCIM
Founding Partner
Scottsdale Office



Spire

Standing 125 feet tall on the southeast corner of Frank Lloyd Wright Boulevard and Scottsdale Road, the Spire has become one of Scottsdale's most recognizable shapes since it was built in 2004. Designed by Frank Lloyd Wright originally for the state capitol building, the Spire instead stands on his namesake road in Scottsdale. This was considered tall in 1994.

Jim Keeley Prediction - "By 2024, it is reasonable to expect having 10 buildings taller than the Spire."

LOCATION

The "Greater Scottsdale Airpark" Boundaries:

NORTH Loop 101 Pima Freeway

SOUTH Thunderbird Rd.

WEST 64th St.

EAST 90th St. - between Raintree & the CAP Canal; 96th St. - between the CAP Canal & Bell Rd.

ZONING

Commercial Office; Light Industrial; Hotel; Commercial Retail; Aviation; Multifamily

SIZE

±3,300 Acres

±34,193,616 SF of Buildings

CURRENTLY DEVELOPED

±3,100 Acres

NUMBER OF COMPANIES

±3,025

BUSINESS CATEGORIES

±130 (i.e., accountants, auto, publishing, aerospace/defense, light manufacturing, retail, high tech, healthcare & biotech, business services, hospitality, retail, etc.)

NUMBERS OF EMPLOYEES

±55,260

NUMBERS OF BUILDINGS - 1,156

(including hotels, auto dealerships, all retail)

Building Usage	Square Feet	Percentage
Office	12,886,844	38%
Industrial/Flex	9,241,921	27%
Hangar	837,772	2%
Retail	8,331,727	24%
Hotels	2,895,352	8%
Total	34,193,616	

The Cogs in the Wheel of the Greater Scottsdale Airpark



January, 2016 - Past, Present & Future to 2030

Jim Keeley, Founding Partner Scottsdale Office, 14080 N. Northsight Blvd., Scottsdale, AZ
480 596 9000 Jim.Keeley@colliers.com

How Do The Centers of Influence Impact the Greater Scottsdale Airpark?

FEE SIMPLE LAND: The Spirit of the Wild West predicated that Rugged Individualism helped build Arizona and the ability to own land. Fee Simple has been a big catalyst for the Greater Scottsdale Airpark. As an employment base, we are in close proximity to State of Arizona land and SRP Indian Community land which allows development on leased land. With Fee Simple land diminishing, it is likely that more State Leased land will become part of the Greater Scottsdale Airpark Employment Base.

CORNUCOPIA OF 130+ INDUSTRIES: From accountants to millennials, auto, publishing, aerospace/defense, light manufacturing, retail, high tech, healthcare & biotech, business services, hospitality, retail, etc.

TRANSPORTATION: Loop 101 Freeway: Prior to the opening in of the Loop 101 freeway in 2000, the Greater Scottsdale Airpark had already established itself as an employment base with 15,000,000 square feet of buildings and 31,000 employees. Since then it has grown to 34,000,000 square feet of buildings and 54,100 employees.

BOB PARSONS: The Leader and the epitome of Rugged Individualism.

- > Go Daddy Founder
- > World's largest Harley Davidson dealership
- > Parson's Extreme Golf "PXG"
- > YAM Properties
- > Philanthropy
- > Sneaky Big Studios - Scottsdale (Arizona's largest production studio)
- > Big Yam, The Parsons Agency

MULTI FAMILY:

- > North Scottsdale Apartments: 240 units
- > Crescent Communities: 275 units
- > Plaza Loft at Kierland Commons: 54 units
- > Optima Kierland Center: 220 units (under construction)
- > Sunrise Commons: 282 unites (planned)
- > The View at Cascade: 187 units

COMMUNICATION: This year will mark the 35th anniversary of the Scottsdale Airpark News, which will continue to cover all the local news and provide a place for the cornucopia of businesses in The Airpark to share their stories. Social media will also continue to play a major role in all of our communications.

TRANSIT ORIENTED DEVELOPMENT (TOD): Driven by our change in culture and Millennials, the Scottsdale Quarter and Kierland Commons are the best current examples of TOD. These two centers bring together ambience, shopping, offices, restaurants, entertainment, condos and apartments.

MAJOR EVENTS:

- > Barrett Jackson Collector Car Auction
- > Waste Management Phoenix Open Golf Tournament
- > Arabian Horse Show
- > International Motorcycle Show
- > Westworld Events
- > International Sportsman's Expo

HIGHER DENSITY: The Scottsdale Quarter, a 1,200,000 SF mall, replaced the former Dial/Henke 200,000 SF facility on 28 acres. That MXD set the new standard and several more will follow in the same fashion.

NEWS IN 2015:

- > Regus: Opens in Raintree Corporate Center to complement the other Airpark office at Kierland. Originally just Executive Suites, they've now expanded to offer "Shared Office Space" and "Co-Working" environments that creates culture and community. "We Work" is another concept now open in 8 major cities and making its way to future employment bases.
- > Harley-Davidson of Scottsdale: Bob Parsons opened the world's largest Harley Davidson Dealership in November which includes a 95-seat chapel and multi-purpose room for weddings and events, a coffee bar, movie theater and many more unique amenities.
- > American Girl: The Mattel-owned brand opened at the Scottsdale Quarter in August to a frenzied crowd. It is the 20th US store to open and includes a 25-seat bistro. The American Girl trend is fueled in part by Millennials who are now parents and loved American Girl growing up.
- > Vanguard: Added 600 employees, taking over the entire 150,000 SF building at 15111 N. Pima Road.
- > Persil: A Henkel detergent brand goes from Europe to Scottsdale .
- > Economic Impact Study: Conducted by Lee McPheters, Director of the JPMorgan Chase Economic Outlook Center at ASU, in April, 2015, determined that aviation activity at the Scottsdale Airport, generated \$536,000,000 in total economic benefits for the region last year.

LAND SALES				
YEAR	# OF LOT SALES	SALES VOLUME	TOTAL ACRES	AVG PRICE/SF
2015	12	\$94,821,765	88.85	\$24.50
2014	13	\$46,716,714	37.57	\$24.00
2013	6	\$26,558,420	25.36	\$24.04
2012	4	\$10,880,978	15.36	\$16.25
2011	3	\$4,803,456	8.21	\$13.43
2010	5	\$6,209,156	9.73	\$14.64
2009	0	N/A	N/A	N/A
2008	3	\$7,250,000	4.45	\$37.40
2007	16	\$55,896,182	51.14	\$25.09

