

# DEVELOPMENT REVIEW BOARD REPORT



Meeting Date: January 19, 2023  
General Plan Element: *Character and Design*  
General Plan Goal: *Foster quality design that enhances Scottsdale as a unique southwestern desert community.*

## ACTION

**Seventh Day Adventist Warehouse Building 14-DR-2022** | Request for approval of project plans for a new 1-story warehouse building comprised of approximately 243,360 square feet of building area on a +/- 18-acre site.

## SUMMARY

### Staff Recommendation

Approve, subject to the attached stipulations (Attachment #6)

### Items for Consideration

- Conformance with Development Review Board Criteria – staff confirms
- Integration of Sensitive Design Principles – staff confirms
- Compatibility with the Greater Airpark Character Area Plan – staff confirms
- Project scope is the first phase of a three-phase development plan
- Relevant cases include: 14-ZN-2019 and 236-SA-2022
- 1 public comment received as of writing this report regarding project clarification

## BACKGROUND

**Location:** 7501, 7509 and 7511 E. Redfield Road

**Zoning:** Planned Airpark Core Development, Aviation (PCP AV) and Planned Airpark Core Development, Employment (PCP EMP)

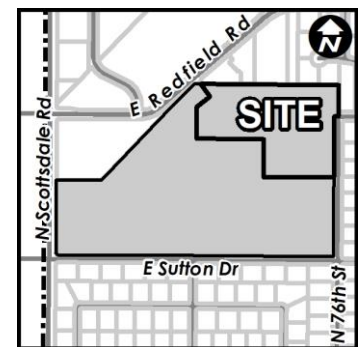
### Adjacent Uses

North: Municipal Uses (Scottsdale's Park and Ride and Municipal Airport) and Private Industrial Uses including 2-story offices

East: Residential Neighborhood (Paradise Valley Ranchos No. 2) and Private Industrial Uses including 2-story offices

South: Residential Neighborhood (Sweetwater Street East I/II)

West: City of Phoenix, Retail and Office Uses



### Property Owner

Arizona Conference Corporation of Seventh Day Adventists

### Applicant

Kurt Jones, Tiffany & Bosco, PA  
(602) 452-2729

### Architect/Designer

LGE Design Group  
(480) 966-4001

### Engineer

Hunter Engineering, Inc.  
(480) 991-3985

## **DEVELOPMENT PROPOSAL**

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The proposed project is for a new 1-story warehouse building located on an 18-acre portion of property with an existing religious and education campus. The overall development plan to the site includes three phases. The new warehouse building will provide the initial access points into the site from E. Redfield Road through an existing access easement, and the City’s Park and Ride site. The design of the proposed warehouse is contemporary with tilt-up concrete panels and anodized dark bronze metal. The style is compatible with other large-scale buildings within the Greater Airpark Character Area. Enhanced landscaping and road improvements are proposed along N. 76<sup>th</sup> Street including a new public trail. An existing private taxiway will provide airport access for a new uncovered airplane staging area to the north of the overall development site.

### **Development Review Board Criteria**

Staff confirms that the development proposal generally meets the applicable Development Review Board Criteria. For a detailed analysis of the Criteria, please see Attachment #4.

### **Sustainability**

The City of Scottsdale promotes the goal of sustainability through the incorporation of appropriate design considerations in the development of the built environment. This development proposal incorporates several design elements that align with the City’s goal of sustainability including recessed windows, deep roof overhangs, use of low water use plant species, and the use of low energy consumption LED lighting. In addition, the proposed development will provide a pedestrian sidewalk connection to an existing public transportation facility.

## **STAFF RECOMMENDED ACTION**

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Staff recommends that the Development Review Board approve the Seventh Day Adventist Warehouse Building development proposal per the attached stipulations, finding that the Character and Design Element of the General Plan and Greater Airpark Character Area and Development Review Board Criteria have been met.

### **RESPONSIBLE DEPARTMENTS**

**Planning and Development Services**  
Current Planning Services

### **STAFF CONTACTS**

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Wayland Barton  
Planner  
480-312-2817    wbarton@Scottsdaleaz.gov

**APPROVED BY**

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Wayland Barton, Report Author

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12/27/2022

Date



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Brad Carr, AICP, LEED-AP, Planning & Development Area Manager  
Development Review Board Liaison  
Phone: 480-312-7713      Email: bcarr@scottsdaleaz.gov

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1/11/2023

Date

**ATTACHMENTS**

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1. Context Aerial
2. Close-up Aerial
3. Applicant's Narrative
4. Development Review Board Criteria Analysis
5. Development Information
6. Stipulations / Zoning Ordinance Requirements
7. Combined Context Aerial and Site Plan
8. Site Plan
9. Open Space Plan
10. Landscape / Hardscape Plan
11. Building Elevations
12. Perspectives
13. Materials and Colors Board
14. Exterior Photometrics Plan
15. Exterior Lighting Cutsheets
16. Zoning Map
17. Preliminary Grading & Drainage Plan / Site Sections
18. Truck Circulation & Access Restriction Plan
19. Phasing Plan
20. Historic Significance Evaluation and Documentation
21. Community Notification Map



Context Aerial

14-DR-2022



Close-up Aerial

14-DR-2022



LGE DESIGN GROUP/CREATION RE  
THUNDERBIRD & SCOTTSDALE ROAD  
DEVELOPMENT REVIEW BOARD

PROJECT NARRATIVE

MAY 20, 2022

**REQUEST**

On behalf of LGE Design Build and Creation RE (“LGE”), the following is a request for the approval of single building industrial building located in the northern portion of the Arizona Conference Corporation of Seventh-day Adventists property located at the northeast corner of Sutton Drive and Scottsdale Road (“Property”). The proposal, approximately 18 acres, requests Development Review Board (“DRB”) approval for the building elevations and site plan for the industrial project (“Project”).

**DEVELOPMENT OVERVIEW**

Arizona Conference Corporation of Seventh-day Adventists Property

In 1942, the airfield, dormitories, classrooms and nearby hangars were built to train pilots for World War II service. After the war, the airfield, buildings and uses were turned over to the Arizona State College for a technical trade school. In 1953, the Arizona State College returned the airfield, classrooms and dormitories back to the federal government who then turned it over to the Arizona Conference Corporation of Seventh-day Adventists (“AZCCSDA”) for their Thunderbird Adventist Academy boarding school. In the early 1960’s, the City annexed the land with the airfield and AZCCSDA campus property. During the mid-1960’s, Elder Daniel Butherus and the Seventh-day Adventists offered the runway and approximately 200 acres to the City. In 1967, the City opened Scottsdale Airport and to honor Elder Butherus, the roadway entrance to the airport was named after him.

The AZCCSDA retained approximately 75 acres and has been operating their religious school and boarding since the Property was granted from the federal government. Approximately 300 students reside on campus within dormitories, while teachers, support staff and members of the Seventh-day Adventist also live on the Property. Along with their religious boarding school, AZCCSDA used their taxiway access to the runway to continue aviation education since the property was granted. The AZCCSDA headquarters and book store, Thunderbird Adventist Academy, Thunderbird Christian Elementary, Thunderbird-Scottsdale Seventh-day Adventist Church and dormitories, faculty housing, ballfields and other open spaces makes up the majority of the campus. The AZCCSDA currently leases property to industrial and airpark type uses along the northern portion of the Property.



### Recent Land Use Entitlements

In 2019, the overall AZCCSDA 75-acre property was rezoned to the Planned Airpark Core Development (“PCP”) zoning districts. Through the use of the Aviation (“AV”), Airpark Mixed-Use (“AMU”), Employment (“Emp”) and Airpark Mixed-Use – Residential (“AMU-R”) sub-districts, the AZCCSDA created a development plan that redevelops underutilized portions of their property to create an employment hub of office, industrial and aeronautical uses consistent with the uses within the Greater Airpark. The zoning provides for flexibility with development standards while protecting the nearby single family residential neighborhoods and the operations of the airport. The Property is approximately 18 acres of the overall 75-acre campus.

### The Project and Surrounding Improvements

The Property is adjacent to Scottsdale Road on the west. Across Scottsdale Road, within the City of Phoenix, are commercial uses along the arterial roadway with large lot single-family developments further to the west. To the northwest is the City’s park & ride facility located at the southeast corner of Scottsdale Road and Thunderbird Road. North of the Property’s industrial uses are private properties along the south side of Thunderbird Road, including a City water campus facility. A taxiway provides access across Thunderbird Road at the northeast portion of the Property. The airport runway is directly north of the campus, across Thunderbird Road. To the east and south, across Miller Road and Sutton Drive are large lot single-family residential neighborhoods. Sutton Drive has no sidewalks while Miller Road has not been constructed or improved along the west side, which is the east side of the campus.

## **REQUEST DETAILS**

### Site Plan

The project proposes a 243,360 square foot (s.f.) building located on approximately 18 acres. The building is situated in an east/west configuration with the majority of loading bays facing south away from any street frontage or views from off-site. As demonstrated by the aerial with the site plan superimposed, the site is primarily screened from the Thunderbird/Redfield Rd. frontage by a water treatment facility and two story offices along the street frontage. The Project is designed with access from Redfield Road and Thunderbird Road from the west. Surface parking lots flank the building along the west and north sides of the building.

The main access point to the site will be from the one (1) existing driveway on Redfield Road approximately 650 feet northeast of the intersection with Thunderbird Road and the City’s Park and Ride Facility (the “Facility”). This driveway is currently the main driveway used by all of the existing uses on the Property outside the main religious and school campus. This current access point is a shared access with the existing airpark buildings and uses that actually front onto Redfield Road.

As part of the zoning approval, the Property has the ability to create a secondary access point through the Facility site, which is adjacent to the west. The Facility operates as a regional park and ride facility with a main signalized access point at Thunderbird Road and N. 73<sup>rd</sup> Street. The main driveway is adjacent to the Property and as part of the rezoning stipulations, AZCCSA has the ability and an actual license, to create a driveway connection from the Property to the Facility driveway.



If the building is divided into several users, the entrances for customer and employees will be along the northern elevation. The southern elevation will be designed with loading bays within a gated area of the site plan. There is a small reservation for an outdoor plane hangar in the northeast corner of the site plan. The potential use of this area for plane storage allows for direct access to the taxiway along the north boundary of the Property. The remainder of the property will be used for landscape buffers and drainage storage.

### Architecture

A clean, modern aesthetic is intended for the overall design of the building. The most visible elevations are characterized by a ribbon window system with a metal hood frame.

The architectural theme of the buildings utilize colors that are common in the area and by providing materials for a modern vocabulary. The materials planned for the buildings include exposed steel beams, columns and steel angles for shade at the entry, concrete tilt up panels with vertical reveals and metal B deck accent panels on the windows for shade, well placed insulated glazing including spandrel glazing. Sloped roof will be either 1/2" or 1/4" per foot across the entire building and will sloped with crickets to drain to internal downspouts, meeting the required 25% footprint of sloped roof area as required with the design guidelines.

The Main entries will be shaded by the large steel structure which includes an open steel trellis system made by 2"x3" steel angles 9" O.C. Building materials, site, parking and landscaping are all to be in accordance with code requirements and standards.

The building section will allow for rooftop screening for all of the mechanical rooftop equipment with the parapet. The height of the tilt up walls will fully screen the mechanical units. These units will be accessed via roof access ladders and hatches internal to the building.

Varying heights and changes along the building with changes in materials and texture add to the character of this development.

The exterior lighting within the proposed project will be integrally designed as a part of the building and outdoor pedestrian spaces with the intent of providing adequate safety while avoiding glare, hot spots and being in compliance with the dark sky ordinance with all the lighting being LED lights.

Signage will be in harmony with the character, scale and context of the building reflecting the appropriate size, materials, color, location and illumination.

### Open Space

As part of the zoning case back in 2019, additional landscape buffers were required along Miller Road to the southeast and east of the Property. A proposed 60-foot-wide minimum landscape buffer is proposed along the eastern boundary along Miller Road. In addition, the site plan places a large drainage basin



along the southeast portion of the site to add to the open space area closest to the residential properties southeast of the site.

### Phasing

As part of this proposal, we are proposing to alter the phasing of the project. During the zoning entitlement process for the Property, it was anticipated that the initial phases (Phase 1) would be along Scottsdale Road and move towards the northeast around the school and religious facility property. This proposal creates an economic development opportunity for the northeast corner of the site. As a result, the phasing plan has been revised and is a part of this submittal. We look forward to implementing this initial phase and addressing all of the applicable requirements outlined in the zoning entitlements for this portion of the Property.

### DEVELOPMENT REVIEW BOARD CRITERIA

The City's zoning ordinance states that the Development Review Board shall be guided by the following criteria when considering an application. They are as follows:

1. The Board shall examine the design and theme of the application for consistency with the design and character components of the applicable guidelines, development standards, Design Standards and Policies Manual, master plans, character plan and General Plan.

***Response:*** *With the rezoning and non-major general plan amendment case in 2019, the proposed design and character of the development is consistent with development standards, DS & PM and character plan. The proposed building is also compatible with the surrounding airpark uses and airport runway operations nearby.*

2. The architectural character, landscaping and master plan design of the proposed development shall:
  - a. Promote a desirable relationship of structures to one another, to open spaces and topography, both on the site and in the surrounding neighborhood.
  - b. Avoid excessive variety and monotonous repetition.
  - c. Recognize the unique climatic and other environmental factors of this region to respond to the Sonoran Desert environment, as specified in the Sensitive Design Principles.

***Response:*** *The character of this area will be enhanced and strengthened by the development. The design considers the distinctive qualities and character of the surrounding context and incorporates them. The building design is sensitive to the context of the area.*

*The development is sensitive to existing topography and landscape while responding to the unique terrain of the site. It blends the natural shape and texture of the land while minimizing disturbances to the natural environment. Additionally, a plant inventory and salvage plan will allow the landscape architect to preserve as much significant vegetation as possible in an attempt to retain the desert character of the site.*



*Additionally, this development protects the character of the Sonoran desert by preserving and restoring plant life, natural habitats and ecological processes.*

*The design responds to the desert environment in that materials with colors and coarse textures associated with this region have been utilized. A variety of textures and natural materials have been used to provide visual interest and richness. Additionally, shade structures, deep roof overhangs and recessed windows have been incorporated towards that end.*

*The landscape design responds to the surrounding environment by utilizing a variety of mature landscape materials indigenous to the arid region. The character of the area will be enhanced through the careful selection of planting materials in terms of scale, density, and arrangement. The design will complement the built environment, taking cues from the existing surrounding development palette, selected to compliment the natural Sonoran Desert and the approved planting selections.*

3. Conform to the recommendations and guidelines in the Environmentally Sensitive Lands (ESL) Ordinance, in the ESL Overlay District; and
  - d. Incorporate unique or characteristic architectural features, including building height, size, shape, color, texture, setback, or architectural details, in the Historic Property Overlay District.

**Response:** *The Property is neither in the ESL or the Historic Property Overlay.*

4. Ingress, egress, internal traffic circulation, off-street parking facilities, loading and service areas and pedestrian ways shall be so designed as to promote safety and convenience.

**Response:** *The main access point to the site will be from the one (1) existing driveway on Redfield Road approximately 650 feet northeast of the intersection with Thunderbird Road and the City's Park and Ride Facility (the "Facility"). This driveway is currently the main driveway used by all of the existing uses on the Property outside the main religious and school campus. This current access point is a shared access with the existing airpark buildings and uses that actually front onto Redfield Road.*

*As part of the zoning approval, the Property has the ability to create a secondary access point through the Facility site, which is adjacent to the west. The Facility operates as a regional park and ride facility with a main signalized access point at Thunderbird Road and N. 73rd Street. The main driveway is adjacent to the Property and as part of the rezoning stipulations, AZCCSA has the ability and an actual license, to create a driveway connection from the Property to the Facility driveway. The loading areas are gated for controlled access and safety.*

5. If provided, mechanical equipment, appurtenances and utilities, and their associated screening shall be integral to the building design. Ground mounted mechanical equipment will be screened either thru the use of architectural site walls, or if located on the roof thru the use of architectural screening compatible with the architecture of the building.



**Response:** *The height of the tilt up walls will fully screen the mechanical units. These units will be accessed via roof access ladders and hatches internal to the building.*

6. Within the Downtown Area, building and site design shall:
  - a. Demonstrate conformance with the Downtown Plan Urban Design & Architectural Guidelines.
  - b. Incorporate urban and architectural design that address human scale and incorporate pedestrian-oriented environment at the street level.
  - c. Reflect contemporary and historic interpretations of Sonoran Desert architectural traditions, by subdividing the overall massing into smaller elements, expressing small scale details, and recessing fenestrations.
  - d. Reflect the design features and materials of the urban neighborhoods in which the development is located; and
  - e. Incorporate enhanced design and aesthetics of building mass, height, materials, and intensity with transitions between adjacent/abutting Type 1 and Type 2 Areas, and adjacent/abutting Type 2 Areas and existing development outside the Downtown Area.

**Response:** *The Property is not within the Downtown Area.*

7. The location of artwork provided in accordance with the Cultural Improvement Program or Public Art Program shall address the following criteria:
  - a. Accessibility to the public.
  - b. Location near pedestrian circulation routes consistent with existing or future development or natural features.
  - c. Location near the primary pedestrian or vehicular entrance of a development.
  - d. Location in conformance with the Design Standards and Policies Manual for locations affecting existing utilities, public utility easements, and vehicular sight distance requirements; and
  - e. Location in conformance to standards for public safety.

**Response:** *There is no proposed Cultural Improvement Program or Public Art Program.*

#### **Additional Project Narrative Development Review Board Criteria**

The Development Review Board has specific criteria within the zoning ordinance, which serve as the basis for the review and approval of a DRB application. The following are the listed criteria and responses to each of the listed criteria.



- Describe how the proposed development is consistent with the Character and Design Chapter of the Scottsdale General Plan, the Zoning Ordinance, any pertinent master plan, scenic corridor guideline, or streetscape guideline.

*Response: The zoning case was approved in 2019 in conformance with minor changes to the the City's General Plan and the applicable Greater Airpark Character Area Plan ("GACAP"). With the amendment, the GACAP indicates this area as Aviation ("AV"), Airpark Mixed-Use ("AMU"), Employment ("Emp") and Airpark Mixed-Use – Residential ("AMU-R"). The zoning was changed to conform to the GACAP with Planned Airpark Core Development ("PCP") zoning sub districts. With the residential, employment and support commercial uses approved for the Property, the project implements the desired land uses indicated with the AMU-AV and AMU-EMP goals and policies*

- Explain how the proposed development will contribute to the general health, welfare, safety, and convenience of persons residing or working in the vicinity.

*Response: The City's General Plan, GACAP and existing zoning entitlements will allow the Property to develop as a part of a larger industrial/employment core. The Project is a component of an overall master plan that envisions a mixture of uses including employees, employers, and goods that will support religious and education AZCCSA campus adjacent to the Property. This type of development plan creates a set of access points used mainly by employees and employers of the surrounding airpark where there is an expectation of commercial type traffic. This location benefits the economic health of the community while minimizing any impacts on the livability of the community.*

- Describe the spatial relationship that will exist between nearby structures and the proposed development, as well as open spaces, and topography, both within the project site and in the surrounding context.

*Response: The Project is located behind existing uses and in an area of the site hidden by landscape buffers and surrounding buildings. The loading areas are further screened limited in one area of the site. The bays are depressed slightly for trucks to pull up to the loading bays. The proposed landscape buffer combined with the mature landscape buffer along the west side of Miller Road will also provide an adequate spatial balance from the Property to other uses nearby.*

- Explain how the site layout will promote safety and convenience relative to ingress, egress, internal circulation for pedestrians and vehicles, parking areas, loading and service areas.

*Response: The main access point to the site will be from the one (1) existing driveway on Redfield Road approximately 650 feet northeast of the intersection with Thunderbird Road and the City's Park and Ride Facility (the "Facility"). This driveway is currently the main driveway used by all of the existing uses on the Property outside the main religious and school campus. This current access point is a shared access with the existing airpark buildings and uses that actually front onto Redfield Road.*



*As part of the zoning approval, the Property has the ability to create a secondary access point through the Facility site, which is adjacent to the west. The Facility operates as a regional park and ride facility with a main signalized access point at Thunderbird Road and N. 73rd Street. The main driveway is adjacent to the Property and as part of the rezoning stipulations, AZCCSA has the ability and an actual license, to create a driveway connection from the Property to the Facility driveway. The loading areas are gated for controlled access and safety.*

- Describe how the architectural characteristics of the proposed development relate to character elements and design features of the structures that are within the surrounding context.

*Response: The architecture is consistent with the style of the surrounding area while including modern and contemporary forms to provide a lasting design. Steel accents service both as identifiers for entry locations, but also as shaded refuge during inclement weather. Trellis accents provide for vegetation incorporation into entry areas. The color palette of natural tans and greys are consistent with the surrounding area.*

- Describe how the design features and details of the proposed development have been utilized to screen all mechanical equipment, and utilities.

*Response: Mechanical equipment and their associated screening will be integral to the building design. Ground mounted mechanical equipment will be screened either thru the use of architectural site walls or landscape elements, or if located on the roof thru the use of architectural screening compatible with the architecture of the building. Rooftop mechanical equipment will be visually screened by raised façade parapets incorporated into the overall design concept without impeding air flow to such equipment.*

- Describe how the proposed development is consistent with the Sensitive Design Principles, pertinent Architectural Design Guidelines, and other design guidelines.

*Response: This development is consistent with the Sensitive Design Principles as it; enhances the area with new development, does not impact any scenic views, does not disturb any natural landscaping or habitats, has proximity to existing public transportation, and provides pedestrian shaded areas.*

- If the proposed development is located within the environmentally sensitive lands (ESL) district, explain how the proposed development complies with the recommendations and guidelines that are described in the environmentally sensitive lands (ESL) ordinance

*Response: The Property is not located within the City's ESL area.*

- If the proposed development is located within the HP, historic property district, then describe how the proposed development has utilized any unique or characteristic architectural features throughout the design of the project.

*Response: The Property is not located within a historic property designation.*



- If the proposed development is located within the downtown district, then describe how the proposed development has incorporated urban character and pedestrian orientation throughout the design of the project.

*Response: The Property is not located within the downtown district.*

- If the proposed development is located within the downtown district, then describe how the proposed development has incorporated traditional or southwestern design vernaculars, subdivided the building form into smaller character elements, emphasized fine-grain detailing, and utilized recessed fenestrations.

*Response: The Property is not located within the downtown district.*

- If the proposed development is located within the downtown district, then describe how the proposed development has incorporated the urban design and architectural design guidelines.

*Response: The Property is not located within the downtown district.*

#### **SCOTTSDALE SENSITIVE DESIGN PRINCIPLES**

1. The design character of any area should be enhanced and strengthened by new development.
  - Building design should consider the distinctive qualities and character of the surrounding context and, as appropriate, incorporate those qualities in its design.
  - Building design should be sensitive to the evolving context of an area over time.

**Response:** *The design character draws from examples of modern architecture in the Airpark Area. Remaining consistent with a desert-contemporary style, the architecture presents true material qualities appropriate for the region. This high-finish design will endure as the balance of the area continues to redevelop for years to come.*

2. Development, through appropriate siting and orientation of buildings, should recognize and preserve established major vistas, as well as protect natural features such as:
  - Scenic views of the Sonoran Desert and mountains
  - Archaeological and historical resources

**Response:** The Project is required to conduct a historic preservation program to document the buildings that are a part of the overall religious and educational campus. The document will be used to capture the history in the program prior to any of the development occurring on the Property. The main campus, used as dorms and classrooms for pilot training during WW II, will remain as the redevelopment of the remainder of this site is to financially back the religious and educational campus remaining operational at this location.

3. Development should be sensitive to existing topography and landscaping.
  - A design should respond to the unique terrain of the site by blending with the natural shape and texture of the land while minimizing disturbances to the natural environment.



**Response:** *The Property is relatively flat and disturbed. Additional landscape buffers will be added to the existing mature oleander hedge along Miller Road to the east to buffer the nearby residential uses.*

4. Development should protect the character of the Sonoran Desert by preserving and restoring natural habitats and ecological processes.

**Response:** *This portion of the overall AZCCSDA site is already disturbed and utilized.*

5. The design of the public realm, including streetscapes, parks, plazas, and civic amenities, is an opportunity to provide identity to the community and to convey its design expectations
  - o Streetscapes should provide continuity among adjacent uses through use of cohesive landscaping, decorative paving, street furniture, public art, and integrated infrastructure elements.

**Response:** *The Property is mainly hidden off of the Thunderbird Rd and Redfield Rd. frontage. The frontage along this site includes a park and ride facility and water treatment facility and an older two-story office type buildings. The entire north side of the adjacent roadways is a chain link fence and the open runway of Scottsdale Airport. The street tree theme along the south side of Thunderbird/Redfield Road is mainly Palo Verde trees.*

6. Developments should integrate alternative modes of transportation, including bicycles and bus access, within the pedestrian network that encourage social contact and interaction within the community.

**Response:** *In addition to vehicular traffic, access throughout the site will also be provided for walking and cycling. There is a bike lane on Thunderbird/Redfield Road but stops as it heads eastbound at the driveway into the Property. There are recent street improvements for this roadway to the east by the City. Internal sidewalks will allow for pedestrians to traverse the site outside the loading area for the building. The connection to the park and ride facility and future development on this campus will allow for several modes of transportation to be utilized for this and future uses.*

7. Development should show consideration for the pedestrian by providing landscaping and shading elements as well as inviting access connections to adjacent developments.
  - o Design elements should be included to reflect a human scale, such as the use of shelter and shade for the pedestrian and a variety of building masses.

**Response:** *Significant landscaping is proposed for this site for buffers and parking lot landscaping. The site will be designed with shaded pedestrian walkways.*

8. Buildings should be designed with a logical hierarchy of masses:
  - o To control the visual impact of a building's height and size
  - o To highlight important building volumes and features, such as the building entry.



**Response:** *Building entries feature contrasting colors, massing, and materials to serve as a discrete wayfinding tool and provide visual hierarchy to the architecture.*

9. The design of the built environment should respond to the desert environment:
  - Interior spaces should be extended into the outdoors both physically and visually when appropriate
  - Materials with colors and coarse textures associated with this region should be utilized.
  - A variety of textures and natural materials should be used to provide visual interest and richness, particularly at the pedestrian level. Materials should be used honestly and reflect their inherent qualities
  - Features such as shade structures, deep roof overhangs and recessed windows should be incorporated.

**Response:** *All entry areas will feature Class-A finishes with primarily glass and steel accents. A color palette has been chosen with a variety of beige, tan, and grey contrasts – consisted with a traditional Arizona desert theme. Entry areas provided shaded overhangs in response to local climate conditions.*

10. Developments should strive to incorporate sustainable and healthy building practices and products.
  - Design strategies and building techniques, which minimize environmental impact, reduce energy consumption, and endure over time, should be utilized.

**Response:** *The development will strive to source environmentally conscious materials, clerestory windows, and, electric vehicle charging stations at all entries. The project will provide covered parking areas with an option to include solar panels for occupant use.*

11. Landscape design should respond to the desert environment by utilizing a variety of mature landscape materials indigenous to the arid region.
  - The character of the area should be emphasized through the careful selection of planting materials in terms of scale, density, and arrangement
  - The landscaping should complement the built environment while relating to the various uses.

**Response:** *The landscape will utilize a variety of tree and shrub sizes including salvaged materials consistent with the C.O.S. guidelines. Special consideration has been given to landscape buffers adjacent to residential areas. Landscaping will service as both an aesthetic amenity as well as aide in sound and light mitigation.*

12. Site design should incorporate techniques for efficient water use by providing desert adapted landscaping and preserving native plants.
  - Water, as a landscape element, should be used judiciously
  - Water features should be placed in locations with high pedestrian activity.

**Response:** *The landscape will be watered with a drip irrigation system that will be zoned by trees, shrub, and exposures.*



13. The extent and quality of lighting should be integrally designed as part of the built environment.
  - o A balance should occur between the ambient light levels and designated focal lighting needs.
  - o Lighting should be designed to minimize glare and invasive overflow, to conserve energy, and to reflect the character of the area.

**Response:** *The project will utilize energy-efficient LED lighting throughout. All exterior lighting will be oriented/shielded away from neighboring parcels. All lighting will comply with local regulations and consideration has been given to proximity to Airport facilities and FAA regulations. Accent lighting will be provided at key entry areas and project signage locations.*

14. Signage should consider the distinctive qualities and character of the surrounding context in terms of size, color, location, and illumination.
  - o Signage should be designed to be complementary to the architecture, landscaping, and design theme for the site, with due consideration for visibility and legibility.

**Response:** *Again, with the building being located a significant distance from the Thunderbird/Redfield Road frontage, the proposed building will most likely have limited signage visibility. Any signage will conform to the City's sign ordinance.*

## **SUMMARY**

The request is to approve a singular 243,360 square foot (s.f.) building located on approximately 18 acres in the northern portion of the AZCCSDA religious and educational site. This area has been used for temporary outdoor and some indoor industrial uses over the course of many years. The recent (2019) rezoning case approved a master plan to develop around the campus to today's design standards and uses for economic development growth for the airpark and campus. The Project will build the vehicular connection to the park and ride facility providing safe and efficient access for truck traffic and employees to the site.

This project will take a neglected piece of property in the core of our community and revitalize it. The community will see a modern state-of-the-art business incubator space. The modern and compelling design will set a new standard for future projects in the area. With thoughtfulness given to both the environmental impact and adjacent uses, the design of this building will be an asset to the local community and city at large.

## **DEVELOPMENT REVIEW BOARD CRITERIA ANALYSIS**

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Per Section 1.904. of the Zoning Ordinance, in considering any application for development, the Development Review Board shall be guided by the following criteria:

1. The Board shall examine the design and theme of the application for consistency with the design and character components of the applicable guidelines, development standards, Design Standards and Policies Manual, master plans, character plan and General Plan.
  - *The applicant states that the project will be consistent with the applicable guidelines and plans for the area as displayed on the proposed site plan and elevations.*
  - *Staff finds that the proposed development is generally consistent with the Zoning Ordinance as well as the Character and Design element of the General Plan, and the Greater Airpark Character Area Plan, which designates the site as employment/commercial/industrial. Additionally, staff has found the Development Plan to be designed in conformance with the Design Standards & Policies Manual.*
2. The architectural character, landscaping and site design of the proposed development shall:
  - a. Promote a desirable relationship of structures to one another, to open spaces and topography, both on the site and in the surrounding neighborhood;
  - b. Avoid excessive variety and monotonous repetition;
  - c. Recognize the unique climatic and other environmental factors of this region to respond to the Sonoran Desert environment, as specified in the Sensitive Design Principles;
  - d. Conform to the recommendations and guidelines in the Environmentally Sensitive Lands (ESL) Ordinance, in the ESL Overlay District; and
  - e. Incorporate unique or characteristic architectural features, including building height, size, shape, color, texture, setback or architectural details, in the Historic Property Overlay District.
  - *The applicant states that the proposed development will incorporate the distinctive qualities and character of the surrounding context. The building design and overall landscaping aims to minimize the disturbance to the natural environment. The varied use of earth-tone finishes, and adequate shading provides visual interest to the building in response to the desert character of the site. A diverse palette of desert native plant materials will be utilized around the site consistent with the overall region.*
  - *Staff finds that the proposed development promotes a desirable relationship with the surrounding character of the site. The proposed building has a contemporary design style that utilizes a variety of materials and adequate articulation to avoid monotonous repetition. Shading is provided above the main entrances on the north side of the building that appropriately responds to the unique climate of the Sonoran Desert. The site is not located within the ESL Overlay District nor the Historic Preservation Overlay District.*
3. Ingress, egress, internal traffic circulation, off-street parking facilities, loading and service areas and pedestrian ways shall be designed as to promote safety and convenience.
  - *The applicant states that the combination of the existing driveway access point and the new access point through the City's Park and Ride facility will provide adequate ingress and egress for the development proposal. The loading bays will be provided to the rear of the building accessed via a secured gate.*
  - *Staff finds the ingress, egress, internal traffic circulation, off-street parking facilities, loading and service areas and pedestrian ways have been designed to promote safety and*

*convenience. The new secondary access through the City's Park and Ride Facility will adhere to the Revocable Driveway License Agreement.*

4. If provided, mechanical equipment, appurtenances and utilities, and their associated screening shall be integral to the building design.
  - *The applicant states that all rooftop mechanical equipment will be screened by the building parapets to the minimum height of the highest part of the unit.*
  - *Staff finds that the proposed mechanical equipment will be screened by the parapet and site walls that are integral to the building design.*
5. Within the Downtown Area, building and site design shall:
  - a. Demonstrate conformance with the Downtown Plan Urban Design & Architectural Guidelines;
  - b. Incorporate urban and architectural design that address human scale and incorporate pedestrian-oriented environment at the street level;
  - c. Reflect contemporary and historic interpretations of Sonoran Desert architectural traditions, by subdividing the overall massing into smaller elements, expressing small scale details, and recessing fenestrations;
  - d. Reflect the design features and materials of the urban neighborhoods in which the development is located; and
  - e. Incorporate enhanced design and aesthetics of building mass, height, materials and intensity with transitions between adjacent/abutting Type 1 and Type 2 Areas, and adjacent/abutting Type 2 Areas and existing development outside the Downtown Area.
  - *This criterion is not applicable*
6. The location of artwork provided in accordance with the Cultural Improvement Program or Public Art Program shall address the following criteria:
  - a. Accessibility to the public;
  - b. Location near pedestrian circulation routes consistent with existing or future development or natural features;
  - c. Location near the primary pedestrian or vehicular entrance of a development;
  - d. Location in conformance with Design Standards and Policies Manual for locations affecting existing utilities, public utility easements, and vehicular sight distance requirements; and
  - e. Location in conformance to standards for public safety.
  - *This criterion is not applicable*

## **DEVELOPMENT INFORMATION**

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### **Zoning History**

The site was annexed into the City in 1962 (Ord. #165) and zoned to the Industrial Park (I-1) zoning designation. Since the initial rezoning of the site, portions of the overall 75-acre site have been rezoned to Planned Airpark Core Development, Airport Mixed-Use Residential (PCP/AMU-R), Planned Airpark Core Development, Airpark Airport Mixed-Use (PCP/AMU), Planned Airpark Core Development, Aviation (PCP/AV), and Planned Airpark Core Development, Employment (PCP/EMP) under case 14-ZN-2019.

### **Community Involvement**

With the submittal of the application, staff notified all property owners within 750 feet of the site. In addition, the applicant has been in communication with property owners surrounding the site. As of the publishing of this report, staff has received one public comment regarding the application.

### **Context**

The overall 75-acre site is located at the northeast corner of East Sutton Road and North Scottsdale Road, 1 block south of East Redfield/Thunderbird Roads. The site primarily contains existing education and campus facilities built in the 1970s for the Seventh Day Adventist Church Conference. The 18-acre subject site for the proposed warehouse is further situated northeast of the overall Seventh Day Adventist site adjacent to North 76th Street and East Thunderbird Road. The warehouse site contains several hangars and other buildings leased to industrial and office uses that were built in as early as the 1940s. The surrounding development includes Scottsdale's Municipal Airport and Park and Ride facility, commercial, office and industrial uses as well as residential neighborhoods.

### **Project Data**

- Existing Use: Religious and educational campus/commercial/employment/industrial uses (overall campus)
- Proposed Use: Wholesale, warehousing and distribution (subject site)
- Parcel Size: Overall Site: 75-acres  
Warehouse (Phase 1): 783,924 square feet / 18-acre (net)
- Total Proposed Building Area: 243,360 square feet
- Floor Area Ratio: 0.3 FAR
- Building Height Allowed: 60-feet (inclusive of rooftop appurtenances)  
42-feet within 300-feet of residential neighborhoods
- Building Height Proposed: 42-feet, 0-inches (inclusive of rooftop appurtenances)
- Parking Required: 322 spaces
- Parking Provided: 322 spaces
- Open Space Required: 195,981 square feet / 4.5-acres
- Open Space Provided: 195,987 square feet / 4.5-acres

**Stipulations for the  
Development Review Board Application:  
Seventh Day Adventist Warehouse Building  
Case Number: 14-DR-2022**

These stipulations are intended to protect the public health, safety, welfare, and the City of Scottsdale.

**APPLICABLE DOCUMENTS AND PLANS:**

1. Except as required by the Scottsdale Revised Code (SRC), the Design Standards and Policies Manual (DSPM), and the other stipulations herein, the site design and construction shall substantially conform to the following documents:
  - a. Architectural elements, including dimensions, materials, form, color, and texture shall be constructed to be consistent with the building elevations submitted by LGE Design Group, with a city staff date of 11/4/2022.
  - b. The location and configuration of all site improvements shall be consistent with the site plan submitted by LGE Design Group, with a city staff date of 11/4/2022.
  - c. Landscape improvements, including quantity, size, and location shall be installed to be consistent with the preliminary landscape plan submitted by TJ McQueen & Associates, Inc., with a city staff date of 11/4/2022.
  - d. The Preliminary Grading and Drainage Plan and Preliminary Drainage Report submitted by Hunter Engineering, Inc. and accepted in concept by the Stormwater Management Department of the Planning and Development Services. The final plans shall address any comments that are in the drainage report.
  - e. The Water and Sewer Basis of Design Report submitted by Hunter Engineering Group, Inc. and accepted as noted by the Water Resources Department, with project owner addressing notes through construction plan set.

**RELEVANT CASES:**

**Ordinance**

- A. At the time of review, the applicable cases for the subject site were 14-ZN-2019 and 236-SA-2022.

**ARCHAEOLOGICAL RESOURCES:**

**Ordinance**

- B. Any development on the property is subject to the requirements of Scottsdale Revised Code, Chapter 46, Article VI, Section 46-134 - Discoveries of archaeological resources during construction.

**ARCHITECTURAL DESIGN:**

**DRB Stipulations**

2. All exterior window glazing shall be recessed a minimum of fifty (50) percent of the wall depth, including glass windows within any tower/clerestory elements. The amount of recess shall be measured from the face of the exterior wall to the face of the glazing, exclusive of external detailing. With the final plan submittal, the developer shall provide head, jamb and sill details clearly showing the amount of recess for all window types.
3. All exterior doors shall be recessed a minimum of thirty (30) percent of the wall depth, the amount of recess shall be measured from the face of the exterior wall to the face of the glazing, exclusive of external detailing. With the final plan submittal the developer shall provide head, jamb and sill details clearly showing the amount of recess for all door types.
4. Any roll-up doors shall orientate all accessory/mechanical components internal to the building.

**SITE DESIGN:**

**Ordinance**

- C. ACCESS RESTRICTIONS. Access to the development site shall conform to the following restrictions:
  - a. Secondary Access Requirement – There shall be a secondary access to either N. Scottsdale Road or E. Redfield Road if the Phase 1 access through the City’s Park and Ride facility is to be removed in the future, per the Revocable Driveway License Agreement. The location of the secondary access shall be subject to Transportation Department Director or designee approval and be constructed prior to the end of the one-year termination notice, per the Revocable Driveway License Agreement. This requirement does not apply if a secondary access is already existing as part of the Phase 2 development.
  - b. Park and Ride (PNR) Parcel – There shall be no freight truck, or construction access through the City’s Park and Ride facility, per the Revocable Driveway Agreement. Access through the PNR shall be limited to passenger motor vehicles and delivery trucks (such as Fed Ex, USPS, UPS, Staples and trucks of similar size) not to exceed 33,000 pounds gross.
  - c. With the final plans submittal, the owner shall provide a traffic signage plan to identify truck access restrictions on-site. The signs shall be generally located consistent with the Truck Circulation Plan submitted by LGE Design Group with a City Staff date of 11/4/2022.
- D. ON-SITE LANE AND AISLE WIDTHS. Site entrances and bypass lanes shall be forty (40) foot minimum width, all others shall be twenty-four (24) feet minimum (excluding curb and gutter).
- E. ON-SITE PEDESTRIAN CONNECTIONS. Six-foot-wide accessible pedestrian routes from the main entry of the building within the development shall be made to each abutting public street that provides a pedestrian sidewalk, per the Pedestrian Circulation Plan dated 11/4/2022.
- F. REFUSE. Refuse for Phase 1 of project shall be in substantial conformance with the Refuse Plan prepared by LGE Design Group with a City Staff date of 11/4/2022 or as otherwise approved by City Engineering staff. The refuse design shall meet all city Code requirements, subject to Development Review Board approval.
- G. ELECTRIC. Prior to issuance of any Certificate of Occupancy or Certification of Shell Building, whichever is first, the property owner shall cause to have undergrounded all overhead utility infrastructure along Miller Road from E. Sutton Drive to E. Thunderbird Road and within project boundaries.

**DRB Stipulations**

5. All drive aisles that are fire lanes shall have a width of twenty-four (24) feet.
6. Prior to issuance of any building permit for the development project, the property owner shall submit plans and receive approval to construct refuse infrastructure in conformance with the Refuse Details Plan prepared by LGE Design Group with a City Staff date of 11/4/2022

**OPEN SPACE:**

**Ordinance**

- H. Open space requirements of ZO Sec. 5.4007.c shall be met on the 18-acre portion of property for the Phase 1 development of the site, per the Open Space Plan prepared by LGE Design Group with a City Staff date of 11/4/2022.

**LANDSCAPE DESIGN:**

**DRB Stipulations**

7. With the final plans submittal, the owner shall update the landscape plans to show all utility lines and ensure that all new trees are placed at least eight (8) feet away from any utility lines.
8. With final plans submittal, the property owner shall update the landscape plans to show that thorny trees, shrubs and cacti will be planted so that their mature size/canopy will be at least 4 feet away from any walkways or parking area curbing.
9. The existing chain link fence along N. 76<sup>th</sup> Street shall be removed where new block walls will be installed, per the Site Wall Plan prepared by LGE Design Group with a City Staff date of 11/4/2022.
10. A sixty (60) foot wide landscape buffer shall be maintained along the N. Miller/N. 76<sup>th</sup> Street property frontage from E. Sutton Drive to E. Thunderbird Road, per the Open Space Plan prepared by LGE Design Group with a City Staff date of 11/4/2022.

**EXTRIOR LIGHTING:**

**Ordinance**

- I. All exterior luminaires mounted eight (8) feet or higher above finished grade, shall be directed downward.
- J. Any exterior luminaire with a total initial lumen output of greater than 1600 lumens shall have an integral lighting shield.
- K. Any exterior luminaire with a total initial lumen output of greater than 3050 lumens shall be directed downward and comply with the Illuminating Engineering Society of North America (IES) requirements for full cutoff.
- L. The initial vertical luminance at 6-foot above grade along the east property line shall not exceed 0.0 foot-candles and shall include pre-curfew and post-curfew lighting hours:
  - a. Pre-Curfew lighting design hours shall be defined as dusk to 10:00 PM, and post-curfew lighting design hours shall be defined as 10:00 PM to dawn.
  - b. All exterior lighting lights adjacent to residential zones shall be turned off during the post-curfew hours with the exception of lights for security purposes.

**DRB Stipulations**

11. Incorporate the following parking lot and site lighting into the project's design:

## CASE NO. 14-DR-2022

- a. The maintained average horizontal luminance level, at grade on the site, shall not exceed 2 foot-candles. All exterior luminaires shall be included in this calculation
- b. The maintained maximum horizontal luminance level, at grade on the site, shall not exceed 8 foot-candles. All exterior luminaires shall be included in this calculation
- c. The initial vertical luminance at 6-foot above grade, along the entire property line shall not exceed 0.8 foot-candles. All exterior luminaires shall be included in this calculation.
- d. All exterior lighting shall have a color temperature of 3,000 Kelvin or less.
- e. The total lumen per luminaire shall not exceed 24,000 lumens.

### **AIRPORT:**

#### **DRB Stipulations**

12. With the construction document submittal, the property owner shall submit an FAA FORM 7460-1 to the FAA for any proposed structures, appurtenances and/or individual construction cranes that penetrate the 100:1 slope. The elevation of the highest point of those structures, including the appurtenances, must be detailed on the FAA form 7460-1 submittal. The property owner shall provide Aviation staff a copy of the FAA determination letter prior to building permit issuance.
13. With the construction document submittal, the property owner shall submit an aircraft noise and overflight disclosure notice that is to be provided to occupants, potential homeowners, employees and/or students. The disclosure form shall be in a form acceptable to the Scottsdale Aviation Director, prior to the issuance of any building permit, not including a native plant permit.

### **STREET INFRASTRUCTURE:**

#### **Ordinance**

- M. All street infrastructure improvements shall be constructed in accordance with this City of Scottsdale (COS) Supplement to MAG Specifications and Details, and the Design Standards and Policies Manual.
- N. PHASE 1 CIRCULATION IMPROVEMENTS. Prior to the issuance of any building permit for the development project, the property owner shall submit and obtain approval of civil construction documents to construct the following improvements:
  - a. THUNDERBIRD/REDFIELD ROAD.
    - i. At which time the cross-access driveway connection through the park & ride facility is constructed, construct an eastbound right turn lane at N 73RD Street.
    - ii. At which time the cross-access driveway connection through the park & ride facility is constructed, construct traffic video detection cameras to the existing traffic signal at the intersection with N 73rd Street.
  - b. MILLER ROAD.
    - i. Widen existing street improvements to a minimum of twenty (20) feet of pavement, centered on section line with ribbon curb on west side, from E. Sutton Drive to E. Thunderbird Road.
    - ii. Construct an eight (8) foot wide trail, adjacent to the property frontage, from E. Sutton Drive to E. Thunderbird Road.

**WATER AND WASTEWATER:**

**Ordinance**

- O. All water and wastewater infrastructure improvements shall be constructed in accordance with this City of Scottsdale (COS) Supplement to MAG Specifications and Details, and the Design Standards and Policies Manual.

**DRAINAGE AND FLOOD CONTROL:**

**DRB Stipulations**

14. With the civil construction document submittal, the property owner shall submit a final drainage report that demonstrates consistency with the DSPM and the case drainage report accepted in concept by the Stormwater Manager or designee. The approved drainage report has some comments that deemed can be addressed during the final plans stage. The approved report has been previously shared on the city Internet File Exchange folder.

