

HUELLMANTEL AFFILIATES

MEMORANDUM

To: City of Scottsdale, Planning & Development Services
From: Lauren Proper Potter, Huellmantel & Affiliates
Re: 9-ZN-2021 – Palo on 75th 1st Review
Date: January 12, 2022

We have been working through the comments provided by the City from the second plan submittal and have addressed each with this response letter. This letter provides detailed answers (in blue) to each of the comments (in grey), which are listed below.

COMMENTS FROM 2ND SUBMITTAL 10/05/2021

Zoning Ordinance and Scottsdale Revise Code Significant Issues:

The following code and ordinance related issues have been identified in the second review and shall be addressed with the resubmittal. Addressing these items is critical to scheduling the application for public hearing and may affect staff's recommendation. Please address the following:

Drainage:

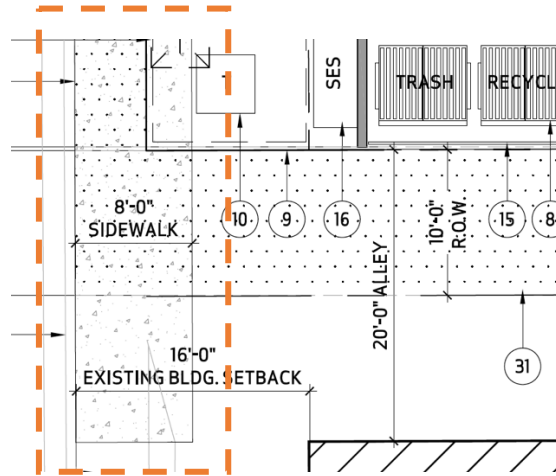
- *The preliminary drainage report still has not been accepted by the Storm Water Division. Please refer to the redlined report in the internet folder and revise accordingly.*
The Drainage Report and G&D plan have both been revised to reflect the comments from the City.

Significant Policy Related Issues:

The following policy related issues have been identified in the second review. Though some of these issues may not be critical to scheduling the application for public hearing, they may affect the staff's recommendation and should be addressed with the resubmittal. Please address the following:

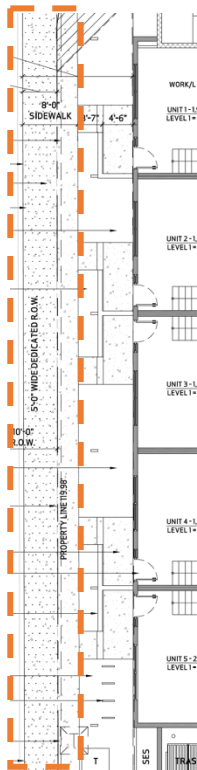
Site Design:

- *The 2nd submittal site plan appears to indicate that the sidewalk along 75th Street will narrow in width as it traverses the alleyway. Please revise the applicable plans to indicate a continuous sidewalk at least 8 feet in width from this development site south to the existing wider sidewalk.*
The site plan has been updated to include an 8' sidewalk along 75th Street across the alley as shown in the image below:

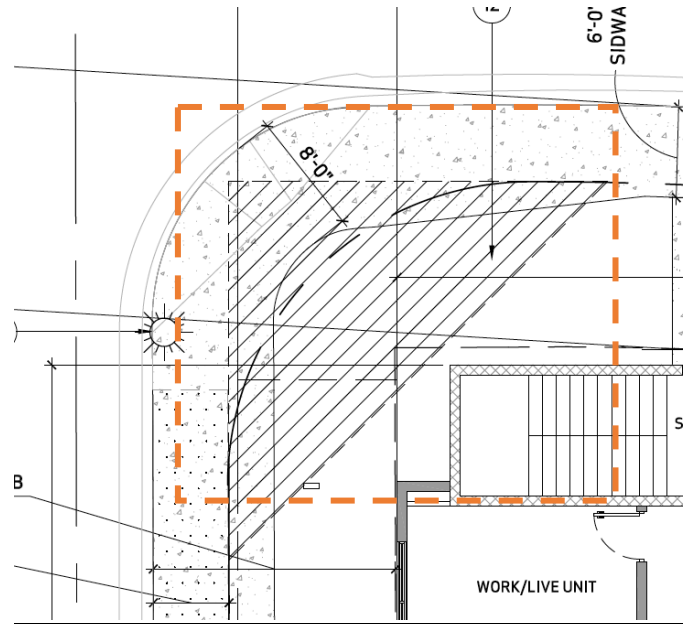


- The 2nd submittal site plan appears to indicate columns supporting the upper level balconies encroaching into the requested 8-foot sidewalk width on 75th Street. Please revise the applicable plans to remedy this situation so that a minimum 8-foot clear width is provided.

The 8' public sidewalk is measured to the outline of the upper level projection and the columns supporting the 2nd level balcony are set back from the face of the projection, resulting in a sidewalk that is greater than 8' as measured to the columns. The images below demonstrate that the measurement is taken to the face of the building projection at the 2nd level with the columns located closer to the ground floor building face and not interfering with the 8' clear distance for the public sidewalk:

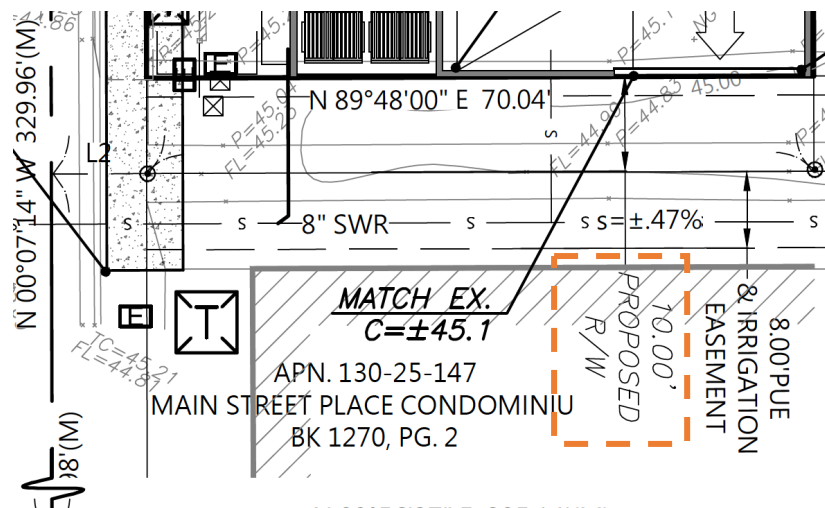


- The 2nd submittal site plan indicates that the portion of sidewalk at the intersection of 75th Street & McKnight will remain as-is. The sidewalk in this area should be replaced with a larger landing area at the intersection, similar to the sidewalk landing on the northeast corner of 75th & Main Street. Please revise applicable plans accordingly. The site plan has been updated to reflect a larger landing area on the sidewalk at 75th Street and McKnight Avenue as shown in the image below:



Engineering:

- Though indicated on the site plan, the preliminary G&D plan does not show the required 10-foot fee-title alley dedication. Please revise the G&D plan accordingly. The Grading and Drainage plan has been updated to reflect that the alley will be a 10' right-of-way dedication as shown below:

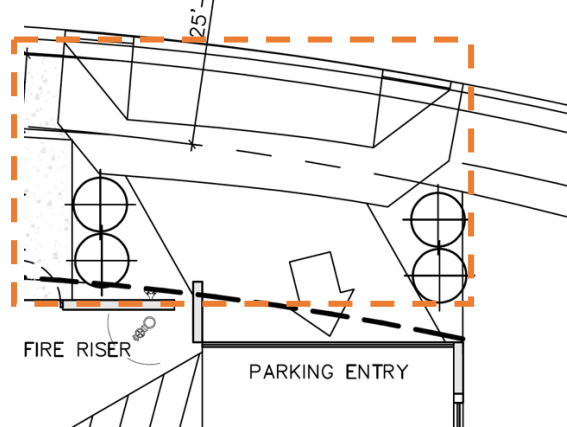


- *Related to the comment above, a minimum 24-foot alley/drive aisle width must be provided due to the condition to the south. The project to the south has garages abutting the alley/drive-aisle. This condition requires a 24-foot aisle width. Please revise the site plan to indicate a minimum 2-foot setback from the property line abutting the alley/drive aisle. Refer to Section 2-1.303 of the DSPM.*