THE GREATER SCOTTSDALE AIRPARK GROWTH - JUNE 1981 - DECEMBER 2014				
DATE	SQUARE FEET	VACANCY	NUMBER OF EMPLOYEES	NUMBER OF BUSINESSES
Jun. 1981	1,580,611	13.9%	±3,320	268
Jun. 1982	1,720,210	10.1%	±3,680	289
Jun. 1983	1,900,188	6.1%	±4,038	316
Jan. 1984	2,288,439	10.9%	±4,504	383
Jun. 1984	2,587,000	10.5%	±5,320	460
Jan. 1985	2,683,394	11.8%	±5,345	480
Jul. 1985	3,206,670	17.3%	±5,500	517
Jan. 1986	3,673,589	18.7%	±5,861	534
Jan. 1987	4,419,226	19.4%	±8,661	733
Aug. 1987	4,811,007	19.0%	±8,912	841
Aug. 1988	4,881,979	16.0%	±9,802	957
Aug. 1989	5,234,493	17.2%	±10,355	930
Aug. 1990	5,396,211	14.3%	±10,120	943
Aug. 1991	5,868,194	14.2%	±10,654	1,082
Aug. 1992	6,141,078	12.6%	±11,345	1,076
Nov. 1993	6,146,866	6.7%	±13,402	1,237
Sep. 1994	6,569,987	3.5%	±13,846	1,257
Sep. 1995	6,932,228	2.3%	±14,132	1,332
Dec. 1996	8,126,066	3.1%	±17,275	1,477
Dec. 1997	9,697,203	3.2%	±19,837	1,624
Dec. 1998	11,085,954	4.1%	±25,891	1,708
Dec. 1999	14,626,300	6.0%	±30,344	1,807
Dec. 2000	15,657,920	7.6%	±31,992	1,868
Dec. 2001	16,282,920	9.8%	±33,015	1,907
Dec. 2002	17,356,120	12.9%	±41,265	2,198
Dec. 2003	18,562,463	11.9%	±44,815	2,288
Dec. 2004	20,553,975	11.5%	±47,218	2,397
Dec. 2005	21,403,975	9.9%	±48,634	2,481
Dec. 2006	22,350,975	9.7%	±50,450	2,554
Dec. 2007	28,454,277	12.9%	±52,500	2,774
Dec. 2008	28,852,256	15.1%	±52,000	2,800
Dec. 2009	28,942,406	29.0%	±48,000	2,576
Dec. 2010	28,962,889	28.8%	±48,500	2,580
Dec. 2011	28,971,900*	23.0%	±49,200	2,601
Dec. 2012	33,928,220*	18.5%	±52,000	2,848
Dec. 2013	33,986,890*	16%	±53,400	2,900
Dec. 2014	34,006,890*	13%	54,100	2,950
Dec. 2015	34,193,616*	11.6%	55,260	3,025

*Using the newest technology (CoStar) we have updated the list of all buildings, including auto dealerships, Costco, Walmart, etc., that we had not included in the past.

Major Companies

Accolade Healthcare
 Alliance Defense Fund
 APL Container Transportation Services
 Apogee Physicians
 AutoNation Ford
 Best Western
 Chase Bank
 Choice Hotels International, Inc.
 Colliers International
 Corporate Jets, Inc.
 Costco
 Cox Cable of Scottsdale
 Dillon Precision Industries
 Discount Tire Company, Inc.
 Early Warning Services
 Earnhardt Cadillac
 Fairmont Scottsdale Princess
 Farmers Insurance
 Fender Musical Instruments Corporation
 First Fidelity Bank
 Flow Dynamics, Inc.
 GE Franchised Finance Corporation
 General Mills
 Go AZ Motorcycles
 Go Daddy Software
 Go Video
 GovNet, Inc.
 GPS Insight
 GTCO CalComp Input Technologies Division
 Harley-Davidson of Scottsdale
 Henkel Corporate Headquarters
 Home Depot
 HomeSmart
 Hypercom Corporation
 IKON
 Ingram Micro
 Interface, Inc.
 International Cruise & Excursions (SEARS)
 IO Capital Princess, LLC
 JDA Software
 Knowledge Net
 Kohl's Department Store
 KYOCERA Solar
 Lowes Home Improvement Warehouse
 Markel West
 Mayo Clinic
 Mercer Advisors, Inc.
 Meritage Corporation
 Metris Company
 MidFirst Bank
 Mobility Electronics
 Morgan Stanley Smith Barney, LLC
 Nautilus Insurance Co.
 North Central University
 Orion Health
 Pegasus Solutions
 Peter Piper Corporate Headquarters
 Phase 2 Solutions
 Prudential Insurance
 Pulte Homes
 Rental Service Corp.
 Redirect Health
 Right Honda and Toyota
 Russo & Steele
 Schumacher European, Ltd. (Mercedes-Benz)
 Scottsdale Association of Realtors
 Scottsdale Prep Academy
 SonicAir
 South Hills Design Corporation
 State Farm Insurance
 Target
 TASER International
 Tesla Motors
 The Tech Group / Tech Poulson
 Trivita
 Universal Technology Institute (UTI)
 U.S. Postal Service
 U-Haul
 Unison Health Care
 Van Chevrolet
 VanGuard
 Wal-Mart Supercenter
 Westin Kierland Resort
 World at Work

BUILDING SALES					
TYPE	# OF SALES	SALES VOLUME	TOTAL SF	AVG PRICE/SF	HIGH/LOW
2015					
Office	24	\$178,174,381	919,060	\$194	\$254/\$98
Industrial	23	\$48,994,677	395,149	\$124	\$192/\$77
Retail	13	\$97,120,600	448,244	\$217	\$767/\$132
Total	60	\$324,289,658	1,762,453		
2014					
Office	30	\$69,800,000	555,965	\$139	\$217/\$79
Industrial	39	\$60,000,000	668,488	\$114	\$257/\$52
Retail	6	\$39,744,000	208,927	\$190	\$414/\$15,
Total	75	\$169,544,000	1,433,380		
2013					
Office	22	\$179,000,800	1,098,164	\$163	\$312/\$97
Industrial	25	\$24,194,800	9,000	\$101	\$149/\$58
Retail	2	\$4,200,000	239,552	\$250	N/A
Total	49	\$207,395,600	1,346,716		
2012					
Office	41	\$204,454,653	998,687	\$114	\$305/\$48
Industrial	28	\$50,662,667	553,022	\$92	\$303/\$25
Retail	7	\$94,522,237	637,434	\$148	\$350/\$101
Total	76	\$349,639,557	2,189,143		
2011					
Office	29	\$27,185,499	261,400	\$104	\$188/\$54
Industrial	38	\$85,246,048	835,745	\$102	\$185/\$58
Retail	11	\$68,809,575	537,574	\$128	\$248/\$85
Total	78	\$181,241,122	1,634,719		
2010					
Office	37	\$66,533,044	608,465	\$109	\$219/\$56
Industrial	24	\$21,838,832	206,253	\$106	\$235/\$67
Retail	5	\$5,797,024	45,855	\$126	\$175/\$69
TOTAL	66	94,168,900	860,573		
2009					
Office	16*	\$37,917,000	308,000	\$123	\$285/\$75
Industrial	3	\$8,197,000	65,027	\$126	\$196/\$80
Retail	0	N/A	N/A	N/A	N/A
TOTAL	19	\$46,114,000	373,027		
*69% of the office sales in 2009 were for office condos smaller than 8,600 SF					
2008					
Office	17	\$23,739,375	92,360	\$257.03	\$348/\$157
Industrial	14	\$23,551,319	144,345	\$163.16	\$324/\$118
Retail	5	\$140,955,000	430,106	\$327.72	\$552/\$167
TOTAL	36	\$188,245,694	666,681		



Jim Keeley SIOR, CCIM
 Founding Partner
 Scottsdale Office
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 www.colliers.com/greaterphoenix

Jim Keeley, SIOR, CCIM, founded Classic Real Estate Corporation, now known as CC Partners, L.L.C., DBA Colliers International, in 1983. The firm conducts the majority of its commercial brokerage business within the Scottsdale Airpark and the Loop 101 employment base and has concluded over 5,000 transactions and \$2.7 billion of business.