**EASEMENTS DEDICATIONS:**

**Ordinance**

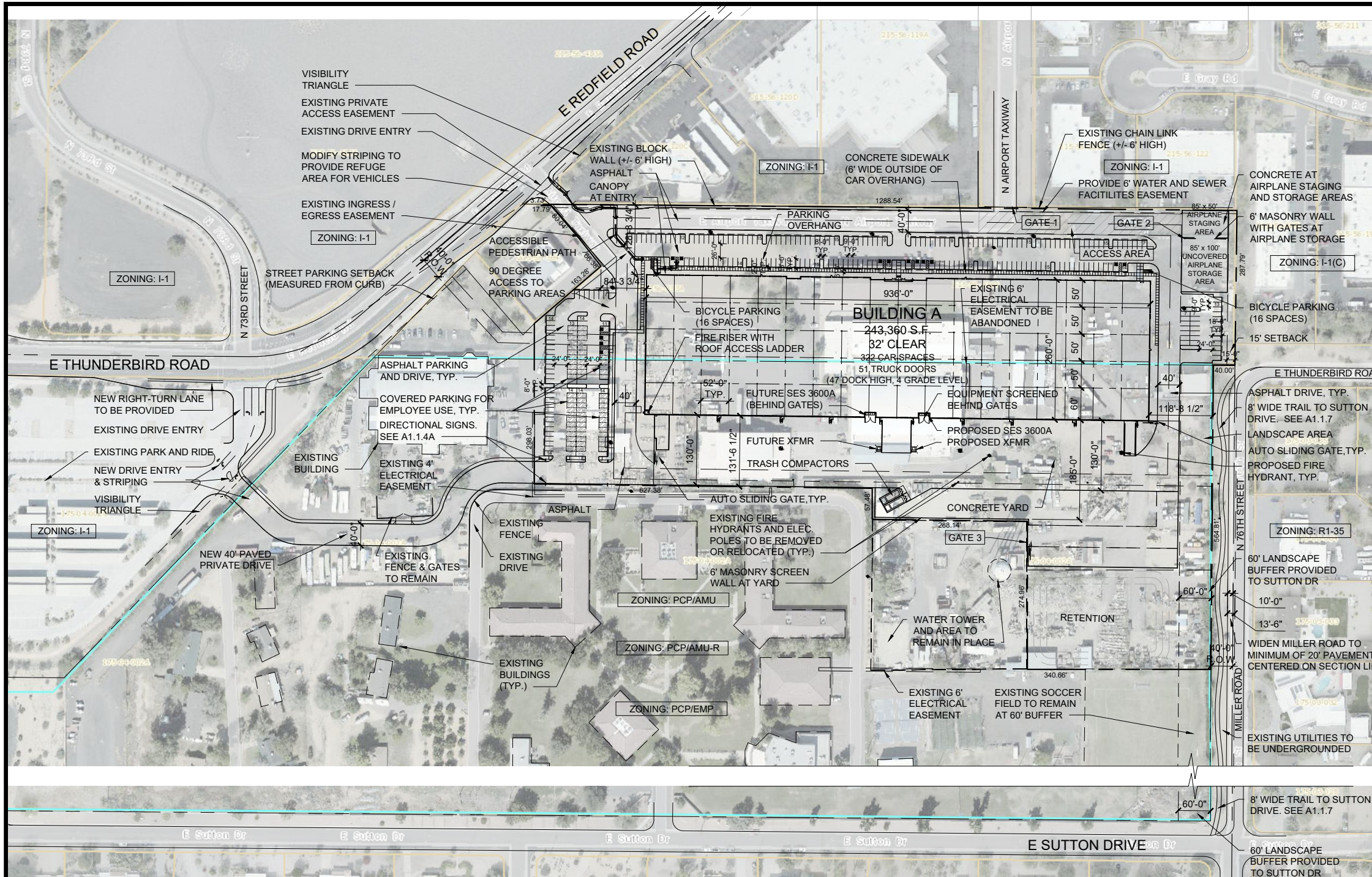
- P. VEHICLE NON-ACCESS EASEMENT. Prior to issuance of any permit for the development project, the property owner shall dedicate a continuous vehicle non-access easement along the Miller Road project frontage, from E Sutton Drive to E Thunderbird Road.
- Q. PUBLIC NON-MOTORIZED ACCESS EASEMENT. Prior to issuance of any permit for the development project, the property owner shall dedicate a continuous Public Non-Motorized Access Easement to the City of Scottsdale to contain the public sidewalks and trails in locations where the improvements are outside of the City right-of-way.
- R. WATER AND SEWER FACILITIES EASEMENT. Prior to issuance of any permit for the development project, the property owner shall dedicate a minimum six (6)-foot wide Water and Sewer Facilities Easement to the City of Scottsdale along the northern project boundary (parcel 215-56-333A) for required maintenance and building separation from the existing public sewer line

**DRB Stipulations**

15. Prior to the issuance of any building permit for the development project, the property owner shall dedicate the following easements to the City of Scottsdale on a final plat or map of dedication:
- a. A Sight Distance Easement, in conformance with figures 5.3-26 and 5.3-27 of Section 5.3 of the DSPM, where a sight distance triangle(s) cross on to the property.
  - b. A Public Motorized Access Easement over shared driveways and drive aisles.
16. Prior to the issuance of any building permit or with Final Plans submittal, the owner shall submit a final plat map for the new parcel boundaries on the development site, per the Preliminary Plat Map prepared by Hunter Engineering, Inc. with a City Staff date of 11/4/2022.

**DRB Stipulations**

17. CONSTRUCTION MANAGEMENT PLAN. Prior to any permit issuance, a construction management plan that includes construction traffic routes, work force vehicle parking, and construction vehicle parking shall be provided to staff for review and approval consistent with the Revocable Driveway License Agreement.



**PROJECT DATA:**

PROJECT NAME: SCOTTSDALE AND THUNDERBIRD  
 PROJECT ADDRESS: SEC OF SCOTTSDALE RD AND THUNDERBIRD RD SCOTTSDALE, ARIZONA  
 DEVELOPER: CREATION RE CONTACT: JAGGER EVERETT 1200 N 52ND STREET PHOENIX, AZ 85008 PHONE: 480-966-4001 E-MAIL: jaggere@creationequity.com  
 APPLICANT: LGE DESIGN GROUP CONTACT: DANNY MACIAS 1200 NORTH 52ND STREET PHOENIX, AZ 85008 PHONE: 480-966-4001 E-MAIL: dannyym@lgedesigngroup.com

PROJECT SCOPE: OFFICE / WAREHOUSE BUILDING  
 ASSESSOR PARCEL NO.: 175-04-002A, 215-56-333C  
 CURRENT ZONING: PCP / AV  
 NET SITE AREA: +/- 783,924 S.F. (18.00 ACRES)  
 GROSS SITE AREA: +/- 807,113 S.F. (18.53 ACRES)

LOT COVERAGE: 31.0%  
 FLOOR AREA RATIO: 243,360 S.F. / 783,924 S.F. = 0.31  
 STORIES: ONE STORY  
 BUILDING AREA (GROSS): 243,360 S.F.  
 OCCUPANCY: B, S-1  
 CONSTRUCTION TYPE: V-B W/ A.F.E.S.  
 SPRINKLERS: YES/FULLY SPRINKLED  
 BUILDING HEIGHT: +/- 42'-0" T.O. PARAPET

**VEHICULAR PARKING CALCS:**

USE	SQUARE FT.	FACTOR	TOTAL
OFFICE	8,000 S.F.	1 / 300 S.F.	27 SPACES
WAREHOUSE	235,360 S.F.	1 / 800 S.F.	295 SPACES
<b>TOTAL PARKING REQUIRED:</b>			<b>322 SPACES</b>

**PARKING PROVIDED:**  
 TOTAL NON-ADA SPACES: 308 SPACES  
 TOTAL ADA SPACES: 14 SPACES  
 TOTAL SPACES ON SITE: 322 SPACES

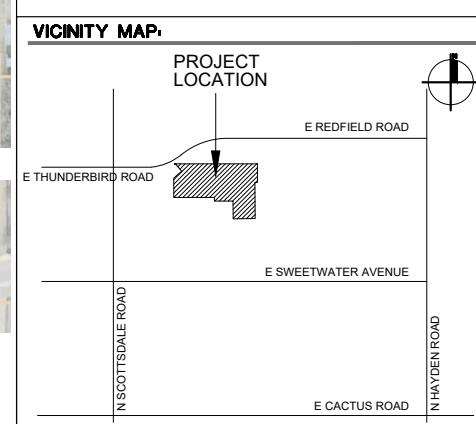
**PARKING/BUILDING RATIO:** 1.32 CARS PER 1,000 S.F.

**ACCESSIBLE PARKING CALCS:**  
 4% OF PROVIDED PARKING PER SEC. 9.105.B.4  
 REQUIRED: 13 SPACES  
 PROVIDED: 14 SPACES

**COVERED ACCESSIBLE PARKING CALCS:**  
 67 TOTAL COVERED PARKING SPACES PROVIDED (4 ADA, 63 NON-ADA)  
 NON-ADA COVERED PARKING PERCENTAGE OF TOTAL NON-ADA PARKING: 63/308 = 20.5%  
 ADA COVERED PARKING AS PERCENTAGE OF TOTAL ADA: 4/14 = 28.6%

**REQUIRED BICYCLE PARKING CALCS:**  
 1 BICYCLE PARKING SPACE REQUIRED PER EVERY 10 VEHICULAR SPACES  
 REQUIRED: 32 SPACES  
 PROVIDED: 32 SPACES

**PROJECT DESCRIPTION:**  
 THIS PROJECT IS A NEW 243,360 SQUARE FOOT OFFICE / WAREHOUSE BUILDING WITH SITE IMPROVEMENTS ON A 18.00 ACRE SITE.

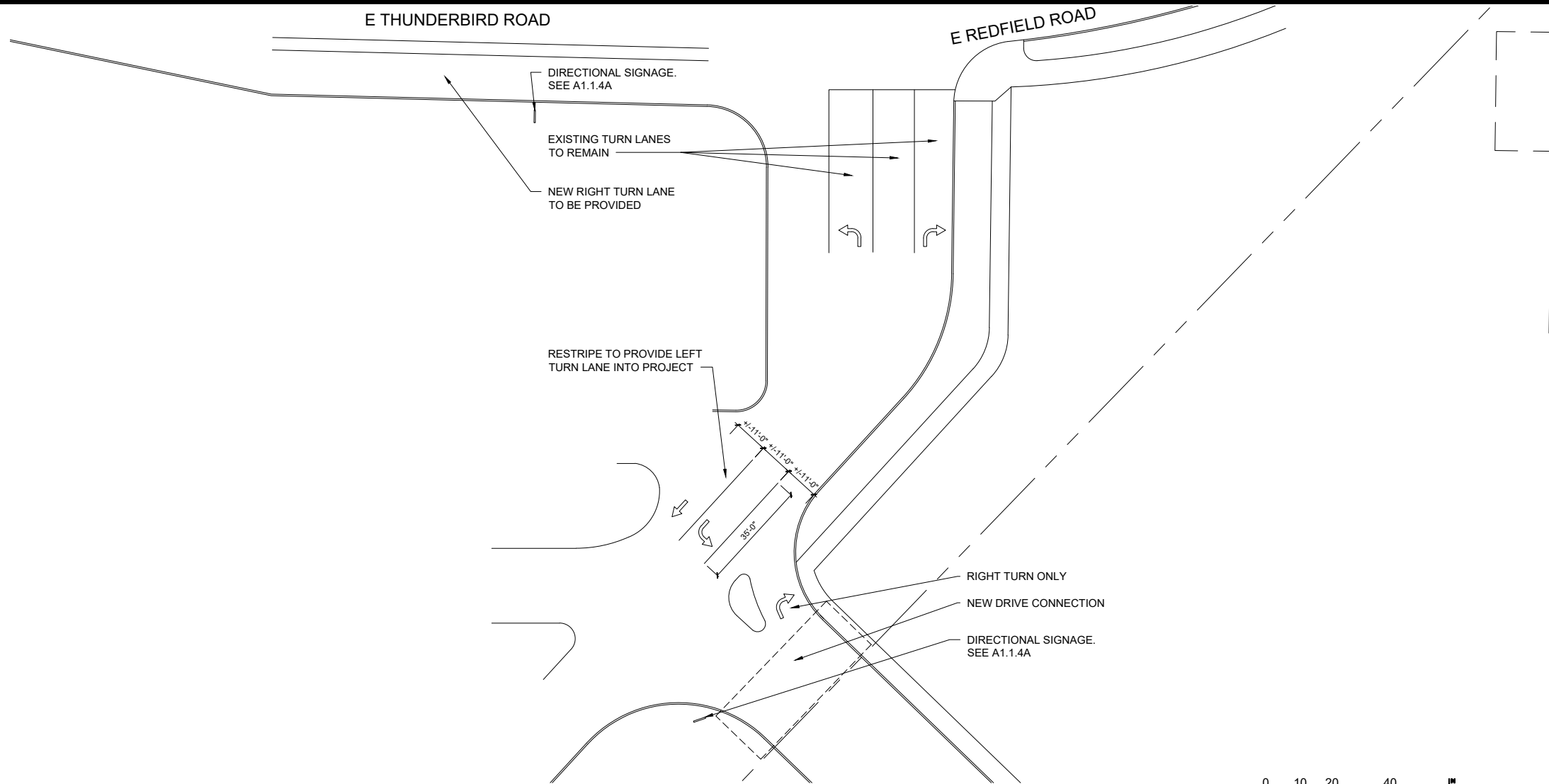


**CITY APPROVALS:**

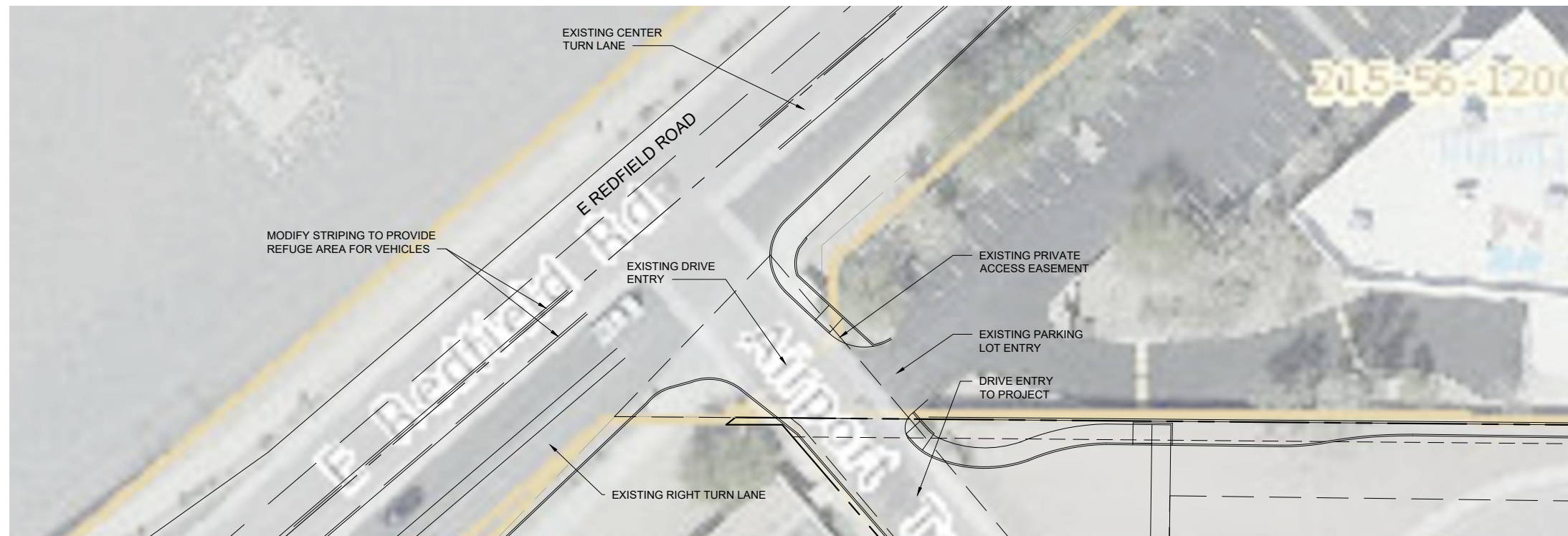
**PRELIMINARY SITE PLAN**

**PLANNING NOTES:**

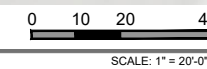
- Sight distance triangles shall be shown on final plans for driveways from commercial sites and any intersections. Area within the safety triangle is to be clear of landscaping, signs, or other visibility obstructions with a height greater than 1.5 feet. Trees within the safety triangle shall be single trunk and have a canopy that begins at 8 feet in height upon installation. All heights are measured from nearest street line elevation.
- Temporary/Security Fencing that is required or is optionally provided shall be in accordance with the Zoning Ordinance and the Design Standards and Policies Manual.
- The temporary/security fence location shall not be modified or the temporary/security fence shall not be removed without the approval of the Planning and Development Services' Inspection Services Division.
- All rights-of-way adjacent to this property shall be landscaped and maintained by the property owner.
- All signs require separate permits and approvals.
- A master sign program shall be subject to the approved of the Development Review Board prior to the issuance of a sign permit for multi-tenant buildings.
- No exterior vending or display shall be allowed.
- Flagpoles, if provided, shall be one piece conical tapered.
- No exterior public address or speaker system shall be allowed.
- Patio umbrellas, if provided, shall be solid colors and shall not have any advertising in the form of signage or logos.
- All exterior mechanical, utility, and communication equipment shall be screened to the height of the tallest unit by parapet or screen wall that matches the architectural color and architectural finish of the building. Ground mounted mechanical, utility, and communication equipment shall be screened by a screen wall that matches the architectural color and architectural finish of the building, which is a minimum of 1'-0" higher than the highest point of tallest unit. (Details are still required.)
- All equipment, utilities, or other appurtenances attached to the building shall be an integral part of the building design in terms of form, color and texture.
- No exterior visible ladders shall be allowed.
- All pole-mounted lighting shall be a maximum of 20 feet in height.
- No chain link fencing shall be allowed.
- No turf areas shall be provided.



**ENLARGED DRIVE ENTRY PLAN - PARK AND RIDE**



**ENLARGED DRIVE ENTRY PLAN - REDFIELD ROAD**



**PROJECT DATA:**

PROJECT NAME: SCOTTSDALE AND THUNDERBIRD  
 ZONING: PCP / AV  
 NET LOT AREA: +/- 783,924 S.F. (18.00 ACRES)

**SCOTTSDALE AND THUNDERBIRD**

SEC OF SCOTTSDALE ROAD AND THUNDERBIRD ROAD  
 SCOTTSDALE, AZ



1200 N. 52nd Street ■ Phoenix, AZ ■ 85008  
 P: 480.966.4001



NUMBER	REVISION	DATE

**CITY APPROVALS:**

**PRELIMINARY NOT FOR CONSTRUCTION**

SHEET TITLE: DRIVE ENTRY PLANS  
 ISSUE DATE: 10/07/22  
 DRAWN BY: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 PROJECT No: \_\_\_\_\_

SHEET: **A1.1.6**

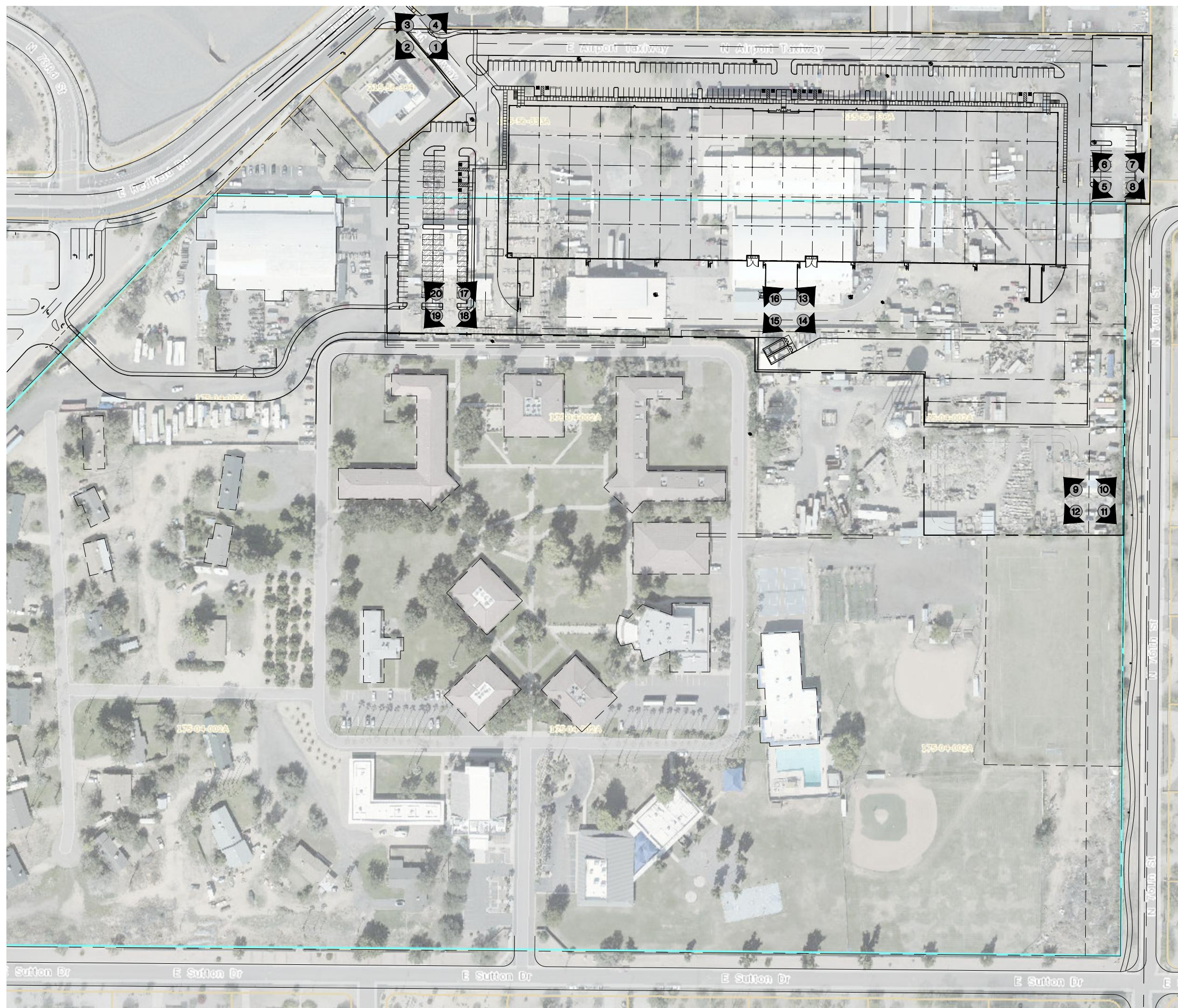
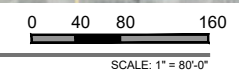


PHOTO EXHIBIT



**PROJECT DATA:**

PROJECT NAME: SCOTTSDALE AND THUNDERBIRD  
 ZONING: PCP / AV  
 NET LOT AREA: +/- 783,924 S.F. (18.00 ACRES)

**CITY APPROVALS:**

PRELIMINARY NOT FOR CONSTRUCTION

SHEET TITLE: PHOTO EXHIBIT  
 ISSUE DATE: 10/07/22  
 DRAWN BY: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 PROJECT No.: \_\_\_\_\_  
 SHEET: **A1.1.7**

**SCOTTSDALE AND THUNDERBIRD**

SEC OF SCOTTSDALE ROAD AND THUNDERBIRD ROAD  
 SCOTTSDALE, AZ



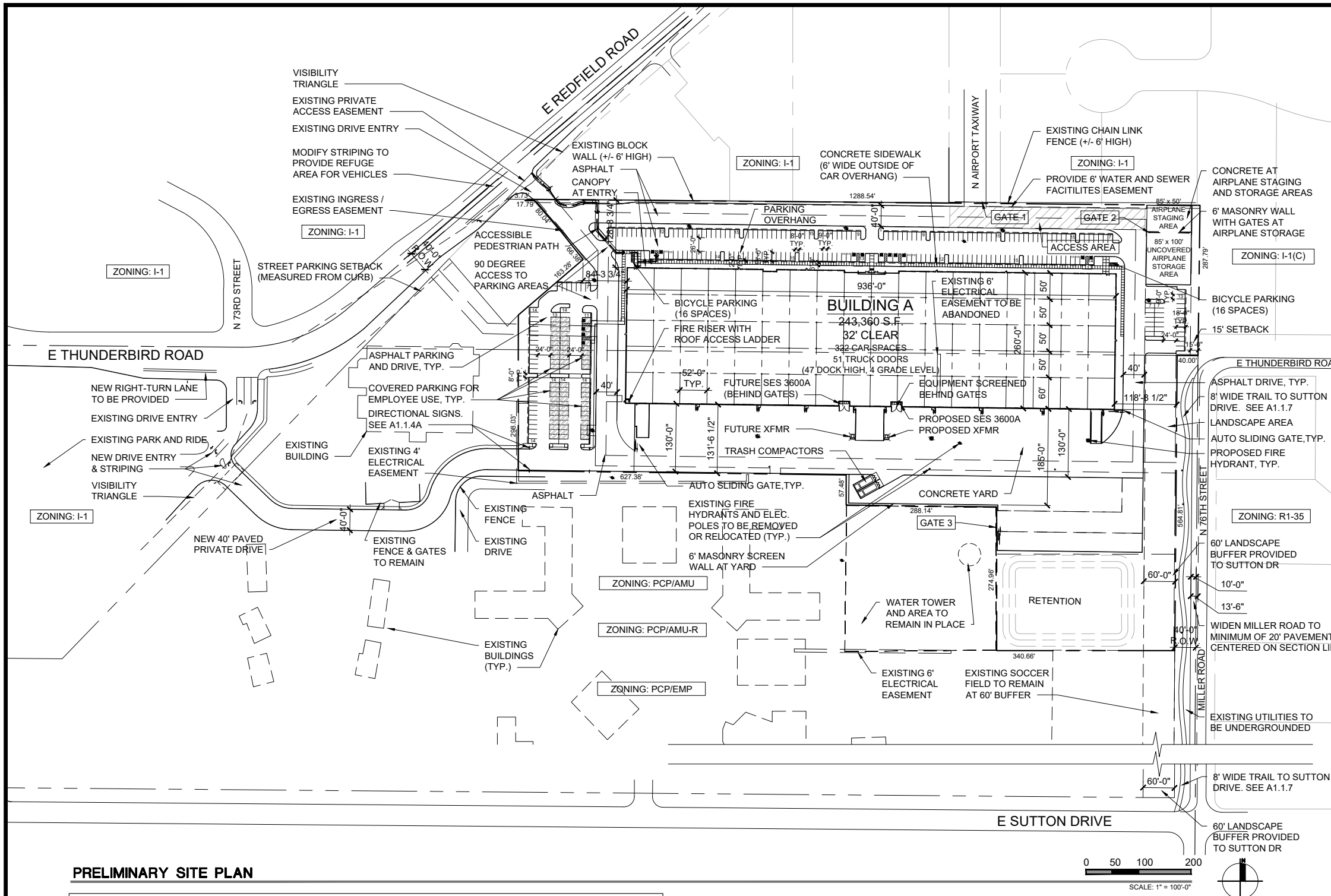
NUMBER	REVISION	DATE

PRELIMINARY NOT FOR CONSTRUCTION

SHEET TITLE: PHOTO EXHIBIT  
 ISSUE DATE: 10/07/22  
 DRAWN BY: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 PROJECT No.: \_\_\_\_\_  
 SHEET: **A1.1.7**



1200 N. 52nd Street • Phoenix, AZ • 85008  
 P: 480.966.4001



**PROJECT DATA:**

PROJECT NAME: SCOTTSDALE AND THUNDERBIRD  
 PROJECT ADDRESS: SEC OF SCOTTSDALE RD AND THUNDERBIRD RD SCOTTSDALE, ARIZONA  
 DEVELOPER: CREATION RE CONTACT: JAGGER EVERETT 1200 N 52ND STREET PHOENIX, AZ 85008 PHONE: 480-966-4001 E-MAIL: jaggere@creationequity.com  
 APPLICANT: LGE DESIGN GROUP CONTACT: DANNY MACIAS 1200 NORTH 52ND STREET PHOENIX, AZ 85008 PHONE: 480-966-4001 E-MAIL: dannyym@lgedesigngroup.com

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 CONSTRUCTION TYPE: V-B W/ A.F.E.S.  
 SPRINKLERS: YES/FULLY SPRINKLED  
 BUILDING HEIGHT: +/- 42'-0" T.O. PARAPET

**VEHICULAR PARKING CALCS:**

USE	SQUARE FT.	FACTOR	TOTAL
OFFICE	8,000 S.F.	1 / 300 S.F.	27 SPACES
WAREHOUSE	235,360 S.F.	1 / 800 S.F.	295 SPACES
<b>TOTAL PARKING REQUIRED:</b>			<b>322 SPACES</b>

**PARKING PROVIDED:**  
 TOTAL NON-ADA SPACES: 308 SPACES  
 TOTAL ADA SPACES: 14 SPACES  
 TOTAL SPACES ON SITE: 322 SPACES

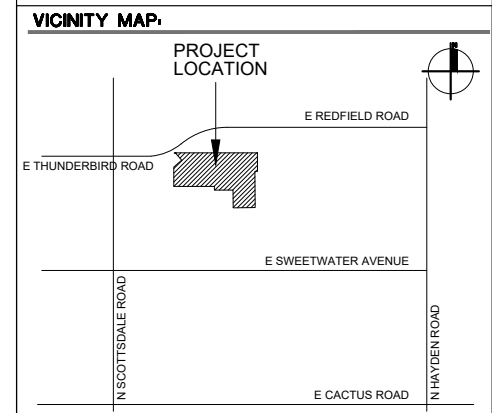
**PARKING/BUILDING RATIO:** 1.32 CARS PER 1,000 S.F.

**ACCESSIBLE PARKING CALCS:**  
 4% OF PROVIDED PARKING PER SEC. 9.105.B.4  
 REQUIRED: 13 SPACES  
 PROVIDED: 14 SPACES

**COVERED ACCESSIBLE PARKING CALCS:**  
 67 TOTAL COVERED PARKING SPACES PROVIDED (4 ADA, 63 NON-ADA)  
 NON-ADA COVERED PARKING PERCENTAGE OF TOTAL NON-ADA PARKING: 63/308 = 20.5%  
 ADA COVERED PARKING AS PERCENTAGE OF TOTAL ADA: 4/14 = 28.6%

**REQUIRED BICYCLE PARKING CALCS:**  
 1 BICYCLE PARKING SPACE REQUIRED PER EVERY 10 VEHICULAR SPACES  
 REQUIRED: 32 SPACES  
 PROVIDED: 32 SPACES

**PROJECT DESCRIPTION:**  
 THIS PROJECT IS A NEW 243,360 SQUARE FOOT OFFICE / WAREHOUSE BUILDING WITH SITE IMPROVEMENTS ON A 18.00 ACRE SITE.



**CITY APPROVALS:**

**PRELIMINARY SITE PLAN**

**PLANNING NOTES:**

Sight distance triangles shall be shown on final plans for driveways from commercial sites and any intersections. Area within the safety triangle is to be clear of landscaping, signs, or other visibility obstructions with a height greater than 1.5 feet. Trees within the safety triangle shall be single trunk and have a canopy that begins at 8 feet in height upon installation. All heights are measured from nearest street line elevation.

Temporary/Security Fencing that is required or is optionally provided shall be in accordance with the Zoning Ordinance and the Design Standards and Policies Manual.

The temporary/security fence location shall not be modified or the temporary/security fence shall not be removed without the approval of the Planning and Development Services' Inspection Services Division.

All rights-of-way adjacent to this property shall be landscaped and maintained by the property owner.

All signs require separate permits and approvals.

A master sign program shall be subject to the approved of the Development Review Board prior to the issuance of a sign permit for multi-tenant buildings.

No exterior vending or display shall be allowed.

Flagpoles, if provided, shall be one piece conical tapered.

No exterior public address or speaker system shall be allowed.

Patio umbrellas, if provided, shall be solid colors and shall not have any advertising in the form of signage or logos.

All exterior mechanical, utility, and communication equipment shall be screened to the height of the tallest unit by parapet or screen wall that matches the architectural color and architectural finish of the building. Ground mounted mechanical, utility, and communication equipment shall be screened by a screen wall that matches the architectural color and architectural finish of the building, which is a minimum of 1'-0" higher than the highest point of tallest unit. (Details are still required.)

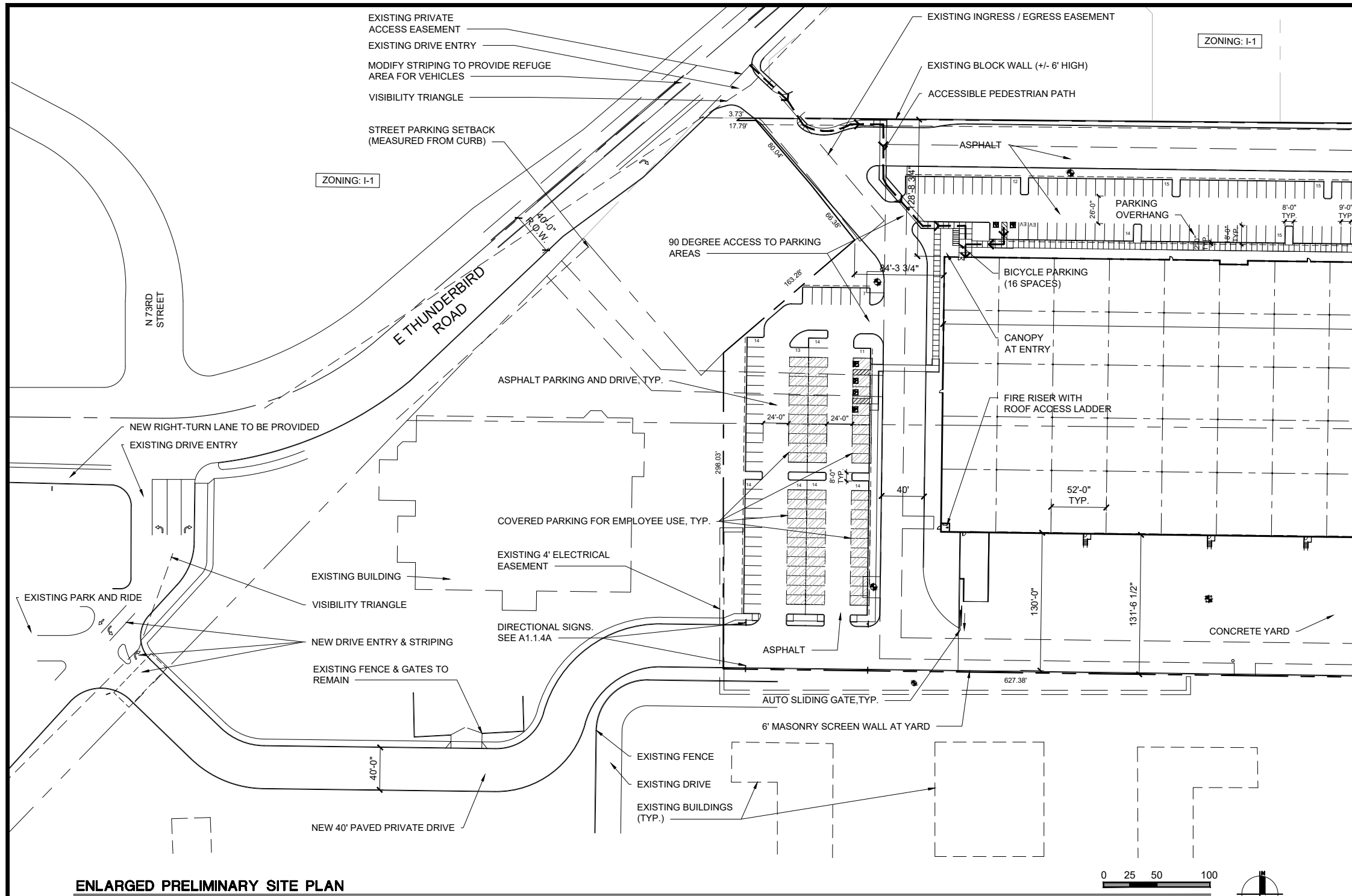
All equipment, utilities, or other appurtenances attached to the building shall be an integral part of the building design in terms of form, color and texture.

No exterior visible ladders shall be allowed.

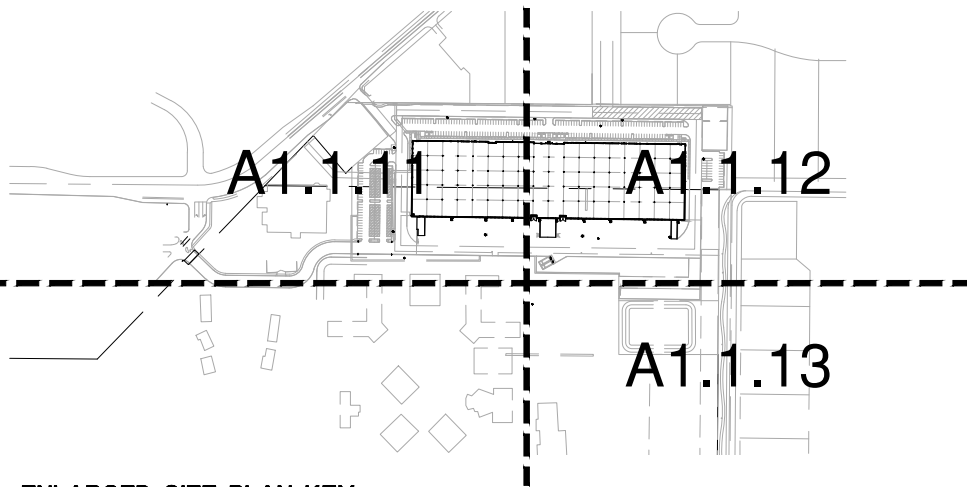
All pole-mounted lighting shall be a maximum of 20 feet in height.

No chain link fencing shall be allowed.

No turf areas shall be provided.



ENLARGED PRELIMINARY SITE PLAN



ENLARGED SITE PLAN KEY

**PLANNING NOTES:**

Sight distance triangles shall be shown on final plans for driveways from commercial sites and any intersections. Area within the safety triangle is to be clear of landscaping, signs, or other visibility obstructions with a height greater than 1.5 feet. Trees within the safety triangle shall be single trunk and have a canopy that begins at 8 feet in height upon installation. All heights are measured from nearest street line elevation.

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All equipment, utilities, or other appurtenances attached to the building shall be an integral part of the building design in terms of form, color and texture.

No exterior visible ladders shall be allowed.

All pole-mounted lighting shall be a maximum of 20 feet in height.

No chain link fencing shall be allowed.

No turf areas shall be provided.

**PROJECT DATA:**

PROJECT NAME: SCOTTSDALE AND THUNDERBIRD  
 PROJECT ADDRESS: SEC OF SCOTTSDALE RD AND THUNDERBIRD RD SCOTTSDALE, ARIZONA  
 DEVELOPER: CREATION RE CONTACT: JAGGER EVERETT 1200 N 52ND STREET PHOENIX, AZ 85008 PHONE: 480-966-4001 E-MAIL: jaggere@creationequity.com  
 APPLICANT: LGE DESIGN GROUP CONTACT: DANNY MACIAS 1200 NORTH 52ND STREET PHOENIX, AZ 85008 PHONE: 480-966-4001 E-MAIL: danny@lgedesigngroup.com  
 PROJECT SCOPE: OFFICE / WAREHOUSE BUILDING  
 ASSESSOR PARCEL NO.: 175-04-002A, 215-56-333C  
 CURRENT ZONING: PCP / AV  
 NET SITE AREA: +/- 783,924 S.F. (18.00 ACRES)  
 GROSS SITE AREA: +/- 807,113 S.F. (18.53 ACRES)  
 LOT COVERAGE: 31.0%  
 FLOOR AREA RATIO: 243,360 S.F. / 783,924 S.F. = 0.31  
 STORIES: ONE STORY  
 BUILDING AREA (GROSS): 243,360 S.F.  
 OCCUPANCY: B, S-1  
 CONSTRUCTION TYPE: V-B W/ A.F.E.S.  
 SPRINKLERS: YES/FULLY SPRINKLED  
 BUILDING HEIGHT: +/- 42'-0" T.O. PARAPET

VEHICULAR PARKING CALCS:

USE	SQUARE FT.	FACTOR	TOTAL
OFFICE	8,000 S.F.	1 / 300 S.F.	27 SPACES
WAREHOUSE	235,360 S.F.	1 / 800 S.F.	295 SPACES
TOTAL PARKING REQUIRED:			322 SPACES

PARKING PROVIDED: 308 SPACES  
 TOTAL NON-ADA SPACES: 14 SPACES  
 TOTAL ADA SPACES: 14 SPACES  
 TOTAL SPACES ON SITE: 322 SPACES

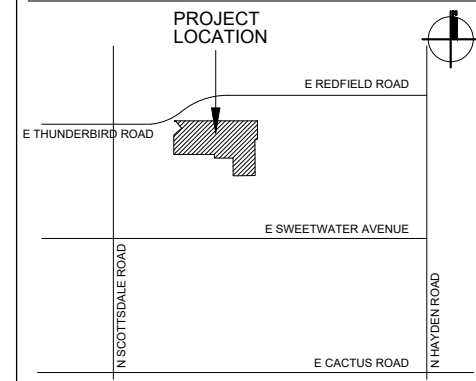
PARKING/BUILDING RATIO: 1.33 CARS PER 1,000 S.F.  
 ACCESSIBLE PARKING CALCS:  
 4% OF PROVIDED PARKING PER SEC. 9.105.B.4  
 REQUIRED: 13 SPACES  
 PROVIDED: 14 SPACES

COVERED ACCESSIBLE PARKING CALCS:  
 67 TOTAL COVERED PARKING SPACES PROVIDED (4 ADA, 63 NON-ADA)  
 NON-ADA COVERED PARKING PERCENTAGE OF TOTAL NON-ADA PARKING: 63/308 = 20.5%  
 ADA COVERED PARKING AS PERCENTAGE OF TOTAL ADA: 4/14 = 28.6%

REQUIRED BICYCLE PARKING CALCS:  
 1 BICYCLE PARKING SPACE REQUIRED PER EVERY 10 VEHICULAR SPACES  
 REQUIRED: 32 SPACES  
 PROVIDED: 32 SPACES

**PROJECT DESCRIPTION:**  
 THIS PROJECT IS A NEW 243,360 SQUARE FOOT OFFICE / WAREHOUSE BUILDING WITH SITE IMPROVEMENTS ON A 18.00 ACRE SITE.

**VICINITY MAP:**



**CITY APPROVALS:**

NUMBER: \_\_\_\_\_ REVISION: \_\_\_\_\_ DATE: \_\_\_\_\_

SHEET TITLE: \_\_\_\_\_  
 SITE PLAN  
 ISSUE DATE: 10/07/22  
 DRAWN BY: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 PROJECT No.: \_\_\_\_\_  
 SHEET: \_\_\_\_\_

SCOTTSDALE AND THUNDERBIRD

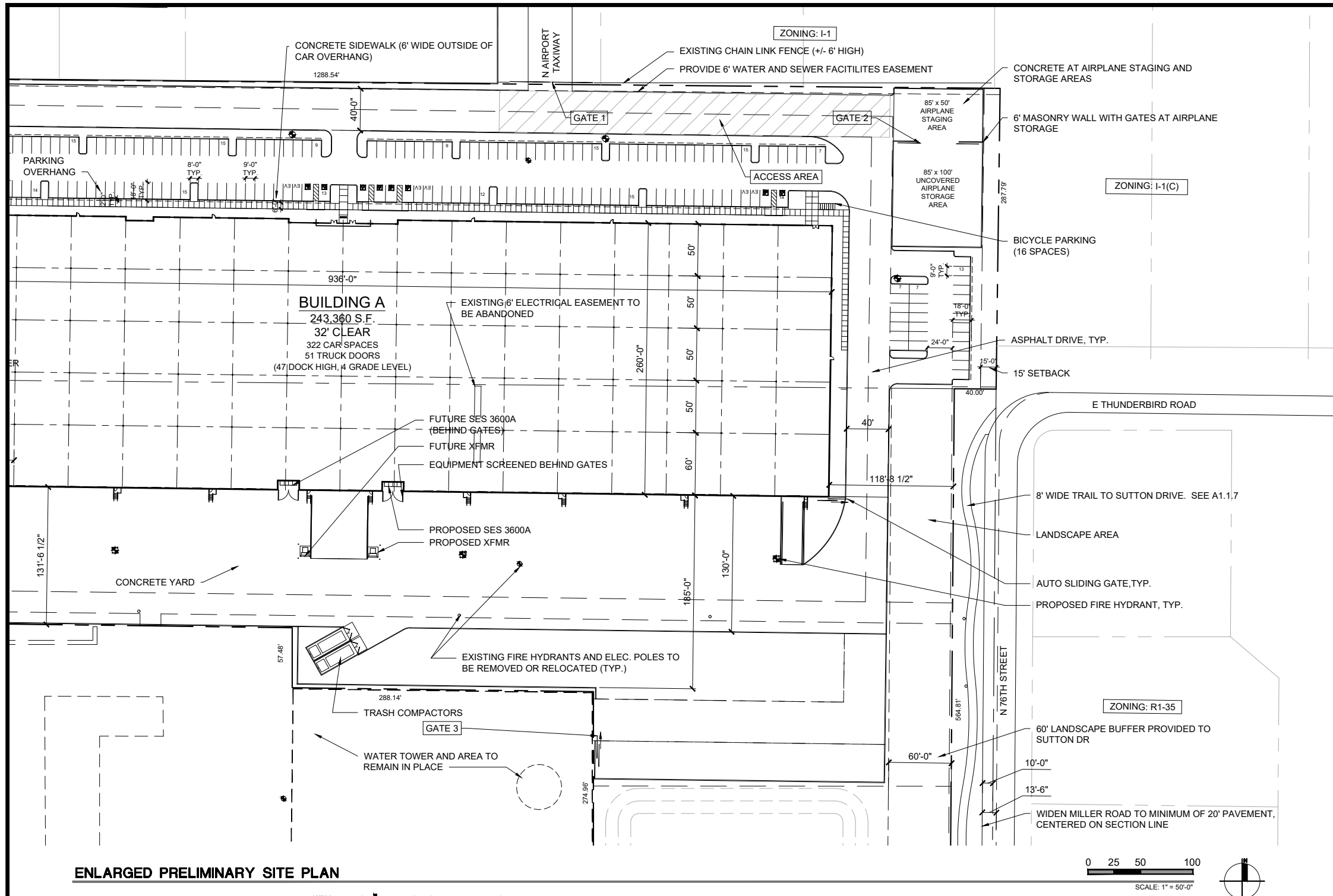


1200 N. 52nd Street ■ Phoenix, AZ ■ 85008  
 P: 480.966.4001

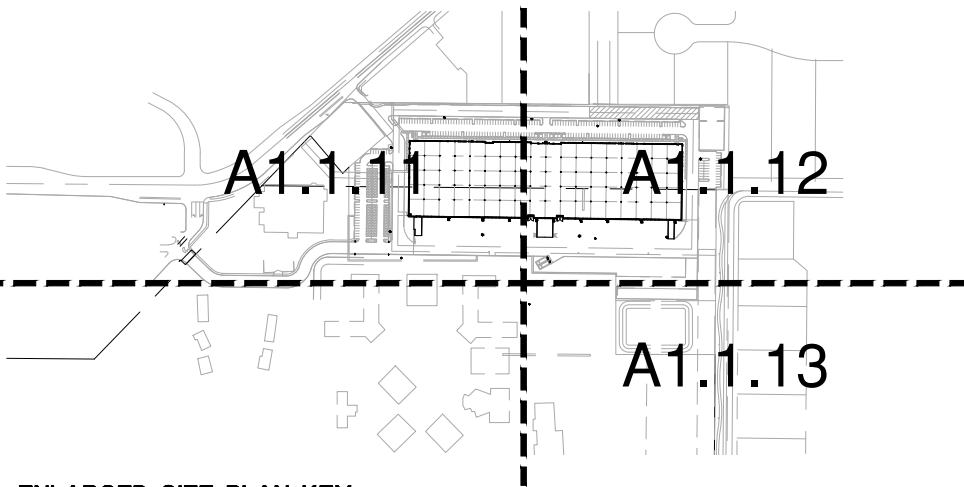


PRELIMINARY NOT FOR CONSTRUCTION

A1.1.11



ENLARGED PRELIMINARY SITE PLAN



ENLARGED SITE PLAN KEY

**PLANNING NOTES:**

Sight distance triangles shall be shown on final plans for driveways from commercial sites and any intersections. Area within the safety triangle is to be clear of landscaping, signs, or other visibility obstructions with a height greater than 1.5 feet. Trees within the safety triangle shall be single trunk and have a canopy that begins at 8 feet in height upon installation. All heights are measured from nearest street line elevation.