We have discussed this comment with Project Coordination Liaison Greg Bloemberg and per our conversation, the project will not be required to provide a 2' setback from the alley property line as the condition referenced in this comment was not created by the property owner but rather is the result of a deficiency at the adjacent project.

- *From 1st review, the driveway, including wings, cannot extend past the property line into the adjacent right-of-way. Please update applicable plans to indicate the driveway, including wings, will be wholly aligned with the project's parcel boundaries. Refer to Section 5-3 of the DSPM.*

The site plan has been revised to indicate that the driveway, including wings, does not extend past the property line as shown in the image below:



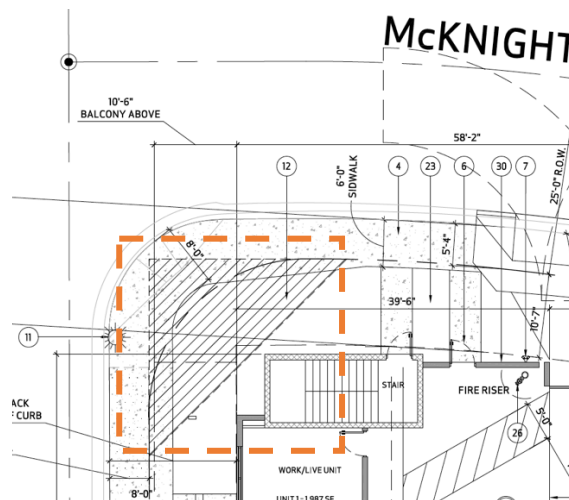
Circulation:

- *Transportation does not support a detached main public sidewalk along 75th Street, as it still appears as if the sidewalk is intended to be a “private” space for the residents. From 1st review, the main public sidewalk should be 8 feet wide and attached to the street curb. Please revise the site plan accordingly. If the developer wishes to have a separate sidewalk abutting the building for residents only, that is fine. Additionally, scooters and unmotorized bikes are permitted on sidewalks and being that this sidewalk is along a street with a 25 MPH speed limit, separation from street curb is not necessary. To discuss in more detail, contact Phil Kercher (480-312-7645 or pkercher@scottsdaleaz.gov). Refer to Section 5-3.110 of the DSPM.*

The site plan has been revised to reflect an attached sidewalk along 75th Street as shown in the image below:



- From 1st review, please revise the site plan to indicate the required corner safety triangle at the intersection of 75th Street & McKnight. It is incorrectly depicted on the 2nd submittal plan. Refer to Section 5-3.123 and Figures 5-3.25 and 5-3.26 of the DSPM. We have revised the site plan to indicate the required corner safety triangle at the intersection of 75th Street and McKnight Avenue.