A special thanks to Colliers International brokers for their input and, in particular, to Cristiana Zarr for the graphic design, Pete O'Neil for research efforts and Jamie McClellan for pulling it all together.

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Accelerating success.

Exhibit 13

2001 General Plan Land Use Existing &
Proposed



- Commercial
- Office
- Employment
- Natural Open Space
- Developed Open Space (Parks)
- G Developed Open Space (Golf Courses)
- Cultural/Institutional or Public Use
- Rural Neighborhoods
- Suburban Neighborhoods
- Urban Neighborhoods
- Mixed-Use Neighborhoods
- Resorts/Tourism
- Shea Corridor
- Mayo Support District
- Regional Use District
- McDowell Sonoran Preserve
- Recommended Study Bounda
- City Boundary

2001 General Plan Existing Land Use



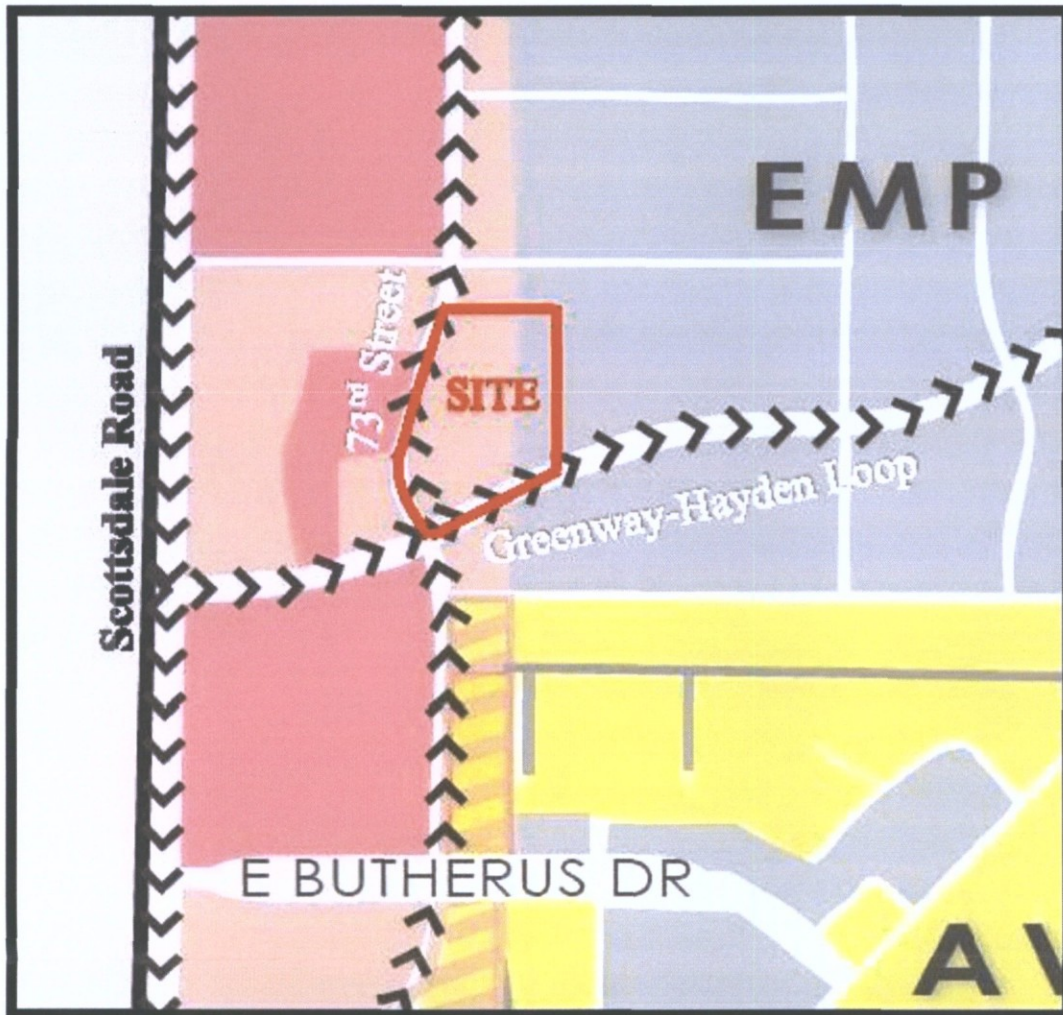
- Commercial
- Office
- Employment
- Natural Open Space
- Developed Open Space (Parks)
- G Developed Open Space (Golf Courses)
- Cultural/Institutional or Public Use
- Rural Neighborhoods
- Suburban Neighborhoods
- Urban Neighborhoods
- Mixed-Use Neighborhoods
- Resorts/Tourism
- Shea Corridor
- Mayo Support District
- Regional Use District
- McDowell Sonoran Preserve
- Recommended Study Bounds
- City Boundary

2001 General Plan Proposed Land Use

Exhibit 14

Greater Airpark Character Area Plan (GACAP)
Land Use – Existing & Proposed

Scottsdale Airpark Land Use – Existing

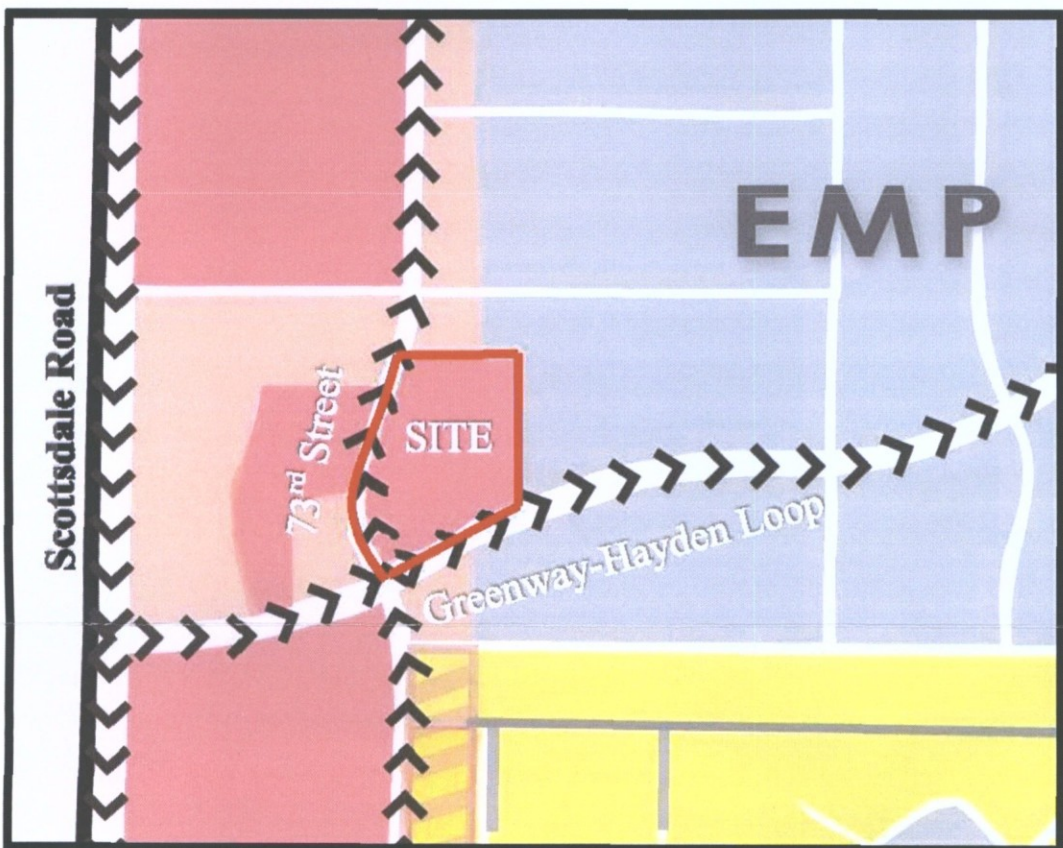


LEGEND

Land Use Plan:

- Airpark Mixed Use-Residential (AMU-R)
- Airpark Mixed Use (AMU)
- Regional Tourism (RT)
- Employment (EMP)
- Aviation (AV)
- Open Space (OS)
- AV or AMU
- Greater Airpark Boundary
- Signature Corridor
- Powerline Corridor
- Central Arizona Project

Scottsdale Airpark Land Use – Proposed



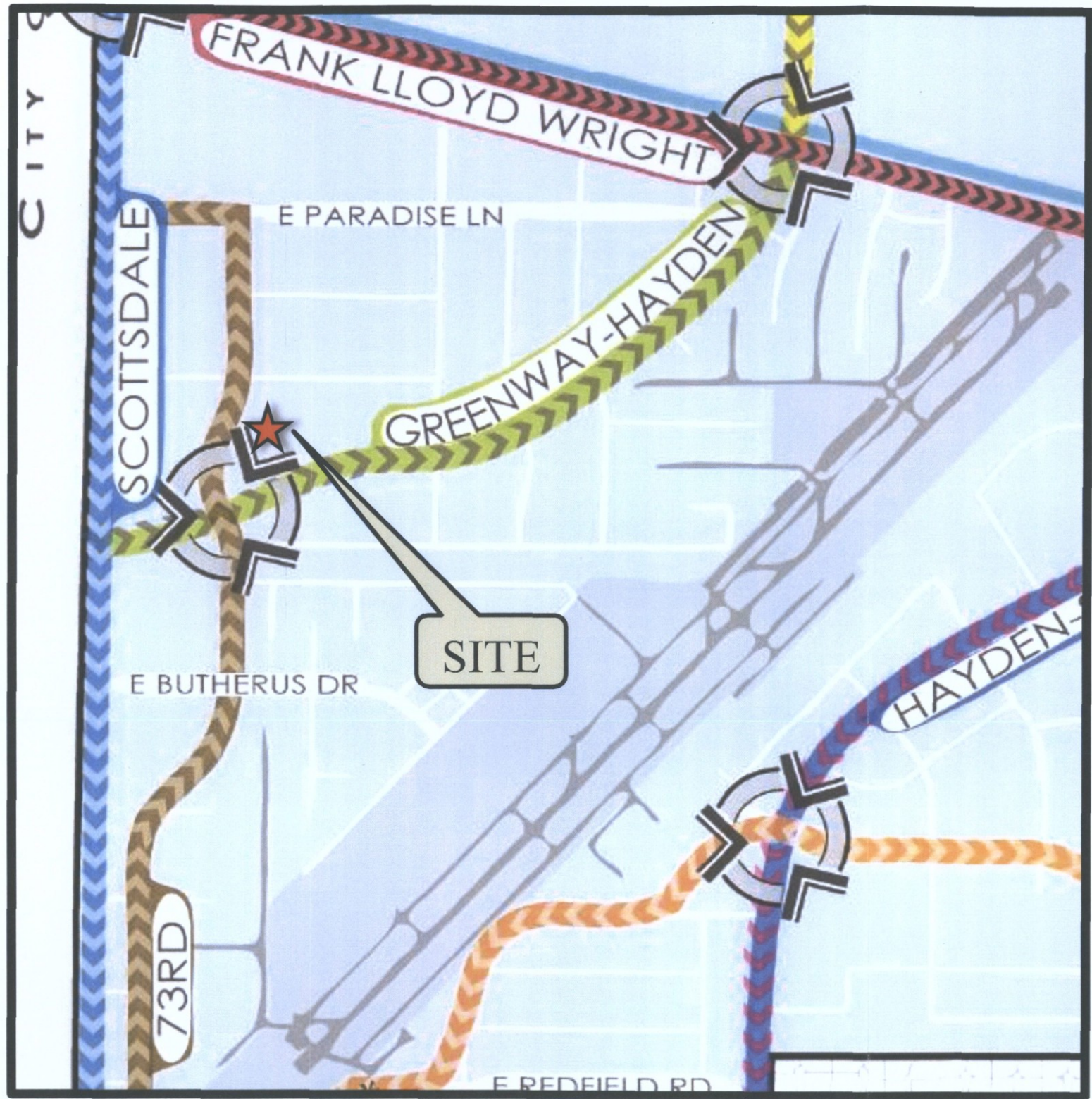
LEGEND

Land Use Plan:

- Airpark Mixed Use-Residential (AMU-R)
- Airpark Mixed Use (AMU)
- Regional Tourism (RT)
- Employment (EMP)
- Aviation (AV)
- Open Space (OS)
- AV or AMU
- Greater Airpark Boundary
- Signature Corridor
- Powerline Corridor
- Central Arizona Project