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All equipment, utilities, or other appurtenances attached to the building shall be an integral part of the building design in terms of form, color and texture.

No exterior visible ladders shall be allowed.

All pole-mounted lighting shall be a maximum of 20 feet in height.

No chain link fencing shall be allowed.

No turf areas shall be provided.

**PROJECT DATA:**

PROJECT NAME: SCOTTSDALE AND THUNDERBIRD  
 PROJECT ADDRESS: SEC OF SCOTTSDALE RD AND THUNDERBIRD RD SCOTTSDALE, ARIZONA  
 DEVELOPER: CREATION RE CONTACT: JAGGER EVERETT 1200 N 52ND STREET PHOENIX, AZ 85008 PHONE: 480-966-4001 E-MAIL: jaggere@creationequity.com  
 APPLICANT: LGE DESIGN GROUP CONTACT: DANNY MACIAS 1200 NORTH 52ND STREET PHOENIX, AZ 85008 PHONE: 480-966-4001 E-MAIL: dannyym@lgedesigngroup.com  
 PROJECT SCOPE: OFFICE / WAREHOUSE BUILDING  
 ASSESSOR PARCEL NO.: 175-04-002A, 215-56-333C  
 CURRENT ZONING: PCP / AV  
 NET SITE AREA: +/- 783,924 S.F. (18.00 ACRES)  
 GROSS SITE AREA: +/- 807,113 S.F. (18.53 ACRES)  
 LOT COVERAGE: 31.0%  
 FLOOR AREA RATIO: 243,360 S.F. / 783,924 S.F. = 0.31  
 STORIES: ONE STORY  
 BUILDING AREA (GROSS): 243,360 S.F.  
 OCCUPANCY: B, S-1  
 CONSTRUCTION TYPE: V-B W/ A.F.E.S.  
 SPRINKLERS: YES/FULLY SPRINKLED  
 BUILDING HEIGHT: +/- 42'-0" T.O. PARAPET

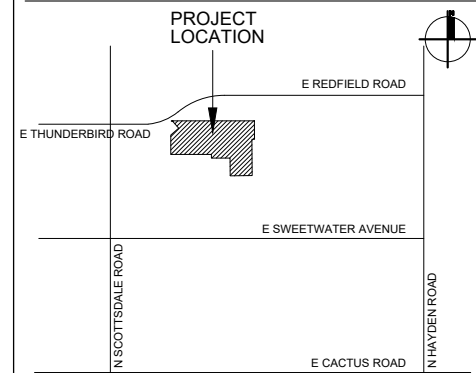
**VEHICULAR PARKING CALCS:**

USE	SQUARE FT.	FACTOR	TOTAL
OFFICE	8,000 S.F.	1 / 300 S.F.	27 SPACES
WAREHOUSE	235,360 S.F.	1 / 800 S.F.	295 SPACES
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<b>PARKING PROVIDED</b>			<b>308 SPACES</b>
TOTAL NON-ADA SPACES			14 SPACES
TOTAL ADA SPACES			322 SPACES
<b>TOTAL SPACES ON SITE</b>			<b>322 SPACES</b>
<b>PARKING/BUILDING RATIO:</b>			<b>1.33 CARS PER 1,000 S.F.</b>
<b>ACCESSIBLE PARKING CALCS:</b>			
4% OF PROVIDED PARKING PER SEC. 9.105.B.4			
REQUIRED:			13 SPACES
PROVIDED:			14 SPACES
<b>COVERED ACCESSIBLE PARKING CALCS:</b>			
67 TOTAL COVERED PARKING SPACES PROVIDED (4 ADA, 63 NON-ADA)			
NON-ADA COVERED PARKING PERCENTAGE OF TOTAL NON-ADA PARKING: 63/308 = 20.5%			
ADA COVERED PARKING AS PERCENTAGE OF TOTAL ADA: 4/14 = 28.6%			
<b>REQUIRED BICYCLE PARKING CALCS:</b>			
1 BICYCLE PARKING SPACE REQUIRED PER EVERY 10 VEHICULAR SPACES			
REQUIRED:			32 SPACES
PROVIDED:			32 SPACES

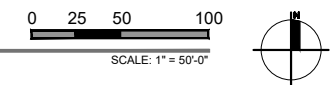
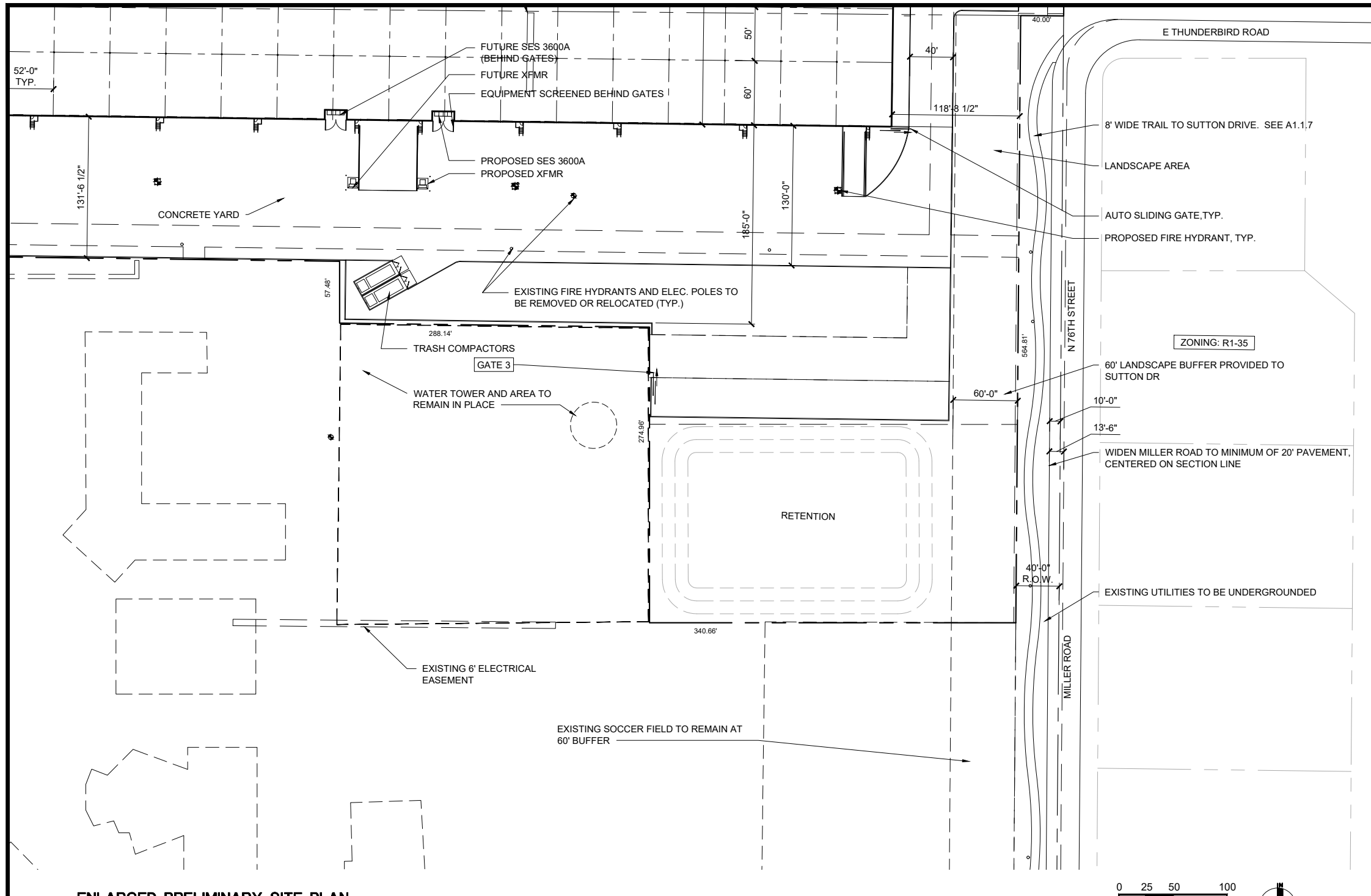
**PROJECT DESCRIPTION:**

THIS PROJECT IS A NEW 243,360 SQUARE FOOT OFFICE / WAREHOUSE BUILDING WITH SITE IMPROVEMENTS ON A 18.00 ACRE SITE.

**VICINITY MAP:**



**CITY APPROVALS:**



**PROJECT DATA:**

PROJECT NAME: SCOTTSDALE AND THUNDERBIRD  
 PROJECT ADDRESS: SEC OF SCOTTSDALE RD AND THUNDERBIRD RD SCOTTSDALE, ARIZONA  
 DEVELOPER: CREATION RE CONTACT: JAGGER EVERETT 1200 N 52ND STREET PHOENIX, AZ 85008 PHONE: 480-966-4001 E-MAIL: jaggere@creationequity.com  
 APPLICANT: LGE DESIGN GROUP CONTACT: DANNY MACIAS 1200 NORTH 52ND STREET PHOENIX, AZ 85008 PHONE: 480-966-4001 E-MAIL: danny@lgedesigngroup.com

PROJECT SCOPE: OFFICE / WAREHOUSE BUILDING  
 ASSESSOR PARCEL NO.: 175-04-002A, 215-56-333C  
 CURRENT ZONING: PCP / AV  
 NET SITE AREA: +/- 783,924 S.F. (18.00 ACRES)  
 GROSS SITE AREA: +/- 807,113 S.F. (18.53 ACRES)

LOT COVERAGE: 31.0%  
 FLOOR AREA RATIO: 243,360 S.F. / 783,924 S.F. = 0.31  
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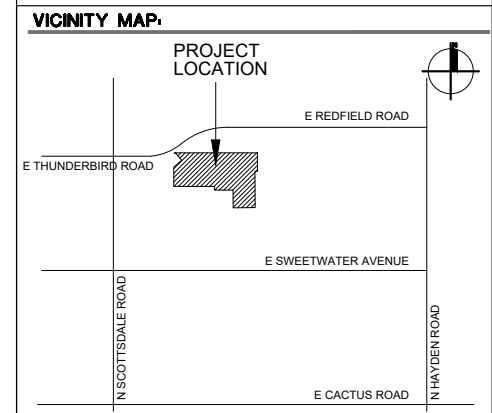
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**ACCESSIBLE PARKING CALCS:**  
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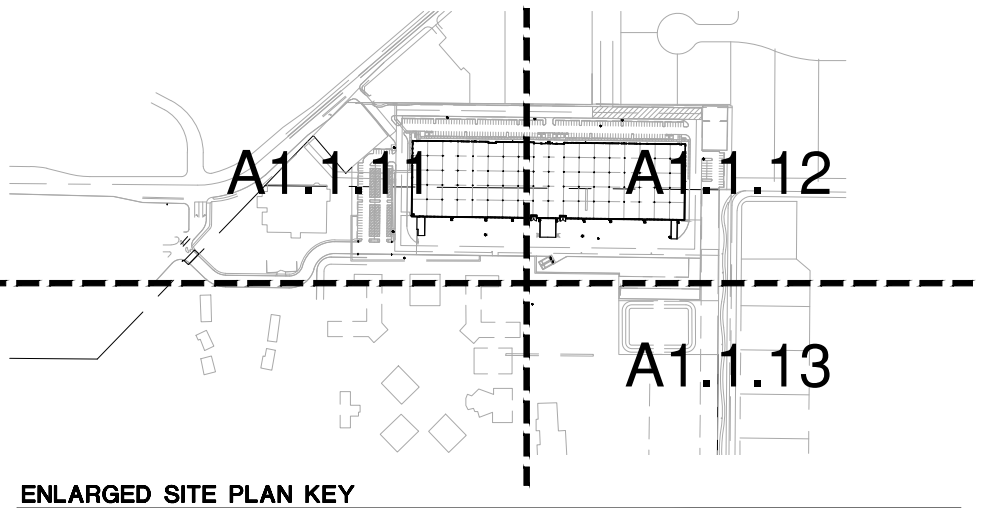
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 PROVIDED: 32 SPACES

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 THIS PROJECT IS A NEW 243,360 SQUARE FOOT OFFICE / WAREHOUSE BUILDING WITH SITE IMPROVEMENTS ON A 18.00 ACRE SITE.



**CITY APPROVALS:**

**ENLARGED PRELIMINARY SITE PLAN**



**PLANNING NOTES:**

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No exterior visible ladders shall be allowed.

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No chain link fencing shall be allowed.

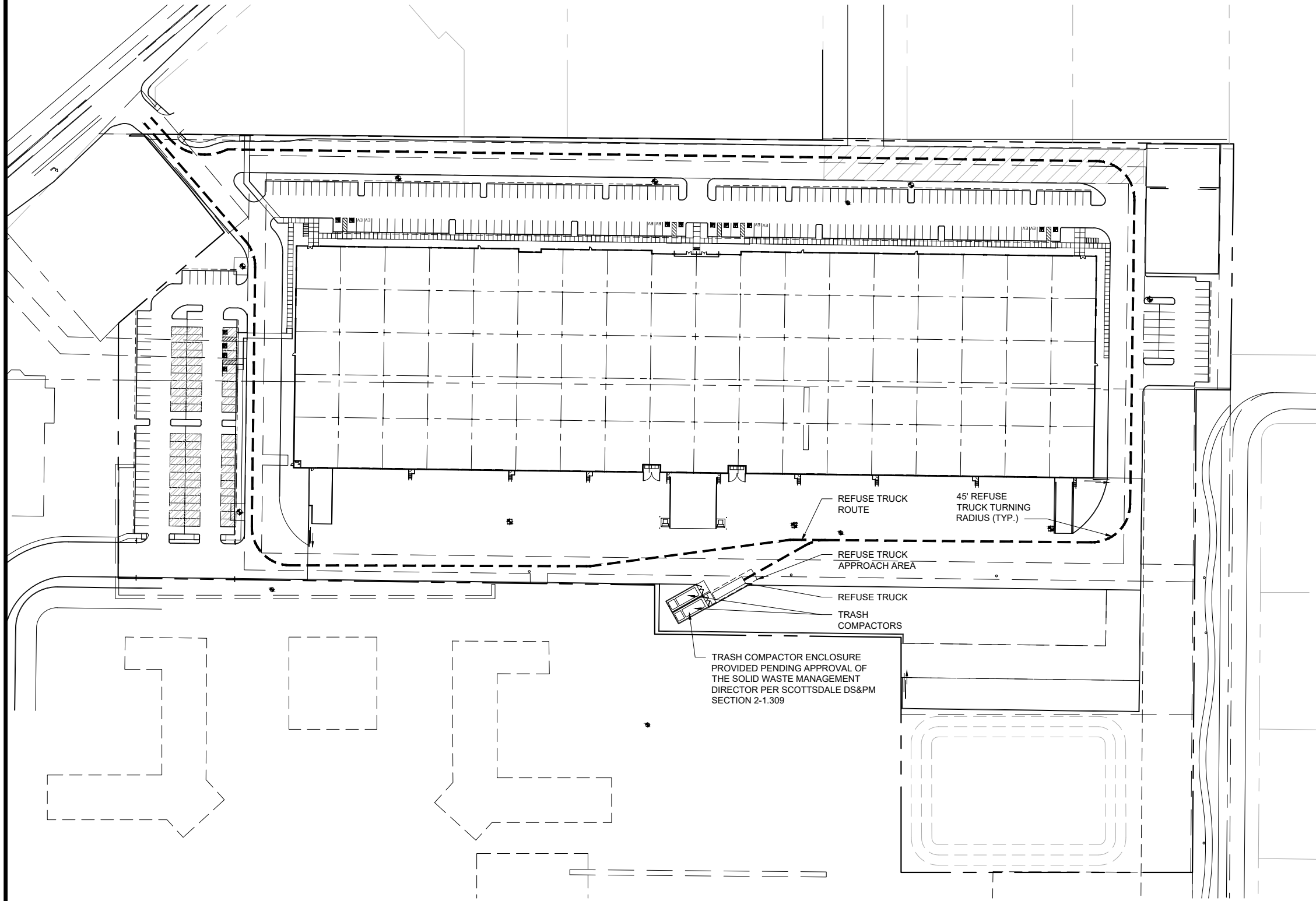
No turf areas shall be provided.

NUMBER	REVISION	DATE

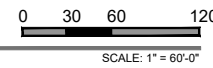
**PRELIMINARY NOT FOR CONSTRUCTION**

SHEET TITLE: \_\_\_\_\_  
 SITE PLAN  
 ISSUE DATE: 10/07/22  
 DRAWN BY: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 PROJECT No.: \_\_\_\_\_

SHEET: **A1.1.13**



**REFUSE PLAN**



**PROJECT DATA:**  
 PROJECT NAME: SCOTTSDALE AND THUNDERBIRD  
 ZONING: PCP / AV  
 NET LOT AREA: +/- 783,924 S.F. (18.00 ACRES)

**CITY APPROVALS:**

**SCOTTSDALE AND THUNDERBIRD**

SEC OF SCOTTSDALE ROAD AND THUNDERBIRD ROAD  
 SCOTTSDALE, AZ

**LGE DESIGNBUILD**

**PRELIMINARY NOT FOR CONSTRUCTION**

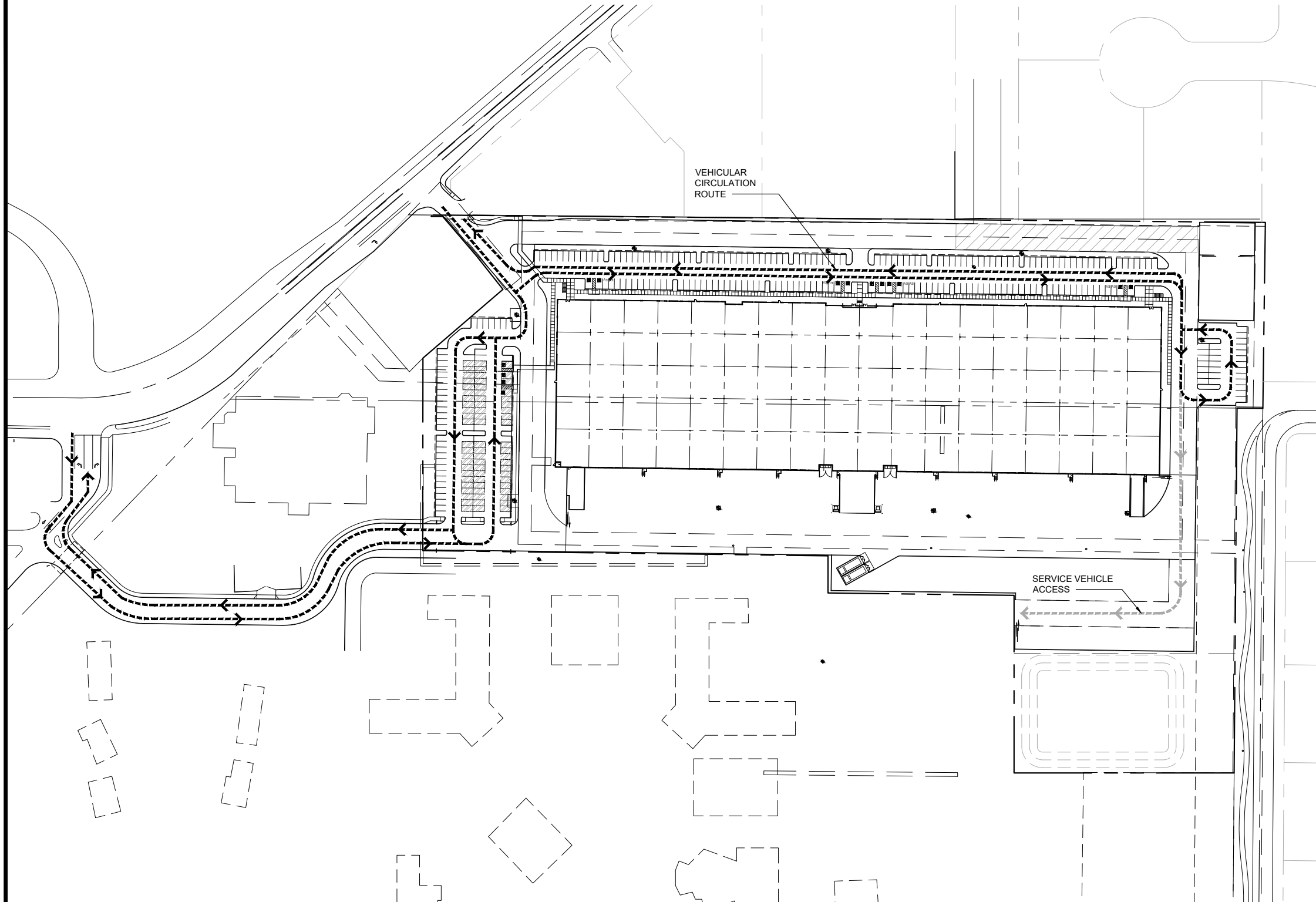
SHEET TITLE: REFUSE PLAN  
 ISSUE DATE: 10/07/22  
 DRAWN BY:  
 CHECKED BY:  
 PROJECT No.:

SHEET: **A1.1.3**

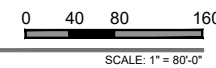
**LGE DESIGNGROUP**  
 1200 N. 52nd Street ■ Phoenix, AZ ■ 85008  
 P: 480.966.4001

10/07/22 - DESIGN REVIEW RESUBMITTAL - 14-DR-2022

14-DR-2022  
 11/4/22



**VEHICULAR CIRCULATION PLAN**



**PROJECT DATA:**  
 PROJECT NAME: SCOTTSDALE AND THUNDERBIRD  
 ZONING: PCP / AV  
 NET LOT AREA: +/- 783,924 S.F. (18.00 ACRES)

**LGE DESIGNGROUP**  
 1200 N. 52nd Street ■ Phoenix, AZ ■ 85008  
 P: 480.966.4001

**SCOTTSDALE AND THUNDERBIRD**

SEC OF SCOTTSDALE ROAD AND THUNDERBIRD ROAD  
 SCOTTSDALE, AZ

**LGE DESIGNBUILD**

NUMBER	REVISION	DATE

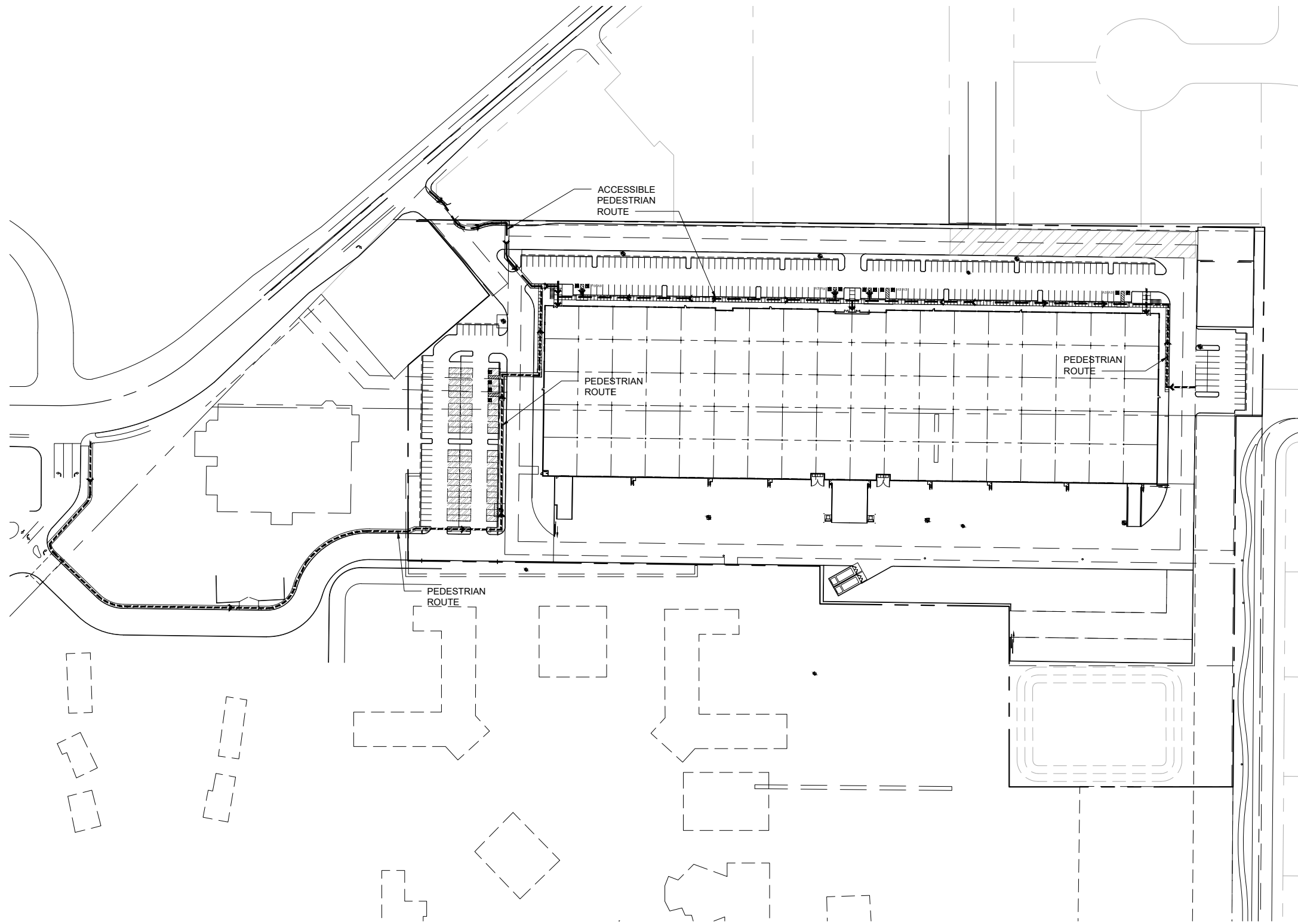
**CITY APPROVALS:**

**PRELIMINARY NOT FOR CONSTRUCTION**

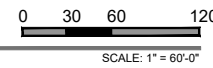
SHEET TITLE: VEHICULAR CIRCULATION PLAN  
 ISSUE DATE: 10/07/22  
 DRAWN BY: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 PROJECT No: \_\_\_\_\_

SHEET: **A1.1.4.B**

10/07/22 - DESIGN REVIEW RESUBMITTAL - 14-DR-2022



**PEDESTRIAN CIRCULATION PLAN**



**PROJECT DATA:**

PROJECT NAME: SCOTTSDALE AND THUNDERBIRD  
 ZONING: PCP / AV  
 NET LOT AREA: +/- 783,924 S.F. (18.00 ACRES)

**SCOTTSDALE AND THUNDERBIRD**

SEC OF SCOTTSDALE ROAD AND THUNDERBIRD ROAD  
 SCOTTSDALE, AZ



NUMBER	REVISION	DATE

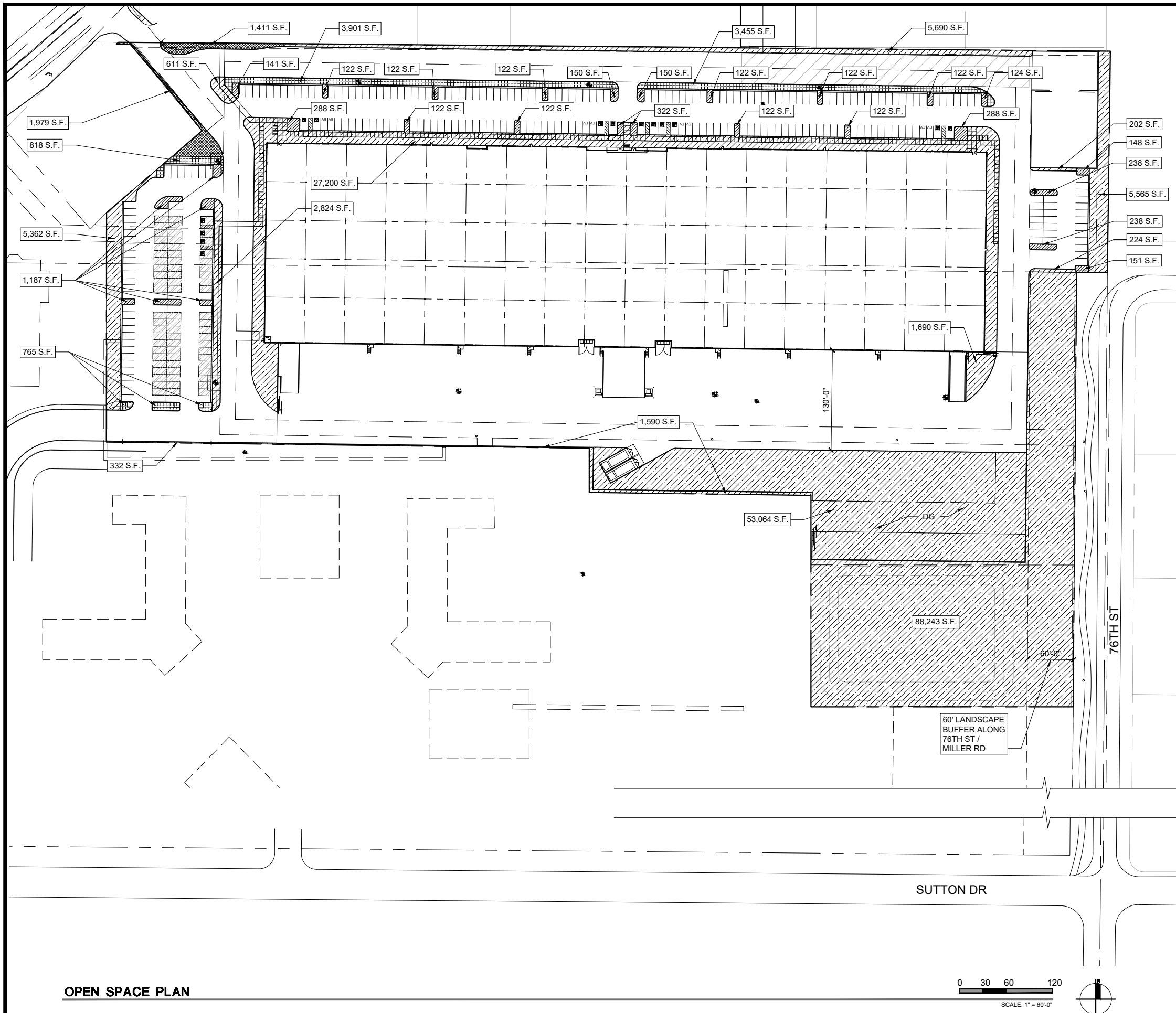
**PRELIMINARY NOT FOR CONSTRUCTION**

SHEET TITLE: PEDESTRIAN CIRCULATION PLAN  
 ISSUE DATE: 10/07/22  
 DRAWN BY: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 PROJECT No: \_\_\_\_\_

SHEET: **A1.1.4.C**

**LGE DESIGNGROUP**  
 1200 N. 52nd Street ■ Phoenix, AZ ■ 85008  
 P: 480.966.4001

10/07/22 - DESIGN REVIEW RESUBMITTAL - 14-DR-2022



**PROJECT DATA:**

PROJECT NAME: SCOTTSDALE AND THUNDERBIRD  
 ZONING: PCP / AV  
 NET LOT AREA: +/- 783,924 S.F. (18.00 ACRES)

**OPEN SPACE CALCULATIONS:**

REQUIRED OPEN SPACE: MINIMUM 25% OF NET LOT AREA OF DEVELOPMENT PLAN PER PCP ZONING (5.4007 C.1.a)

OPEN SPACE REQUIRED: (NOT INCLUDING PARKING LOT LANDSCAPING) 783,924 S.F. x 25% = 195,981 S.F.

OPEN SPACE PROVIDED: (NOT INCLUDING PARKING LOT LANDSCAPING) 195,987 S.F. (25.00%)

PARKING LOT LANDSCAPING: PARKING LOT AREA x 15%

AREA REQUIRED: 90,036 S.F. x 15% = 13,506 S.F.

AREA PROVIDED: 13,584 S.F.

PARKING ISLAND LANDSCAPING: PARKING LOT LANDSCAPING x 33%

AREA REQUIRED: 13,506 S.F. x 33% = 4,457 S.F.

AREA PROVIDED: 4,645 S.F.

- DENOTES FRONT OPEN SPACE  
3,390 S.F.
- DENOTES OTHER OPEN SPACE  
192,597 S.F.
- DENOTES PARKING LOT LANDSCAPING  
8,939 S.F.
- DENOTES PARKING ISLAND LANDSCAPING  
4,645 S.F.

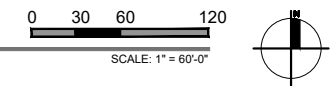
**CITY APPROVALS:**

NUMBER	REVISION	DATE

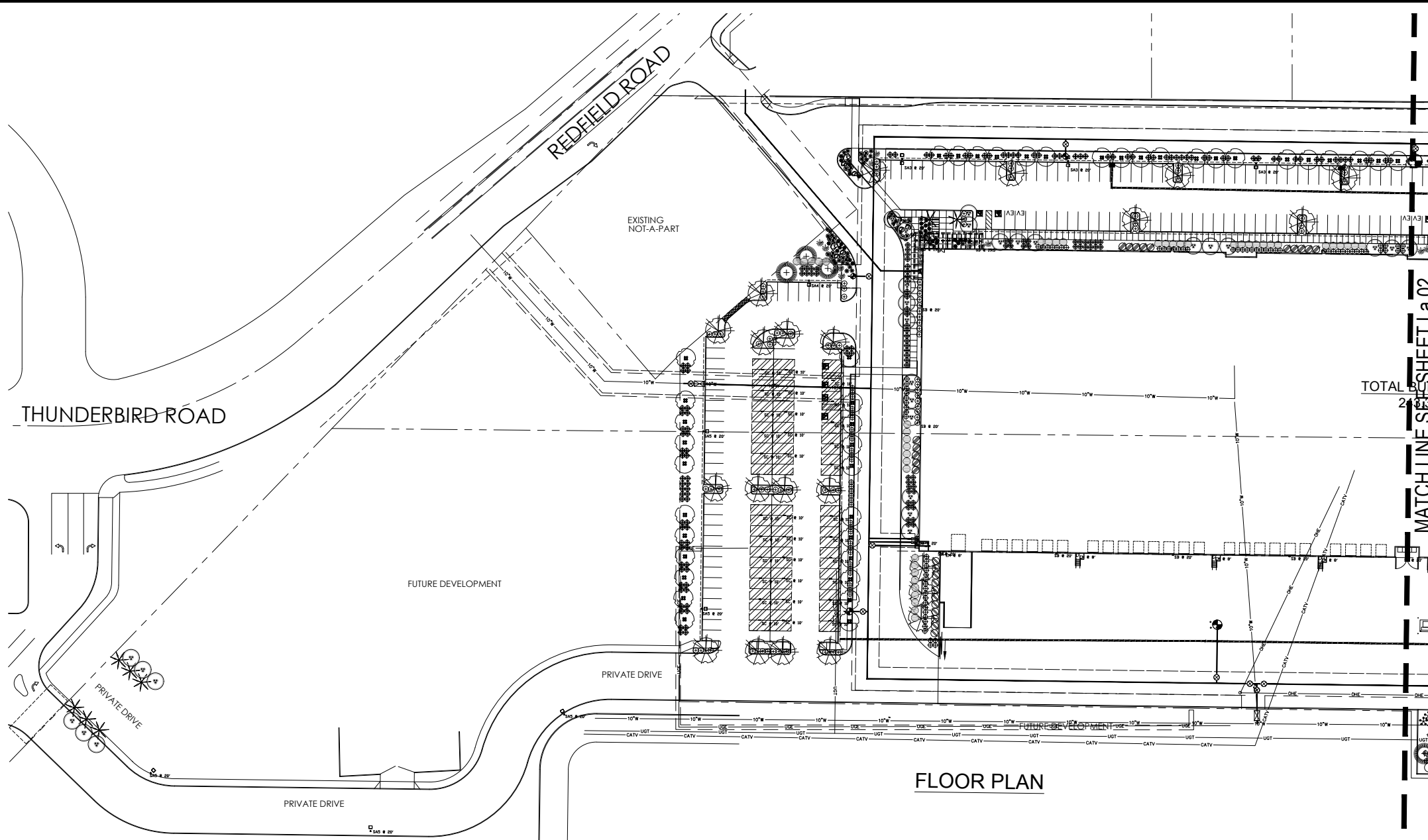
PRELIMINARY NOT FOR CONSTRUCTION

SHEET TITLE: OPEN SPACE PLAN  
 ISSUE DATE: 10/07/22  
 DRAWN BY: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 PROJECT No: \_\_\_\_\_  
 SHEET: \_\_\_\_\_

**OPEN SPACE PLAN**



**ATTACHMENT #9**



**FLOOR PLAN**

**CITY OF SCOTTSDALE LANDSCAPE NOTES:**

- AN AUTOMATIC IRRIGATION SYSTEM WILL BE INSTALLED GUARANTEEING 100% COVERAGE TO ALL LANDSCAPE AREAS.
- ALL LANDSCAPE AREAS WILL BE TOP-DRESSED WITH A 2" DEPTH OF DECOMPOSED GRANITE.
- PROVIDE 8% SLOPE AWAY FROM WALK OR CURB FOR 5' ALONG ALL STREETS.
- ALL RIGHT OF WAYS ADJACENT TO THIS PROPERTY SHALL BE LANDSCAPED AND MAINTAINED BY THE PROPERTY OWNER
- ANY EXISTING LANDSCAPE MATERIALS INCLUDING TREES DAMAGED OR DESTROYED AS A RESULT OF THIS CONSTRUCTION SHALL BE REPLACED, TO THE SATISFACTION OF CITY STAFF, WITH LIKE KIND AND SIZE PRIOR TO RECEIVING A CERTIFICATE OF OCCUPANCY.
- AREAS WITHIN THE SIGHT DISTANCE TRIANGLES IS TO BE CLEAR OF LANDSCAPING, SIGNS, OR OTHER VISIBILITY OBSTRUCTIONS WITH A HEIGHT GREATER THAN 1'-6". TREES WITHIN THE SAFETY TRIANGLE SHALL HAVE A CANOPY THAT BEGINS AT 8 FEET IN HEIGHT UPON INSTALLATION. ALL HEIGHTS ARE MEASURED FROM NEAREST STREET LINE ELEVATION.
- ALL RIGHT-OF-WAY ADJACENT TO THIS PROPERTY SHALL BE LANDSCAPED AND MAINTAINED BY THE PROPERTY OWNER.
- ALL SLOPES ON SITE ARE 4:1 MAX
- NO TURF AREAS ARE TO BE PROVIDED.
- SEE ARCHITECTURAL SITE PLAN FOR SETBACK DIMENSIONS.
- SEE ARCHITECTURAL FOR SITE LIGHTING LOCATIONS. SEE ELECT. DRAWINGS FOR ALL LIGHTING SPECIFICATIONS.
- SEE ARCHITECTURAL FOR SITE WALL ELEVATIONS, COLORS
- SEE CIVIL DRAWINGS FOR ALL RETENTION AREAS, SECTIONS, AND SLOPE RATIOS.
- SEE ARCHITECTURAL FOR BIKE RACK DETAILS.
- ALL SIGNS REQUIRE SEPARATE APPROVALS & PERMITS.
- "SETBACK ALL SPRAY & STREAM TYPE IRRIGATION HEADS 1'-0" FROM BACK OF CURB OR SIDEWALK TO REDUCE OVER SPRAY".
- A MINIMUM 50 PERCENTAGE (UNLESS OTHERWISE STIPULATED BY THE DEVELOPMENT REVIEW BOARD, and/or THE ZONING ORDINANCE REQUIREMENTS) OF THE PROVIDED TREES SHALL BE MATURE TREES, PURSUANT TO THE CITY OF SCOTTSDALE'S ZONING ORDINANCE ARTICLE X, SECTION 10.301, AS DEFINED IN THE CITY OF SCOTTSDALE'S ZONING ORDINANCE ARTICLE III, SECTION 3.100.
- A SINGLE TRUNK TREE'S CALIPER SIZE, THAT IS TO BE EQUAL TO OR LESS THAN 4-INCHES, SHALL BE DETERMINED BY UTILIZING THE SMALLEST DIAMETER OF THE TRUNK 6-INCHES ABOVE FINISHED GRADE ADJACENT TO THE TRUNK.
- A TREE CALIPER SIZE, FOR SINGLE TRUNK TREES WHICH HAVE A DIAMETER GREATER THAN 4-INCHES, SHALL BE DETERMINED BY UTILIZING THE SMALLEST DIAMETER OF THE TRUNK 12-INCHES ABOVE FINISHED GRADE ADJACENT TO THE TRUNK.
- A MULTI TRUNK TREE'S CALIPER SIZE IS MEASURED AT 6-INCHES ABOVE THE LOCATION THAT THE TRUNK SPLITS ORIGINATES, OR 6-INCHES ABOVE FINISHED GRADE OF ALL TRUNKS ORIGINATE FROM THE SOIL.