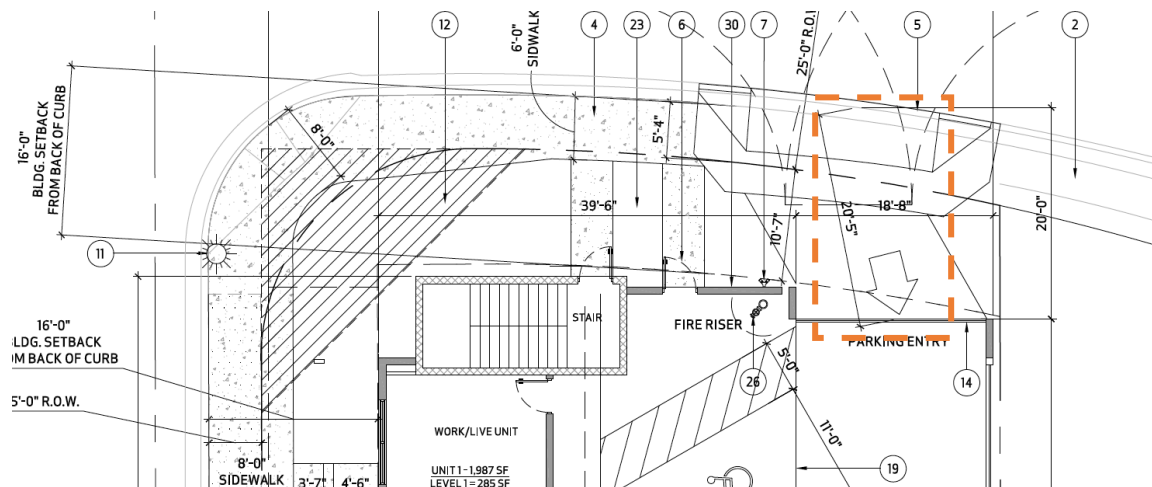
- From 1st review, please revise the site plan to indicate the required sight visibility triangles at the driveway intersection with McKnight Ave. Refer to the above section and figures in the DSPM. Because the driveway onto McKnight is one-way for egress only, SVTs are not provided.
- From 1st review, the proposed driveway on McKnight does not meet City standards for minimum spacing – 30 feet from the property line. Also, as depicted it appears to encroach onto the adjacent property. Please revise applicable plans accordingly.

The proposed driveway does not encroach onto the adjacent property and is entirely contained within the subject site. Because the site is less than 60' wide, there is not a practical method to maintain a driveway within 30' of any property line. We have discussed this challenge with Greg Bloemberg and Phil Kercher and have agreed that the driveway location as proposed is acceptable.

- *Please revise the applicable plans to indicate a minimum setback of 20 feet from back of curb to the proposed gated entry. Refer to Section 2-1.302 and Figure 2-1.2 of the DSPM.*

We have discussed this guideline with Greg Bloemberg and Phil Kercher as it relates to the challenges presented by the site's narrow configuration while maintaining the property owner's desire to access-control the parking area. We are able to provide 20'-5" from the back of the curb to the gate, but it has been since pointed out to our team that the requirement is 20' from the back of the sidewalk.

As a compromise, the property owner will provide a gate that automatically opens (but does not remain open) during business hours to allow unrestricted access to the work/live space to address concerns over access. Additional justification is provided in the revised Project Narrative provided with this resubmittal, but the property owner has concerns with being able to prevent members of the public from utilizing the private parking spaces at Palo during high volume events in the area when public parking can be a challenge. Because Palo is a small-sale project that does not provide excess parking for the public, access control is an important component of ensuring adequate parking on site for future residents. We have increased the distance to the gate as much as possible and are proposing 20'-5" as shown below:



Preliminary Drainage Report:

- *(see appendix D)*
The Preliminary Drainage Report has been revised to include this comment on page 2.
- *stormwater storage requirements*

The Preliminary Drainage Report has been revised to reflect this comment on page 2.

- *Due to site area being less than 0.5 acres, a stormwater storage waiver for in-lieu fee is being submitted to mitigate the increase in post-development stormwater storage requirements*

The Preliminary Drainage Report has been revised to include this comment on page 3.

- *Provide signature*

A revised Request for Stormwater Storage Waiver with the signature has been provided with this resubmittal.

Thank you for your review of our plans. Please feel free to reach out with any additional questions. I can be reached at (480) 921-2800 or via e-mail at lauren@huellmantel.com.

Best regards,

A handwritten signature in black ink, consisting of several overlapping loops and a horizontal line at the bottom, identifying the sender as Lauren Proper Potter.

Lauren Proper Potter