Exhibit 15

Airpark Signature Corridors



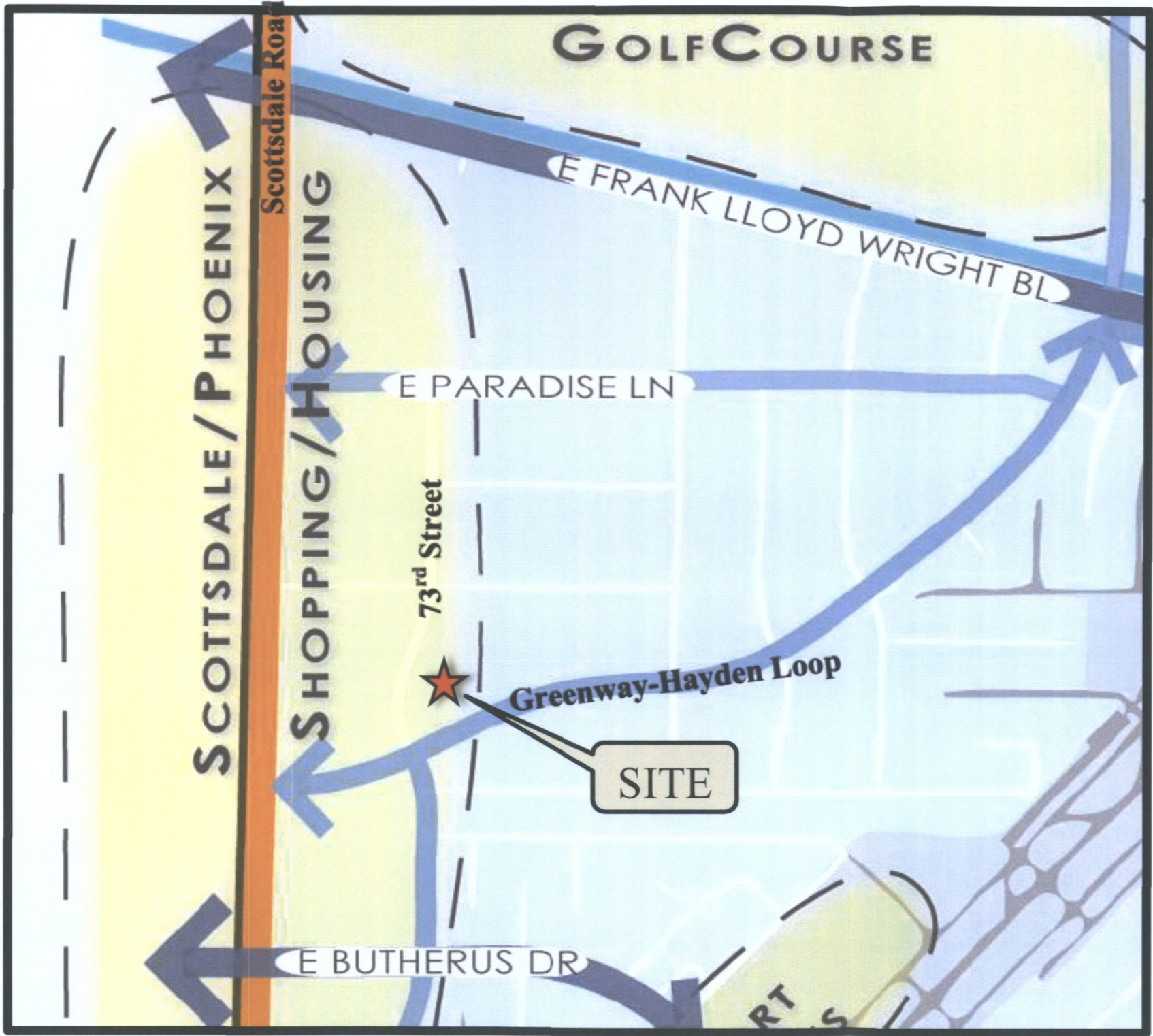
LEGEND

- Signature Corridors
- Greater Airpark Boundary
 - ≡≡≡ Powerline Corridor
 - Central Arizona Project
 - ▤▤▤ Signature Corridors
 - ⊕⊕⊕ Landmark Intersections

Airpark Signature Corridors

Exhibit 16

Airpark Future Transit Connections



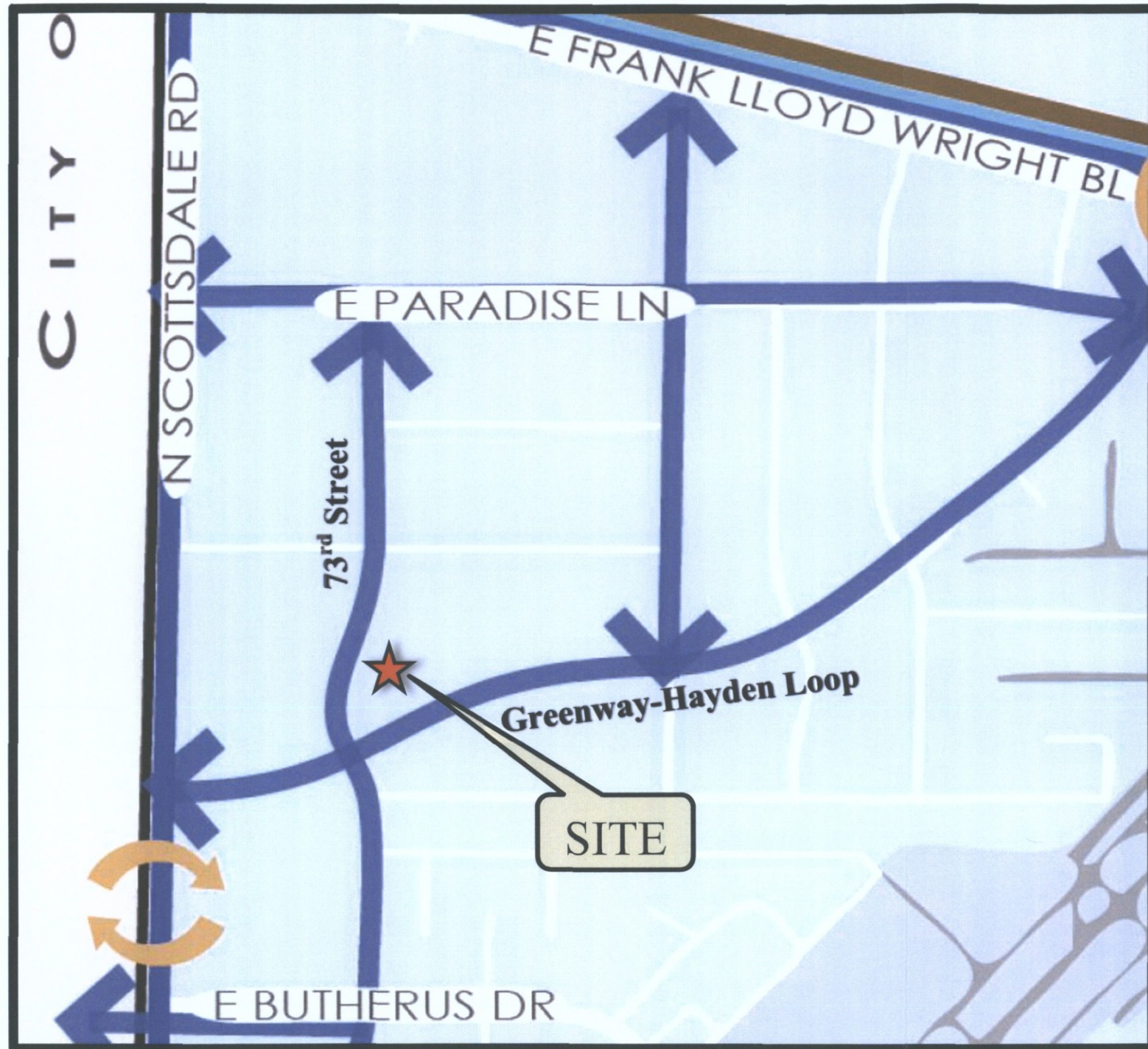
LEGEND

- Conceptual Transit Connections
- Greater Airpark Boundary
 - ≡ Powerline Corridor
 - Central Arizona Project
 - ↔ Existing Transit Route
 - ↔ Potential Future Transit Route
 - ↔ Future High Capacity Transit Corridor
 - Activity Centers
 - Park and Ride