**THORNEY PLANT NOTE:**

THORNEY TREES, SHRUBS AND CACTI SHALL BE PLANTED SO THAT THEIR MATURE SIZE / CANOPY WILL BE AT LEAST 4 FEET AWAY FROM ANY WALKWAYS OR PARKING AREA CURBING. PLEASE REFER TO DSPM SEC. 2-1.501.L

**LANDSCAPE LEGEND**

<p><b>TREES</b></p> <ul style="list-style-type: none"> <li>CERCIDIUM 'HYBRID' - DESERT MUSEUM PALO VERDE 2" CALIP., 6.5T, 4.5W (82)</li> <li>PISTACIA X 'RED-PUSH' RED PUSH PISTACHE 2" CALIP., 6.5T, 4W (2)</li> <li>PROSOPIS 'RIO-SALADO' HYBRID THORNLESS MESQUITE 1.5" CALIP., 6T, 3.5W (95)</li> <li>OLNEYA TESOTA IRONWOOD 2" CALIP., 5T, 3.5W (47)</li> <li>PHOENIX DACTYLIFERA DATE PALM (MATCHING) 20' T.F. DIAMOND CUT, STRAIGHT, NO AERIAL ROOTS SHOWING. (10)</li> <li>ACACIA ANEURA MULGA (SRP/APS APPROVED) 2" CALIP., 5.5T, 4W (57)</li> <li>CAESALPINIA CACALACO CASCALOTE 1.5" CALIP. (MULTI) 4T, 3.5W (50)</li> </ul>	<p><b>LARGE SHRUBS</b></p> <ul style="list-style-type: none"> <li>TECOMA 'ORANGE JUBILEE' ORANGE JUBILEE 5 GALLON (11)</li> <li>CAESALPINIA MEXICANA MEXICAN BIRD OF PARADISE 5 GALLON (119)</li> <li>DODONEA VISCOSA GREEN HOP BUSH 5 GALLON (96)</li> <li>EREMOPHILA MACULATA VALENTINE EMU BUSH 5 GALLON (133)</li> <li>LEUCOPHYLLUM FRUTESCENS 'GREEN CLOUD' 5 GALLON (62)</li> </ul>	<p><b>ACCENTS</b></p> <ul style="list-style-type: none"> <li>HESPERALOE PARVIFLORA RED YUCCA 5 GALLON (388)</li> <li>HESPERALOE PARVIFLORA YELLOW YUCCA 5 GALLON (108)</li> <li>ECHINOCACTUS GRUSONII GOLDEN BARREL CACTUS 10" ROUND (MATCHING) (49)</li> <li>DASYLIRON WHEELERII DESERT SPOON 5 GALLON (275)</li> <li>PACHYCEERUS MARGINATUS MEXICAN FENCE POST 3-STALK GROUPING (2.5, 2, 1" TALL) (15)</li> <li>AGAVE DESMETIANA SMOOTH AGAVE 5 GALLON (30)</li> <li>EUPHORBIA RESINIFERA MOROCCAN MOUND 5 GALLON (77)</li> </ul>	<p><b>GROUND COVER</b></p> <ul style="list-style-type: none"> <li>LANTANA MONTEVIDENSIS 'GOLD MOUND' 1 GALLON (194)</li> <li>ACACIA REDOLENS 'DESERT CARPET' tm 5 GALLON (158)</li> <li>EREMOPHILA GLABRA 'MINGENEW GOLD' 5 GALLON (331)</li> </ul> <p><b>INERT MATERIALS</b></p> <ul style="list-style-type: none"> <li>3"x3"x3" SURFACE SELECT GRANITE BOULDER (BURY 1/3) MINIMUM 2000lbs EACH (14)</li> <li>1/2" SCREENED ROCK PROS MAHOGANY DECOMPOSED GRANITE 2" DEPTH IN ALL LANDSCAPE AREAS</li> </ul>
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**LANDSCAPE PLAN APPROVED**  
CITY OF SCOTTSDALE

CASE NUMBER	APPROVED	DATE
CONSTRUCTION AND INSTALLATION SHALL BE IN ACCORDANCE WITH THIS PLAN AND ANY AND ALL DEVIATIONS WILL REQUIRE REAPPROVAL. LANDSCAPE INSTALLATION TO BE APPROVED BY CITY OF SCOTTSDALE INSPECTION SERVICES BEFORE CERT. OF OCCUPANCY IS ISSUED.		
<b>Case No: XX - DR - 2022</b>		

**T.J. McQUEEN & ASSOCIATES, INC.**  
LANDSCAPE ARCHITECTURE  
URBAN DESIGN  
SITE PLANNING

10450 N. 74th Street, Suite 120  
Scottsdale, Arizona 85258  
P: (602)265-0320

EMAIL: timmccqueen@tjma.net



**LGE DESIGNGROUP**

1200 N. 52nd Street • Phoenix, AZ 85008  
P: 480.966.4001

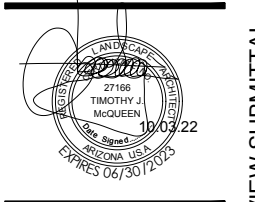
**SCOTTSDALE AND THUNDERBIRD**

SEC OF SCOTTSDALE ROAD AND THUNDERBIRD ROAD  
SCOTTSDALE, AZ

**LGE DESIGNBUILD**

NUMBER	REVISION	DATE

NUMBER	REVISION	DATE



SHEET TITLE:  
**LANDSCAPE PLAN**

ISSUE DATE: 05/13/22

DRAWN BY:

CHECKED BY:

PROJECT No.:

SHEET:

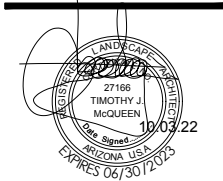
**La.01**

05/13/22 - DESIGN REVIEW SUBMITTAL

**SCOTTSDALE AND THUNDERBIRD**

SEC OF SCOTTSDALE ROAD AND THUNDERBIRD ROAD  
 SCOTTSDALE, AZ

NUMBER	REVISION	DATE



SHEET TITLE: **LANDSCAPE PLAN**

ISSUE DATE: 05/13/22

DRAWN BY: \_\_\_\_\_

CHECKED BY: \_\_\_\_\_

PROJECT No.: \_\_\_\_\_

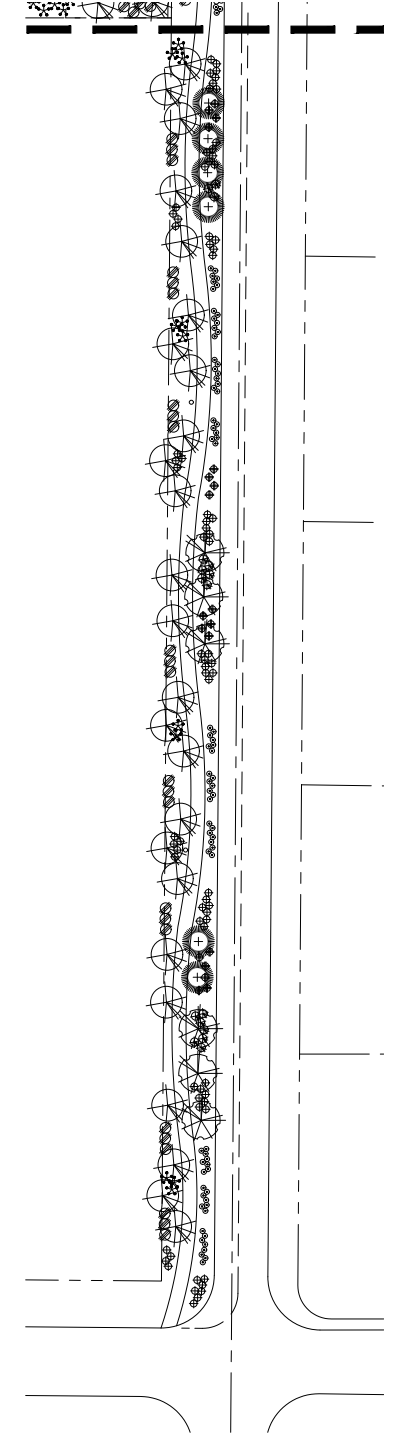
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05/13/22 - DESIGN REVIEW SUBMITTAL

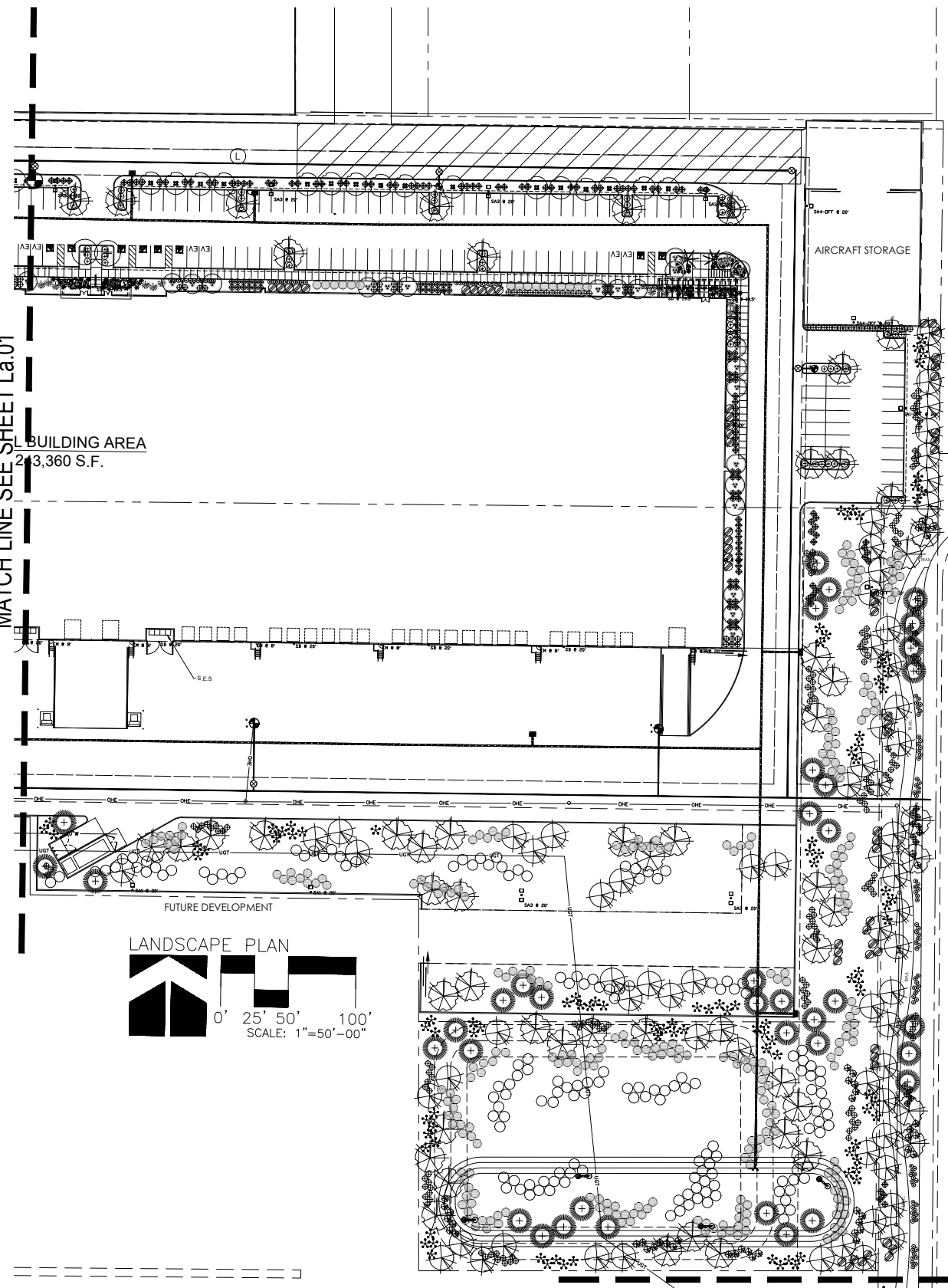
**LANDSCAPE LEGEND**

- TREES**
- CERCIDIUM 'HYBRID'  
DESERT MUSEUM PALO VERDE  
2" CALIP., 6.5T, 4.5W (82)
  - PISTACIA X 'RED-PUSH'  
RED PUSH PISTACHE  
2" CALIP., 6.5T, 4W (2)
  - PROSOPIS 'RIO-SALADO'  
HYBRID THORNLESS MESQUITE  
1.5" CALIP., 6T, 3.5W (95)
  - OLNEYA TESOTA  
IRONWOOD  
2" CALIP., 5T, 3.5W (47)
  - PHOENIX DACTYLIFERA  
DATE PALM (MATCHING)  
20' T.F. DIAMOND CUT, STRAIGHT,  
NO AERIAL ROOTS SHOWING. (10)
  - ACACIA ANEURA  
MULGA (SRP/APS APPROVED)  
2" CALIP., 5.5T, 4W (57)
  - CAESALPINIA CACALACO  
CASCALOTE  
1.5" CALIP. (MULTI) 4T, 3.5W (50)
- LARGE SHRUBS**
- TECOMA 'ORANGE JUBILEE'  
ORANGE JUBILEE  
5 GALLON (11)
  - CAESALPINIA MEXICANA  
MEXICAN BIRD OF PARADISE  
5 GALLON (119)
  - DODONEA VISCOSA  
GREEN HOP BUSH  
5 GALLON (96)
- MEDIUM SHRUBS**
- EREMOPHILA MACULATA  
VALENTINE EMU BUSH  
5 GALLON (133)
  - LEUCOPHYLLUM FRUTESCENS  
'GREEN CLOUD'  
5 GALLON (62)
- GROUND COVER**
- LANTANA MONTEVIDENSIS  
'GOLD MOUND'  
1 GALLON (194)
  - ACACIA REDOLENS  
'DESERT CARPET' 1m  
5 GALLON (158)
  - EREMOPHILA GLABRA  
'MINGENEW GOLD'  
5 GALLON (331)
- INERT MATERIALS**
- 3'x3' SURFACE SELECT  
GRANITE BOULDER (BURY 1/3)  
MINIMUM 2000lbs EACH (14)
  - 1/2" SCREENED ROCK PROS MAHOGANY  
DECOMPOSED GRANITE  
2" DEPTH IN ALL LANDSCAPE AREAS

MATCH LINE SEE BELOW RIGHT



MATCH LINE SEE SHEET La.01

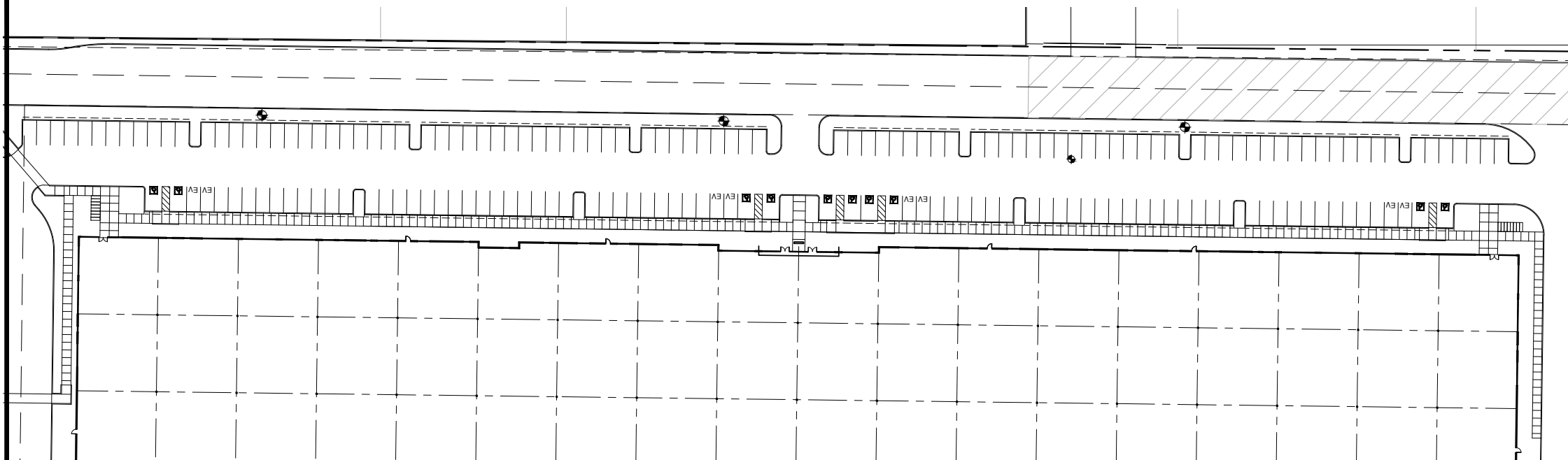


MATCH LINE SEE ABOVE LEFT

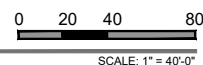
T.J. McQUEEN & ASSOCIATES, INC.  
 LANDSCAPE ARCHITECTURE  
 URBAN DESIGN  
 SITE PLANNING  
 10450 N. 74th Street, Suite 120  
 Scottsdale, Arizona 85258  
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 EMAIL: timmcqueen@tjma.net



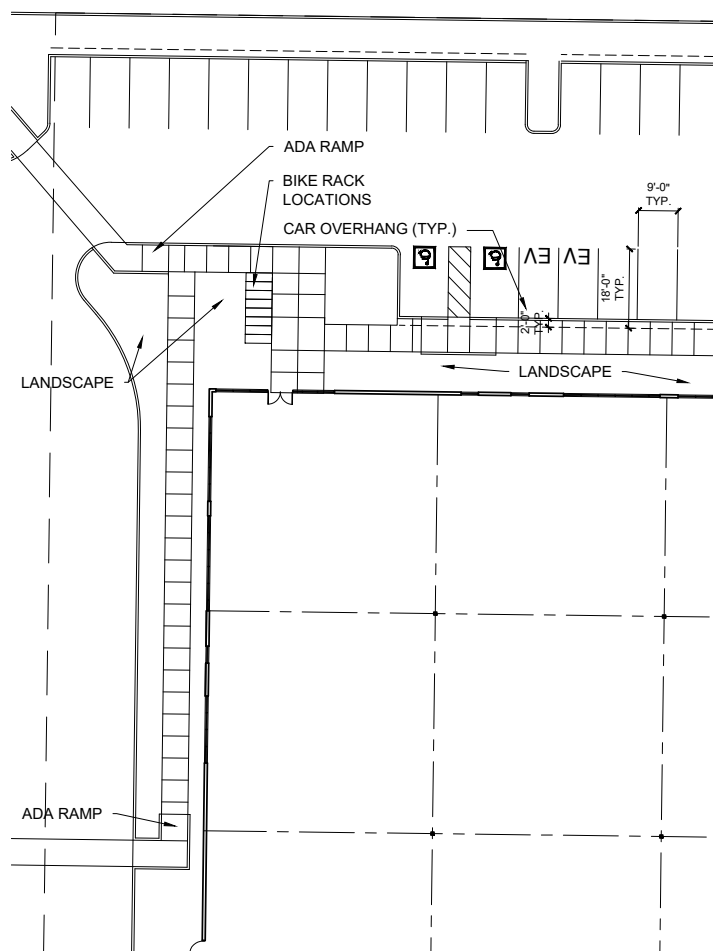
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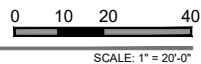
**HARDSCAPE PLAN - OVERALL**



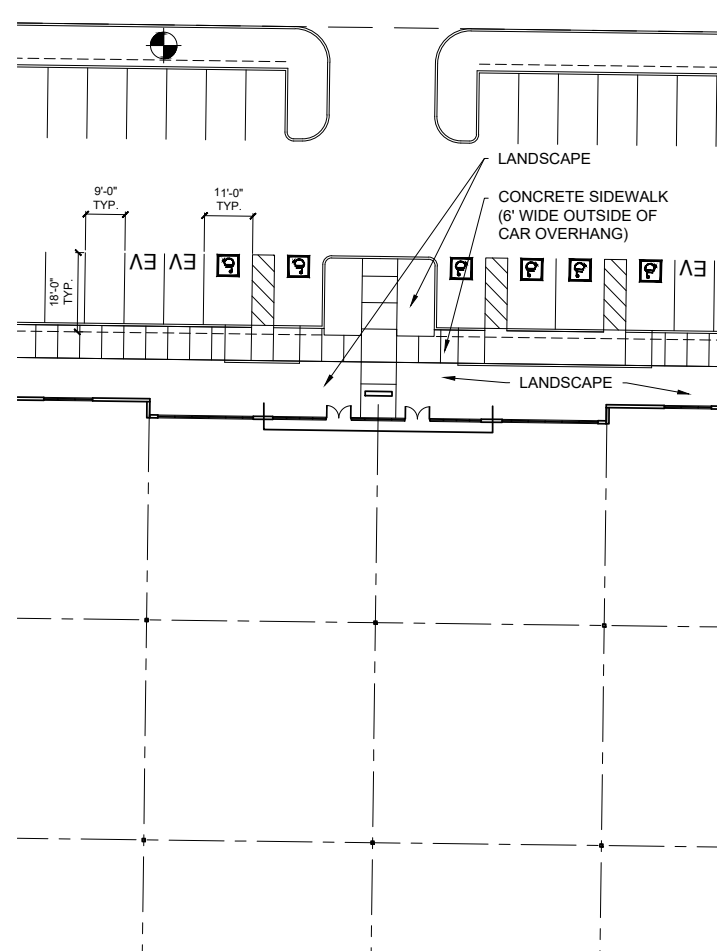
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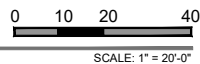
**HARDSCAPE PLAN - WEST ENTRY**



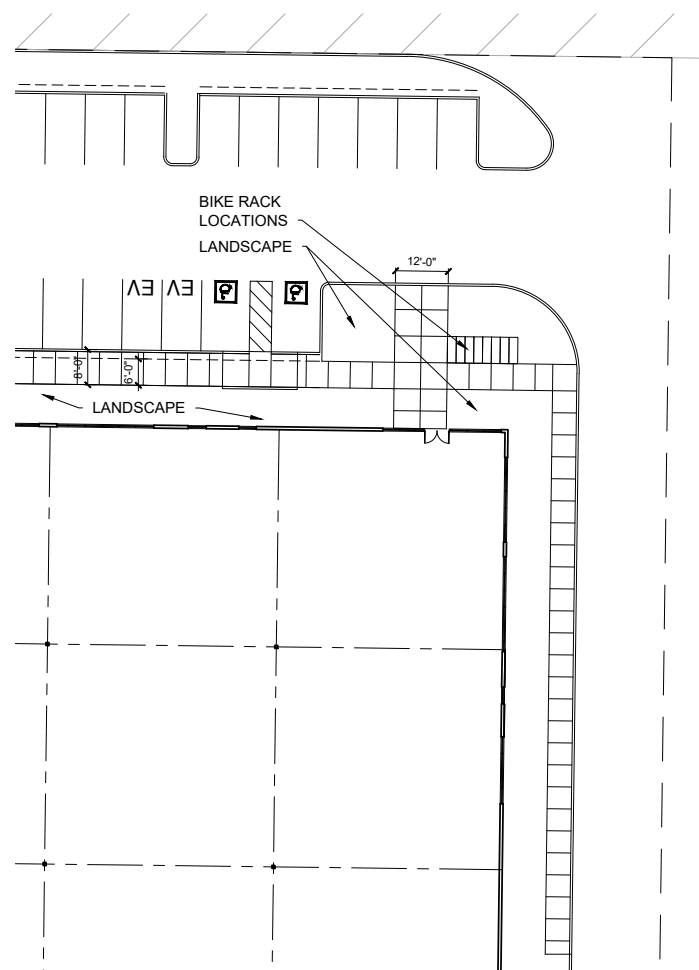
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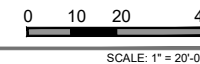
**HARDSCAPE PLAN - CENTER ENTRY**



SCALE: 1" = 20'-0"



**HARDSCAPE PLAN - EAST ENTRY**



SCALE: 1" = 20'-0"

**PROJECT DATA:**

PROJECT NAME: SCOTTSDALE AND THUNDERBIRD  
 ZONING: PCP / AV  
 NET LOT AREA: +/- 783,924 S.F. (18.00 ACRES)

**SCOTTSDALE AND THUNDERBIRD**

SEC OF SCOTTSDALE ROAD AND THUNDERBIRD ROAD  
 SCOTTSDALE, AZ



NUMBER REVISION DATE

**PRELIMINARY NOT FOR CONSTRUCTION**

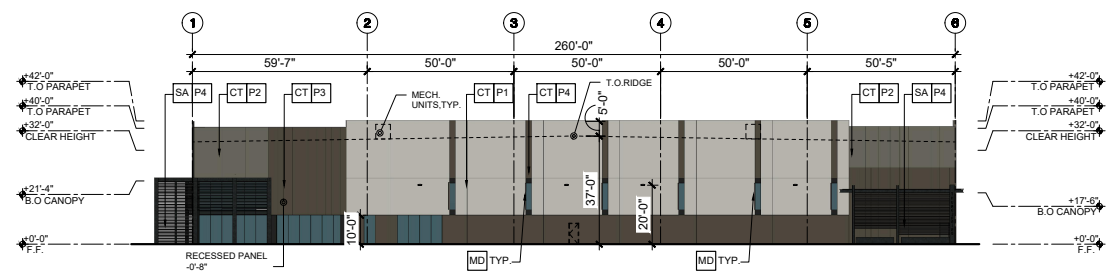
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 ISSUE DATE: 10/07/22  
 DRAWN BY:  
 CHECKED BY:  
 PROJECT No.:

SHEET: **A1.15**

**LGE DESIGNGROUP**  
 1200 N. 52nd Street ■ Phoenix, AZ ■ 85008  
 P: 480.966.4001

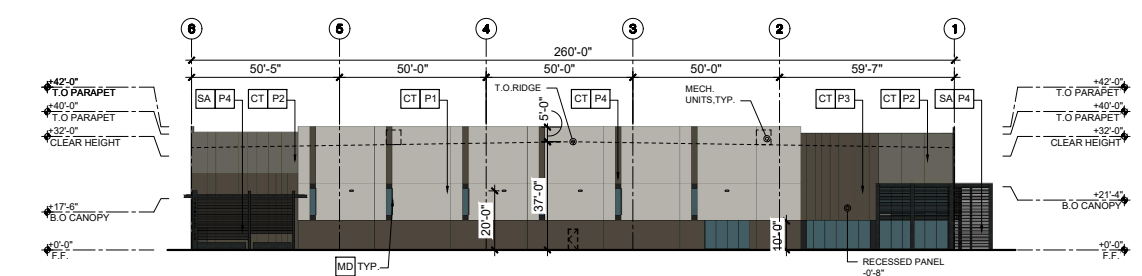
10/07/22 - DESIGN REVIEW RESUBMITTAL - 14-DR-2022

14-DR-2022  
 11/4/22



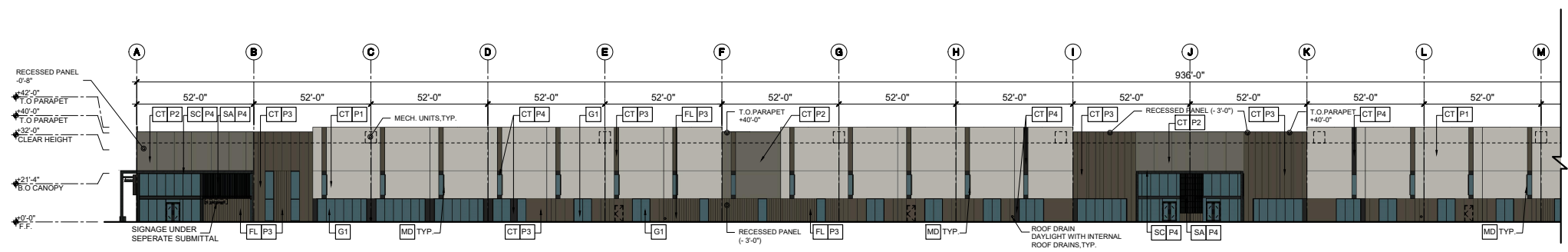
WEST ELEVATION

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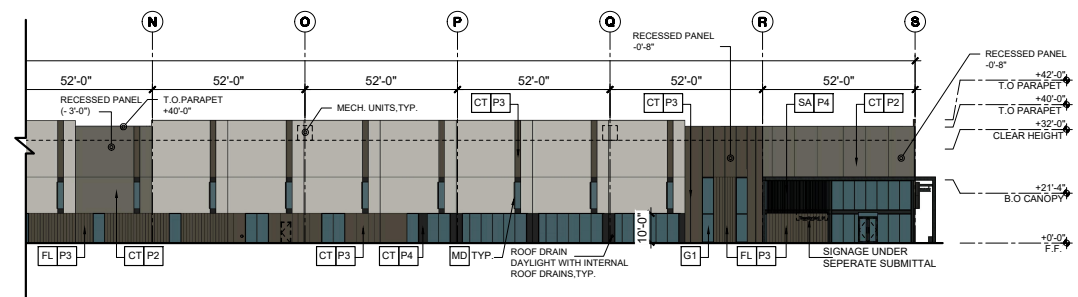
EAST ELEVATION

SCALE: 1" = 30'-0"



NORTH ELEVATION

SCALE: 1" = 30'-0"



NORTH ELEVATION (CONT.)

SCALE: 1" = 30'-0"

**MATERIAL/FINISH SCHEDULE:**

- KEY: DESCRIPTION:
- EXTERIOR WALLS:
- CT CONCRETE TILT PANEL W/ VERTICAL 1/2" X 1/2" DEEP REVEALS & 1/2" "V" SCORE LINES- PAINTED. REFER TO LAYOUT FOR THICKNESS INFORMATION
  - SA STEEL ANGLE SCREEN-2"X3" STEEL ANGLES 9" O.C. ATTACHED TO TUBE STEEL BEAMS PAINTED P4
  - MD METAL B-DECK OVER STEEL FRAMING

**CANOPY KEY:**

- KEY: DESCRIPTION:
- SC METAL CANOPY- STEEL FASCIA BEAMS WITH OPEN TRELIS STEEL ANGLE INFILL.

**GLAZING KEY:**

- MATERIAL: DESCRIPTION:
- FRAMES: ALUMINUM STOREFRONT - 4 1/2" (SEALANT JOINT VERTS) DARK BRONZE ANODIZED (AB-7)

EXTERIOR GLAZING

- G1 SOLARBAN 80 1" INSULATED GLAZING LOW-E SOLARCOOL SOLARBLUE

**PAINT KEY:**

- KEY: DESCRIPTION:
- P1 DUNN EDWARDS (BEIGE) 'FOGGY DAY' # DE6226 (LRV 71)
  - P2 DUNN EDWARDS (MEDIUM DROWN) 'METAL FRINGE' # DET626 (LRV 21)
  - P3 DUNN EDWARDS (BROWN) 'DOWNING EARTH' # DET634 (LRV 11)
  - P4 DUNN EDWARDS (BLACK) 'BLACK BEAN' # DE6385 (LRV 7)

**SCOTTSDALE AND THUNDERBIRD**

SEC OF SCOTTSDALE ROAD AND THUNDERBIRD ROAD  
SCOTTSDALE, AZ



NUMBER	REVISION	DATE

SHEET TITLE:  
EXTERIOR ELEVATIONS

ISSUE DATE: 10/07/22

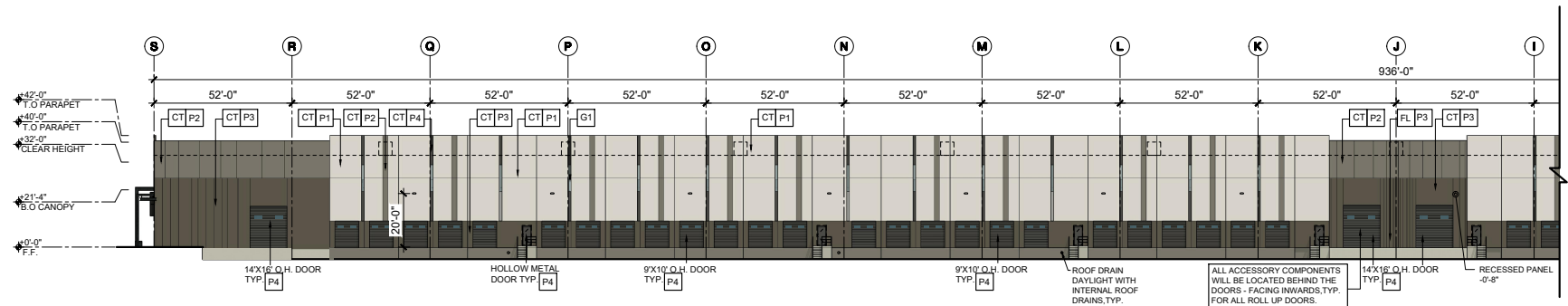
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PROJECT No.:

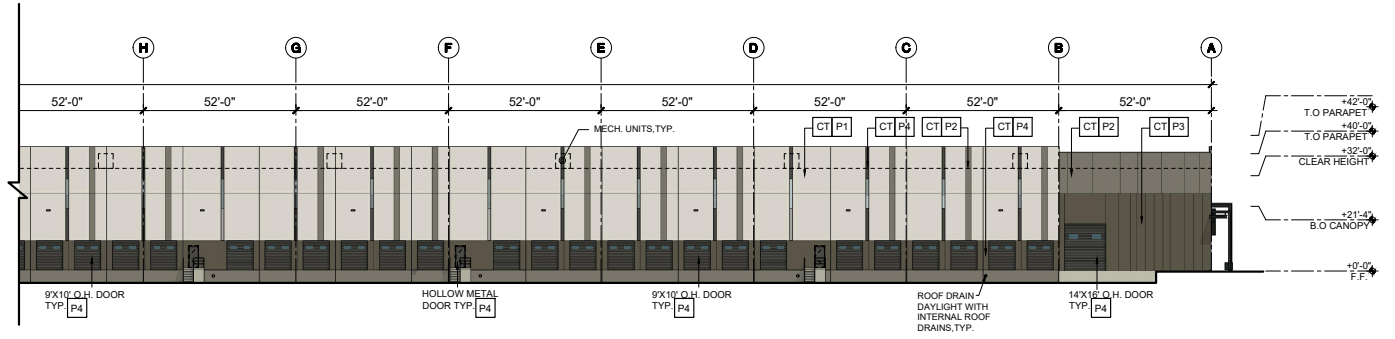
SHEET:

**A4.1.1**



**SOUTH ELEVATION**

SCALE: 1" = 30'-0"



**SOUTH ELEVATION (CONT.)**

SCALE: 1" = 30'-0"

**MATERIAL/FINISH SCHEDULE:**

- KEY: DESCRIPTION:
- EXTERIOR WALLS:
- [CT] CONCRETE TILT PANEL W/ VERTICAL 1/2" X 1/2" DEEP REVEALS & 1/2" "J" SCORE LINES- PAINTED. REFER TO LAYOUT FOR THICKNESS INFORMATION
  - [SA] STEEL ANGLE SCREEN-2"x3" STEEL ANGLES 9" O.C. ATTACHED TO TUBE STEEL BEAMS PAINTED P4
  - [MD] METAL B-DECK OVER STEEL FRAMING
- CANOPY KEY:**
- KEY: DESCRIPTION:
- [SC] METAL CANOPY- STEEL FASCIA BEAMS WITH OPEN TRELIS STEEL ANGLE INFILL.

**GLAZING KEY:**

- MATERIAL: DESCRIPTION:
- FRAMES: ALUMINUM STOREFRONT - 4 1/2" (SEALANT JOINT VERTS) DARK BRONZE ANODIZED (AB-7)
- EXTERIOR GLAZING:
- [G1] SOLARBAN 80 1" INSULATED GLAZING LOW-E SOLARCOOL SOLARBLUE

**PAINT KEY:**

- KEY: DESCRIPTION:
- [P1] DUNN EDWARDS (BEIGE) 'FOGGY DAY' # DE6226 (LRV 71)
  - [P2] DUNN EDWARDS (MEDIUM DROWN) 'METAL FRINGE' # DET626 (LRV 21)
  - [P3] DUNN EDWARDS (BROWN) 'DOWNING EARTH' # DET634 (LRV 11)
  - [P4] DUNN EDWARDS (BLACK) 'BLACK BEAN' # DE6385 (LRV 7)

**LGE DESIGNGROUP**

1200 N. 52nd Street ■ Phoenix, AZ ■ 85008  
P: 480.966.4001

**SCOTTSDALE AND THUNDERBIRD**

SEC OF SCOTTSDALE ROAD AND THUNDERBIRD ROAD  
SCOTTSDALE, AZ

**LGE DESIGNBUILD**

NUMBER	REVISION	DATE

SHEET TITLE:  
EXTERIOR ELEVATIONS

ISSUE DATE: 10/07/22

DRAWN BY:

CHECKED BY:

PROJECT No.:

SHEET:

**A4.1.2**



Scottsdale and Thunderbird

Scottsdale, Arizona  
2022 . 10 . 07

**LGE** | DESIGNGROUP

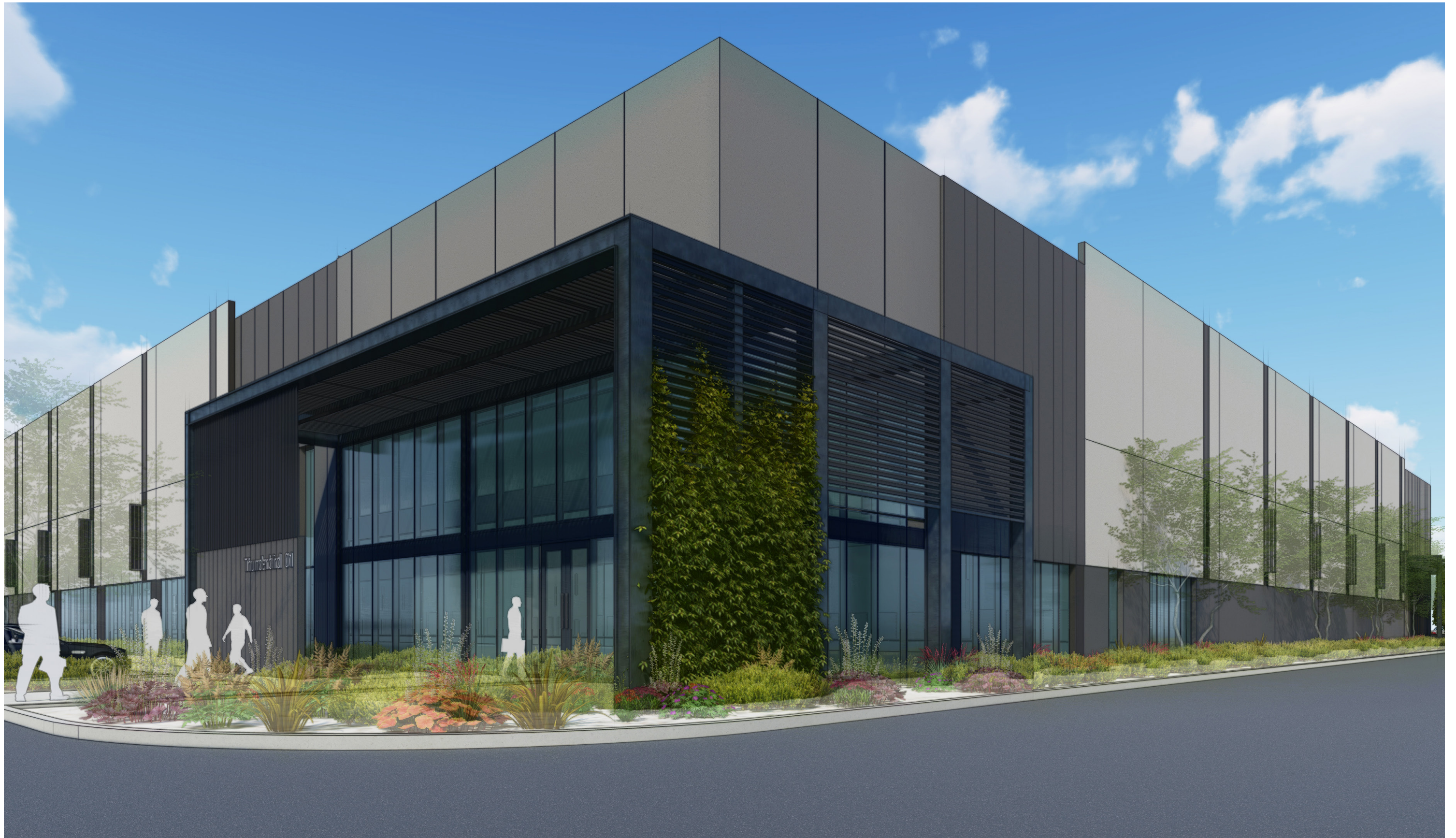
CONCEPT VIEW - NORTH ELEVATION

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ATTACHMENT #12

**LGE** | DESIGNBUILD



Scottsdale and Thunderbird

Scottsdale, Arizona  
2022 . 10 . 07

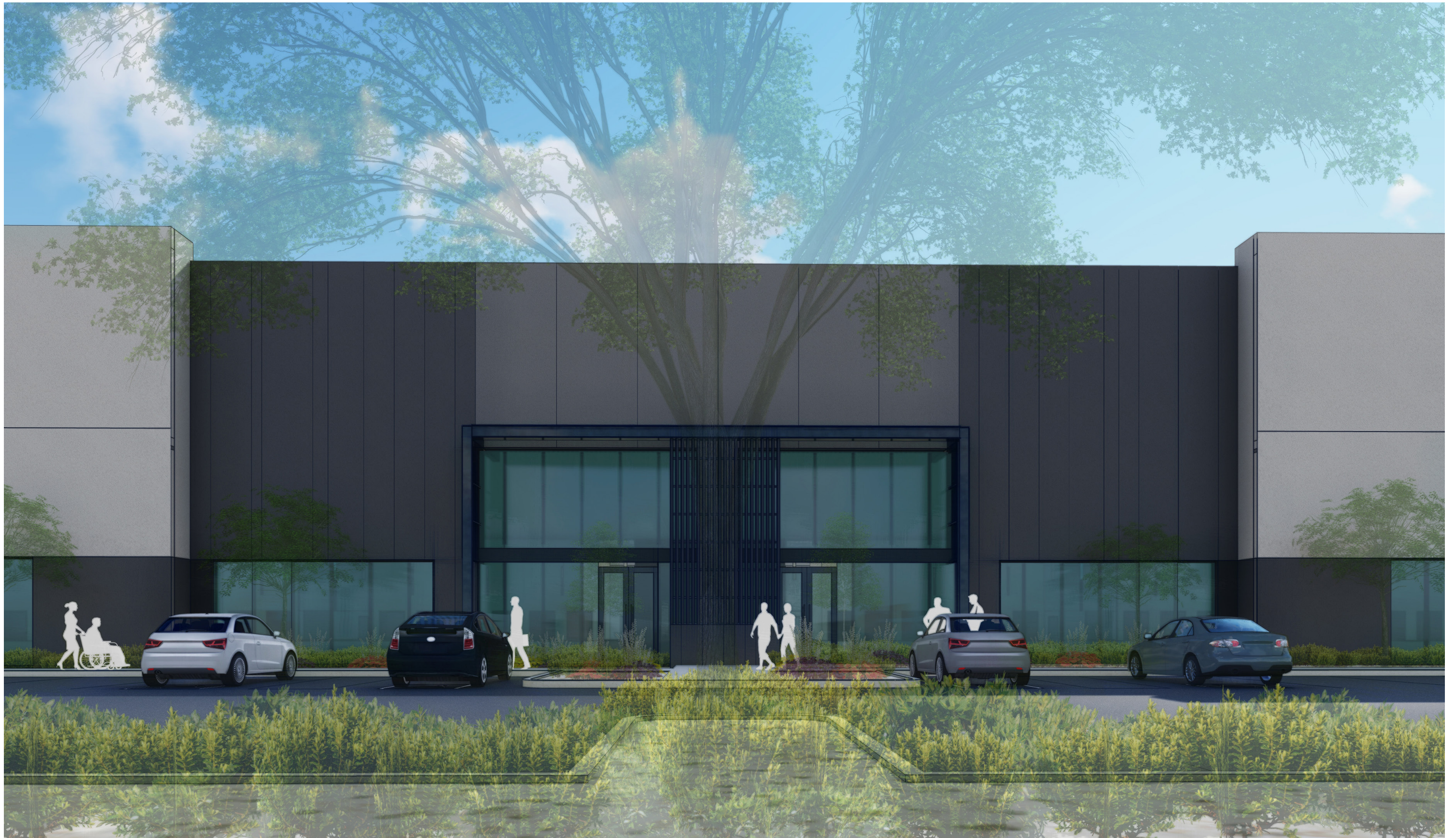
**LGE** | DESIGNGROUP

CONCEPT VIEW - NWC CORNER VIEW

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**LGE** | DESIGNBUILD



Scottsdale and Thunderbird

Scottsdale, Arizona  
2022 . 10 . 07

**LGE** | DESIGNGROUP

CONCEPT VIEW - TENANT ENTRY, NORTH ELEVATION

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**LGE** | DESIGNBUILD



Scottsdale and Thunderbird

Scottsdale, Arizona  
2022 . 10 . 07

**LGE** | DESIGNGROUP

CONCEPT VIEW - SW CORNER

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**LGE** | DESIGNBUILD



Scottsdale and Thunderbird

Scottsdale, Arizona  
2022 . 10 . 07

**LGE** | DESIGNGROUP

CONCEPT VIEW - NW CORNER

This artist rendering is for conceptual design only and should not be referred to as a construction document.

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**LGE** | DESIGNBUILD



P1 - Foggy Day  
DE6226 LRV 71  
Dunn Edwards



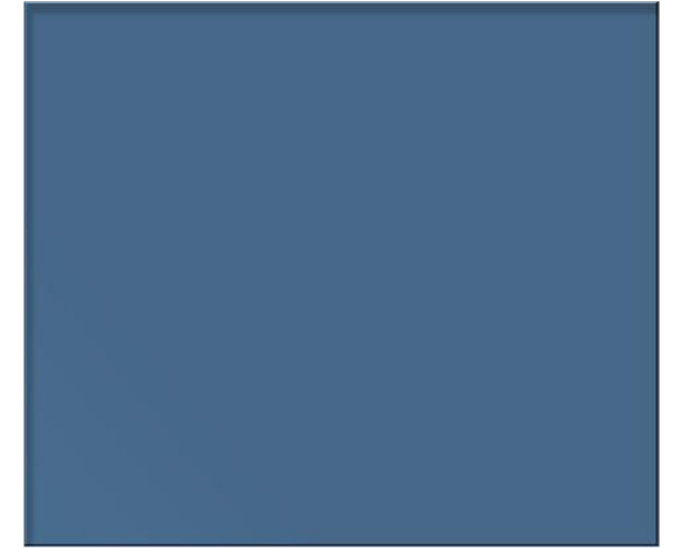
P2 - Metal Fringe  
DET626 LRV 21  
Dunn Edwards



P3 - Downing Earth  
DET634LRV 10  
Dunn Edwards



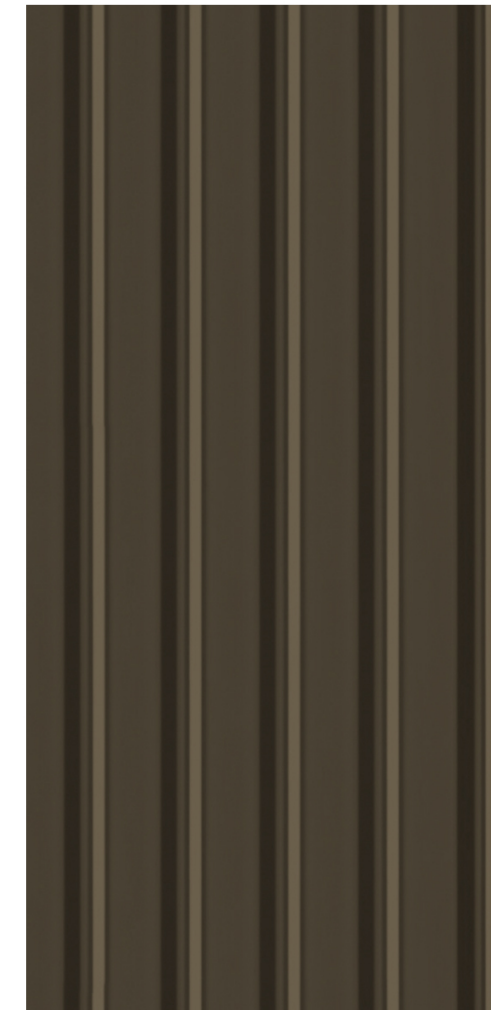
P4 - Black Bean  
DE6385 LRV 7  
Dunn Edwards



Glass - Solarcool Solarblue Annealed



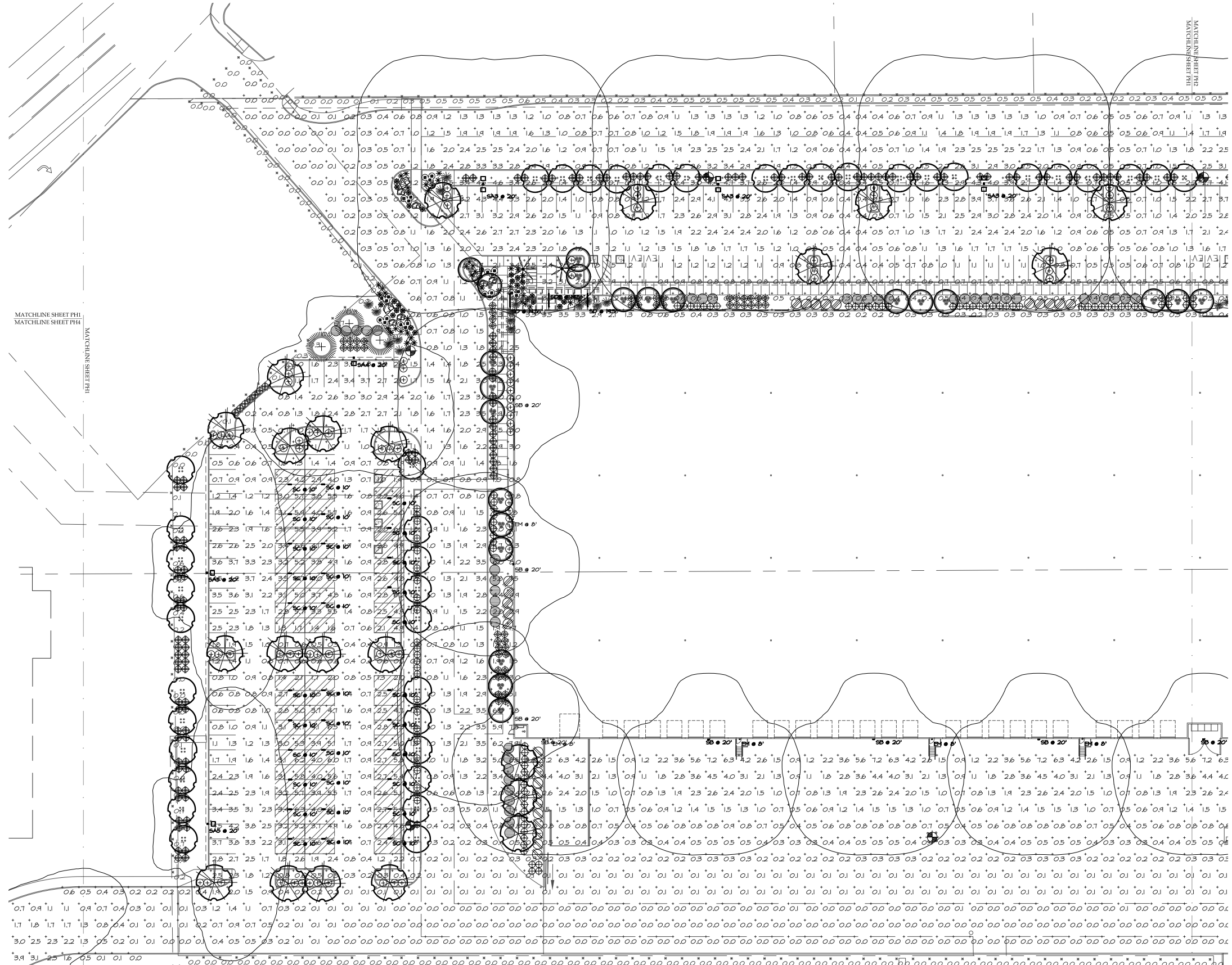
Rendering View



MD - METAL B-DECK



Anodized Aluminum  
Storefront Dark Bronze (AB-7) Arcadia



MATCHLINE SHEET PH1  
MATCHLINE SHEET PH4

HELLERBES INTRODUCTION

MATCHLINE SHEET PH1  
MATCHLINE SHEET PH4

HELLERBES INTRODUCTION

**NOTE:**  
THE PRE-CURFEW LIGHTING DESIGN HOURS SHALL BE DEFINED AS DUSK TO 10:00 PM, AND THE POST-CURFEW LIGHTING DESIGN HOURS SHALL BE DEFINED AS 10:00 PM TO DAWN. ALL EXTERIOR LIGHTS SHALL BE TURNED OFF DURING POST-CURFEW WITH THE EXCEPTION OF LIGHTS FOR SECURITY PURPOSES.  
A PROGRAMMABLE TIMER, AND PHOTOCELLS SHALL CONTROL THE PRE- AND POST-CURFEW LIGHTS; PHOTOCELLS SHALL BE MOUNTED ON THE NORTH SIDE OF BLDG. THE PROGRAMMABLE TIMER MAY CONTAIN A MAXIMUM OF 1-HOUR OVER RIDE WITH AN AUTOMATIC TURN OFF FOR AFTER HOURS, AND SPECIAL EVENTS USE ONLY.

**PHOTOMETRIC (PRE-CURFEW) SITE PLAN - WEST**

1"=30'-0"



**ATTACHMENT #14**

**TONY WOO ENGINEERING, INC.**  
1 W. DEER VALLEY RD.  
SUITE 203  
PHOENIX, AZ 85027  
TEL (602) 279-8092  
JOB # 622011  
CONTACT: GREG HAMPTON  
GREG@TWE.PHXCOXMAIL.COM.

**MANUFACTURERS SHOP DRAWING FOR REFERENCE ONLY**

SHEET TITLE:  
PHOTOMETRIC (PRE-CURFEW) SITE PLAN - WEST  
ISSUE DATE: 05/17/22  
DRAWN BY:  
CHECKED BY:  
PROJECT No.:  
SHEET:

**PH1**

**SCOTTSDALE AND THUNDERBIRD**

SEC OF SCOTTSDALE ROAD AND THUNDERBIRD ROAD  
SCOTTSDALE, AZ

**LGE DESIGNBUILD**

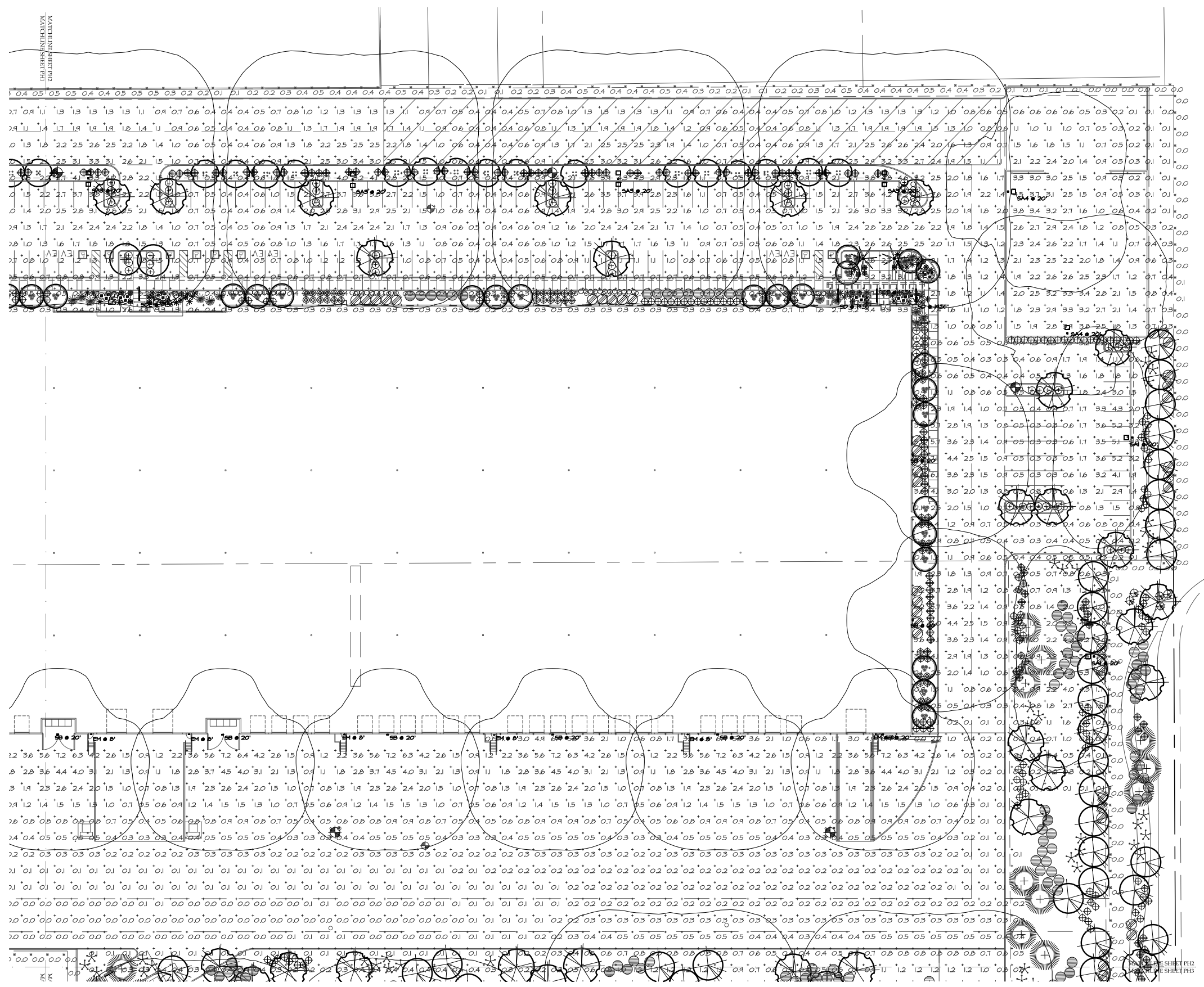
NUMBER	REVISION	DATE

**LGE DESIGNGROUP**

1200 N. 52nd Street • Phoenix, AZ • 85008  
P: 480.966.4001

05/17/22 - DESIGN REVIEW SUBMITTAL

14-DR-2022  
11/4/22



**PHOTOMETRIC (PRE-CURFEW) SITE PLAN - EAST**

1"=30'-0"



**TONY WOO ENGINEERING, INC.**  
 1 W. DEER VALLEY RD.  
 SUITE 203  
 PHOENIX, AZ 85027  
 TEL (602) 279-8092

JOB # 622011  
 CONTACT: GREG HAMPTON  
 GREG@TWE.PHXCOXMAIL.COM.

SHEET TITLE:  
 PHOTOMETRIC (PRE-CURFEW) SITE  
 PLAN - EAST  
 ISSUE DATE: 05/17/22  
 DRAWN BY:  
 CHECKED BY:  
 PROJECT No.:

**MANUFACTURERS  
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 FOR REFERENCE  
 ONLY**

**LGE DESIGNBUILD**

NUMBER	REVISION	DATE

**SCOTTSDALE AND THUNDERBIRD**

SEC OF SCOTTSDALE ROAD AND THUNDERBIRD ROAD  
 SCOTTSDALE, AZ

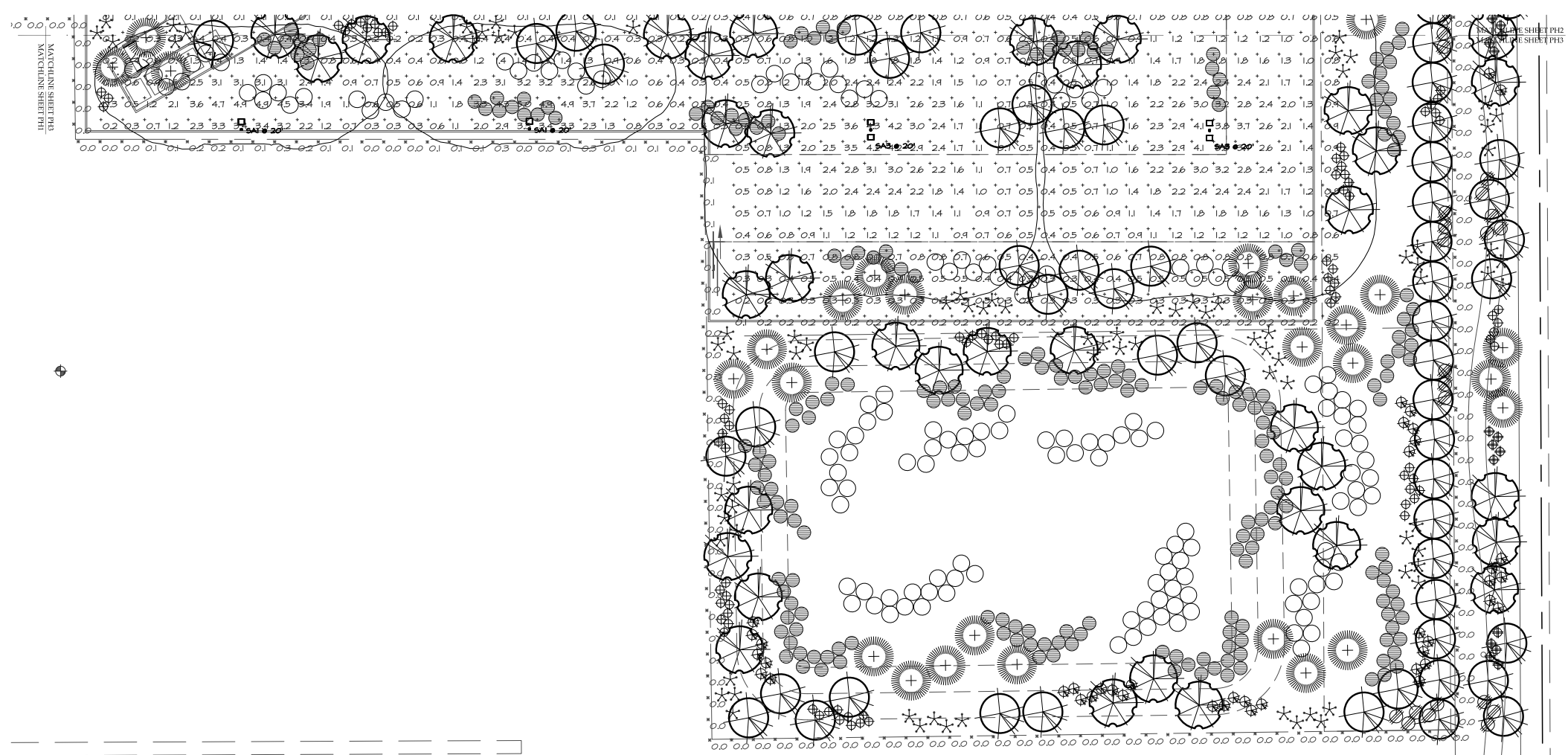
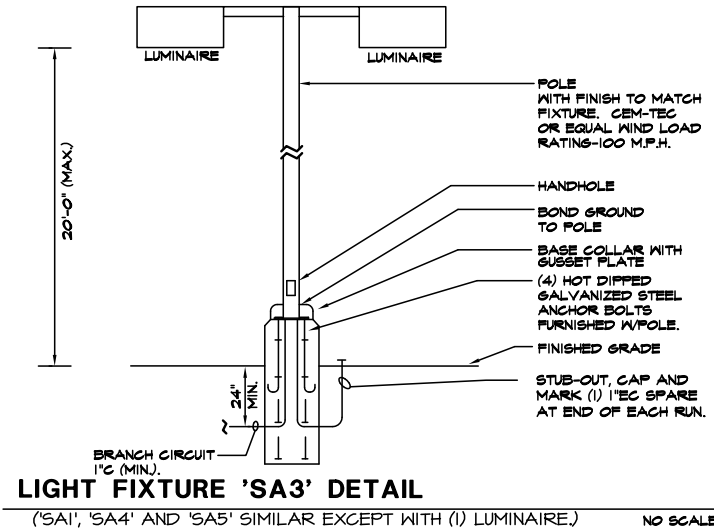
**LGE DESIGNGROUP**

1200 N. 52nd Street • Phoenix, AZ • 85008  
 P: 480.966.4001

05/17/22 - DESIGN REVIEW SUBMITTAL

Statistics						
Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
SITE - FC @ GRADE	+	1.3 fc	8.0 fc	0.0 fc	N/A	N/A
PROPERTY LINE - FC @ 0' AFG	X	0.1 fc	0.7 fc	0.0 fc	N/A	N/A

Schedule										
Symbol	Label	Quantity	Manufacturer	Catalog Number	Description	Lamp	Filename	Lumens Per Lamp	Light Loss Factor	Wattage
EM	EM	11	Lithonia Lighting	AFF OEL (FINISH) UVOLT LTP SDRT FCT	AFF premium FCT (Forward Throw)	LED	AFF_FCT.lvs	451	0	5.48
SA1	SA1	4	Lithonia Lighting	DSX1 LED P5 30K BLC MVOLT SPA (FINISH) / SSS 17.5' WITH 2.5' BASE	DSX1 LED P5 30K BLC MVOLT	3000K LED	DSX1_LED_P5_30K_BLC_MVOLT.lvs	12047	0.91	138
SA3	SA3	9	Lithonia Lighting	(2) DSX1 LED P5 30K T5W MVOLT SPA (FINISH) / SSS 17.5' WITH 2.5' BASE	TWINHEAD DSX1 LED P5 30K T5W MVOLT	3000K LED	DSX1_LED_P5_30K_T5_W_MVOLT.lvs	15156	0.91	276
SA4	SA4	3	Lithonia Lighting	DSX1 LED P5 30K T2M MVOLT SPA (FINISH) / SSS 17.5' WITH 2.5' BASE	DSX1 LED P5 30K T2M MVOLT	3000K LED	DSX1_LED_P5_30K_T2M_MVOLT.lvs	14694	0.91	138
SA5	SA5	5	Lithonia Lighting	DSX1 LED P5 30K T2M MVOLT SPA (FINISH) / SSS 17.5' WITH 2.5' BASE	DSX1 LED P5 30K T2M MVOLT	3000K LED	DSX1_LED_P5_30K_T2M_MVOLT.lvs	14663	0.91	138
SB	SB	15	Lithonia Lighting	WDGE4 LED P3 30K 80CRI RFT MVOLT SRM (FINISH)	WDGE4 LED WITH P3 - PERFORMANCE PACKAGE, 3000K, 70CRI, FORWARD THROW OPTIC	3000K LED	WDGE4_LED_P3_70CRI_LRFT_30K.lvs	18889	0.91	124.86
SC	SC	33	Lithonia Lighting	DMW2 2000LM WD AFL MVOLT GZ10 30K 80CRI	DMW2 2000LM WD AFL MVOLT GZ10 30K 80CRI (GLEDS)	3000K LED	DMW2_2000LM_WD_AFL_MVOLT_GZ10_30K_80CRI.lvs	2100	0.91	18.78
SD	SD	4	Pinnacle Architectural Lighting	EX3 WET N 830HO 8' (POSITION) S U FSD 1 0' (FINISH)	8' SURFACE MOUNTED LINEAR-HIGH OUTPUT	3000K LED	EX3-WET-N-830HO-4.lvs	22	0.91	69.2
SDE	SDE	3	Pinnacle Architectural Lighting	EX3 WET N 830HO 8' (POSITION) S U FSD 1 4' (FINISH)	8' SURFACE MOUNTED LINEAR-HIGH OUTPUT WIEM BATTERY PACK	3000K LED	EX3-WET-N-830HO-4.lvs	22	0.91	69.2



PHOTOMETRIC (PRE-CURFEW) SITE PLAN - SOUTH EAST

**TONY WOO ENGINEERING, INC.**  
 1 W. DEER VALLEY RD.  
 SUITE 203  
 PHOENIX, AZ 85027  
 TEL (602) 279-8092

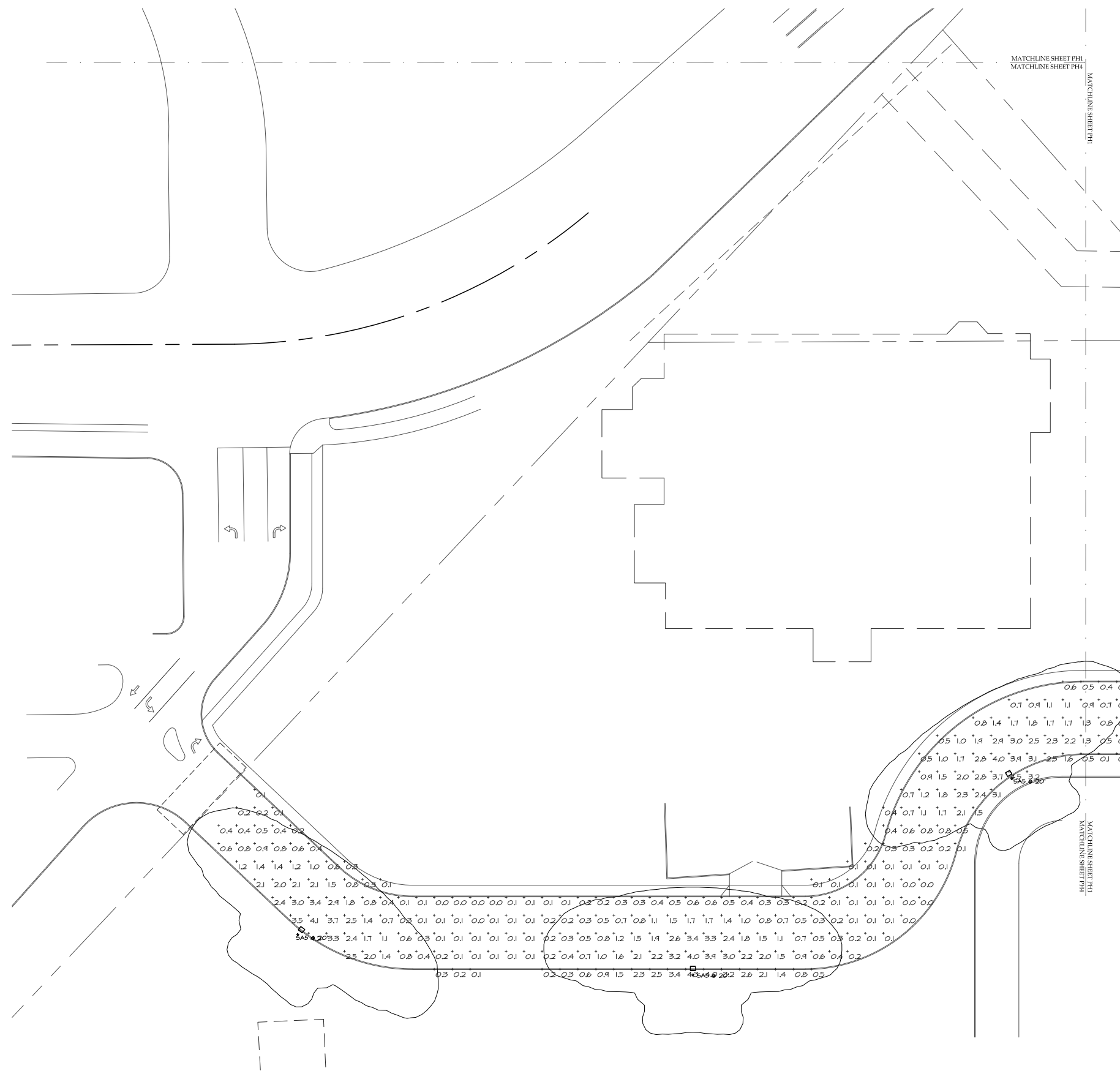
JOB # 622011  
 CONTACT: GREG HAMPTON  
 GREG@TWE.PHX.COXMAIL.COM.

**MANUFACTURERS SHOP DRAWING FOR REFERENCE ONLY**

SHEET TITLE:  
 PHOTOMETRIC (PRE-CURFEW) SITE PLAN - SOUTH EAST  
 ISSUE DATE: 05/17/22

DRAWN BY:  
 CHECKED BY:  
 PROJECT No.:

SHEET: **PH3**



**PHOTOMETRIC (PRE-CURFEW) SITE PLAN - SOUTH WEST**

1"=30'-0"



**TONY WOO ENGINEERING, INC.**  
 1 W. DEER VALLEY RD.  
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JOB # 622011

CONTACT: GREG HAMPTON  
 GREG@TWE.PHX.COXMAIL.COM.

**LGE DESIGNGROUP**  
 1200 N. 52nd Street • Phoenix, AZ • 85008  
 P: 480.966.4001

**SCOTTSDALE AND THUNDERBIRD**

SEC OF SCOTTSDALE ROAD AND THUNDERBIRD ROAD  
 SCOTTSDALE, AZ

**LGE DESIGNBUILD**

NUMBER	REVISION	DATE

**MANUFACTURER'S  
 SHOP DRAWING  
 FOR REFERENCE  
 ONLY**

SHEET TITLE:  
 PHOTOMETRIC (PRE-CURFEW) SITE  
 PLAN - SOUTH WEST

ISSUE DATE: 05/17/22

DRAWN BY:

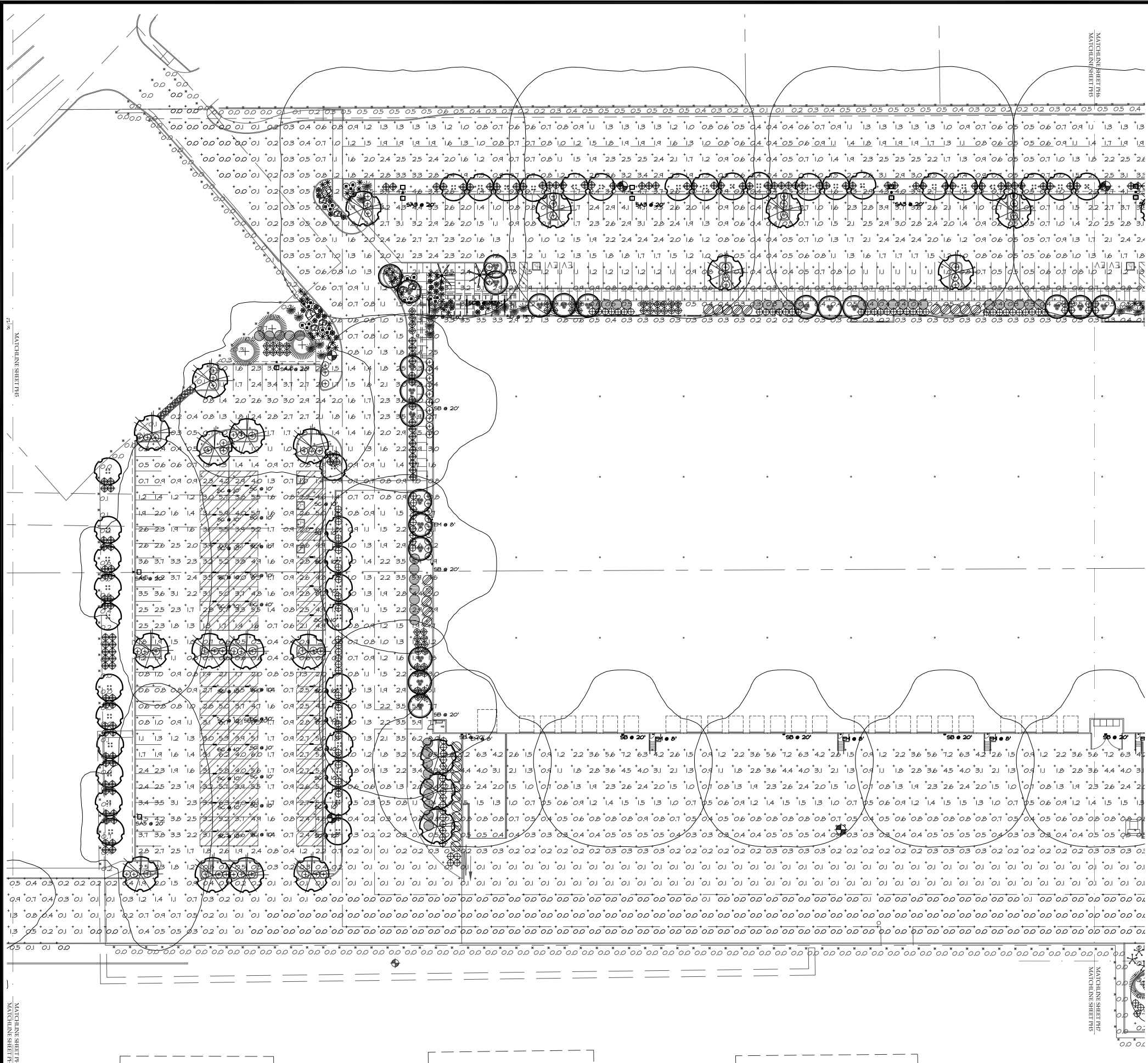
CHECKED BY:

PROJECT No.:

SHEET:

**PH4**

05/17/22 - DESIGN REVIEW SUBMITTAL



**PHOTOMETRIC (POST-CURFEW) SITE PLAN - WEST**

1"=30'-0"



**SCOTTSDALE AND THUNDERBIRD**

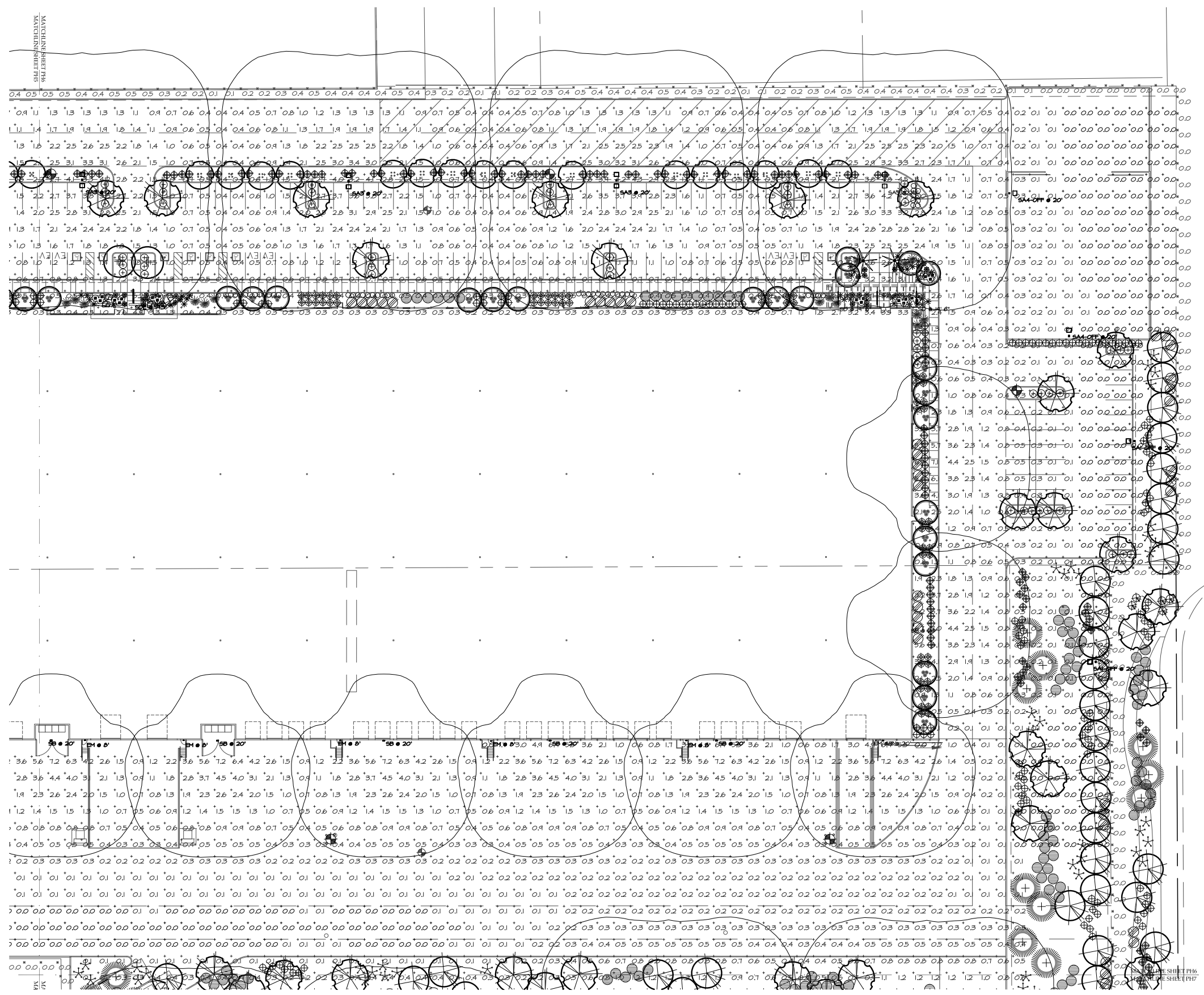
SEC OF SCOTTSDALE ROAD AND THUNDERBIRD ROAD  
 SCOTTSDALE, AZ

NUMBER	REVISION	DATE

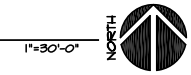
**MANUFACTURERS  
 SHOP DRAWING  
 FOR REFERENCE  
 ONLY**

SHEET TITLE:	PHOTOMETRIC (POST-CURFEW) SITE PLAN - WEST
ISSUE DATE:	05/17/22
DRAWN BY:	
CHECKED BY:	
PROJECT No.:	
SHEET:	<b>PH5</b>

**TONY WOO ENGINEERING, INC.**  
 1 W. DEER VALLEY RD.  
 SUITE 203  
 PHOENIX, AZ 85027  
 TEL (602) 279-8092  
 JOB # 622011  
 CONTACT: GREG HAMPTON  
 GREG@TWE.PHXCOXMAIL.COM.



**PHOTOMETRIC (POST-CURFEW) SITE PLAN - EAST**



**TONY WOO ENGINEERING, INC.**  
 1 W. DEER VALLEY RD.  
 SUITE 203  
 PHOENIX, AZ 85027  
 TEL (602) 279-8092

JOB # 622011  
 CONTACT: GREG HAMPTON  
 GREG@TWE.PHX.COM

**LGE DESIGNGROUP**  
 1200 N. 52nd Street • Phoenix, AZ • 85008  
 P: 480.966.4001

**SCOTTSDALE AND THUNDERBIRD**  
 SEC OF SCOTTSDALE ROAD AND THUNDERBIRD ROAD  
 SCOTTSDALE, AZ

**LGE DESIGNBUILD**

NUMBER	REVISION	DATE

**MANUFACTURERS SHOP DRAWING FOR REFERENCE ONLY**

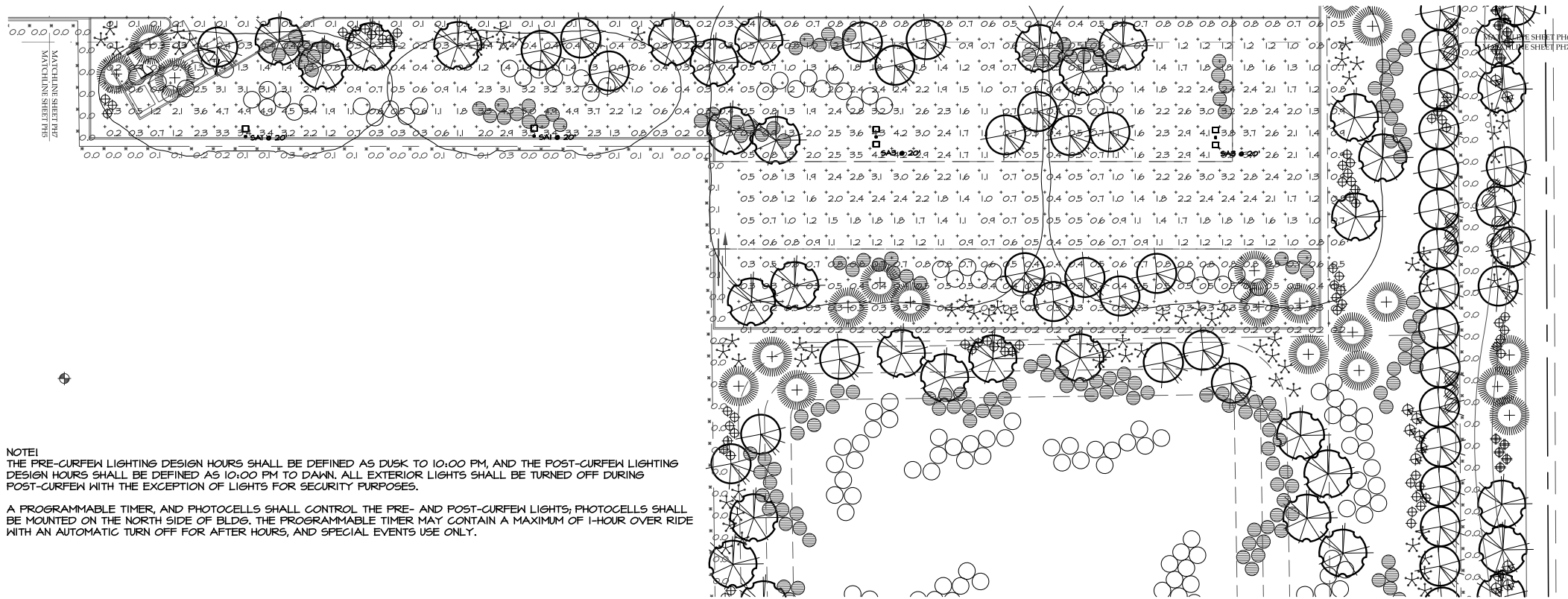
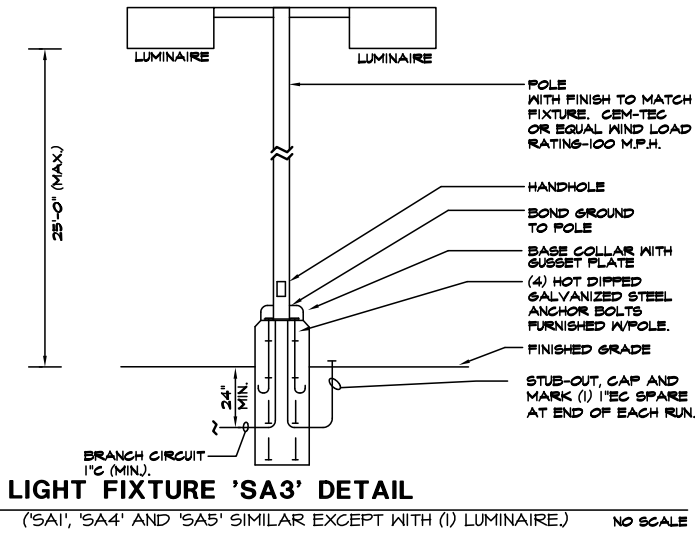
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 PHOTOMETRIC (POST-CURFEW)  
 SITE PLAN - EAST  
 ISSUE DATE: 05/17/22  
 DRAWN BY:  
 CHECKED BY:  
 PROJECT No.:

SHEET: **PH6**

05/17/22 - DESIGN REVIEW SUBMITTAL

Statistics						
Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
SITE - FC @ GRADE	+	1.2 fc	8.0 fc	0.0 fc	N/A	N/A
PROPERTY LINE - FC @ 5' AFG	X	0.1 fc	0.7 fc	0.0 fc	N/A	N/A

Schedule										
Symbol	Label	Quantity	Manufacturer	Catalog Number	Description	Lamp	Filename	Lumens Per Lamp	Light Loss Factor	Wattage
☒	EM	11	Lithonia Lighting	AFF OEL (FINISH) UVOLT LTP SORT FCT	AFF premium FCT (Forward Throw)	LED	AFF_FCT.ies	451	0	5.48
☐	SA1	2	Lithonia Lighting	DSX1 LED P5 30K BLC MVOLT SPA (FINISH) / SSS 17.5" WITH 2.5" BASE	DSX1 LED P5 30K BLC MVOLT	3000K LED	DSX1_LED_P5_30K_BLC_MVOLT.ies	12047	0.91	138
☐	SA1-OFF	2	Lithonia Lighting	DSX1 LED P5 30K BLC MVOLT SPA (FINISH) / SSS 17.5" WITH 2.5" BASE	DSX1 LED P5 30K BLC MVOLT TURNED OFF FOR POST-CURFEW	3000K LED	DSX1_LED_P5_30K_BLC_MVOLT.ies	12047	0	138
☐	SA3	9	Lithonia Lighting	(2) DSX1 LED P5 30K TSW MVOLT SPA (FINISH) / SSS 17.5" WITH 2.5" BASE	TWIN-HEAD DSX1 LED P5 30K TSW MVOLT	3000K LED	DSX1_LED_P5_30K_TSW_MVOLT.ies	15156	0.91	276
☐	SA4	1	Lithonia Lighting	DSX1 LED P5 30K TTFM MVOLT SPA (FINISH) / SSS 17.5" WITH 2.5" BASE	DSX1 LED P5 30K TTFM MVOLT	3000K LED	DSX1_LED_P5_30K_TTFM_MVOLT.ies	14694	0.91	138
☐	SA4-OFF	2	Lithonia Lighting	DSX1 LED P5 30K TTFM MVOLT SPA (FINISH) / SSS 17.5" WITH 2.5" BASE	DSX1 LED P5 30K TTFM MVOLT TURNED OFF FOR POST-CURFEW	3000K LED	DSX1_LED_P5_30K_TTFM_MVOLT.ies	14694	0	138
☐	SA5	5	Lithonia Lighting	DSX1 LED P5 30K T2M MVOLT SPA (FINISH) / SSS 17.5" WITH 2.5" BASE	DSX1 LED P5 30K T2M MVOLT	3000K LED	DSX1_LED_P5_30K_T2M_MVOLT.ies	14663	0.91	138
☐	SA5	15	Lithonia Lighting	WDGE4 LED P3 30K 80CRI RFT MVOLT SRM (FINISH)	WDGE4 LED WITH P3-PERFORMANCE PACKAGE, 3000K 70CRI FORWARD THROW OPTIC	3000K LED	WDGE4_LED_P3_30K_T2M_MVOLT.ies	16889	0.91	124.86
☐	SB	33	Lithonia Lighting	DMW2 2000LM WD AFL MVOLT G210 30K 80CRI	DMW2 2000LM WD AFL MVOLT G21 30K 80CRI (GLEDS)	3000K LED	DMW2_2000LM_WD_AFL_MVOLT_G210_30K_80CRI.ies	2100	0.91	18.78
☐	SC	4	Pinnacle Architectural Lighting	EX3 WET-N-830HO 8' (POSITION) S U FSD 1 0 (FINISH)	8' SURFACE MOUNTED LINEAR-HIGH OUTPUT	3000K LED	EX3-WET-N-830HO-4.IES	22	0.91	69.2
☐	SD	4	Pinnacle Architectural Lighting	EX3 WET-N-830HO 8' (POSITION) S U FSD 1 4 E (FINISH)	8' SURFACE MOUNTED LINEAR-HIGH OUTPUT WIEM BATTERY PACK	3000K LED	EX3-WET-N-830HO-4.IES	22	0.91	69.2
☐	SDE	3	Pinnacle Architectural Lighting	EX3 WET-N-830HO 8' (POSITION) S U FSD 1 4 E (FINISH)	8' SURFACE MOUNTED LINEAR-HIGH OUTPUT WIEM BATTERY PACK	3000K LED	EX3-WET-N-830HO-4.IES	22	0.91	69.2



**NOTE1**  
 THE PRE-CURFEW LIGHTING DESIGN HOURS SHALL BE DEFINED AS DUSK TO 10:00 PM, AND THE POST-CURFEW LIGHTING DESIGN HOURS SHALL BE DEFINED AS 10:00 PM TO DAWN. ALL EXTERIOR LIGHTS SHALL BE TURNED OFF DURING POST-CURFEW WITH THE EXCEPTION OF LIGHTS FOR SECURITY PURPOSES.

A PROGRAMMABLE TIMER, AND PHOTOCELLS SHALL CONTROL THE PRE- AND POST-CURFEW LIGHTS; PHOTOCELLS SHALL BE MOUNTED ON THE NORTH SIDE OF BLDGS. THE PROGRAMMABLE TIMER MAY CONTAIN A MAXIMUM OF 1-HOUR OVER RIDE WITH AN AUTOMATIC TURN OFF FOR AFTER HOURS, AND SPECIAL EVENTS USE ONLY.

**PHOTOMETRIC (POST-CURFEW) SITE PLAN - SOUTH EAST**

1"=30'-0"



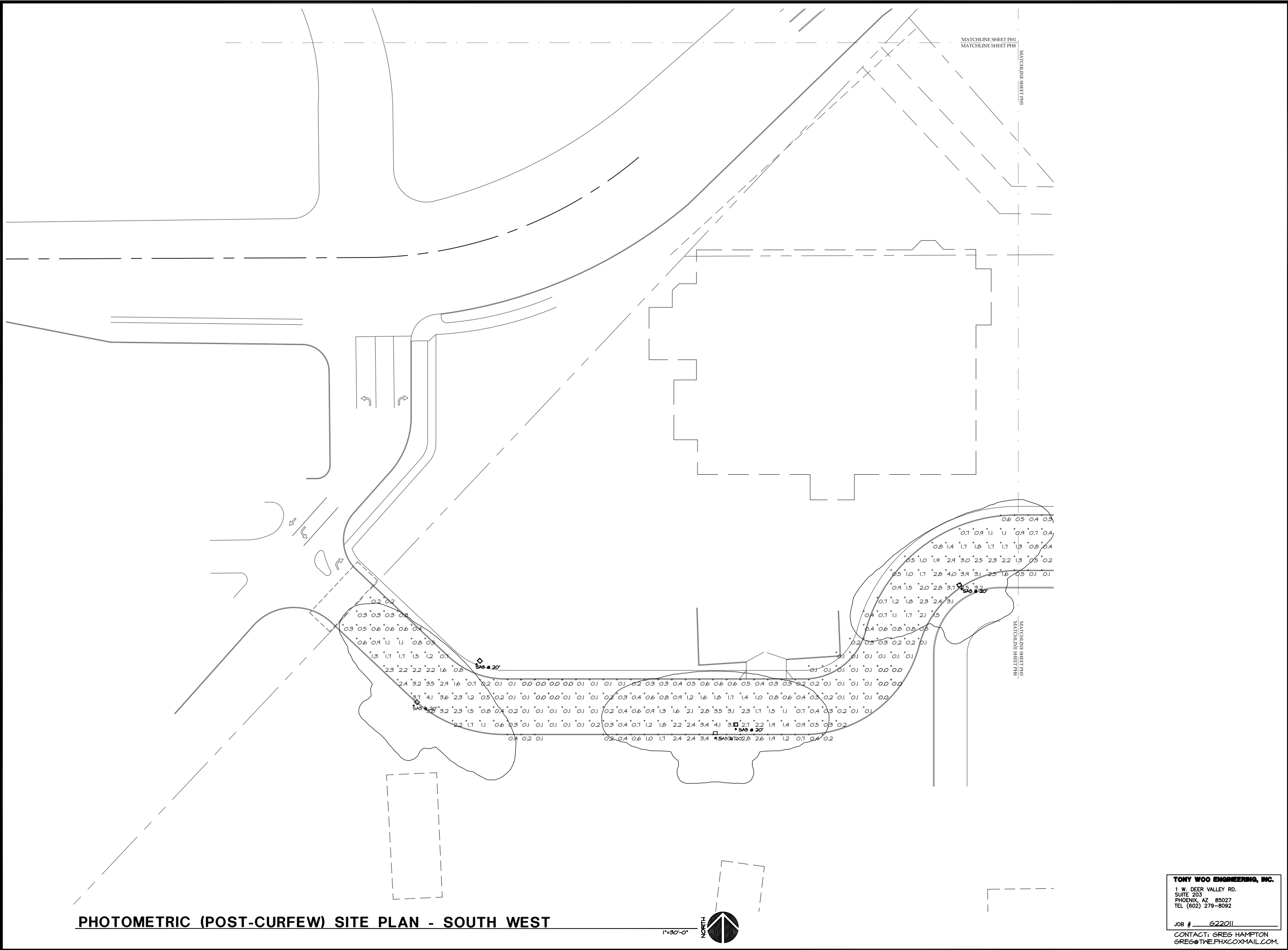
**TONY WOO ENGINEERING, INC.**  
 1 W. DEER VALLEY RD.  
 SUITE 203  
 PHOENIX, AZ 85027  
 TEL (602) 279-8092

JOB # 622011  
 CONTACT: GREG HAMPTON  
 GREG@TWE.PHX.COXMAIL.COM.