Airpark Future Transit

Exhibit 17

Airpark Pedestrian & Bicycle Connectivity



Airpark Pedestrian & Bicycle Connections

LEGEND

Conceptual Greater Airpark
Pedestrian/Bicycle Connectivity

— Greater Airpark Boundary

≡ Powerline Corridor

— Central Arizona Project
Aqueduct

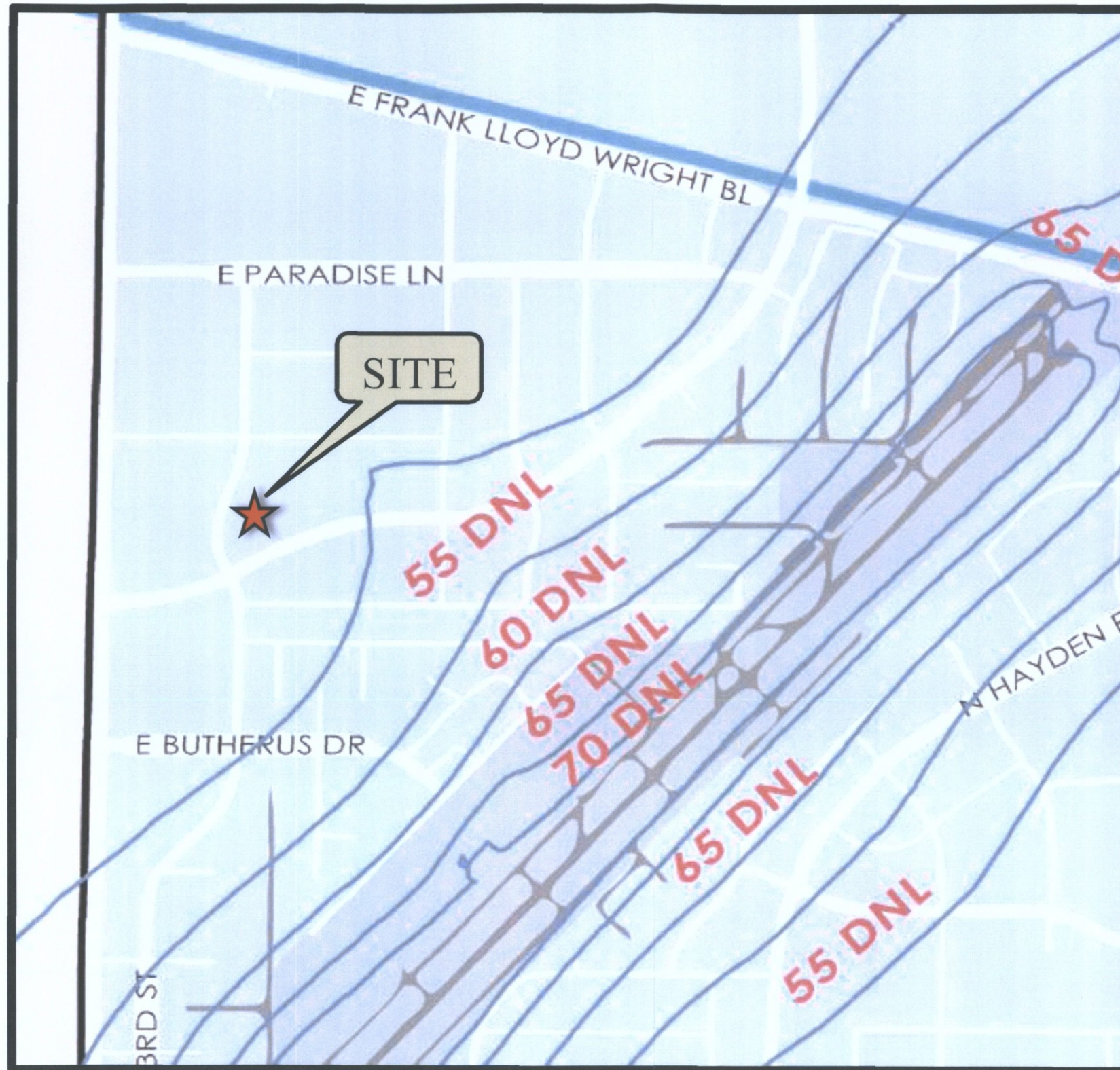
→ Pedestrian/ Bicycle Corridors

→ Trails

↻ Important Pedestrian Linkage

Exhibit 18

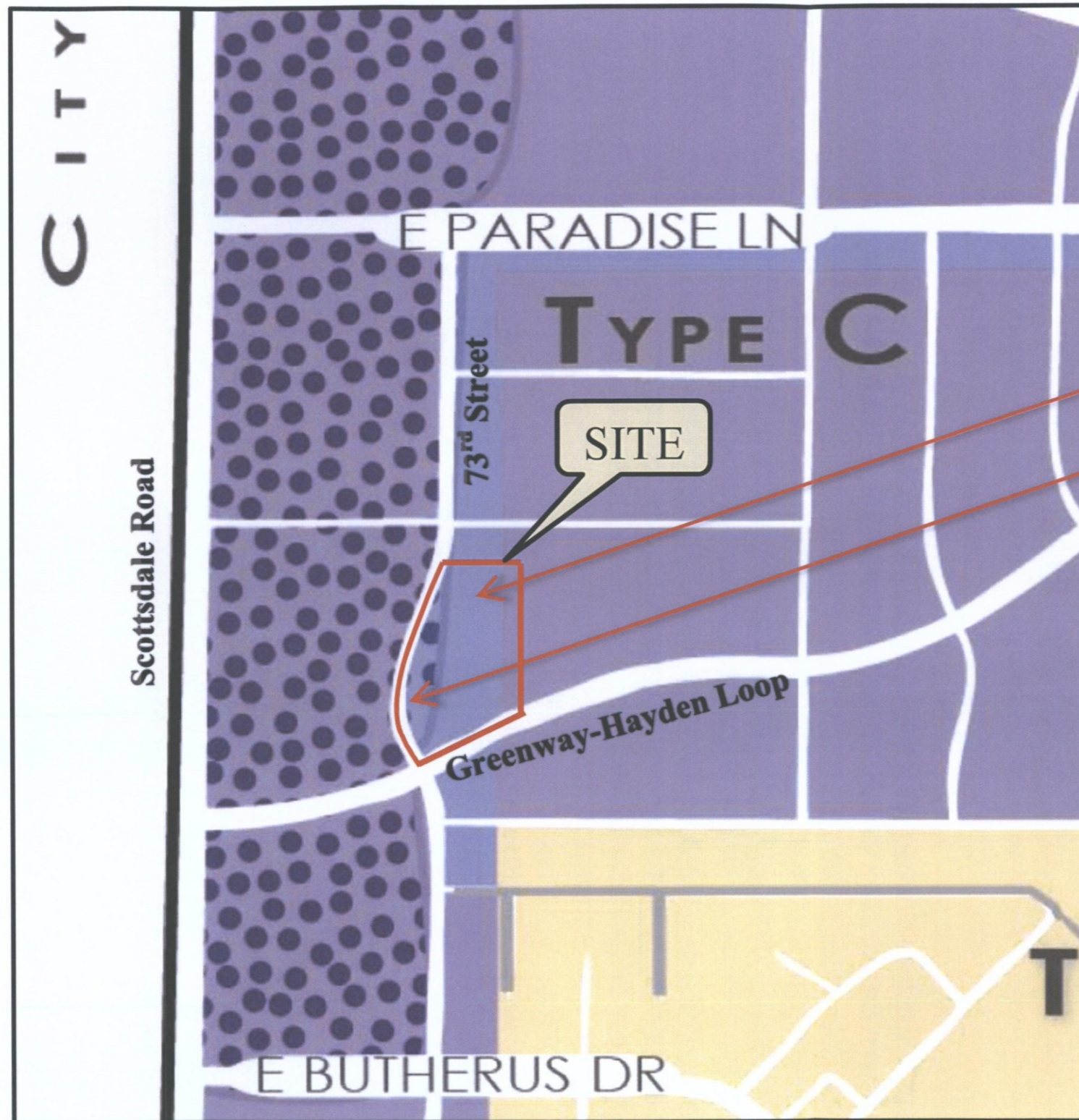
Airpark Noise Contours



Scottsdale Airpark Noise Contours Exhibit


Exhibit 19

Airpark Development Types





LEGEND


Conceptual Development Types:

 Type A- Medium Scale

 Type B- Large Mass

 Type C- Higher Scale

 Regional Core-
Greatest Intensity

 Greater Airpark Boundary

 Sensitive Edge

 Powerline Corridor

 Central Arizona Project

 Open Space

Airpark Development Types Exhibit



**Request for Site Visits and/or Inspections
Development Application (Case Submittals)**

This request concerns all property identified in the development application.