**MANUFACTURERS SHOP DRAWING FOR REFERENCE ONLY**

SHEET TITLE:  
 PHOTOMETRIC (POST-CURFEW) SITE PLAN - SOUTHEAST  
 ISSUE DATE: 05/17/22  
 DRAWN BY:  
 CHECKED BY:  
 PROJECT No.:

**PH7**



**PHOTOMETRIC (POST-CURFEW) SITE PLAN - SOUTH WEST**

1"=30'-0"



**TONY WOO ENGINEERING, INC.**  
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 CONTACT: GREG HAMPTON  
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**LGE DESIGNGROUP**  
 1200 N. 52nd Street • Phoenix, AZ • 85008  
 P: 480.966.4001

**SCOTTSDALE AND THUNDERBIRD**  
 SEC OF SCOTTSDALE ROAD AND THUNDERBIRD ROAD  
 SCOTTSDALE, AZ

**LGE DESIGNBUILD**

NUMBER	REVISION	DATE

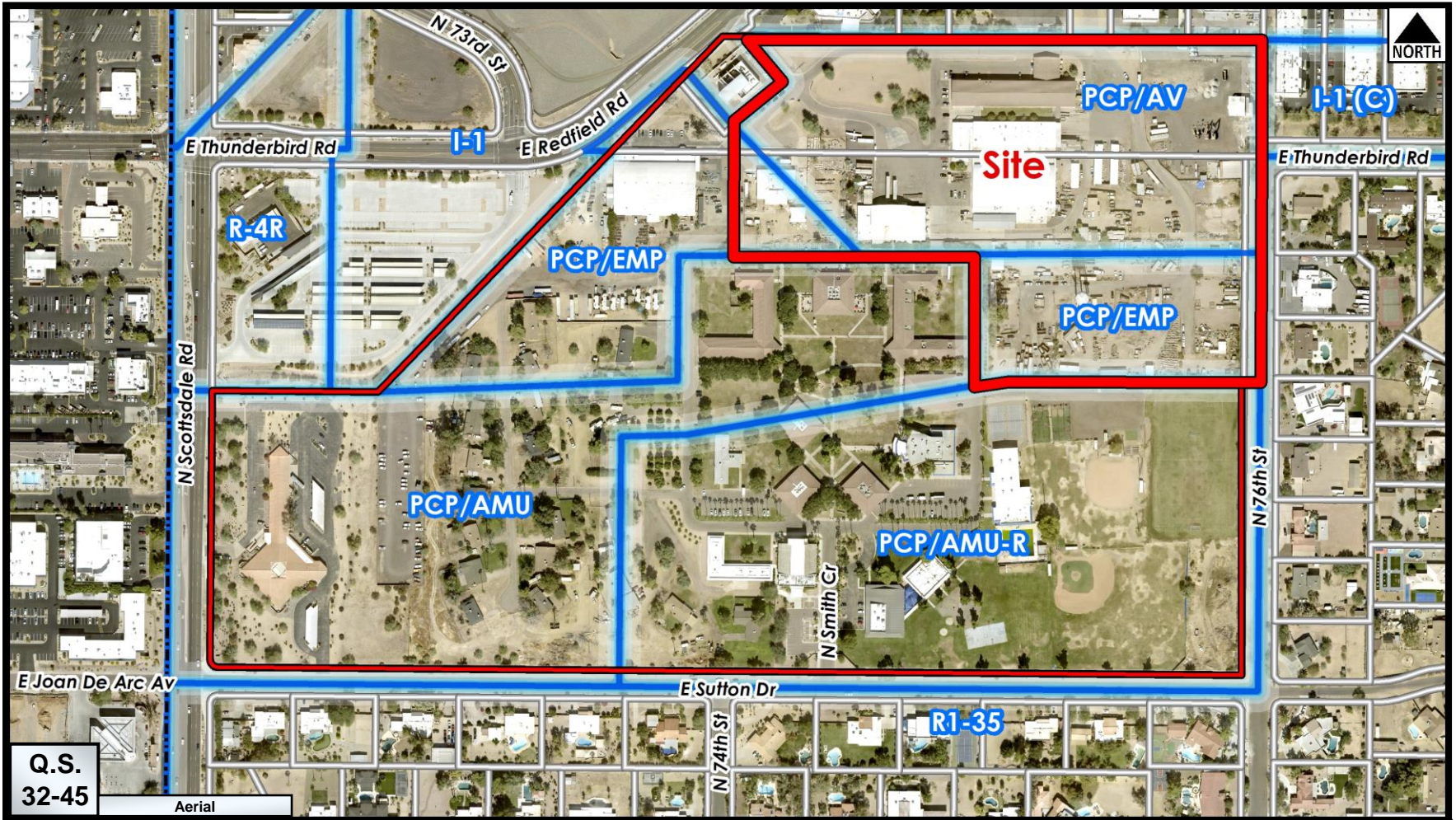
**MANUFACTURER'S SHOP DRAWING FOR REFERENCE ONLY**

SHEET TITLE:  
 PHOTOMETRIC (POST-CURFEW) SITE PLAN - SOUTH WEST  
 ISSUE DATE: 05/17/22  
 DRAWN BY:  
 CHECKED BY:  
 PROJECT No.:  
 SHEET:

**PH8**

05/17/22 - DESIGN REVIEW SUBMITTAL





Q.S.  
32-45

Aerial

Zoning Aerial

14-DR-2022

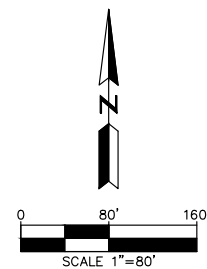
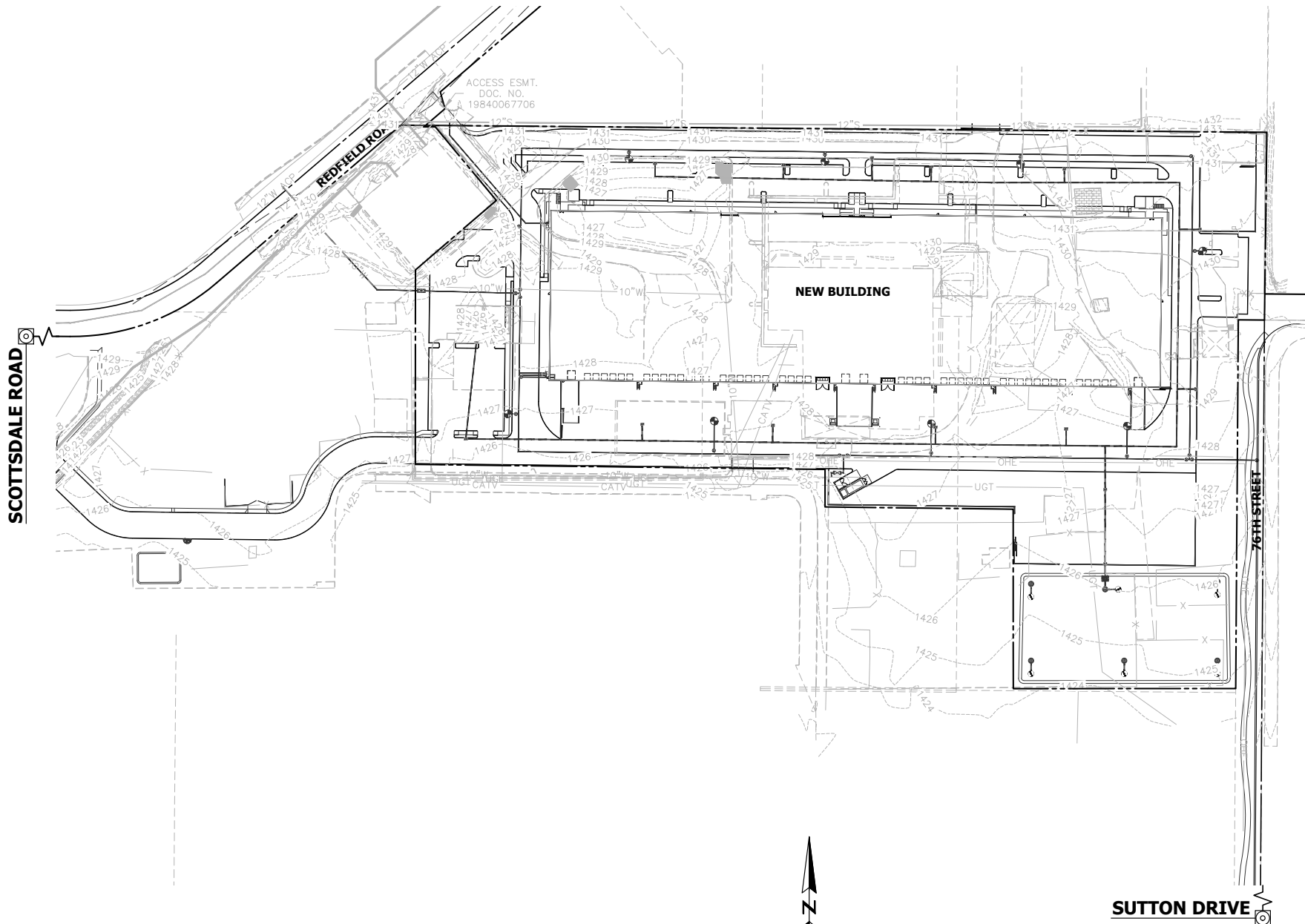
# CONCEPTUAL COVER FOR SCOTTSDALE AND THUNDERBIRD

## SEC OF SCOTTSDALE AND THUNDERBIRD SCOTTSDALE, ARIZONA

A PORTION OF NORTHWEST QUARTER OF SECTION 14, TOWNSHIP 3 NORTH,  
RANGE 4 EAST OF THE GILA AND SALT RIVER BASE AND MERIDIAN,  
MARICOPA COUNTY, ARIZONA.

### SHEET INDEX

	CIVIL	SHEET
CONCEPTUAL COVER SHEET	.C1	1
CONCEPTUAL GRADING & DRAINAGE PLAN	.C2	2
CONCEPTUAL UTILITY PLAN	.C3	3
CONCEPTUAL TYPICAL SECTIONS	.C4	4



ATTACHMENT #17

### DEVELOPER

LGE DESIGN BUILD  
1200 N 52ND ST  
PHOENIX, ARIZONA 85008  
PHONE: (480) 966-4001  
CONTACT: BLAKE WELLS  
EMAIL: BWELLS@LGEDESIGNGROUP.COM

### ARCHITECT

LGE DESIGN GROUP  
1200 N 52ND ST  
PHOENIX, ARIZONA 85008  
PHONE: (480) 966-4001  
CONTACT: MIKE RUSSO  
EMAIL: MRUSSO@LGEDESIGNGROUP.COM

### CIVIL ENGINEER

HUNTER ENGINEERING, INC.  
10450 N. 74TH STREET, SUITE #200  
SCOTTSDALE, ARIZONA 85258  
PHONE: (480) 991-3985  
FAX: (480) 991-3986  
CONTACT: LARRY TALBOTT, P.E.  
EMAIL: LTALBOTT@HUNTERENGINEERINGPC.COM

### EXISTING LEGEND

CENTERLINE	---
RIGHT OF WAY	----
PROPERTY LINE	----
EASEMENT	----
MAJOR CONTOUR	---1175---
MINOR CONTOUR	---1170---
STORM PIPE	---SD---
WATERLINE	---W---
SANITARY SEWERLINE	---SS---
ELECTRICAL RISER	⊕
WATER METER	⊗
WATER VALVE	⊙
FIRE HYDRANT	⊕
STREET LIGHT	⊙
STORM DRAIN MANHOLE	⊕
SANITARY SEWER MANHOLE	⊙
SPOT GRADE	x 1012.64 GUT
CONCRETE	[Pattern]

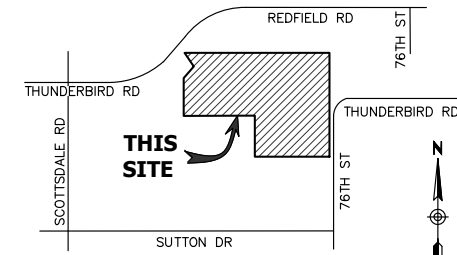
### PROPOSED LEGEND

CONTOUR	---76---
EASEMENT	----
STORM PIPE	---
SECTION CALLOUT	⊕
DIRECTION OF SLOPE	↖
CATCH BASIN	⊕
GRADE BREAK	⊕
CONCRETE	[Pattern]
CONCRETE	[Pattern]
SPOT GRADE	90.30C
FLOOD PLAIN	----

### FLOOD PLAIN DESIGNATION

COMMUNITY NUMBER	PANEL #	SUFFIX	DATE OF FIRM (INDEX DATE)	FIRM ZONE	BASE FLOOD ELEVATION (IN AO ZONE USE DEPTH)
04013C	1760 10-16-13	L	7/20/2021	ZONE X	N/A

ENGINEER'S CERTIFICATION: THE LOWEST FLOOR ELEVATION(S) AND/OR FLOOD PROOFING ELEVATION(S) ON THIS PLAN ARE SUFFICIENTLY HIGH TO PROVIDE PROTECTION FROM FLOODING CAUSED BY A ONE-HUNDRED YEAR STORM, AND ARE IN ACCORDANCE WITH CITY OF SCOTTSDALE REVISED CODE, CHAPTER 37 - FLOODWAYS & FLOOD PLAINS ORDINANCE.



### VICINITY MAP

### BENCHMARK

MCDOT BENCHMARK POINT NAME 26034-1 DESCRIBED AS A BRASS CAP IN HANDHOLE, STAMPED "CITY OF SCOTTSDALE" 0.5' BELOW GRADE, LOCATED AT THE INTERSECTION OF SCOTTSDALE ROAD & THUNDERBIRD ROAD. NAVD88 DATUM=1427.04

### BASIS OF BEARING

BASIS OF BEARING FOR THIS SURVEY IS A MEASURED BEARING OF SOUTH 89°32'00" EAST, ALONG THE NORTH LINE OF THE NORTHWEST QUARTER OF SECTION 14, TOWNSHIP 3 NORTH, RANGE 4 EAST OF THE GILA AND SALT RIVER BASE AND MERIDIAN, MARICOPA COUNTY, ARIZONA.

### UTILITY NOTE

WATER SERVICE FOR THIS SITE IS PROVIDED BY EPCOR WATER. SEWER SERVICE IS PROVIDED BY THE CITY OF SCOTTSDALE

### LEGAL DESCRIPTION

PARCEL NO. 1:  
ALL OF SECTION 11, TOWNSHIP 3 NORTH, RANGE 4 EAST OF THE GILA AND SALT RIVER BASE AND MERIDIAN, MARICOPA COUNTY, ARIZONA.  
EXCEPT ALL URANIUM, THORIUM AND OTHER MATERIALS DETERMINED TO BE PECULIARLY ESSENTIAL TO THE PRODUCTION OF FISSIONABLE MATERIAL AS RESERVED TO THE UNITED STATES OF AMERICA IN QUIT CLAIM DEED RECORDED IN DOCKET 1185, PAGE 139

PARCEL NO. 2:  
THE NORTH HALF OF THE NORTHWEST QUARTER OF SECTION 14, TOWNSHIP 3 NORTH, RANGE 4 EAST OF THE GILA AND SALT RIVER BASE AND MERIDIAN, MARICOPA COUNTY, ARIZONA.  
EXCEPT THAT PART LYING WITHIN THAT CERTAIN TRACT OF LAND CONVEYED TO THE CITY OF SCOTTSDALE RECORDED IN DOCKET 5231, PAGE 207 AND  
EXCEPT ALL URANIUM, THORIUM AND OTHER MATERIALS DETERMINED TO BE PECULIARLY ESSENTIAL TO THE PRODUCTION OF FISSIONABLE MATERIAL AS RESERVED TO THE UNITED STATES OF AMERICA IN QUIT CLAIM DEED RECORDED IN DOCKET 1185, PAGE 139  
NOTE: DESCRIPTION TO BE FURNISHED AND APPROVED PRIOR TO THE CLOSING OF THIS TRANSACTION.

### APN:

215-56-333C  
175-04-002A

### SITE AREA:

783,925 S.F. (18.00 ACRES)

NO.	DATE	REVISION	BY

DESIGN BY: WG  
DRAWN BY: DC  
CHECKED BY: LT

HUNTER ENGINEERING CIVIL AND SURVEY

10450 NORTH 74TH STREET, SUITE 200  
SCOTTSDALE, AZ 85258  
T 480 991 3985  
F 480 991 3986



COVER SHEET FOR SCOTTSDALE AND THUNDERBIRD SEC OF SCOTTSDALE AND THUNDERBIRD SCOTTSDALE, ARIZONA

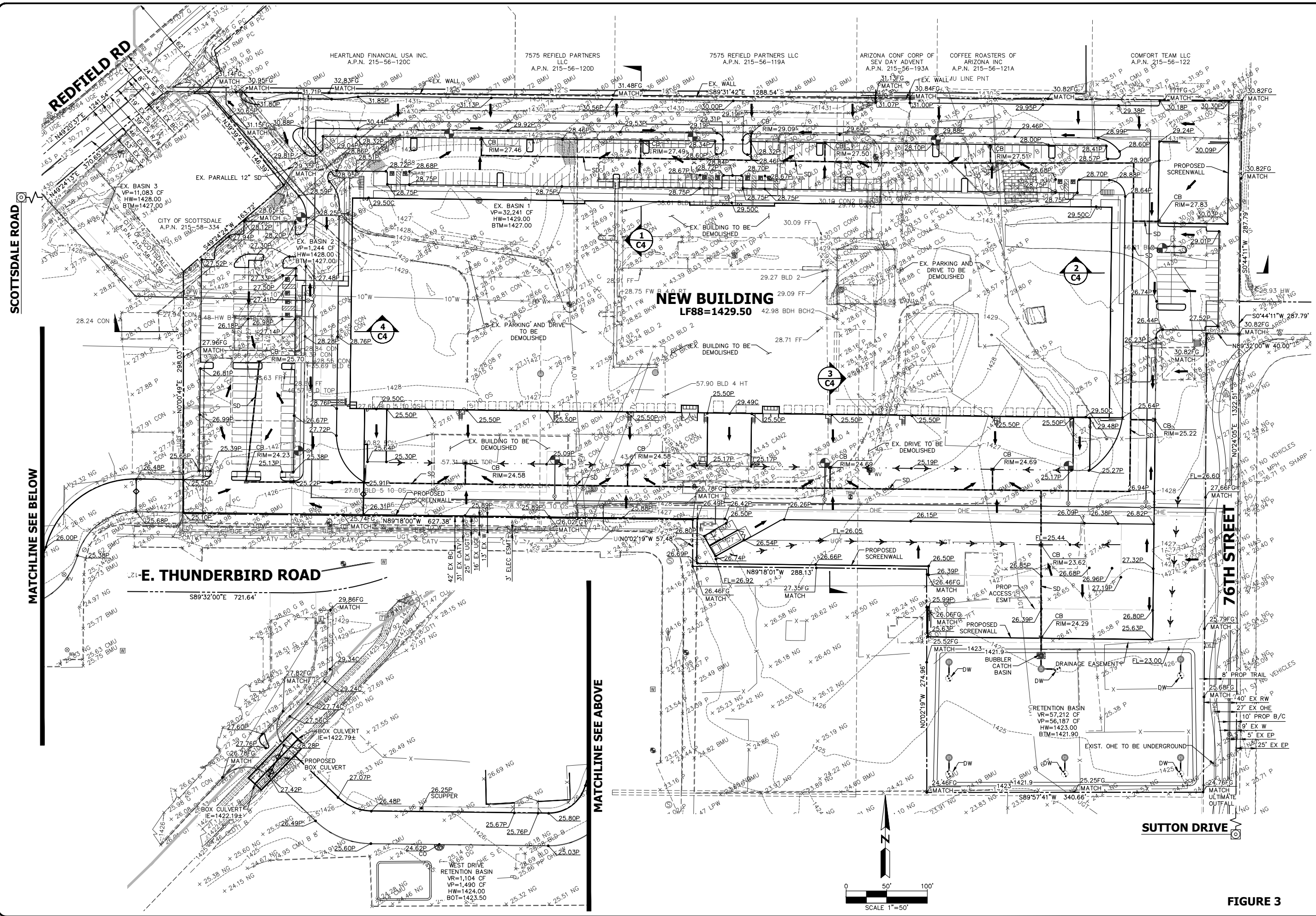


THESE PLANS ARE NOT APPROVED FOR CONSTRUCTION WITHOUT AN APPROVED SIGNATURE FROM THE GOVERNING MUNICIPALITY.

PROJECT NAME:  
SCOTTSDALE AND THUNDERBIRD

HE NO.: LGEC308  
SCALE: NTS

SHEET:  
C1



NO.	DATE	REVISION

DESIGN BY: WC  
 DRAWN BY: DC  
 CHECKED BY: LT

**HUNTER**  
 ENGINEERING  
 CIVIL AND SURVEY  
 10450 NORTH 74TH STREET  
 SUITE 200  
 SCOTTSDALE, AZ 85258  
 T 480 991 3985  
 F 480 991 3986



**CONCEPTUAL GRADING AND DRAINAGE FOR SCOTTSDALE AND THUNDERBIRD SEC OF SCOTTSDALE AND THUNDERBIRD SCOTTSDALE, ARIZONA**

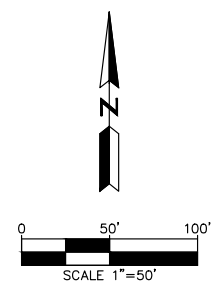
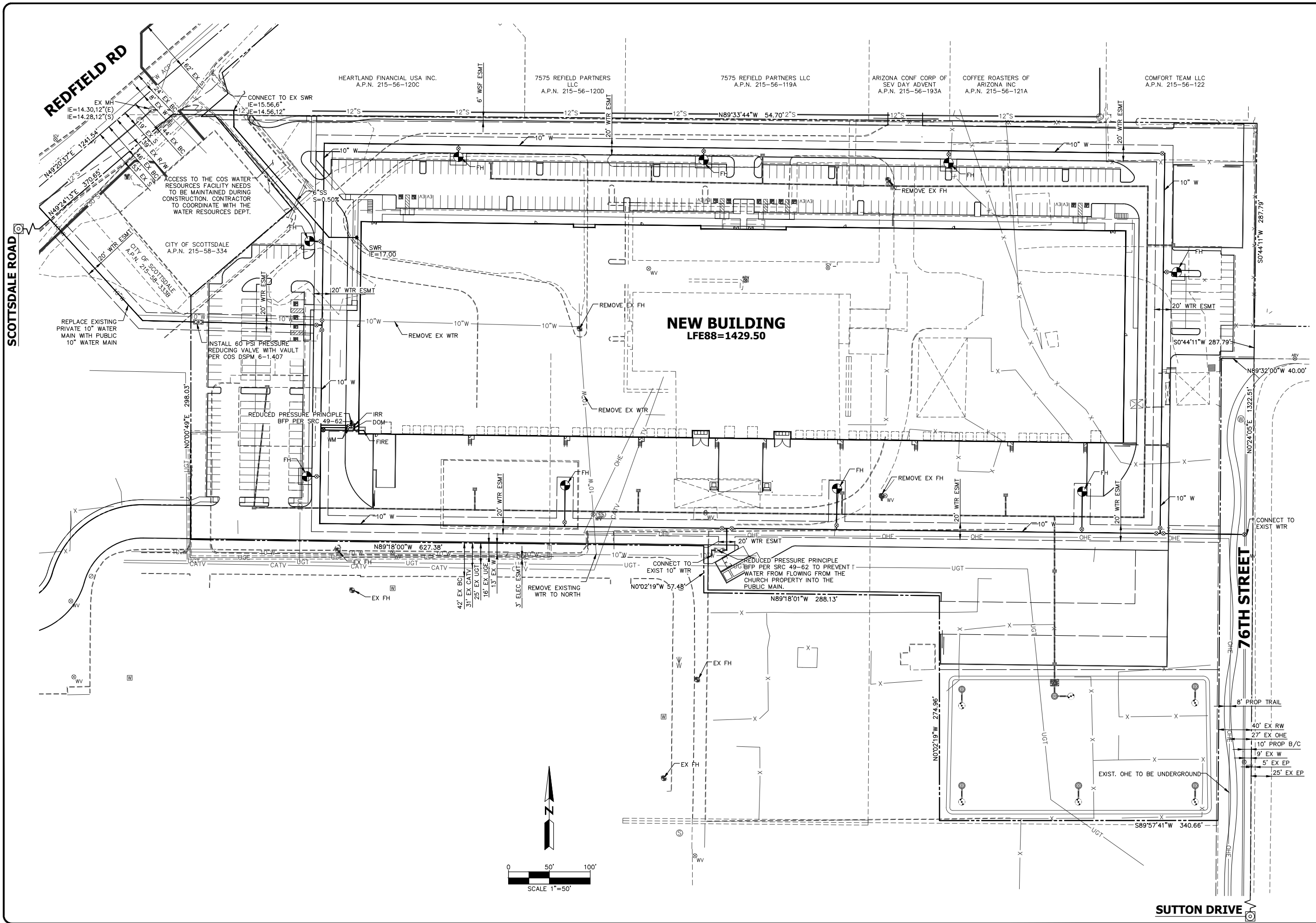
THESE PLANS ARE NOT APPROVED FOR CONSTRUCTION WITHOUT AN APPROVED SIGNATURE FROM THE GOVERNING MUNICIPALITY.

PROJECT NAME:  
**SCOTTSDALE AND THUNDERBIRD**

HE NO.: LGEC308  
 SCALE: 1"=50'

SHEET:  
**C2**

**FIGURE 3**



NO.	DATE	REVISION	BY

DESIGN BY: **WG**  
 DRAWN BY: **DC**  
 CHECKED BY: **LT**

**HUNTER**  
 ENGINEERING  
 CIVIL AND SURVEY  
 10450 NORTH 74TH STREET  
 SUITE 200  
 SCOTTSDALE, AZ 85258  
 T 480 991 3985  
 F 480 991 3986



**CONCEPTUAL UTILITY PLAN  
 FOR  
 SCOTTSDALE AND THUNDERBIRD  
 SEC OF SCOTTSDALE AND THUNDERBIRD  
 SCOTTSDALE, ARIZONA**

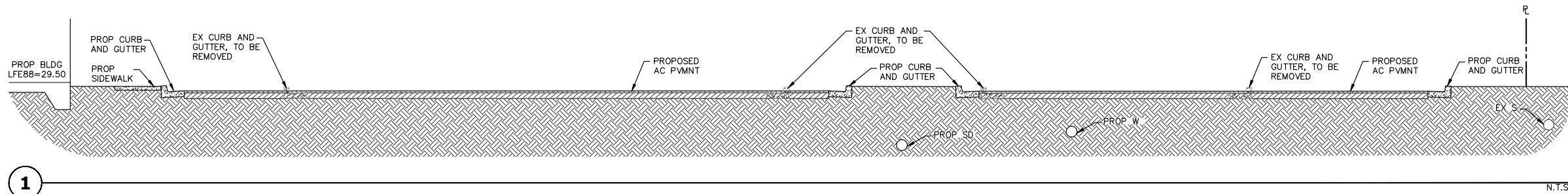
CONTRACT NUMBER: 14-DR-2022  
 DRAWING DATE: 11/4/22  
**ARZONAS11**  
 CALL 811 OR CLICK ARIZONAS11.COM

THESE PLANS ARE NOT APPROVED FOR CONSTRUCTION WITHOUT AN APPROVED SIGNATURE FROM THE GOVERNING MUNICIPALITY.

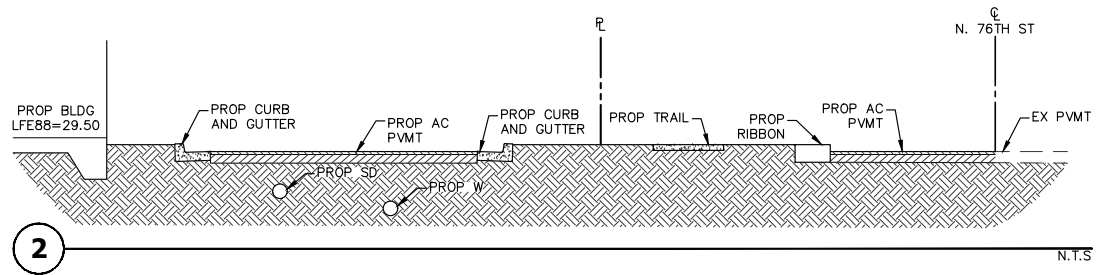
**PROJECT NAME:  
 SCOTTSDALE  
 AND  
 THUNDERBIRD**

HE NO.: LGE308  
 SCALE: 1"=50'

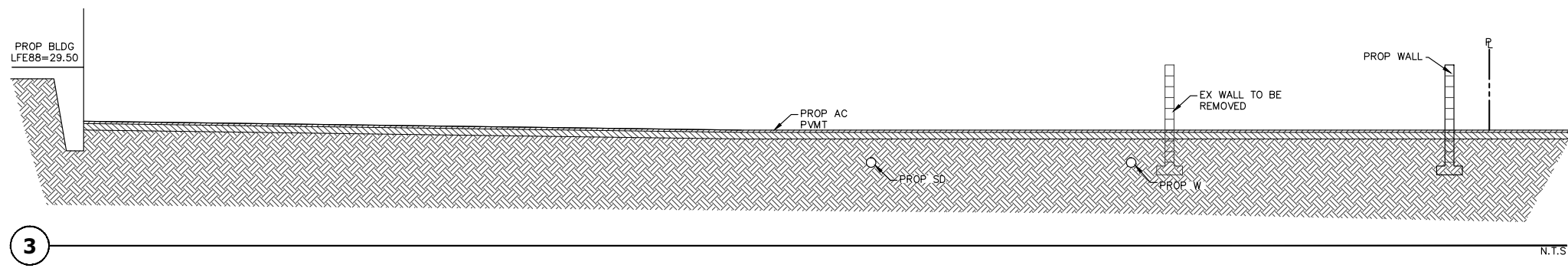
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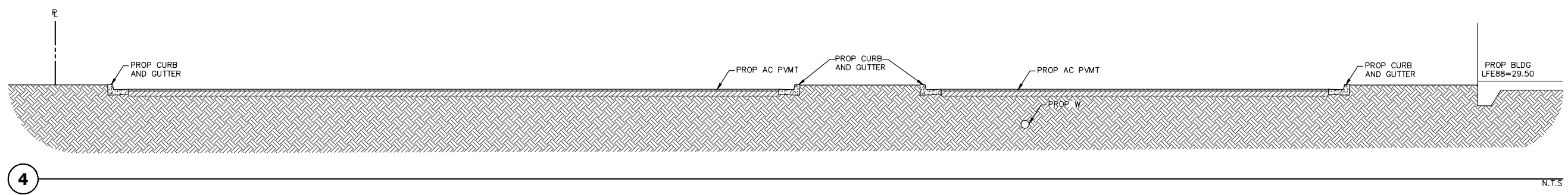
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2



3



4

NO.	DATE	REVISION	BY

DESIGN BY: WG  
DRAWN BY: DC  
CHECKED BY: LT

**HUNTER**  
ENGINEERING  
10450 NORTH 74TH STREET,  
SUITE 200  
SCOTTSDALE, AZ 85258  
T 480 991 3985  
F 480 991 3986



**CONCEPTUAL TYPICAL SECTIONS  
FOR  
SCOTTSDALE AND THUNDERBIRD  
SEC OF SCOTTSDALE AND THUNDERBIRD  
SCOTTSDALE, ARIZONA**

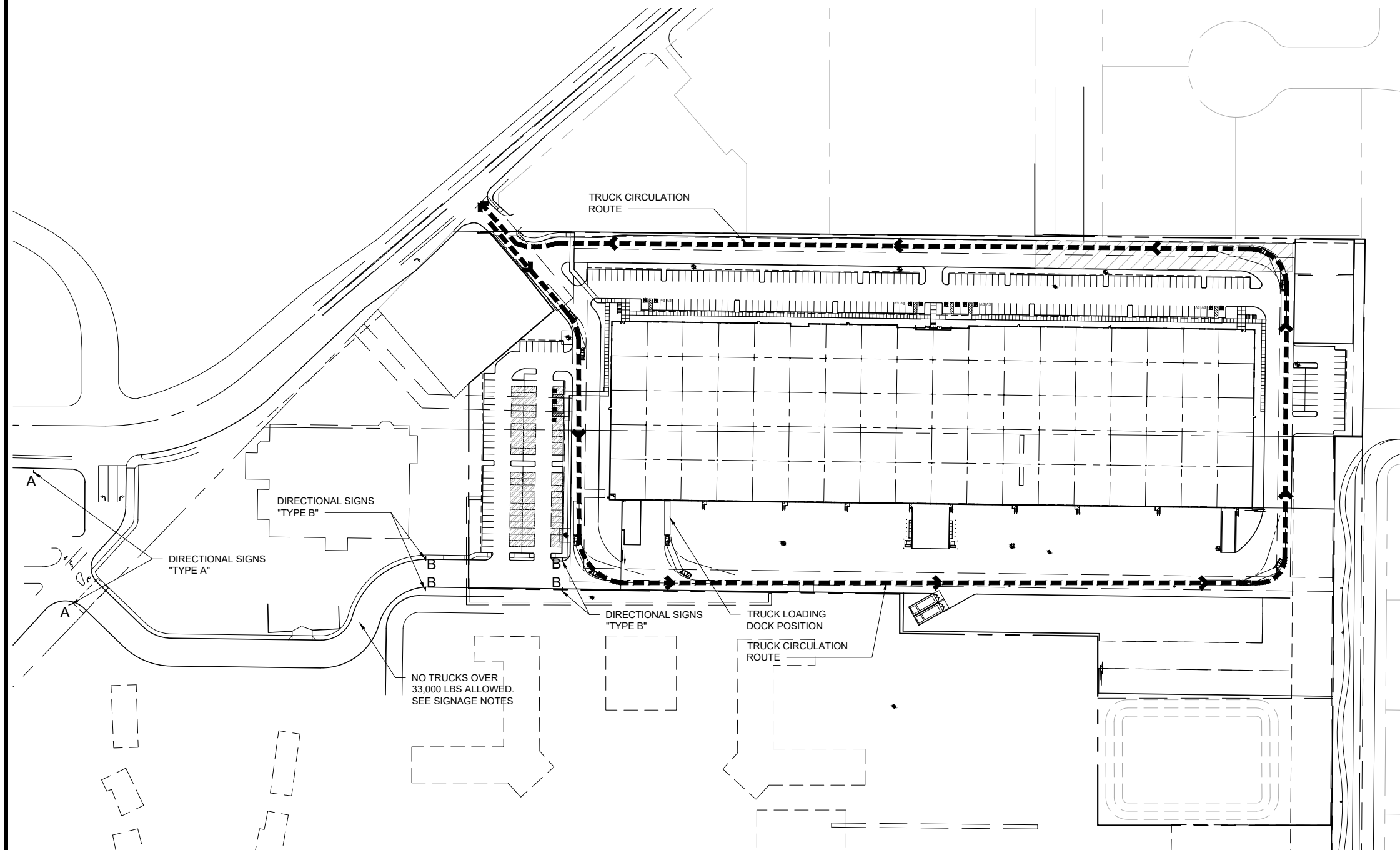


THESE PLANS ARE NOT APPROVED FOR CONSTRUCTION WITHOUT AN APPROVED SIGNATURE FROM THE GOVERNING MUNICIPALITY.

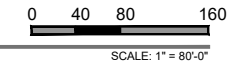
PROJECT NAME:  
SCOTTSDALE  
AND  
THUNDERBIRD

HE NO.: LGE308  
SCALE: NTS

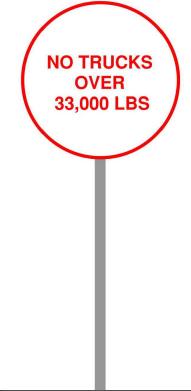
SHEET:  
**C4**



**TRUCK CIRCULATION PLAN**



**DIRECTIONAL SIGN TYPE A**



**DIRECTIONAL SIGN TYPE B**

**PROJECT DATA:**

PROJECT NAME:	SCOTTSDALE AND THUNDERBIRD
ZONING:	PCP / AV
NET LOT AREA:	+/- 783.924 S.F. (18.00 ACRES)
BUILDING HEIGHT:	+/- 40'-6" T.O.PARAPET

**LGE DESIGNGROUP**  
 1200 N. 52nd Street ■ Phoenix, AZ ■ 85008  
 P: 480.966.4001

**SCOTTSDALE AND THUNDERBIRD**

SEC OF SCOTTSDALE ROAD AND THUNDERBIRD ROAD  
 SCOTTSDALE, AZ

**LGE DESIGNBUILD**

NUMBER	REVISION	DATE

**CITY APPROVALS:**

PRELIMINARY NOT FOR CONSTRUCTION

SHEET TITLE: VEHICULAR CIRCULATION PLAN  
 ISSUE DATE: 10/07/22  
 DRAWN BY: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 PROJECT No: \_\_\_\_\_

**A1.1.4.A**

Proposed Revised Phasing Plan



Phase 1

Phase 2

Phase 2

Phase 3

# HISTORIC SIGNIFICANCE EVALUATION AND DOCUMENTATION of Military and Industrial Buildings at the LGE Design Build Proposed **SCOTTSDALE & THUNDERBIRD** **INDUSTRIAL PROJECT SITE**

7401-7501 East Redfield Road, Scottsdale, AZ 85260 – Portions of the Thunderbird Adventist Academy Campus

Final Report ... 2 November 2022

Prepared for:

**LGE DESIGN BUILD LLC**

1200 N. 52nd St., Phoenix, AZ 85008

David Sellers – President, CEO

[desellers@lgedesignbuild.com](mailto:desellers@lgedesignbuild.com)

with

**CREATION EQUITY LLC**

Grant Kingdon – V.P. of Transactions

602.600.6363 w; 602.509.4864 m

[grantk@creationequity.com](mailto:grantk@creationequity.com); [www.creationequity.com](http://www.creationequity.com)

Blake Wells – V.P. of Preconstruction

480-966-4001 w; 602-370-0262 m

[blakew@lgedesignbuild.com](mailto:blakew@lgedesignbuild.com)

Jagger Everett – Dir. of Development

602-600-6363 w; 602-769-6013 m

[jaggere@creationequity.com](mailto:jaggere@creationequity.com)

Prepared by:

**RYDEN ARCHITECTS, INC.**

2241 East Mountain View Road – Phoenix, AZ 85028

Don W. Ryden, AIA – President

602-253-5381 w; 602-616-7381 m

with

**LARRY MISHLER PHOTOGRAPHY** – 489-838-8881 w

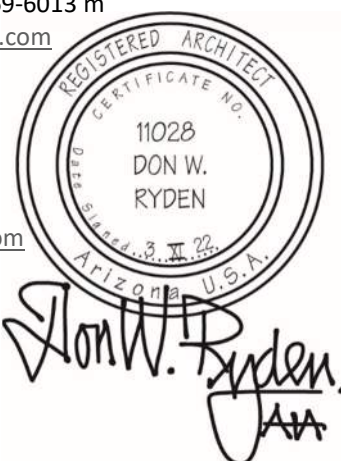
and

**JOAN FUDALA, HISTORIAN** – 480-585-6952 w

[don@rydenarchitects.com](mailto:don@rydenarchitects.com)

[printjobs@cox.net](mailto:printjobs@cox.net)

[jfudala@cox.net](mailto:jfudala@cox.net)



**RYDEN ARCHITECTS, INC.** – Don W. Ryden, AIA © 2022

2241 East Mountain View Road – Phoenix, AZ 85028

602-253-5381 w – [don@rydenarchitects.com](mailto:don@rydenarchitects.com)

[1]

Historic Significance Evaluation & Documentation  
**Buildings at Scottsdale & Thunderbird Industrial Project**

TbirdHngrs.EvalSig.Final3.2xi22 – Final ... 2 Nov 2022

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# 1. INTRODUCTION

## PROJECT BACKGROUND AND GOALS

### HISTORY OVERVIEW OF THE THUNDERBIRD ADVENTIST ACADEMY PROPERTY

The US Army Air Corps (AAC) constructed a pilot training school at Thunderbird Airfield No. 2 that was operated by civilian contractor Southwest Airways from June 1942 until October 1944. Following WWII, the ownership and uses of the property passed to Arizona State College (now Arizona State University) in 1947 for use as a technical school for vocational training of veterans through the GI Bill benefits. In 1953, the Seventh-day Adventist Church Conference purchased the 720-acre military property, outside Scottsdale city limits, for a Christian education boarding campus.

In 1963, the Thunderbird Adventist Academy (TAA) commissioned the development of the large industrial park at Scottsdale Road and Thunderbird Road. In 1966, the Adventists transferred for a modest sum the airfield portion of the property to the City of Scottsdale for enhancement as a municipal airport.

The Academy used the surviving military buildings until the early 1970s when most residential and classroom buildings were demolished and replaced with a new education campus. TAA continued using the WWII-period hangars for the vocational training shops and aircraft storage until those education programs were discontinued. The academic-focused, Christian high school boarding academy continues to this day. The hangars and other buildings are currently leased to a ceramic tile producer (Tabarka Studio), metal fabricator (English Forge), event equipment rentals, countertop fabricator (Old World Granite), architectural salvage importer (Antiquities Imports), and architect and interior designer (Oz Architects).

### PROPOSED SCOTTSDALE & THUNDERBIRD INDUSTRIAL DEVELOPMENT PROJECT

Property developers, Creation Equity & LGE Design Build, and the landowner Thunderbird Seventh-day Adventist Academy have entered into a long-term land lease of some 18 acres of the site for purposes of redevelopment.

Creation Equity has commissioned Ryden Architects, Inc. to provide historic preservation and design consultation services to comply with the stipulation (31. Historic Preservation Program) from the rezoning case with the City of Scottsdale and the Seventh-day Adventist Academy.

***31. HISTORIC PRESERVATION PROGRAM. With the Development Review Board Application, the property owner shall submit a report, based on the Documentation Requirements for the Seventh-day Adventist Rezoning, regarding the documentation of the existing buildings and evaluation of the historic significance to the city of the existing buildings and associated site improvements on the Thunderbird Adventist Academy campus (APN 175-04-002A, 215-56-193A and 215-56-333A), per SRC Chapter 2, Article V, Division 9, Sec. 2.313(b)(4). THE REPORT SHALL BE FOR DOCUMENTATION PURPOSES AND SHALL NOT IMPLY A COMMITMENT FOR PRESERVATION OF ANY BUILDING OR STRUCTURE.***

## HISTORICAL AND ARCHITECTURAL EVALUATION CONSIDERATIONS

The preliminary site development plan calls for the existing structures and site features to be removed for new infrastructure, paving, and landscaping for a single 243,360-square-foot fabrication/warehouse building. Thus, when applied to this situation and property, this report will be useful for its prescribed research, significance evaluation, and documentation of the buildings and site. It would not be needed for architectural assessment of preservation treatments of the buildings appropriate for adaptive uses.

Because the stipulation requires the “evaluation of the historic significance to the city of the existing buildings and associated site improvements on the Thunderbird Adventist Academy campus,” we understand that the City Historic Preservation Office (CHPO) has not yet determined *which, if any,* buildings are significant or *why* buildings are significant. Because the stipulation requires “the documentation of the existing buildings and associated site improvements,” we understand that the CHPO anticipates that there *may* be historically significant resources that deserve recordation for community benefit. Thus, in accordance with the stipulation and in compliance with City historic preservation assessment standards, Ryden Architects has prepared this historic resource report regarding the existing buildings and features at the Thunderbird Adventist Academy campus by compiling a history overview, by evaluating local historic significance, and by documenting the physical characteristics which convey that significance.

To our knowledge, no scholarly evaluation of the property has been conducted previously to evaluate historic significance or to consider potential eligibility for City historic designation. In complying with City HP criteria for evaluation and documentation, it is necessary to tell the story of the three hangars and the site features although only two hangars stand on the leased portion of the entire TAA property. Regardless of the subdividing lease lines, three hangars are still part of the historical and architectural context of the entire original site.

The three broad *historic contexts* in the Salt River Valley between 1942 and 1972 associated with the users and buildings of the current TAA property include WWII AAF pilot training, post-war GI Bill vocational training, and private boarding schools. For purposes of evaluating and documenting the TAA property and the proposed lease parcels, three *areas of significance* investigated to identify their level of importance in events and patterns of history and for design/construction as associated with the original Thunderbird Airfield No. 2 property:

- 1) Military – the three original aviation hangars and associated site features important as rare surviving military aviation architecture in Arizona, and/or for association with the Army Air Corps cadet pilot training programs at Thunderbird Airfield No. 2 between 1942 and 1944; or
- 2) Education – the three original aviation hangars and associated site features important for their adaptation as a campus between 1947 and 1953 for the Arizona State College Vocational Training School program attended primarily by veteran using their G.I. Bill education benefits; or
- 3) Education – the three original aviation hangars, industrial buildings, educational and residential buildings, and associated site features constructed by Thunderbird Adventist

Academy important as a privately operated boarding school campus for primary and secondary education and for flight and vocational training between 1953 and 1972.

The goal of this report is to provide an illustrated narrative document for the sake of scholarly research and community heritage recordation. It has been our experience regarding rare or significant buildings marked for rehabilitation, redevelopment, or demolition, that the City or State Historic Preservation Officer may require documentation based on appropriate aspects of National Parks Service's standards. This method of resource documentation includes historic narrative and significance evaluation, architectural description, measured drawings, and professional photographs.

## **ARCHAEOLOGY INVESTIGATION CONSIDERATIONS**

*The Interim ASM-SHPO Reporting Guidance* cited by the City Historic Preservation Officer applies to potential archaeological resources rather than to "standing structures" as archaeologists call buildings. This generalized interim survey guideline assumes that the 18-acre project site has already been archivally researched by historians and field surveyed by archaeologists. However, that type or degree of heritage study and evaluation likely has not occurred on this project site heavily impacted for 70 years by a municipal airport, K-12 educational campus, and private industrial park development. Our project research may identify historic period buildings or structures that have been demolished or sites where human activities occurred related to the flight or vocational schools. That information may be used for scoping future archaeological survey, testing, and data recovery by a professional archaeology firm to evaluate historic (or perhaps prehistoric) subsurface resources and artifacts.

## **METHODS OF EVALUATION AND DOCUMENTATION**

To initiate historic preservation consultation between LGE Design Build and the City of Scottsdale Development Review Board and CHPO, Ryden Architects prepared this report addressing historic significance evaluation and building documentation. This report will be the basis for discussion of historic resource management considerations in the site planning of property for the proposed Scottsdale & Thunderbird Road Industrial Development Project.

The study area for this report formally includes only the three northern parcels of TAA affected by the land lease and industrial redevelopment project. However, the historic-period site overlaps the development project boundaries. Thus, it is necessary to consider adjacent land west of the project, for it contains one of the three original and surviving aviation hangars associated with Thunderbird Airfield No. 2. Although we toured the West Hangar with the tenant, we did not document it with measured drawings or photos. In evaluating historic significance and integrity of the property, the three original hangars must be considered not only individually but also as a complete array of related structures within their setting.

## **SRC CRITERIA FOR HISTORIC RESOURCE EVALUATION AND DOCUMENTATION**

Historic Preservation Program Stipulation 31 of the City of Scottsdale rezoning case requests the developer to prepare a report regarding "**documentation of the existing buildings and evaluation of the historic significance to the city of the existing buildings and associated site improvements on the Thunderbird Adventist Academy campus**". The stipulation concludes by stating "THE REPORT SHALL BE FOR DOCUMENTATION PURPOSES AND SHALL NOT IMPLY A COMMITMENT FOR PRESERVATION OF ANY

BUILDING OR STRUCTURE.” The intent of LGE Design Build is to develop the property where two of three hangars are located and not preserve the structures. The historic period of study from 1942 to 1972 includes periods of operation by the US Army Air Corps, Arizona State College, and Thunderbird Adventist Academy. Without implying a commitment to preserve historic resources, this study evaluates historic significance of the property. We use the criteria of evaluation for potential local historic designation per Scottsdale SRC Chapter 2, Article VI – Supplementary Districts, Section 6.113, Criteria A – Historic Resource.

Scottsdale regulations cite the National Register criteria and methods, as amended, as the local standards for the historic evaluation and documentation of property resources. The CHPO follows *National Register Bulletin 15: How to Apply the National Register Criteria for Evaluation*, and the NPS *HABS/HAER Guidelines* (i.e., Historic American Buildings Survey/Historic American Engineering Record). These HABS documentation guidelines describe three possible levels of detail and archival quality. Ryden Architects uses an abbreviated, customized form of this documentation method for this project. Furthermore, study of this aviation-related property also follows *National Register Bulletin 43: Guidelines for Evaluating and Documenting Historic Aviation Properties*.

## **PROJECT WORK PROGRAM**

### **1.0 HISTORIC PRESERVATION & DESIGN CONSULTATION**

- 1.1 On-call historic preservation and design consultation
- 1.2 Historic preservation project scoping and initiation

### **2.0 RESEARCH PHASE**

- 2.1 Historical data compilation
- 2.2 Architectural field observations and documentation
- 2.3 Photography of buildings and site
- 2.4 Buildings floor plan measurement
- 2.5 Data integration of history, architecture, plans, and photos

### **3.0 DATA ANALYSIS PHASE**

- 3.1 Property documentation
  - .1 Historical overview
  - .2 Architectural data
  - .3 Site plan and measured floor plans
  - .4 Photographic data
- 3.2 Summary of Historic Significance and Integrity

### **4.0 REPORT PREPARATION PHASE**

- 4.1 Preliminary report for review by Client
- 4.2 Draft report for review by Client and City
- 4.3 Final report for approval by Client and City

---

## 10 INITIAL BUILDINGS AND SITE FEATURES

These buildings and features were identified in the original scope of work for study, evaluation, and documentation in the proposed industrial development project area.

1. H-2 South Hangar (WWII-period building)
2. H-1 East Hangar (WWII-period building)
3. R-1 Materials Shed
4. W-1 Workshops Building
5. W-3 Steel Building
6. W-2 Fabrication and Distribution Building
7. R-3 East Storage Shed
8. B-1 Architect Office Building (originally School Laundry Building)
9. C-1 Site of Airfield Control Tower (demolished – concrete slab in storage yard)
10. C-2 Other Site Features (Laundry Loading Dock)

## ADDITIONAL 10 BUILDINGS AND SITE FEATURES

Although not included in the original scope of work, initial research revealed that these buildings and features should be considered additionally in evaluating the significance of the property.

11. C-3 Furniture Loading Dock
  12. C-4 Site of School Bus Fuel Station (demolished – concrete slab)
  13. C-5 Site of Airfield Headquarters Building (demolished – gravel-surfaced storage yard)
  14. R-2 Open shed
  15. R-4 Open shed
  16. R-5 Open paired sheds
  17. B-2 Prefabricated Building
  18. W-4 Prefabricated Building
  19. W-5 Prefabricated Building
  20. H-3 West Hangar (WWII-period building not within proposed industrial development site)
-

## 2. PHYSICAL DESCRIPTION

### SETTING AND SITE

The East, South and West Aviation Hangars at the site of the World War II Thunderbird Airfield No. 2 are a set of wood-framed airplane hangars with barrel-vault roofs. (The East Hangar is outside the project site.) Each hangar features a single, open interior space spanned by exposed bowstring trusses. The hangars are on the north part of what is now the Thunderbird Adventist Academy (TAA). Unlike the well-landscaped education campus to the south, this asphalt-paved site has a military/industrial character. This image is conveyed by the historic-period hangars surrounded by non-historic industrial buildings, open sheds, prefabricated steel buildings, and subgrade loading docks. These later buildings, structures, and stormwater retention basin intrude to a degree on the paved open space designed to allow for the taxi and staging of airplanes.

Thunderbird Adventist Academy campus stands directly south of the hangars on the former location of the T2 barracks, classrooms, recreation hall, and swimming pool. During the 1960s, the TAA replaced most of these Army Air Corps buildings that had been laid out in an artistically geometric, symmetrical pattern. Linking the north and south components of the facility, the central axis of that pattern extended from the airbase entrance gates northward through the Control Tower (demolished) to the aircraft landing field (demolished) and beyond toward the desert horizon. The Control Tower marked the crossing of orthogonal axes that squared the symmetrical layout of the three hangars and Headquarters Building (demolished). As the axis mundi (i.e., axis of the world), the centerpiece Control Tower (demolished) functionally anchored six directions of architecture (i.e., front-back, left-right, up-down) and three angles of aviation (i.e., yaw, pitch, roll) for motion through calibrated space.