Pre-application No: 733 - PA - 2015

Project Name: District at the Quarter"

Project Address: 15501 N. 73rd St. Scottsdale, AZ 85260

STATEMENT OF AUTHORITY:

1. I am the owner of the property, or I am the duly and lawfully appointed agent of the property and have the authority from the owner to sign this request on the owner's behalf. If the land has more than one owner, then I am the agent for all owners, and the word "owner" refer to them all.
2. I have the authority from the owner to act for the owner before the City of Scottsdale regarding any and all development application regulatory or related matter of every description involving all property identified in the development application.

STATEMENT OF REQUEST FOR SITE VISITS AND/OR INSPECTIONS

1. I hereby request that the City of Scottsdale's staff conduct site visits and/or inspections of the property identified in the development application in order to efficiently process the application.
2. I understand that even though I have requested the City of Scottsdale's staff conduct site visits and/or inspections, city staff may determine that a site visit and/or an inspection is not necessary, and may opt not to perform the site visit and/or an inspection.

Property owner/Property owners agent: Paul E. Gilbert, Esq. Beus Gilbert PLLC

Print Name
Paul E. Gilbert
Signature

City Use Only:	
Submittal Date: _____	Case number: _____
Planning, Neighborhood & Transportation Division 7447 E Indian School Road, Suite 105, Scottsdale, AZ 85251 ♦ Phone: 480-312-7000 ♦ Fax: 480-312-7088	



Submittal Fee

Project Name: GREENWAY HAYDEN LOOP APTS Pre-App#: 733-PA-2015
Fee Type: GP Fee Amount: \$ 1950⁰⁰
Staff Name: KAREN Signature: Karen Phone: x _____ Date: _____

Planning, Neighborhood & Transportation Division

7447 E Indian School Road, Suite 105, Scottsdale, AZ 85251 • Phone: 480-312-2500 • Fax: 480-312-7088



City of Scottsdale Cash Transmittal

105841

105841
 00893359
 3/30/2016 PLN-1STOP
 KHANAS HPTC600512
 3/30/2016 5:00 PM
 \$1,950.00

Received From :

Beus Gilbert PLLC c/o Dennis M. Newcombe
 701 North 44th Street
 Phoenix, AZ 85008
 480-429-3065

Bill To :

Beus Gilbert PLLC c/o Dennis M. Newcombe
 701 North 44th Street
 Phoenix, AZ 85008
 480-429-3065

Reference # 733-PA-2015
Address 15501 N DIAL BL
Subdivision SCOTTSDALE AIRPARK NORTH
Marketing Name
MCR 390-33
APN 215-44-007

Lot Number 5
County No
Gross Lot Area 0
NAOS Lot Area 0
Net Lot Area
Number of Units 1
Density

Issued Date 3/30/2016
Paid Date 3/30/2016
Payment Type CHECK
Cost Center
Metes/Bounds No
Water Zone
Water Type
Sewer Type
Meter Size
 QS 35-45

Owner Information

Rowley Blue Water Investments LLC
 7231 East Princess Boulevard,
 Scottsdale, AZ 85255
 602-749-2171

Code	Description	Additional	Qty	Amount	Account Number
3173	GENERAL PLAN APPLICATION		1	\$1,950.00	100-21300-44221

3-GP-2016
3/30/2016

Total Amount **\$1,950.00**

 Applicant Signature

(When a credit card is used as payment I agree to pay the above total amount according to the Card Issuer Agreement.)

TO HAVE WATER METER SET - CALL 480-312-5650 AND REFER TO TRANSMITTAL # 105841

#9

GPA

Request To Submit Concurrent Development Applications

Acknowledgment and Agreement

The City of Scottsdale recognizes that a property owner may desire to submit concurrent development applications for separate purposes where one or more of the development applications are related to another development application. City Staff may agree to process concurrently where one or more the development applications related to the approval of another development application upon receipt of a complete form signed by the property owner.

Development Application Types		
Please check the appropriate box of the types of applications that you are requesting to submit concurrently		
Zoning	Development Review	Signs
<input type="checkbox"/> Text Amendment (TA)	<input type="checkbox"/> Development Review (Major) (DR)	<input type="checkbox"/> Master Sign Program (MS)
<input checked="" type="checkbox"/> Rezoning (ZN)	<input type="checkbox"/> Development Review (Minor) (SA)	<input type="checkbox"/> Community Sign District (MS)
<input type="checkbox"/> In-fill Incentive (II)	<input type="checkbox"/> Wash Modification (WM)	Other
<input type="checkbox"/> Conditional Use Permit (UP)	<input type="checkbox"/> Historic Property (HP)	<input type="checkbox"/> Annexation/De-annexation (AN)
Exemptions to the Zoning Ordinance	Land Divisions (PP)	<input checked="" type="checkbox"/> General Plan Amendment (GP)
<input type="checkbox"/> Hardship Exemption (HE)	<input type="checkbox"/> Subdivisions	<input type="checkbox"/> In-Lieu Parking (IP)
<input type="checkbox"/> Special Exception (SX)	<input type="checkbox"/> Condominium Conversion	<input type="checkbox"/> Abandonment (AB)
<input type="checkbox"/> Variance (BA)	<input type="checkbox"/> Perimeter Exceptions	Other Application Type Not Listed
<input type="checkbox"/> Minor Amendment (MA)	<input type="checkbox"/> Plat Correction/Revision	<input type="checkbox"/>

Owner: Marcia Rowley

Company: Rowley Blue Water Investments LLC

Address: 10030 N. 25th Ave. Scottsdale, AZ 85201

Phone: 602-749-2171 Fax: _____

E-mail: marcia.rowley@icecenterprise.com

As the property owner, by providing my signature below, I acknowledge and agree: 1) that the concurrent development applications are processed at the property owner's risk; 2) to hold the City harmless of all cost, expense, claims, or other liability arising in connection with the concurrent development applications; 3) to the City of Scottsdale's Substantive Policy Statement pertaining to Concurrent Applications; 4) to placing a development application on hold in order to continue processing a concurrent development application that is related to an another development application; and 5) that upon completion of the City review(s) of the development applications, one or of the development application may not be approved.

Property owner (Print Name): Marcia Rowley Title: Chief Marketing Officer

Marcia Rowley Signature Date: 03/25/2016

Official Use Only: Submittal Date: _____

Request: Approved or Denied

Staff Name (Print): _____

Staff Signature: _____ Date: _____

3-GP-2016
3/30/2016



Development Application

Development Application Type:

Please check the appropriate box of the Type(s) of Application(s) you are requesting

Zoning	Development Review	Signs
<input type="checkbox"/> Text Amendment (TA)	<input type="checkbox"/> Development Review (Major) (DR)	<input type="checkbox"/> Master Sign Program (MS)
<input checked="" type="checkbox"/> Rezoning (ZN)	<input type="checkbox"/> Development Review (Minor) (SA)	<input type="checkbox"/> Community Sign District (MS)
<input type="checkbox"/> In-fill Incentive (II)	<input type="checkbox"/> Wash Modification (WM)	Other:
<input type="checkbox"/> Conditional Use Permit (UP)	<input type="checkbox"/> Historic Property (HP)	<input type="checkbox"/> Annexation/De-annexation (AN)
Exemptions to the Zoning Ordinance	Land Divisions (PP)	<input checked="" type="checkbox"/> General Plan Amendment (GP)
<input type="checkbox"/> Hardship Exemption (HE)	<input type="checkbox"/> Subdivisions	<input type="checkbox"/> In-Lieu Parking (IP)
<input type="checkbox"/> Special Exception (SX)	<input type="checkbox"/> Condominium Conversion	<input type="checkbox"/> Abandonment (AB)
<input type="checkbox"/> Variance (BA)	<input type="checkbox"/> Perimeter Exceptions	Other Application Type Not Listed
<input type="checkbox"/> Minor Amendment (MA)	<input type="checkbox"/> Plat Correction/Revision	<input type="checkbox"/>

Project Name: "District at the Quarter"

Property's Address: 15501 N. 73rd St. Scottsdale, AZ

Property's Current Zoning District Designation: I-1

The property owner shall designate an agent/applicant for the Development Application. This person shall be the owner's contact for the City regarding this Development Application. The agent/applicant shall be responsible for communicating all City information to the owner and the owner application team.

Owner: Marcia Rowley	Agent/Applicant: Paul E. Gilbert, Esq.
Company: Rowley Blue Water Investments LLC	Company: Beus Gilbert PLLC
Address: 10030 N. 25th Ave. Scottsdale, AZ 85201	Address: 701 N. 44th St.
Phone: (602)-749-2171 Fax:	Phone: (480)-429-3065 Fax: (480)-429-3100
E-mail: marcia.rowley@icecenterprise.com	E-mail: dnewcombe@beusgilbert.com
Designer: Jack Selman	Engineer: Joe Cirone
Company: Architects Orange	Company: Rick Engineering Company
Address: 144 N. Orange St. Orange, CA 92866	Address: 6150 N. 16th St.
Phone: (714)-639-9860 Fax:	Phone: (602)-957-3350 Fax:
E-mail: jack@architectsorange.com	E-mail: jcirone@rickengineering.com

Please indicate in the checkbox below the requested review methodology (please see the descriptions on page 2).

- This is not required for the following Development Application types: AN, AB, BA, II, GP, TA, PE and ZN. These applications will be reviewed in a format similar to the Enhanced Application Review methodology.

<input checked="" type="checkbox"/> Enhanced Application Review:	I hereby authorize the City of Scottsdale to review this application utilizing the Enhanced Application Review methodology.
<input type="checkbox"/> Standard Application Review:	I hereby authorize the City of Scottsdale to review this application utilizing the Standard Application Review methodology.

Owner Signature

Agent/Applicant Signature

Official Use Only

Submittal Date:

Development Application No.:

3-GP-2016
3/30/2016

Planning, Neighborhood & Transportation

7447 East Indian School Road Suite 105, Scottsdale, Arizona 85251 Phone: 480-312-7000 Fax: 480-312-7088

City of Scottsdale's Website: www.scottsdaleaz.gov



Development Application

Review Methodologies

Review Methodologies

The City of Scottsdale maintains a business and resident friendly approach to new development and improvements to existing developments. In order to provide for flexibility in the review of Development Applications, and Applications for Permitting, the City of Scottsdale provides two methodologies from which an owner or agent may choose to have the City process the application. The methodologies are:

1. Enhanced Application Review Methodology

Within the parameters of the Regulatory Bill-of-Rights of the Arizona Revised Statutes, the Enhanced Application Review method is intended to increase the likelihood that the applicant will obtain an earlier favorable written decision or recommendation upon completion of the city's reviews. To accomplish this objective, the Enhanced Application Review allows:

- the applicant and City staff to maintain open and frequent communication (written, electronic, telephone, meeting, etc.) during the application review;
- City staff and the applicant to collaboratively work together regarding an application; and
- City staff to make requests for additional information and the applicant to submit revisions to address code, ordinance, or policy deficiencies in an expeditious manner.

Generally, the on-going communication and the collaborative work environment will allow the review of an application to be expedited within the published Staff Review Time frames.

2. Standard Application Review Methodology

Under the Standard Application Review, the application is processed in accordance with the Regulatory Bill-of-Rights of the Arizona Revised Statutes. These provisions significantly minimize the applicant's ability to collaboratively work with City Staff to resolve application code, ordinance, or policy deficiencies during the review of an application. After the completion of the city's review, a written approval or denial, recommendation of approval or denial, or a written request for additional information will be provided.

The City is not required to provide an applicant the opportunity to resolve application deficiencies, and staff is not permitted to discuss or request additional information that may otherwise resolve a deficiency during the time the City has the application. Since the applicant's ability to collaboratively work with Staff's to resolve deficiencies is limited, the total Staff Review Time and the likelihood of a written denial, or recommendation of denial is significantly increased.

In addition to the information above, please review the Development Application, and/or the Application for Permitting flow charts. These flow charts provide a step-by-step graphic representation of the application processes for the associated review methodologies.

Note:

1. Please see the Current Planning Services and Long Range Planning Services Substantive Policy Statements and Staff Review Timeframes for Development Applications, number III.

Planning, Neighborhood & Transportation

7447 East Indian School Road Suite 105, Scottsdale, Arizona 85251 Phone: 480-312-7000 Fax: 480-312-7088

City of Scottsdale's Website: www.scottsdaleaz.gov



Development Application

Arizona Revised Statutes Notice

§9-834. Prohibited acts by municipalities and employees; enforcement; notice

- A. A municipality shall not base a licensing decision in whole or in part on a licensing requirement or condition that is not specifically authorized by statute, rule, ordinance or code. A general grant of authority does not constitute a basis for imposing a licensing requirement or condition unless the authority specifically authorizes the requirement or condition.
- B. Unless specifically authorized, a municipality shall avoid duplication of other laws that do not enhance regulatory clarity and shall avoid dual permitting to the maximum extent practicable.
- C. This section does not prohibit municipal flexibility to issue licenses or adopt ordinances or codes.
- D. A municipality shall not request or initiate discussions with a person about waiving that person's rights.
- E. This section may be enforced in a private civil action and relief may be awarded against a municipality. The court may award reasonable attorney fees, damages and all fees associated with the license application to a party that prevails in an action against a municipality for a violation of this section.
- F. A municipal employee may not intentionally or knowingly violate this section. A violation of this section is cause for disciplinary action or dismissal pursuant to the municipality's adopted personnel policy.
- G. This section does not abrogate the immunity provided by section 12-820.01 or 12-820.02.