Although the hangars have sustained some post-historic-period modifications, the characteristic barrel vault roof form and open interiors—the most historically significant element of each building—continue to convey the hangars’ original use and visual character. The historic concrete aprons at the hangar doors and the open space of the flightline are still in place although modified by paving patterns, curbs, concrete slabs, and a stormwater retaining basin.

### BUILDING TYPES

#### **WWII AVIATION HANGARS – H-1 East, H-2 South, H-3 West**

The aviation hangars are the earliest surviving buildings on site and the only standing structures associated with the WWII Period and Post-war Period. The East and West Hangars mirror one another across the original taxi way. When first constructed using a modular component system, the two hangars were nearly identical. The rectangular hangars each have a single, volume of ten 20-foot structural bays for a length of approximately 200 feet by an open-span width of 100 feet and an interior clear height of 16 feet below the bottom chords of the trusses. Each hangar is timber-framed and features a bowstring-trussed barrel-vault roof that is the character-defining feature of this building type. The primary interior of the vaulted hangars features a single, cavernous open space with a concrete floor slab and corrugated side walls topped with a continuous ribbon of wood clerestory windows.

Originally, both end walls of the East and West Hangars would roll fully open to allow unimpeded access for aircraft. The South Hangar has an opening wall only on its north façade. As characteristic of military hangars, enormous multi-leaved wooden doors, set on a three-track rail system, would by-pass one another and stack in front of the low wings flanking the vaulted portion of the façade. The hangars feature an exposed, bolted timber bowstring truss and purlin roof system. This structural system supports a barrel roof formed by sheets of 5V-crimp galvanized steel panels. As an effective design demonstration of economy in structure, the curved steel roofing panels span between purlins without a wood roof deck. A fire sprinkler system has been installed among the historic-period fluorescent light fixtures on the roof trusses. According to the English Forge tenant, the several misting machines mounted on the East Hangar trusses are original features that humidified the open hangar for mechanics' comfort during the dry summer months. Further archival photographic or documentary evidence is needed to verify the age and origin of the machines.

5V-crimp steel panels and corrugated galvanized panels are used as the primary interior wall sheathing on both the side walls of the building and extends to a 12-foot height. Above this sheathing, filtered light enters the space through the corrugated fiberglass exterior wall material, creating a translucent ribbon of clerestory windows. As a result of the limited application of wooden members creating the illusion of mullions and muntins, some of these windows appear to be split into 6-pane sections. This window detail is original to the structure, as evidenced in a photo dating from 1944. Translucent fiberglass panels span between timber structural wall members; on the exterior of each building, some of this fiberglass is sheathed with corrugated metal. Both the roof and gable ends are constructed of 5V-crimp galvanized steel sheets.

The original sets of multi-leaved doors have been removed from their tracks to fix them in place flush as end walls. The immobile multi-leaved doors were set into the end facades of the buildings to open at two-thirds of the total hangar width to allow adaptive installation of modern roll-up garage doors. A rod-suspended wooden canopy with steel panel eaves originally supported the top tracks for the rolling doors. This canopy at the top of the doors extends beyond the bow-shaped gable ends to allow the doors to fully open. In appropriate places, steel cross-cable visible from the interior stiffen both the wall and roof structures, as well as panels of the multi-leaved rolling doors.

Originally, one-story, shed-roofed projections extended from the long sides of the East and West Hangars, which housed classrooms, offices, tool rooms, and storage. The shorter South Hangar has only one large door on the north façade. The shed building projections surround the other three sides.

In general, primary character-defining features of the hangars have survived and convey the significance of these buildings. The character-defining features retain in their relationship to the seven aspects of integrity identified by City historic preservation assessment standards. They include the building's location, design, materials, workmanship, and feeling. Although the hangars have been sheathed in modern steel panels, the primary original materials, such as timber framing, steel members, and galvanized steel panels are still intact and visible especially at the interiors. Initial visual observation suggests that much of the historic exterior finish likely would be revealed if the sheathing were removed. Further physical investigation is required to verify that the historic-period exterior materials still exist beneath a reversible remodeling treatment.

The post-historic-period steel panel sheathing of the hangars is similar in pattern to that of the original siding still intact and observable in places from the interior. Replacement materials, such as corrugated fiberglass, convey the historic visual character and feeling of the original material. Retention of original massing and proportion, as well as the overall spatial quality of the historically significant interiors, conveys the original and subsequent uses, visual qualities, and historic character of the hangars. Although physical condition (level of repair) may be fair, the integrity (level of authenticity) of these surviving characteristics convey the historic significance of the hangars.

### 3. STATEMENT OF SIGNIFICANCE

#### HISTORICAL OVERVIEW

What the U.S. Army Air Corps and Southwest Airways created on a remote parcel of Sonoran Desert land during World War II significantly impacted the evolving image, economy, and civic infrastructure of Scottsdale, Arizona for the next 80 years. The Army Air Corps built and oversaw Thunderbird Field No. 2, one of four military pilot training airfields established in the Salt River Valley. The facility was operated following military standards by contractor Southwest Airways whose civilian aviators trained the cadet pilots.

The newly completed Thunderbird Field No. 2 welcomed the Army's first Stearman military training biplane in June 1942. That auspicious event launching Scottsdale as an aviation hub of the Southwest. "T2," as the airfield was nicknamed, also revealed the potential of this area as a future employment center, economic and tourism engine, and education center for Scottsdale. In the mission of training military pilots, T2 also provided work experience and organizational skills for its civilian employees many of whom would become civic and business leaders for Scottsdale.

#### **WWII CADET PILOT TRAINING SCHOOL AT THUNDERBIRD FIELD NO. 2**

In 1939, with war being fought around the world, some national leaders anticipated the United States would eventually get involved in the global conflict. U.S. Army Air Corps General Henry H. "Hap" Arnold established the Civilian Pilot Training (CPT) Program in 1939. The Army Air Corps began seeking out suitable land and civilian contractors to operate basic pilot training schools to prepare military aviators for potential wartime service. Central Arizona, particularly the metro Phoenix area, was considered a highly desirable location for CPT schools, with a year-round temperate, "blue skies" climate, huge tracts of inexpensive vacant land available, good rail connections, and a favorable tax and labor environment.

Seeing opportunities for wartime business diversity, renowned Hollywood producer Leland Hayward, along with aviation expert Jack Connelly, and *Life Magazine* and Hollywood photographer John Swope, founded Southwest Airways in 1940-41, and based the new company in the metro Phoenix area. Gaining CPT contracts with the Army Air Corps/Force, between 1940 and 1942, Southwest Airways opened four Civilian Pilot Training schools to train military pilots. A detachment located at Sky Harbor Airport opened in October 1940 where the CPT program also trained cadet allies from Latin America. Thunderbird Field No. 1 in Glendale opened in March 1941 to train Americans and cadets from the allied Chinese Air Force. Falcon Field in Mesa opened in September 1941 primarily to train Royal Air Force pilots. Thunderbird Field No. 2 north of Scottsdale opened in June 1942 to provide U.S. Army Air Corps aviation cadets their basic flight training.

The federal government bought the land and funded construction of the CPT airfields and facilities. Southwest Airways managed operations under a contract with the Army Air Corps/Force. To fund their World War II Southwest Airways operations in the Valley of the Sun, Hayward, Connelly, and Swope brought in Hollywood friends and colleagues as investors. Among Southwest Airways' many investors were Jimmy Stewart, Henry Fonda, Cary Grant, Hoagy Carmichael, Darryl Zanuck, Gregory Peck, Dashiell Hammett, and Charles Laughton.

Based on the success of its first three CPT fields, and heightened demand for military pilots following the attack on Pearl Harbor December 7, 1941, Southwest Airways opened Thunderbird Field No. 2 on June 22, 1942. Located nine miles north of the small farming community of Scottsdale, the airfield encompassed 720 acres of previously undeveloped desert. The open airfield acreage (no formal runways) was unpaved and needed frequent oiling to keep the dust down. The 20 newly constructed buildings included hangars, control tower, administration building, classrooms, cadet dormitories, cafeteria, recreation hall, etc. They were located (prior to their demolition during the early 1970s) on the south side of the take-off/landing/flight line area, where Thunderbird Adventists Academy is located today in 2022.

The site plan of T2 complemented the regionally inspired architectural layout created for Thunderbird Field No. 1 by award-winning artist/designer Millard Sheets. The T1 buildings were arranged to be seen from the air as an abstract Native American thunderbird with its wings spread in flight. Conjuring lightning from its eyes and thunder with its powerful wings, the thunderbird is said to be an enormous supernatural being and benevolent guardian spirit that brings success in war and long life. The T2 campus buildings were arranged around the edges of a landscaped rectangular campus in zig-zag lighting fashion like patterns on a Navajo blanket.

The base had a small cadre of active-duty Army Air Force (the Army Air Corps was renamed Army Air Force in 1941) military personnel to ensure that Southwest's pilot training complied with Army Air Force standards. Southwest Airways principal John Swope was the T2 Field Manager; Ernie Pelton was Chief Pilot; Mike DesMarais was Director of Training. Most of the flight instructors, maintenance, flight support, and administrative employees were civilian residents of Scottsdale and Salt River Pima-Maricopa Indian Community. The commute to T2 – usually by bus -- was long and dusty on then-unpaved Scottsdale Road from unincorporated Scottsdale (population just over 1,000).

Emerging from the Great Depression, employment at Thunderbird Field No. 2 became important to Scottsdale as a source of income for many previously farm-oriented residents, and as a significant boost to Scottsdale's agricultural-based economy through payroll and local purchasing. War work at T2 also helped offset the economic and transportation effect of World War II on Scottsdale's seasonal tourism industry. The early local dude ranches were impacted by wartime gasoline and tire rationing as well as priority for travel given to moving wartime supplies and personnel.

Scottsdale residents held a variety of positions at T2 during the war and then went on to civic positions and businesses that helped Scottsdale launch its modern, post-war era. Malcolm White was a flight instructor; he was a Scottsdale business owner and became Scottsdale's first Mayor after incorporation in 1951, serving two terms. Dorothy Cavalliere Ketchum Roberts was a parachute rigger; she worked in her family's blacksmith and restaurant businesses and served as Scottsdale's Town Clerk from 1952 through the early 1960s. Scottsdale residents Lucy Lutes and Mattie Whinery were among the first women certified as T2 aircraft mechanics. Virgie Lutes Brown was a T2 switchboard operator; she became Scottsdale's first Town Clerk in 1951. Mildred Bartholow closed her Adobe House Guest Ranch in Scottsdale 'for the duration of the war' and became the T2 cafeteria manager. Mary Alice Cavalliere worked with Bartholow at T2 as a cook. Jack Bauer worked as a security guard at T2; after the war he was a long-time Scottsdale Post Office employee. Ralph Hamer worked in the T2 [aircraft] Stewards Department; he was a long-time Scottsdale café owner/operator.

Before arriving at T2 and the other military airfields around Phoenix, aviation cadets went through pre-flight training at Santa Ana army base in Southern California, where they were put through a basic-training-like program of military indoctrination. After arrival at Thunderbird Field No. 2, the aviation cadets – many of whom had never been in an airplane – had 10 weeks of basic flight training consisting of 98 hours of ground school and 65 hours of flight instruction. Flight instruction was given in Stearman PT-17s that had been introduced in 1934. These lightweight, stable machines were bi-wing, two-seat aircraft that had fabric-covered wood-framed wings and a single, piston-driven radial engine. With few instruments aboard, flying was done in daylight hours following the Air Corps visual flight rules (VFR). Pinnacle Peak in Scottsdale’s McDowell Mountains was a key navigational landmark for T2 cadets to ‘find their way home’ on their first solo flights. In addition to ground school academics and flying training, cadets had physical training and water survival training (“ditching”) in the field’s swimming pool.

True to their Hollywood owners/founders, T1, T2, and Falcon Field served as a movie set for the 1942 filming of “Thunder Birds: Soldiers of the Air” starring Gene Tierney, Preston Foster, and John Sutton. To thank the cadets for serving as ‘extras,’ the film’s producer and a Southwest Airways investor Darryl Zanuck donated a swimming pool to each airfield. There were other Hollywood connections. During his tenure as T2 field manager, John Swope married actress Dorothy McGuire. They rented a house from the Ellis family at Cattle Track and entertained celebrities visiting the Southwest Airways training bases. Leland Hayward was married to actress Margaret Sullivan; they attended parties at the Swopes’ rented home in Scottsdale.

Thunderbird Field No. 2 enjoyed great rapport and support from its nearby community Scottsdale. Thousands from throughout metro Phoenix, curious to see the off-limits airfield, attended a public open house to celebrate T2’s first anniversary in July 1943. Aviation cadets were invited into residents’ homes for weekend and holiday meals. Local resorts like the Hotel Westward Ho and Camelback Inn were the sites for cadet graduation ceremonies and parties.

A federally funded war-worker housing project, Thunderbird Homes, was built in Scottsdale in 1943, and several military and civilian families from Thunderbird Field No. 2 lived there. Located at Marshall Way and Second Street, Thunderbird Homes was closed and disassembled in 1960. Many of its wood-framed buildings were relocated and repurposed throughout Scottsdale.

As testament to their love of Scottsdale and fond memories of their time at Thunderbird Airfield No. 2, the Thunderbird Pilots Association held reunions at what became Scottsdale Airport in 1967. Scottsdale Mayor Malcolm White, a former T2 instructor pilot, often hosted the well-attended events. During its two and a half years of operation, Thunderbird Field No. 2 trained 5,500 aviation cadets. Many became decorated combat pilots in World War II and subsequent conflicts. Once introduced to the sunny climate and unlimited opportunities in the greater Scottsdale area, many returned after the war and settled here, creating a population and business boom that significantly changed Scottsdale from a farming/ranching community to a tourism, high-tech, medical, and entrepreneurial destination with a thriving, diverse economy.

The success of Thunderbird Field No. 2 demonstrated the ‘air-worthiness’ of Scottsdale as a future aviation center and as a desirable location for companies and their employees – successes that persist to this day. Thunderbird Field No. 2 also launched this area as a place for education and training at all levels and specialties, which continues today, ranging from the Thunderbird Adventist Academy, to

several flight training schools at Scottsdale Airport, to trade schools and branch campuses in the surrounding Scottsdale Airpark.

## **ARIZONA STATE COLLEGE ADAPTS POST-WAR THUNDERBIRD FIELD NO. 2**

When their wartime function was no longer necessary, the U.S. government offered to transfer many of its former training bases throughout the country to organizations that would continue to operate them for educational or flight training purposes. Thunderbird Field No. 2 was decommissioned as a military pilot training base in October 1944. It sat idle until the post-war government program to divest surplus wartime properties found an interested party. In 1947, Arizona State College (ASC, which became a university, ASU, in 1958) acquired the 720-acre Thunderbird Field No. 2 from the War Assets Administration.

Since the Army Air Force and its civilian contract operator Southwest Airways had ceased its aviation cadet training and closed T2 in October 1944 the site and its then-33 buildings had been vacant for three years in the dusty, remote desert nine miles north of yet-to-be-incorporated Scottsdale. But with a windfall on its hands, the college geared up the former aircraft hangars, administration building, classrooms, dormitories, and recreation hall to accommodate students. In October 1947, the college moved its Vocational Training Program from the cramped Farm Shop buildings on Mill Avenue in Tempe north 14 miles to Thunderbird Field No. 2. L.F. Riggins, who had run war industry training at ASC in the early 1940s, became the director of the new Arizona State College Vocational School.

Attracting mostly male World War II veterans using their new G.I. Bill benefits, ASC Vocational School students studied auto mechanics and electrics, auto upholstery, diesel and heavy equipment repair, car painting and body and fender work, refrigeration and air conditioning, welding, farm shop training and furniture upholstery and repair. During its first year, 118 students were enrolled in courses. The timing of the school's debut was spot on. Veterans needed training. Local businesses needed skilled workers to meet demand of a ration-free post-war consumer economy. Since Detroit had a hard time keeping up with demand for new cars, which had not been built during World War II, ASC Vocational School students turned reclaimed old cars into serviceable vehicles for area families and businesses. The former aircraft hangars of T2 were repurposed into car and truck repair training facilities.

According to documents in the ASU archives, few of the vocational training courses offered at the Thunderbird campus carried college credit but were designed to meet the employment objectives of specific trades. Students did not have to meet the usual college entrance requirements and could enroll at any time during the year. Although designed primarily for veterans, other civilian adults were accepted into the courses which were 48 to 104 weeks long. Tuition was \$50 per month. The ASC Vocational School kept in contact with the U.S. Veterans Administration and other appropriate agencies to ensure that the training offered met employment opportunities and skill demands. A proposal to add aviation training to the technical school's curriculum was floated, but never approved, so most of the land at the campus – some 650 acres of take-off and landing area – was not used.

The Thunderbird campus was described in the 1948 ASC Saguaro yearbook: "A community within itself, the Vocational School has its own sewage and water system, dormitories, kitchen and dining hall, recreation hall and three huge hangars which house the shops and classrooms. The school operates a fleet of trucks and busses [sic], the busses [sic] being used to transport students to and from the field at no charge. Students have their own athletic department and swimming pool and are issued activity

tickets, which enable them to take advantage of the college activities in Tempe. With additional equipment being installed constantly, the Arizona State College Vocational School can look to this year as only the beginning and the best yet to come.” A report in ASU’s archives described the landscaping as well planned by the original Thunderbird Field No. 2 operators – elm, mulberry, silk oak and bottle trees lined the walkways and roadways, with bougainvillea, hibiscus, cassia, jasmine, and other shrubs thriving throughout the grounds.

ASC Vocational School students benefited from the variety of recreational facilities built to entertain aviation cadets during World War II. A large recreation center in the middle of the campus included a student lounge (complete with a fireplace), a faculty lounge and a large dance floor and activity room where numerous school dances took place. Students traveled from the main Tempe campus to enjoy the Thunderbird swimming pool where the college chapter of the American Red Cross supplied its lifeguards.

Students got involved in community activities as well. The Vocational School had its own entry in the ASC Homecoming Parade. In April 1949 the school and its 250 students hosted an open house, which over 5,000 Scottsdale and Valley residents attended. Picnic tables were set up around campus, and Mr. Riggins, his staff of instructors and students gave tours of the shop areas and classrooms. In 1950, collegiate rodeo contestants from 14 Western states met for a two-day ASC Aggie Club-hosted rodeo in a new arena built on the Vocational School campus. According to the February 16, 1950 *Scottsdale Progress*, Dr. Grady Gammage was on hand to dedicate the arena. At one ASC rodeo held at the Thunderbird campus, future Scottsdale Mayor Bill Schrader, then an ASC student on the main Tempe campus, was thrown from his horse with resultant broken bones.

The November 30, 1950 *Scottsdale Progress* reported: “There’s an oddity on the Arizona desert at Thunderbird Field II north of Scottsdale. It’s a \$625,000 state institution that hasn’t cost the state a cent. The institution, Arizona State College at Tempe Vocational School...still [has] no prospect of having to dip into state taxpayers’ pockets to run the school. The field and the bulk of its equipment have been obtained as war surplus from the federal government. Students directed by instructors have put their labor and skill into building and placing equipment in top condition and maintaining it. And what financial needs exist are met by the students’ tuition...Today it has the capacity of 350 students, 40 instructors and employees, \$125,000 in equipment including six busses [sic], and school improvements have nudged the value of the former wartime flight training field to \$500,000.” The article also said that graduates had been placed in jobs regularly and that state industry had been calling the school for help in getting competent employees.

The ASC Vocational School at the former Thunderbird Field No. 2 closed on June 15, 1951 due to decreased enrollment, sharply down from a peak of 300 students. It briefly reopened later in 1951 as the training site for U.S. Air Force clerk-typists who were learning their trade via a contract ASC had with the Air Force. Enrollment declined as war veterans completed their technical training and found jobs or launched careers. Since transportation from the Tempe campus was a perennial problem, other, non-vocational uses for the remote campus were not viable.

As its use by ASC dwindled, cows could be seen grazing on the land. Several proposals would have breathed new life into the campus. One idea was to use it as an education center for retirees. An article in the *Phoenix Gazette* in February 1953 said the school “would provide them with instruction on how to make the best use of their remaining life span.” It was also considered as the location for a

rehabilitation center for juvenile delinquents. None of these ideas took root, so the airfield/campus again stood vacant.

## **SEVENTH-DAY ADVENTIST ACADEMY ACQUIRES THUNDERBIRD FIELD NO. 2**

By 1953, ASC had run out of ways to use the facilities and land, and in April returned the entire property to the federal government. In July 1953, the U.S. government signed the land over to the Arizona Conference of the Seventh Day Adventists. According to a 1986 report to the City of Scottsdale done by Roman Associates, at the time the Adventists Conference purchased the 720-acre airfield, the appraised value was \$235,784.

The Adventists relocated its Thunderbird Academy K-12 day and boarding school to the site, using the cadet dormitories and classrooms for their high school. Thunderbird Academy used the former T2 flight facilities to offer flying training to students and missionaries. The Academy allowed local private pilots daytime use of the 'unimproved' (no manned control tower) landing field. Thunderbird Academy also retained the former ASC Vocational School's furniture-making facilities in one of the original T2 hangars for many years, with student apprentices producing both furniture and income. The Academy built two tall industrial buildings near the east hangar to support their vocational program with workspace and warehouse distribution. To serve the needs of the boarding school, a laundry building was constructed immediately north of the T2 Administration Building. When the Academy dropped vocational training from its curriculum, the industrial buildings and hangars were leased to private fabrication businesses.

By the early 1960s, the Arizona Conference Seventh Day Adventists realized that they had more land than they needed, and that by selling land they would be able to fund replacements for T2's deteriorating wartime buildings being used by students and faculty of Thunderbird Academy. Seventh Day Adventist Elder Daniel Butherus and others consulted with Kansas City architect/developer George Tewksbury on what to do with the surplus land. In 1963, Tewksbury recommended that the Conference give the aviation portion of the land to the City of Scottsdale to use as a municipal airport, and to sell the surrounding land for development into an industrial park. Tewksbury himself bought 635 acres for \$3 million, according to the Roman Report of 1986.

## **SCOTTSDALE AIRPORT AND AIRPARK TAKE OFF FROM THUNDERBIRD FIELD NO. 2**

Urged by the Scottsdale Chamber of Commerce since the mid-1950s, the City of Scottsdale expressed the desire to have a municipal airport. In 1963, the City had annexed a large desert area including the Thunderbird Field No. 2 property following a so-called 'annexation war' with Phoenix (who also wanted the former wartime airfield as a reliever airport). The Adventists offered to transfer the airfield portion of their property to Scottsdale for a modest sum.

Having acquired the Adventist property in 1966, the City of Scottsdale began organizing a municipal airport authority, finding a fixed-base operator, and constructing initial facilities to serve general aviation traffic. The City paved and lighted a portion of the oiled-earth T2 airfield as a single 4,800-foot-long runway for modern corporate jets. When the airport opened on June 16, 1967, it had a beacon, runway lights, and 160 aircraft tie-downs. The first business jets landed in August of 1967. Robert Wachs, the first fixed base operator (FBO), worked out of a trailer with one aviation fuel truck. One week after opening a convention of 200 dentists flew into "SDL" (the code for Scottsdale Airport) to attend a conference. By December 1969, 127 aircraft and 20 helicopters were based at Scottsdale

Municipal Airport. By 2017, about 400 aircraft were based there making it the state's busiest single-runway corporate jet airports. It was not until 1968-69 that the first Scottsdale Airport terminal building opened, and the airport didn't get an FAA control tower until December 1974. This first control tower was located on the west side of the field; a new tower on the east side opened in 1989. Both control towers were dedicated by then U.S. Senator Barry Goldwater, a life-long pilot, aviation booster and 'patron saint' of Scottsdale Airport.

According to the Scottsdale Airport website, "Scottsdale Airport is one of the premier corporate jet facilities in the state. Its success was achieved, in great part, by the efforts of Scottsdale civic and community leaders." The City claims it to be a national model for an airport-based business parks. In 1995, the Arizona Department of Transportation presented Scottsdale with the Arizona Airport of the Year award. In addition, one of the Fixed Base Operators located at Scottsdale Airport has been rated among the top 50 by 'Professional Pilots Magazine' every year since 1993. Today Scottsdale Airport is one of the busiest single runway airports in the U.S.

What had been the 720-acre Thunderbird Field No. 2 during World War II had become the Scottsdale Airport/Airpark which by the late 1990s was one of the top three employment centers in Arizona. Scottsdale Airport and its surrounding Airpark are major economic assets for the City of Scottsdale. Centrally located in Scottsdale's 2,600-acre clean industry-zoned area, the Airport and Airpark are primary sources of employment. The Airpark area serves as a base for over 85 major companies and is home to nearly 3,233 small and medium-sized businesses with over 59,000 employees." With completion of the Loop 101 Pima Freeway in 2002, regional access to Scottsdale Airport/Airpark was greatly enhanced, further elevating its status as an area for employment, flight training and operation, tourism, recreation, education, dining, shopping, and residing.

In 2018, the airport's aging terminal building was razed and replaced by the Scottsdale Aviation Business Center which included office space for the U.S. Customs Service, Civil Air Patrol, and other flight operations. As a tribute to its founding as Thunderbird Field No. 2, a centerpiece of the City of Scottsdale's Aviation Business Center is the display of an actual Stearman PT-17 aircraft like that flown by T2 aviation cadets in the 1940s. It was flown to Scottsdale Airport in April 2017 and is a project of the Thunderbird Field II Veterans Memorial Inc. ([www.tbird2.org](http://www.tbird2.org)).

## EXECUTIVE SUMMARY OF HISTORIC SIGNIFICANCE

The functions, buildings, and spaces of the WWII Thunderbird Field No. 2 property have been adapted during 80 years by its several major users – U.S. Army Air Corps, Arizona State College Vocational School, Thunderbird Adventist Academy, and Scottsdale Airport/Airpark. In pursuit of their own goals, these entities each built upon the structures, efforts, and successes of their predecessors. The visible results of exponential growth here demonstrate that the users did not so much add to success as they multiplied success.

Thus, the physical reminders of the military origins of T2 and its significance to the city have almost disappeared. The military airfield has become an exemplary municipal airport and a clean-industry business park. In 2022, all original Army Air Corps buildings but the three 1942 hangars have been demolished or replaced at the Thunderbird Adventist Academy campus. (Two hangars are within the site of the current Scottsdale & Thunderbird Industrial Development Project.) The surviving hangars and their flightline area are visual cues illustrating five chapters in the continuing Scottsdale success story. Much of Scottsdale's post-World War II urban growth and economic success can be traced to the seeds sown by the Army Air Corps' wartime cadet pilot training operation at Thunderbird Field No. 2.

## Historic Highlights of the Thunderbird Field No. 2 Property

- The U.S. Army Air Corps through the Civilian Pilot Training Program searched the nation for the most advantageous environments and sites for establishing pilot training airfields and facilities. The Salt River Valley was selected for four such facilities which included Thunderbird Field No. 2 near Scottsdale.
- Thunderbird provided an opportunity for a cadre of Hollywood film makers to invest in America's future by successfully developing and operating pilot training airfields for the Army Air Corps in the Salt River Valley.
- Thunderbird is where farm town Scottsdale successfully fought the home-front battle that helped the nation win the war.
- Thunderbird was a significant "civic incubator" that provided life lessons of cooperation for its hundreds of wartime civilian employees many of whom succeeded as community leaders in post-war Scottsdale.
- Here blue skies, desert climate, and scenic appeal lured back WWII veterans to succeed in learning skills, finding jobs, starting businesses, and establishing family homes in Scottsdale and the Valley.
- Here the Arizona State College Vocational School adapted WWII T2 facilities to provide a successful education program in practical job skills especially for returning veterans as enabled by their G.I. Bill education and loan benefits.
- Here the Adventist Conference adapted WWII T2 facilities to establish and grow a successful K-12 academy to educate children and teach practical skills for missionary work.
- Thunderbird's success as a cadet pilot training field demonstrated that the location and environment was perfect for future civilian aviation operations which manifested as the Scottsdale Municipal Airport.
- Thunderbird's role as a major wartime employer set the tone for the development of the site as the Scottsdale Airpark and its surrounding industrial zone, now among the three most successful business centers in Arizona.
- Parallel and related to the series of uses and adaptations of the Thunderbird Field No. 2 campus and airfield, Scottsdale has evolved into a world-renowned tourism, business, healthcare, education, clean industry, and residential destination.

## HISTORIC OVERVIEW RESEARCH RESOURCES

Regarding history of Thunderbird 2 Airfield, AZ State College Vocational School, Thunderbird Adventist Academy, Scottsdale Airport/Airpark Area – compiled by Joan Fudala, as of August 25, 2022.

### Thunderbird 2 Airfield – 1942-44: Southwest Airways/US Army Air Corps

- Scottsdale Historical Society (Bob Fowler)
- Scottsdale Public Library Digital Collection (Sara Housley)
- Thunderbird Academy, Seventh Day Adventists
- Thunderbird Field II Veterans Memorial, Inc. (Steve Ziomek, Chairman/President)  
[www.thunderbirdpilots.com](http://www.thunderbirdpilots.com) (mostly about Thunderbird Field, Glendale)
- Arizona Dept. of Libraries & Archives/AZ Memory Project
- Arizona Historical Society @ AZ Heritage Center Papago Park (esp. Joe Wischler collection – photo albums, movie films of Falcon & Thunderbird fields)
- Arizona State University Archives, Shannon Walker, archivist (*The Thunderbird* magazines, photos, documents)
- Pima Air & Space Museum/Arizona Aviation Hall of Fame (Hayward/Connelly enshrined in 2000) – Southwest Airways photos and documents in archives
- *A History of the Scottsdale Municipal Airport*, 1986 by Roman Associates (digital PDF copy available at [www.scottsdalelibrary.org/historical](http://www.scottsdalelibrary.org/historical))
- *Arizona Republic/Phoenix Gazette* archives (see [www.newspapers.com](http://www.newspapers.com))
- Thunderbird II cadet yearbooks
- Family of Malcolm White – he was T2 instructor pilot, 1<sup>st</sup> Mayor of Scottsdale, host of T-bird Pilots reunions (daughter Mary White Keltner; grandson Dean Keltner – both active in Scottsdale Historical Society)
- Family of Dorothy Cavalliere Ketchum Roberts (she was a parachute rigger at T2, early Town of Scottsdale Town Clerk, member of the Cavalliere blacksmith shop family)
- Janie Ellis, Cattletrack Art Enclave (re: Swopes' former rental home there)
- Paul Messinger, mortuary owner, civic leader and historian (personal recollections of his boyhood when T2 was operating)
- Paul Markow Photography - Phoenix (son of Southwest Airways photog Bob Markow; has archives)
- Estate of Mark Swope (son of John Swope, field mgr. & photographer) – California or his sister in Washington
- Arizona Military Museum @ Papago Park (verify possible resources)
- Salt River Pima-Maricopa Indian Community – contact Thomas Wright, staff archaeologist, 480-362-7623 (verify possible resources)

### Arizona State Teachers College – 1947-1953: Technical Trade or Vocational School

- Arizona State University Archives (Shannon Walker/Rob Spindler)
- ASU yearbooks, 1948-51
- *A History of the Scottsdale Municipal Airport*, 1986 by Roman Associates (digital PDF copy available at [www.scottsdalelibrary.org/historical](http://www.scottsdalelibrary.org/historical))
- *Arizona Republic/Phoenix Gazette* archives
- *Scottsdale Progress* archives (at [www.newspaperarchives.com](http://www.newspaperarchives.com) or on microfiche at Scottsdale Civic Center Library)

### **Thunderbird Adventist Academy – 1953-present: Seventh Day Adventists**

- Arizona Conference, Seventh Day Adventists
- Thunderbird Academy administration
- T-bird Academy yearbooks
- *Scottsdale Progress* archives (at Scottsdale Civic Center Library)

### **Scottsdale Municipal Airport – 1967-present**

- Scottsdale Airport Administrative Offices (Sara Ferrara)
- Scottsdale Historical Society (Bob Fowler)
- Scottsdale Public Library Digital Collection (Sara Housley)
- *Scottsdale Progress* archives (at Scottsdale Civic Center Library)
- *Arizona Republic/Phoenix Gazette* archives
- City of Scottsdale Public Affairs Office/City News Network programs/film footage (Kelly Corsette, City of Scottsdale Public Affairs Officer and past president of Arizona Historical Society's Central Chapter and a former US Navy PAO)
- *Scottsdale Airpark News* archives (Scottsdale Civic Center library, the current publisher Times Publishing Steve Strickbine or Diana Smith, former publisher)
- Andrea Michaels, daughter of the late Bill Arthur, who was a WWII pilot, 'grandfather of Scottsdale Airport/Airpark, long-time SDL Commissioner and builder/owner of Thunderbird Suites Hotel at Scottsdale Airport
- Past Scottsdale Airport Commissioners, former Scottsdale Airport directors/staff
- FAA/FSDO personnel; FBO operators

### **Scottsdale Airpark Area – 1964-present**

- Scottsdale Historical Society (Bob Fowler)
- Scottsdale Public Library Digital Collection (Sara Housley)
- Scottsdale Airpark News archives (library, Times Publishing and Diana Smith)
- *Scottsdale Progress* archives (at Scottsdale Civic Center Library)
- *Arizona Republic/Phoenix Gazette* archives
- City of Scottsdale Economic Development Dept. (current stats)
- Jim Keeley, Colliers Classic Real Estate (has compiled a comprehensive report on Scottsdale Airport/Airpark trends, businesses, land use since 1980s)
- Scottsdale Area Chamber of Commerce airport/airpark committee

## **ARCHITECTURAL OVERVIEW**

### **VISUAL CHARACTER OF THE PROPERTY AND ITS COMPONENTS**

At first impression, the visual character of the project study area is undeniably negative due to the agglomeration of incongruous architectural styles and materials, accumulation of buildings of disparate shapes and sizes, under-maintained facades and finishes, superficial appearance upgrades, awkward layout of roads and parking, glaring expanses of pavements, and a dearth of live landscaping.

Overlay this eclectic architectural backdrop with a bewildering muddle of untidy construction materials yards, rickety open sheds packed with splintered palettes and rusted metal, scattered cargo containers and dumpster bins, clustered big-rig trailers and ski boats, hodgepodes of security fences and barbed wire, jumbles of cast-off equipment and furniture, and an omnium-gatherum of derelict detritus. When taken together, there seems to be no redeeming value in these motley buildings on a cluttered site where hoarding has achieved an industrial scale and visual disarray has reached a chaotic plateau.

By integrating archival history research with physical building investigation, the historic significance of the underlying physical elements associated with events may be revealed, evaluated, and documented.

### **SETTING AND SITE OF THE AVIATION HANGARS**

Begin investigation and evaluation with the most obvious central outdoor area. The open space of the hangar aprons and their shared taxiway area remains in place although its ground plane has been affected by modern grading, drainage, and paving modifications. These civil engineering improvements and roadways at 45 degrees to the compass confuse the eye when picturing the uniform, level character of a taxiway and adjacent buildings laid out orthogonally. Nonetheless, most of the open space north of the hangars and south of Redfield Road and the airport runway still exists giving a sense of how the training aircraft originally moved or parked between the hangars and the airfield.

The site plan symmetry of this important working space has been visually affected by the loss (between of the Aviation Control Tower (C-1, demolished between 1998-2000), the symbolic functional center of the airbase, and of the Aviation Headquarters Building (C-5, demolished between 1979-1986), the structure that visually balanced with the South Hangar. The Control Tower and HQ Building sites are covered by a fenced architectural salvage yard associated with the architect's office in the adapted Laundry Building (B-1). To further impact the visual symmetry, the TAA School Laundry Building was constructed in a yard immediately north of the HQ Building. For many years, those airbase and school buildings stood together nearly touching. The Laundry Loading Dock (C-2) was added after the HQ Building was demolished. The Open Shed (R-2) at the northwest corner of the South Hangar (H-2) was built during the early years of TAA. The irregular polygon concrete slab in the taxiway north of the South Hangar, covers the site of the demolished TAA school bus fueling station underground tanks (C-4).

As the TAA center of vocational education beginning in 1953, the East Hangar (H-1) has been visually impacted by development of additional industrial buildings to the north. The location, size, and architectural character of the later buildings detract from the East Hangar. The enclosure of the narrow yard between the East Hangar and the 1950s Fabrication Building (W-1) connects the structures without physically harming the East Hangar.

## 4. EVALUATION FINDINGS

### HISTORICAL SIGNIFICANCE FINDINGS

#### AGE

Constructed in 1942, the aviation structures meet the 50-year age requirement for historic designation. Several other industrial buildings within the study area meet the age requirement but are not associated with the historic significance of the Southwest Airways Civilian Pilot Training schools program flight training and early post-war veteran education.

#### **Periods found significant**

Southwest Airways Civilian Pilot Training schools program (1942-44) – cadet pilot flight training facility as Thunderbird 2 Airfield (T2)

Post- Southwest Airways Civilian Pilot Training schools program Period (1947-53) – adapted for veteran vocational training school by AZ State College (ASC)

#### **Period found not significant**

Suburbanization Period (1953-72) – K-12 boarding school as Thunderbird Adventist Academy (TAA)

#### SIGNIFICANCE

##### **Level of Significance**

Property found locally significant as related to growth and development of Scottsdale and the Salt River Valley during the periods of significance.

##### **Significance Criterion A: Historic Events and Trends**

*Areas of Significance found applicable as primary themes*

##### ***Military***

Thunderbird Airfield No. 2 is significant as one of three WWII airfields around the Salt River Valley that were built and operated by the Southwest Airways Civilian Pilot Training school program for the Army Air Corps. The disciplined military order of the T2 airfield and campus layout, building element hierarchy, aviation imagery, structural efficiency, materials economy, and multi-purpose resilience together convey the unified coordination of the Southwest Airways Civilian Pilot Training school program partners including the patriotic project developers, private building contractors, dedicated flight trainers, young cadet pilots, and eager local work force. The T2 hangars may be the last remaining WWII-period military-use buildings in Scottsdale.

##### ***Education***

The Thunderbird Airfield No. 2 had a profound influence on traditional farm town Scottsdale and the Salt River Pima-Maricopa Indian Community by providing war effort employment that helped offset the

1940s local agriculture and tourism downturn. Following allied victory, Arizona State College easily adapted the deactivated T2 buildings for vocational training for locals, especially for veterans using GI Bill education benefits. The T2 campus was the place where the government met its debt to vets. Through wartime defense work and postwar vocational education, the T2 programs served as a “civic incubator” for a young generation of optimistic business and political leaders ready for the population boom soon to transform Scottsdale as part of an emerging Modern metropolis.

**Area of Significance found not applicable**

### **Education**

The Thunderbird Adventist Academy, a faith-based boarding school initially used and enlarged the T2 buildings to provide primary and secondary parochial education for children of their religious community. Their initial education program included flight and vocational training as important elements of overseas missionary preparation. They utilized the hangars for aviation and skills training (notably furniture manufacturing). The goals and programs of the Academy did not have the same degree of impact on Scottsdale as had the T2 airfield and the ASC technical school.

### **Significance Criterion B: Important Person**

*Found not applicable*

## **ARCHITECTURAL SIGNIFICANCE FINDINGS**

### **Significance Criterion C: Design and Construction**

**Area of Significance found applicable as a secondary theme of the aviation hangars and their setting**

#### ***Structural Engineering***

The engineering design and construction of the hangars successfully combines the materials, methods, and men available during that collision of wartime need and Great Depression shortage. Each hangar is constructed from a kit of modular structural members, connectors, and sheathing. The kit allowed for design customization for available space and modification for changing functions. The reliance on wood rather than steel for building long-span aviation hangars reminds us that metals were prioritized for fabricating ships, tanks, planes, and weapons.

Southwest Airways may have used the prefabricated modular systems for hangars and buildings they developed and operated at the three sister training airfields. In 1942, the AAC aviation hangars around the Valley were among buildings having the largest footprints and longest clear spans (100 feet) at that time. Further comparative research is needed.

#### ***Architecture***

The original set of three nearly intact aviation hangars of Thunderbird 2 Airfield are a rare surviving example of Southwest Airways Civilian Pilot Training schools program structures in the Salt River Valley.

Spatially, the site plan pattern of the *original* Southwest Airways facility with its component areas and buildings not only possessed the distinctive functional characteristics of WWII military aviation facilities but also a uniquely artistic composition of buildings as seen from the air. The geometric symmetry and proportions of the AAC campus carried through to the flightline's headquarters, control tower, and three hangars. Southwest Airways commissioned nationally renowned artist/designer Millard Sheets to design artistic site plans of T1 (an Anasazi thunderbird), T2 (a Navajo blanket), and Falcon Field (a hollow square in oval). Although the original T2 buildings were demolished when TAA constructed a new campus the underlying symmetrical pattern of T2 airfield and campus is still somewhat evident.

#### **Significance Criterion D: Archaeology**

Found ***not applicable***

### **INTEGRITY AND CONDITION FINDINGS**

In evaluating the eligibility of properties for potential listing on the National Register, it is essential to assess them through the seven aspects of integrity (i.e., location, setting, feeling, design, materials, workmanship, and association). In historic preservation parlance, *integrity* refers to the ability of a property to convey its historic significance. A quick intuitive test considers, "if a person associated with the building during its historic period could return today, would that person still recognize the place?"

Integrity must not be confused with *condition* which refers to the level of repair required to return the property, whole or in part, to service or completeness. Obviously, if the condition level is very low, then integrity may be affected. If a remodeled old building has lost essential character-defining elements, it may have good condition but poor integrity. Historic designation eligibility evaluation relies primarily on integrity rather than on condition.

#### **AVIATION HANGARS**

When our preliminary assessment of the military and industrial buildings had eliminated those less than 50 years and those not associated with historically significant WWII flight training and postwar veteran education, our efforts for evaluating architectural significance and integrity focused on the hangars.

Even after 80 years of service and modifications for many users, the hangars retain their essential characteristics of architectural form and structural systems that convey their significance for WWII cadet flight training and postwar veteran vocational education. The two original hangars within the project area survive with moderate effects on integrity of materials by post-1953 additions and in-kind sheathing of the original exterior materials.

#### **Character-defining Elements** – highlighted in *italics*

*The building form* of the iconic military aviation hangar is still seen in the shape of the *high barrel-vault roof* of the aircraft area *flanked by low shed-roofs* of the support spaces. The *shape* and *symmetry* of the end façades still express the original full-width operation of *rolling door panels*. The side facades retain their *clerestory ribbon windows*. The exposed *modular structural system* of *heavy timber posts* and clear-span wood *bowstring trusses* dominate the *voluminous interior space*.

## Location

The two aviation hangars of the project site stand in their original locations and relative spatial relationships with each other, to the third hangar, and to the open spaces of the associated flightline. The important axis of alignment and original layout of these military buildings in their setting remains unchanged. Although visually effected by surrounding modern development, the hangars retain their locational relationship to the landscaped educational/residential campus (buildings replaced by TAA) and to the airport runway (now Scottsdale Municipal Airport). Still in their original location, the hangars convey their historic association with aviation and the origins of the adjacent Scottsdale Airport.

## Setting

The most significant setting for the two hangars on the project site is the original flightline area that they shared historically with the third hangar on TAA property. Although the open space survives, the original character of the hangar aprons and flightline has been physically impacted by imposition of modern improvements for grading, drainage, paving, and roads. The homogeneous continuity of the historic flightline's oiled gravel area has been visually confused by modern road and drainage structures.

The formal symmetry and balance of original airfield site planning has been affected by the demolition of the Control Tower (C-1) and Headquarters Building (C-5), and by the construction of the TAA Laundry Building (B-1) and Laundry Loading Dock (C-2). Fenced storage yards of salvaged architectural features atop the sites of the missing Control Tower (C-1) and Headquarters Building (C-5) further obscure the perception of original order and symmetry.

Although the TAA industrial buildings north of the East Hangar (H-1) seem to crowd it, they do not step far into the taxiway area or effect of the airfield site plan symmetry. The secondary façade of the East Hangar has been visually affected by the clutter of modern open storage sheds (R-3, R-4, R-5), Furniture Loading Dock (C-3), and storage yards with Prefabricated Buildings (B-2, W-4, W-5). The random scattering of building materials, cargo containers, construction vehicles to the east effect the setting for the East Hangar's secondary façade. The East Hangar's setting of its tertiary south façade has been little effected by the tall Open Shed (R-1).

## Design

The character-defining barrel-vault forms of the hangars are readily evident from all sides except the north elevation of the East Hangar. The exterior of the East Hangar (H-1) is obscured on its tertiary north façade by the adjacent Fabrication Building (W-1). The mirror-image south façade remains visible and little affected by a small addition and adjacent Open Shed (R-1).

The character-defining barrel-vaulted interior volume and the wood bowstring truss-and-post structure remain intact and visible at each hangar. The flanking one-story row of rooms on the south side of the East Hangar that originally served as offices and storage has been remodeled with modern office interiors that do not adversely affect the character or meaning of the whole building.

The giant rolling hangar doors survive but are fixed in place to serve as end walls. The bypass panels have been removed from their tracks and set end to end in a single flush plane that looks like an exposed stud wall. They have been left partly open or have been cut to install modern, manageable roll-up garage doors. The original tracks and wheels can be seen at most door panels.

## Materials

Although the original exterior wall metal siding of the hangars has been sheathed in-kind with modern painted metal siding panels, the overall character of the exterior remains little effected. It is common to find at historic buildings that original siding or roofing was not removed but simply re-covered for purposes of expedient weatherproofing or aesthetic enhancement. Limited observation of the interior side of the walls suggests that areas of historic metal siding attached to wood boards may still exist beneath modern metal siding. Further limited dismantling is needed to investigate the conditions and extent of possible surviving exterior wall finishes. In general, casual observation suggests that very little character-defining material and few exterior features (e.g., doors and windows) have been removed from the hangars in the 80-year course of remodeling projects. Rather, the historic buildings seem to be encapsulated relatively intact.

## Workmanship

A remarkable character-defining aspect of the hangars is the obvious workmanship and evident labor organization in the rapid assembly of these giant prefabricated kit-of-parts structures. Close observation reveals how simple in construction yet complex in concept these efficient modular-system hangars are. Close inspection of structural members and connections proves there was no waste in materials or motion when constructing the hangars. Diligent workmanlike efforts are evident in the integrity of the details.

## Feeling

The remarkable size, scale, symmetry, and atmosphere of the building, especially evident when first stepping inside the hangar, may convey to some emotions associated with the imagery of WWII. Even the structure and materials of the hangar interior speak to its wartime vintage. Minor changes to the interiors are lost in the overwhelming volume of the space defined in one-point perspective by the strong, orderly patterns of the roof structure.

## Association

The buildings operate by Southwest Airways and site features of the property are historically significant to Scottsdale for their association with the Southwest Airways Civilian Pilot Training schools program (1942-1944) and the Arizona State College Vocational School (1947-1953) for residents and returning veterans assisted by their GI Bill benefits. Thunderbird 2 Airfield, through the Southwest Airways Civilian Pilot Training schools program, is directly associated with three sister-city airfields across the Valley, i.e., Glendale Thunderbird 1 Airfield, Mesa Falcon Field, and Phoenix Sky Harbor Airport.

The “multiplier factor” of effects on Scottsdale and Metro Phoenix of the T2 Airfield and of the ASC Vocational School is evident in the growth and development of the community. Countless families in the Valley can trace their family histories and attribute their residency here through the wartime and peacetime experiences at the Thunderbird 2 and other Southwest Airways facilities. Many local people working at the facilities during the 1940s and 1950s, became political and business leaders in Scottsdale as it grew exponentially with veterans returning to start families and careers.

## INVENTORY LIST

### Military and Industrial Buildings at the Proposed **SCOTTSDALE & THUNDERBIRD INDUSTRIAL PROJECT SITE**

7401-7501 East Redfield Road – Scottsdale, AZ 85260

Findings: T2 Airfield is historically significant locally as WWII AAC cadet pilot training facility (1942-1944) and as a post-war Arizona State College Vocational School (1947-1953) for veterans through the GI Bill

<i>Inv. No.</i>	<i>Original Use or Building Type</i>	<i>Current Use</i>	<i>Original User</i>	<i>Construction Date or Range*</i>	<i>Architectural Style</i>	<i>CHPO Significance Opinion/Reason</i>
B-1	school laundry	archt office	TAA	1960-1976	Modern	NS/S
B-2	prefab enclosed bldg	storage	tenant	2008-2011	Industrial	NS/A
C-1	aviation control tower	demolished	AAC	1942	n/a	NS/I
C-2	loading dock w/ramp	loading dock	TAA	1960-1976	No style	NS/S
C-3	loading dock w/ramp	loading dock	TAA	1979-1986	No style	NS/S
C-4	school bus fuel station	demolished	TAA	1979-1986	n/a	NS/I
C-5	airfield headquarters	demolished	AAC	1942	n/a	NS/I
H-1	aviation hangar	tile fabr/metal	AAC	1942	Indl/Aviation	LS
H-2	aviation hangar	storage/maint	AAC	1942	Indl/Aviation	LS
H-3	aviation hangar	countertop fabr	AAC	1942	Indl/Aviation	LS
R-1	open shed	vacant	TAA	1969-1976	No style	NS/S
R-2	open shed	storage	TAA	1969-1976	No style	NS/A
R-3	open shed	tile storage	tenant	2008-2011	No style	NS/A
R-4	open shed	tile storage	tenant	2011-2017	No style	NS/A
R-5	open paired sheds	tile storage	tenant	2011-2017	No style	NS/A
W-1	fabrication/showroom	fabr/showrm	TAA	1953-1961	Vernacular	NS/S
W-2	fabrication/distribution	fabr/distrib	TAA	1979-1986	Industrial	NS/A
W-3	prefab enclosed bldg	workshop	TAA	1979-1986	Industrial	NS/A
W-4	prefab enclosed bldg	equip storage	tenant	2000-2001	Industrial	NS/A
W-5	prefab enclosed bldg	vehicle storage	tenant	2001-2008	Industrial	NS/A

\*Where documentary verification of construction dates is not possible, the series of aerial photographs provide a range of time.

#### **ABBREVIATIONS**

**AAC** = Army Air Corps; **ASC** = Arizona State College; **T2** = Thunderbird 2 Airfield; **TAA** = Thunderbird Adventist Academy

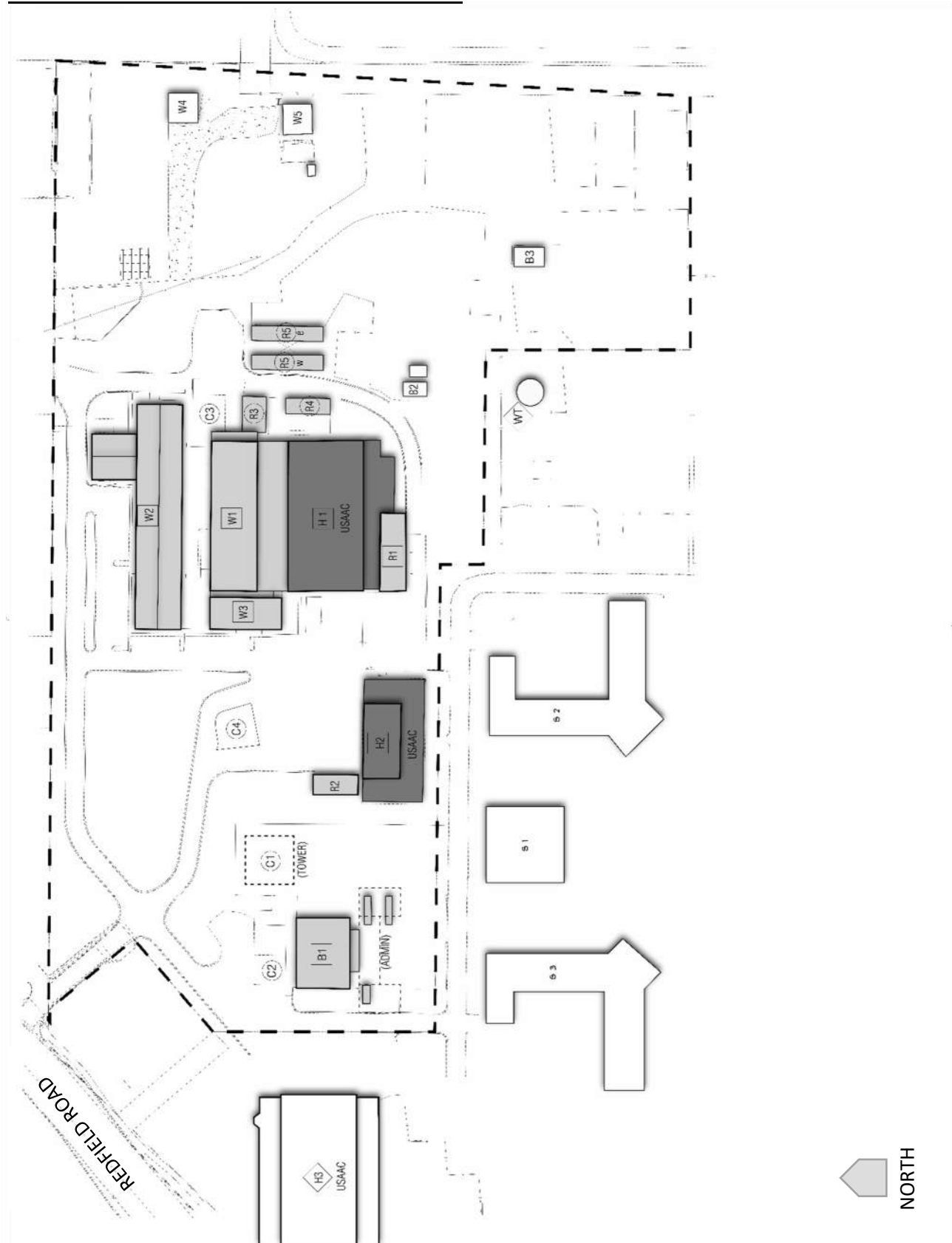
#### **Contributing Resources**

**NR** = National Register listed; **LS** = Likely Significant; **PE** = Possibly Significant; **FS** = Further Study needed; **n/a** = not applicable

#### **Non-contributing Resources**

**NS** = Not Significant due to **NS/A** = Age Insufficient; **NS/S** = Significance Inadequate; **NS/I** = Integrity Loss; **SE/R** = Recoverable

# HISTORIC RESOURCE SURVEY SITE PLAN



**RYDEN ARCHITECTS, INC.** – Don W. Ryden, AIA © 2022  
 2241 East Mountain View Road – Phoenix, AZ 85028  
 602-253-5381 w – [don@rydenarchitects.com](mailto:don@rydenarchitects.com)

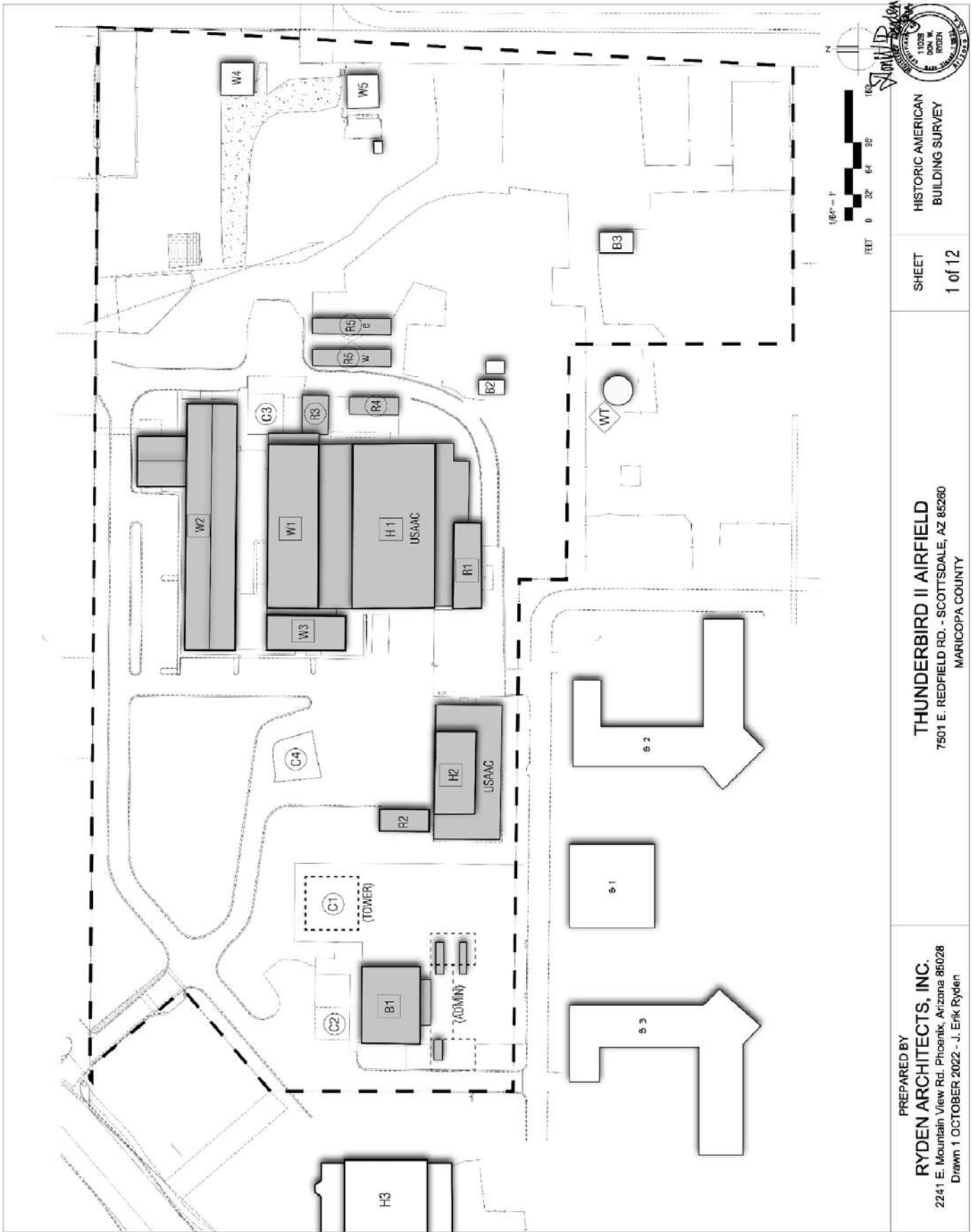
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*Historic Significance Evaluation & Documentation*  
**Buildings at Scottsdale & Thunderbird Industrial Project**  
 TbirdHngrs.EvalSig.Final3.2xi22 – Final ... 2 Nov 2022

## 5. HISTORIC RESOURCE DOCUMENTATION

### AS-FOUND CONDITIONS FLOOR PLANS

1. Site Plan
2. H-1 East Hangar
3. H-2 South Hangar
4. W-1 Workshops Building
5. W-2 Fabrication and Distribution Building
6. W-3 Steel Building
7. B-1 Architect Office Building
8. R-1 Materials Shed
9. R-2 Open shed
10. R-3 East Storage Shed
11. R-4 Open shed
12. R-5 Open paired sheds

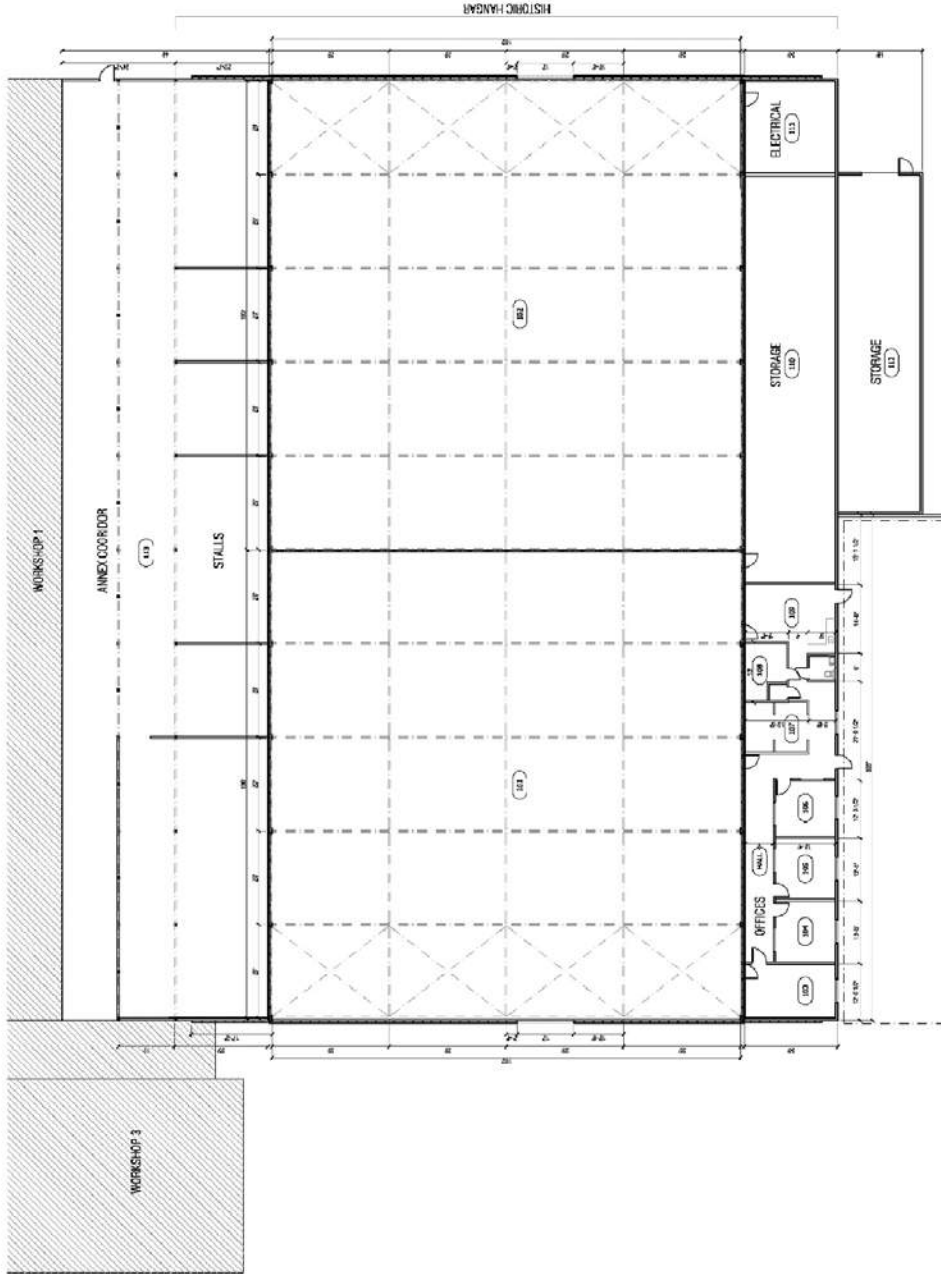


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BUILDING SURVEY

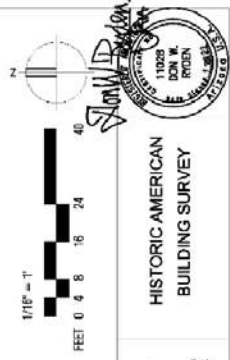
SHEET  
1 of 12

**THUNDERBIRD II AIRFIELD**  
7501 E. REDFIELD RD. - SCOTTSDALE, AZ 85260  
MARICOPA COUNTY

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HANGAR 1 (EAST)  
FLOOR PLAN

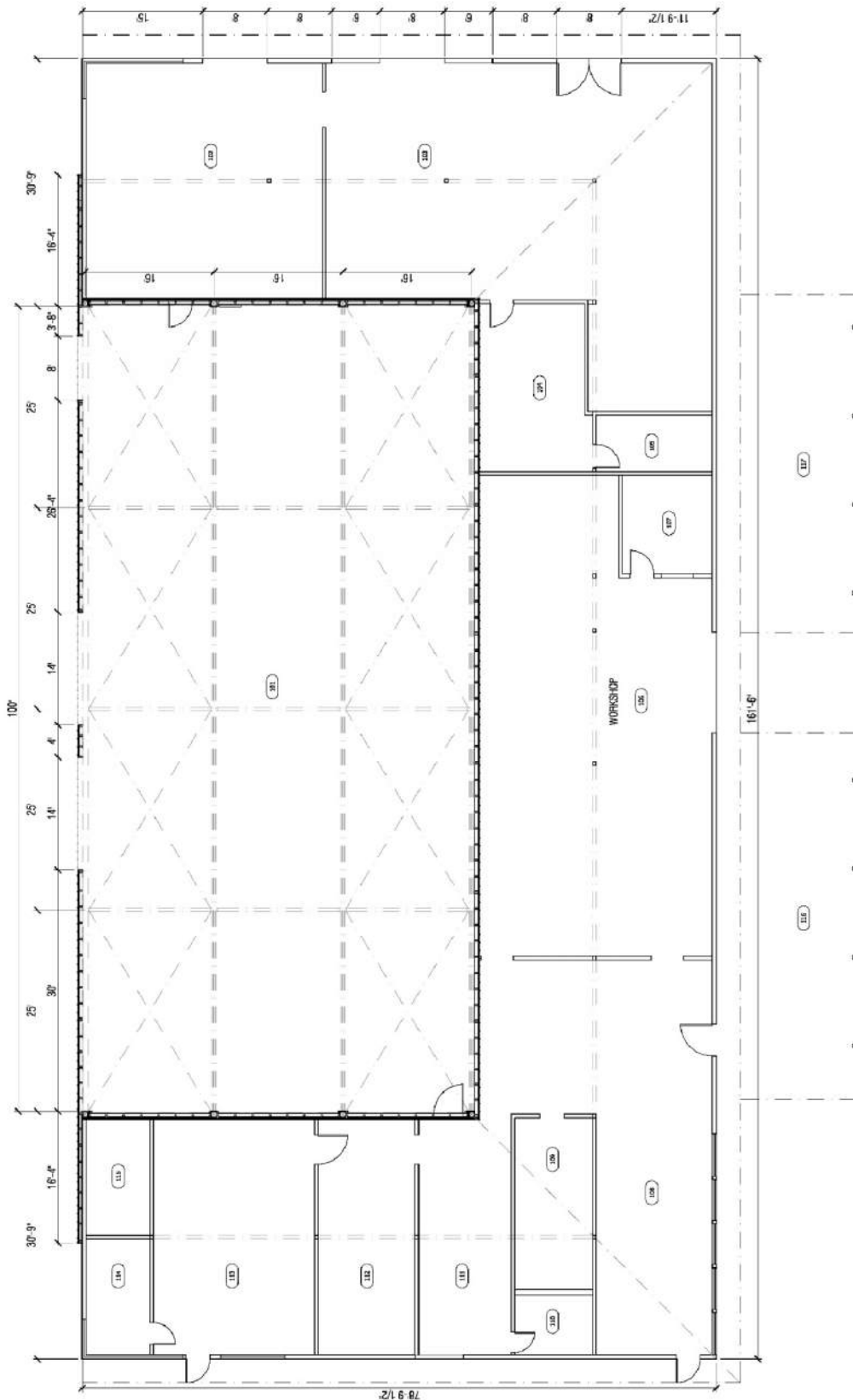


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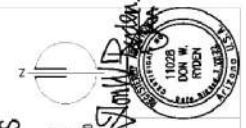
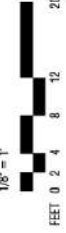
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HANGAR 2 (SOUTH)

FLOOR PLAN

FLOOR PLANS  
1/8" = 1'

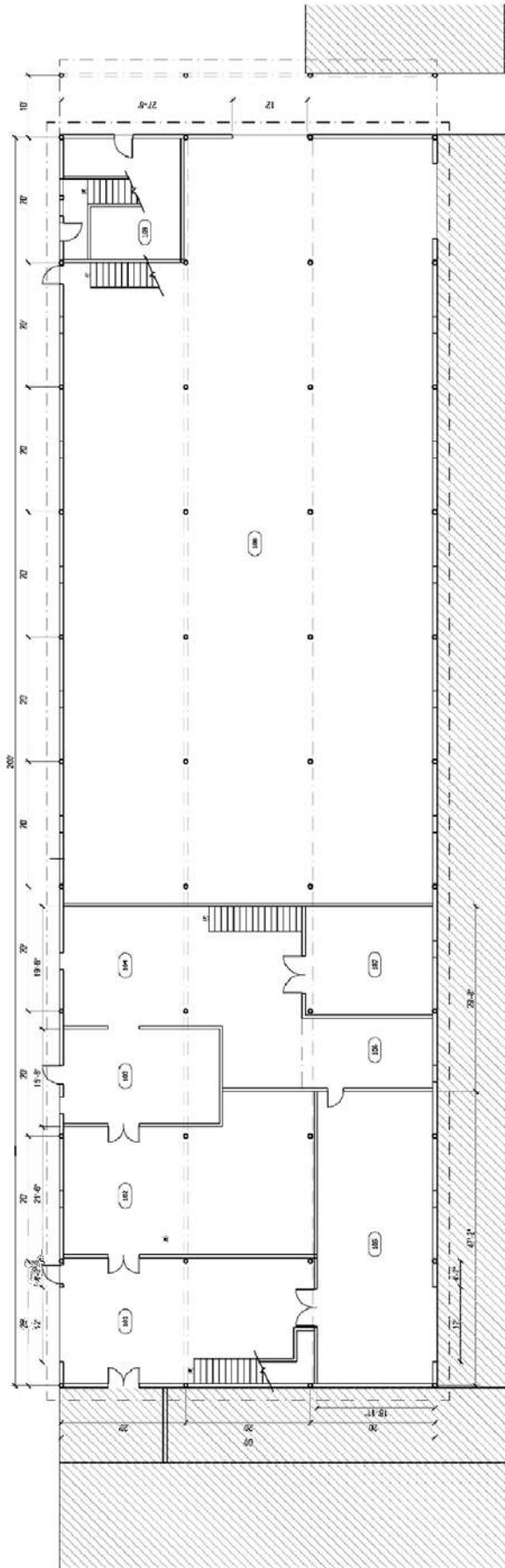


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**WORKSHOP 1**  
FLOOR PLAN

FLOOR PLANS

3/32" = 1'



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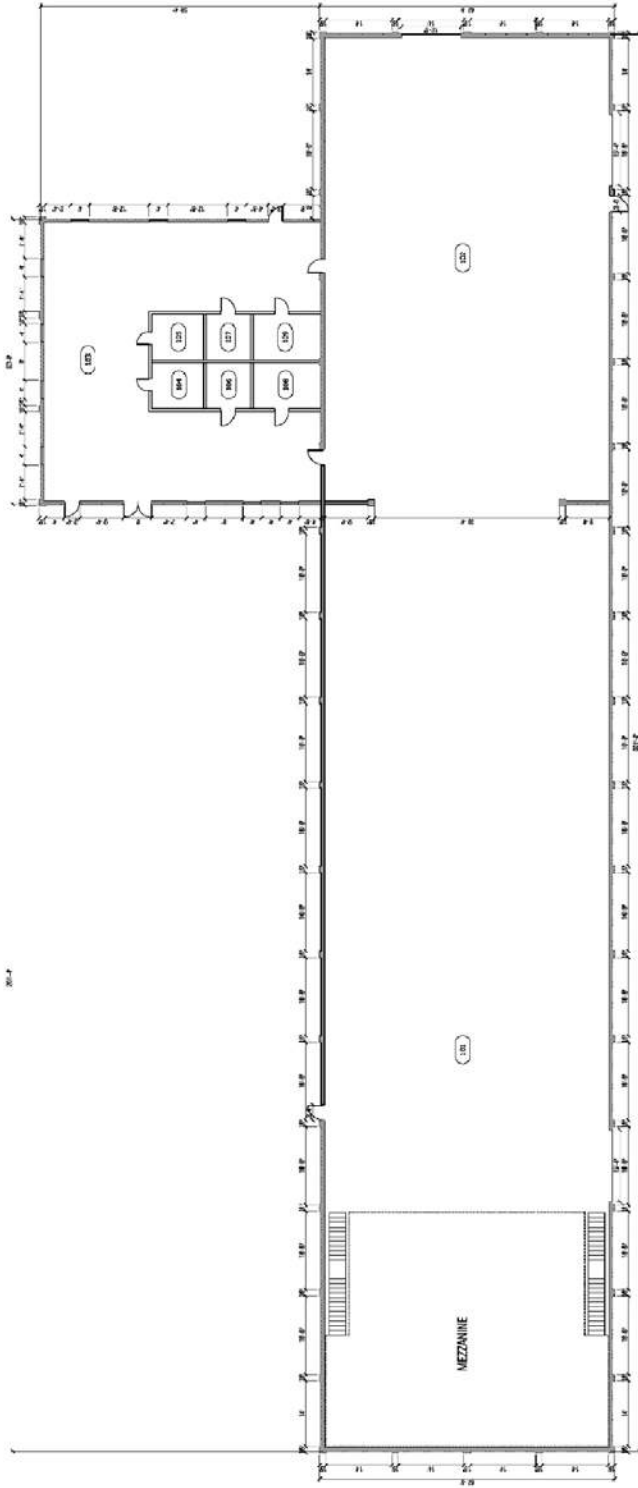


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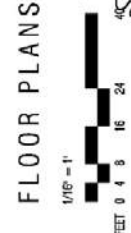
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**WORKSHOP 2**  
FLOOR PLAN

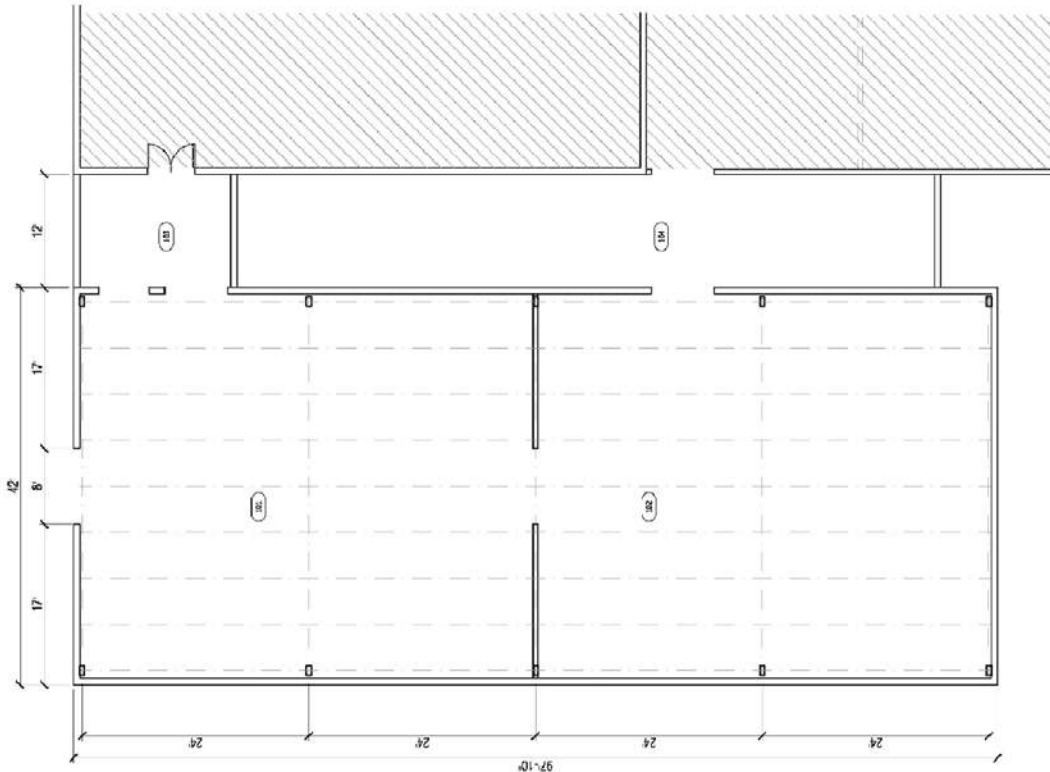


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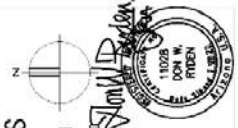
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**WORKSHOP 3**  
FLOOR PLAN

**FLOOR PLANS**  
1/8" = 1'  
FEET 0 2 4 6 8 12



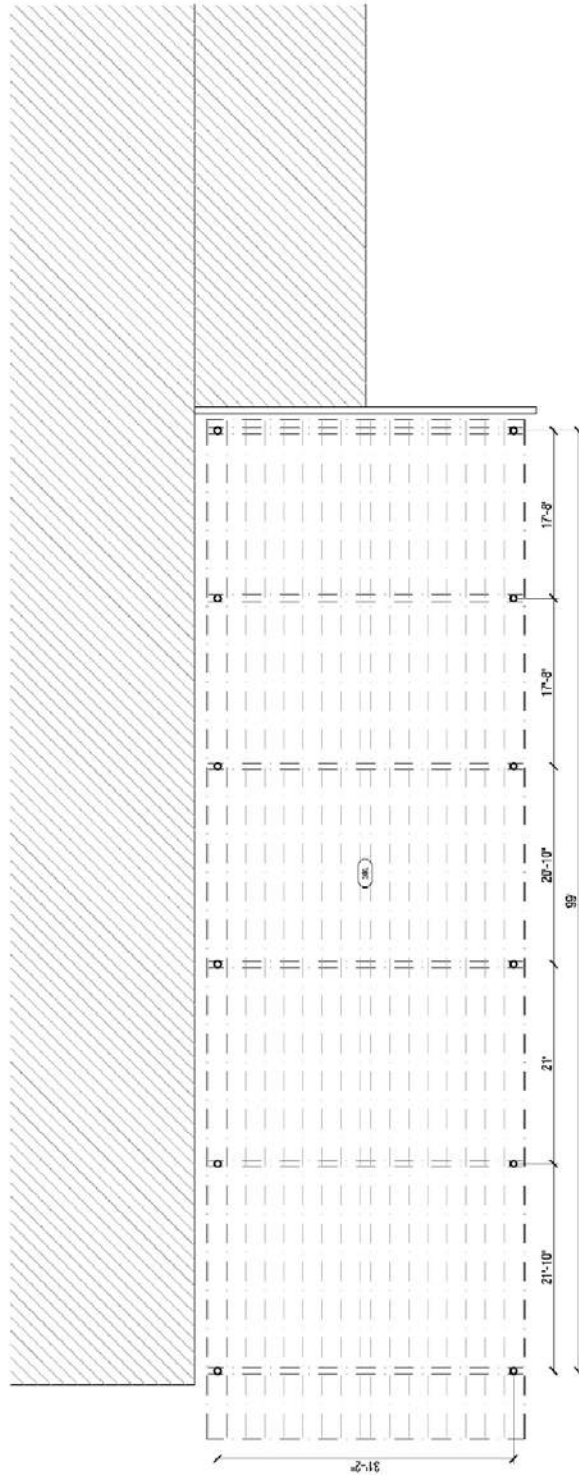
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6 of 12

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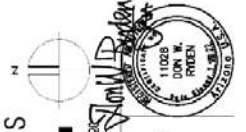
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**RAMADA 1**  
FLOOR PLAN

FLOOR PLANS  
1/8" = 1'

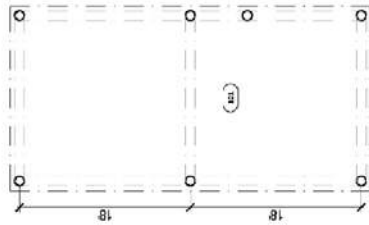


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**RAMADA 2**  
FLOOR PLAN

FLOOR PLANS  
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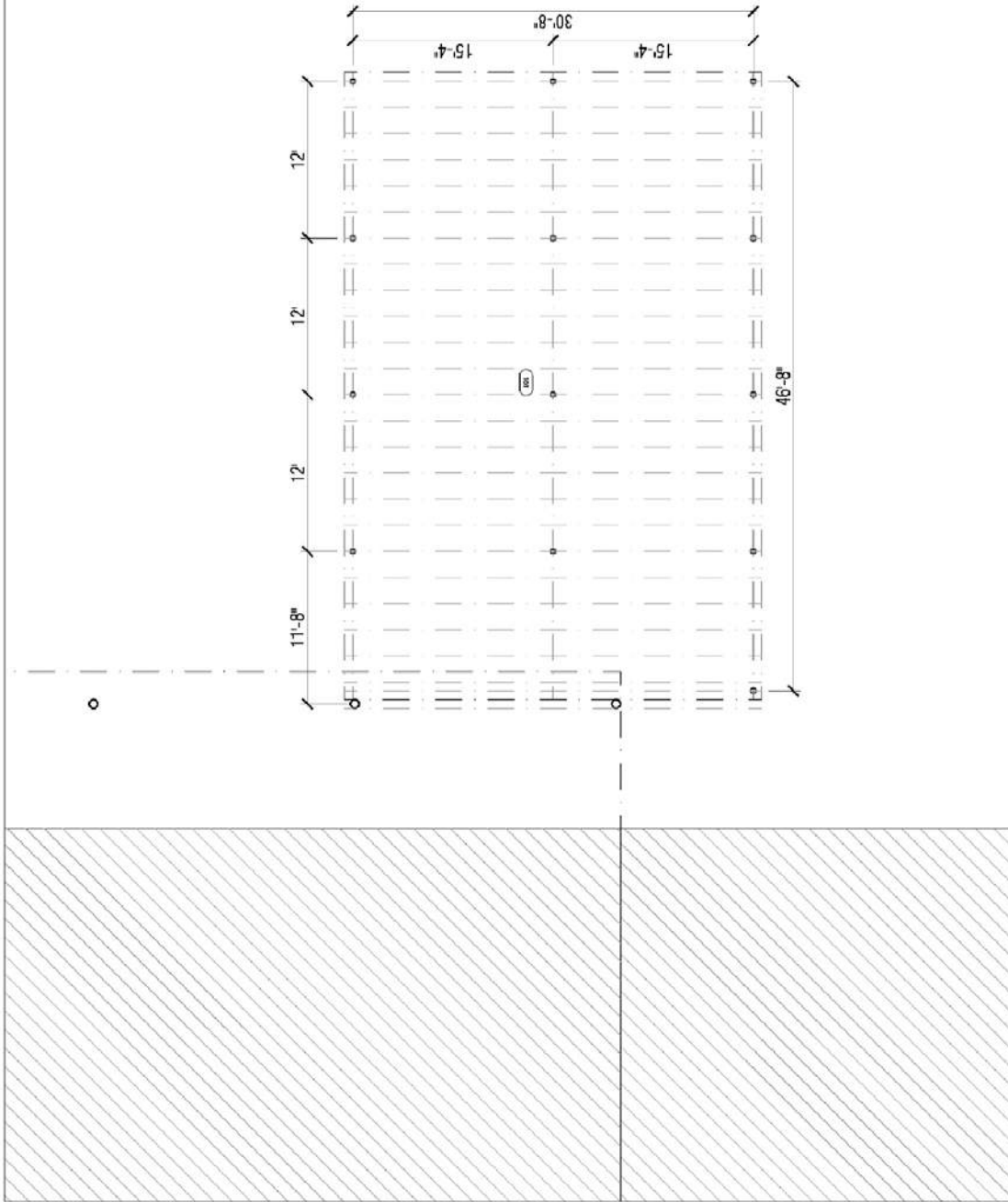


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**RAMADA 3**  
FLOOR PLAN

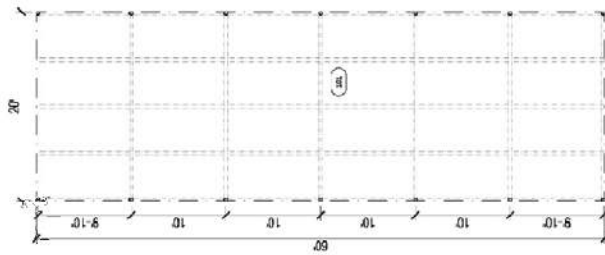
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1/8" = 1'  
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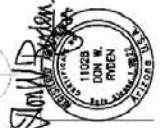
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**RAMADA 4**  
FLOOR PLAN

FLOOR PLANS

1/8" = 1'

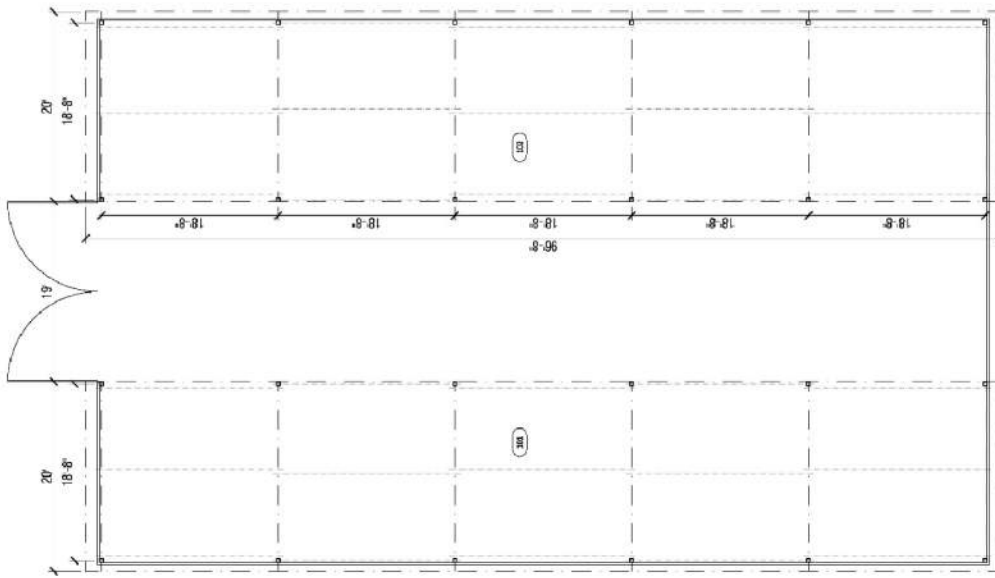


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**RAMADA 5**  
FLOOR PLAN

FLOOR PLANS

1/8" = 1'



HISTORIC AMERICAN  
BUILDING SURVEY

SHEET  
12 of 12

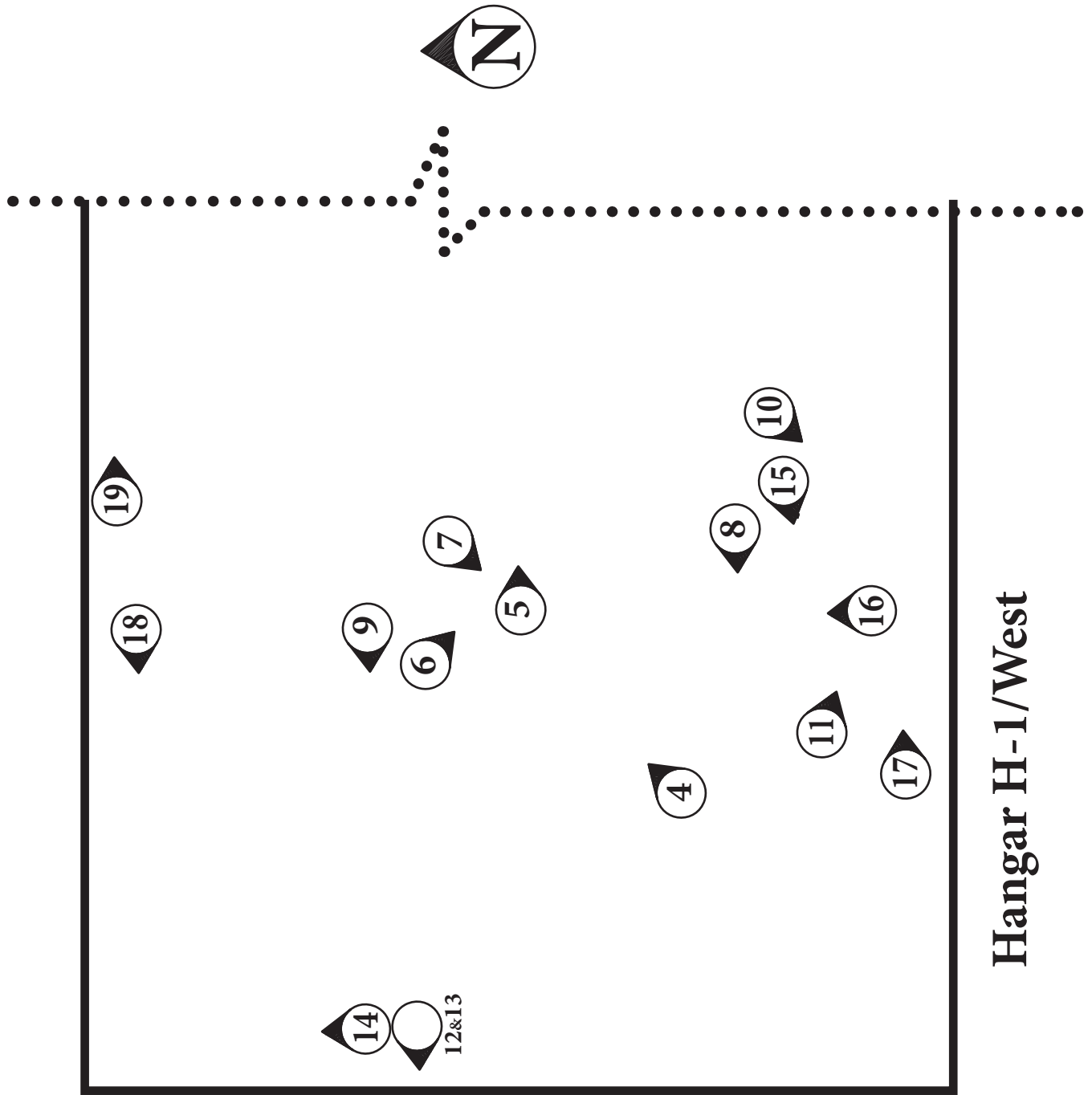
**THUNDERBIRD II AIRFIELD**  
7501 E. REDFIELD RD. - SCOTTSDALE, AZ 85280  
MARICOPA COUNTY

PREPARED BY  
**RYDEN ARCHITECTS, INC.**  
2241 E. Mountain View Rd. Phoenix, Arizona 85028  
Drawn 1 OCTOBER 2022 - J. Erik Ryden

# AS-FOUND CONDITIONS PHOTOGRAPHS – SEPTEMBER 2022

Photography by Larry Mishler Photography





Hangar H-1/West

# HANGAR H-1/WEST



H1.1 (DSC\_7956)

Looking East



H1.2 (DSC\_7696)

Looking Northeast



H1.3 (DSC\_7961)

Looking East



H1.4 (DSC\_7660)

Looking Northeast



H1.5 (DSC\_7650)

Looking East



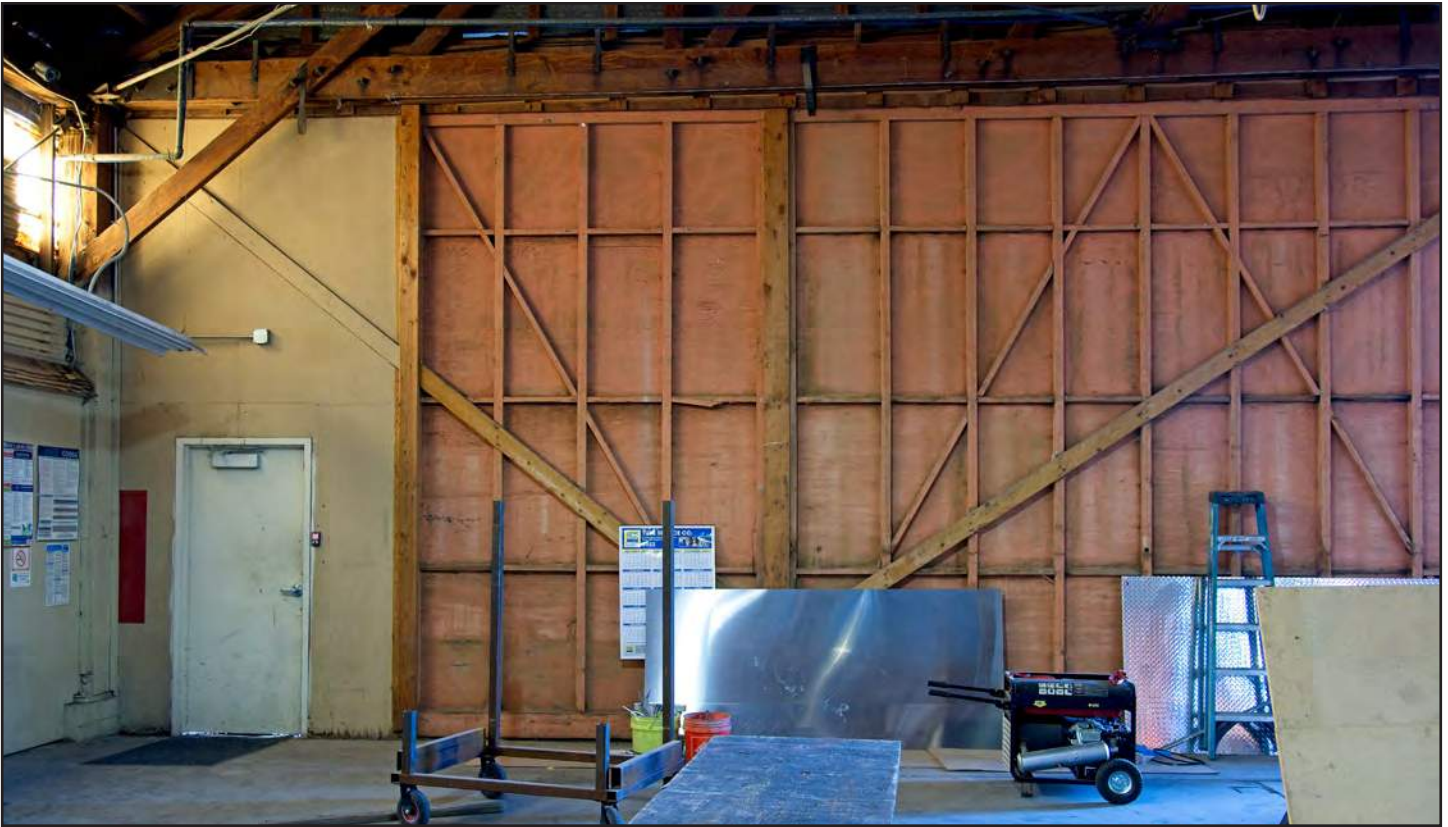
H1.6 (DSC\_7647)

Looking Southeast



H1.7 (DSC\_7648)

Looking Southwest



H1.8 (DSC\_7637)

Looking West



H1.9 (DSC\_7645)

Looking West



H1.10 (DSC\_7630)

Looking Southwest



H1.11 (DSC\_7636)

Looking Southeast



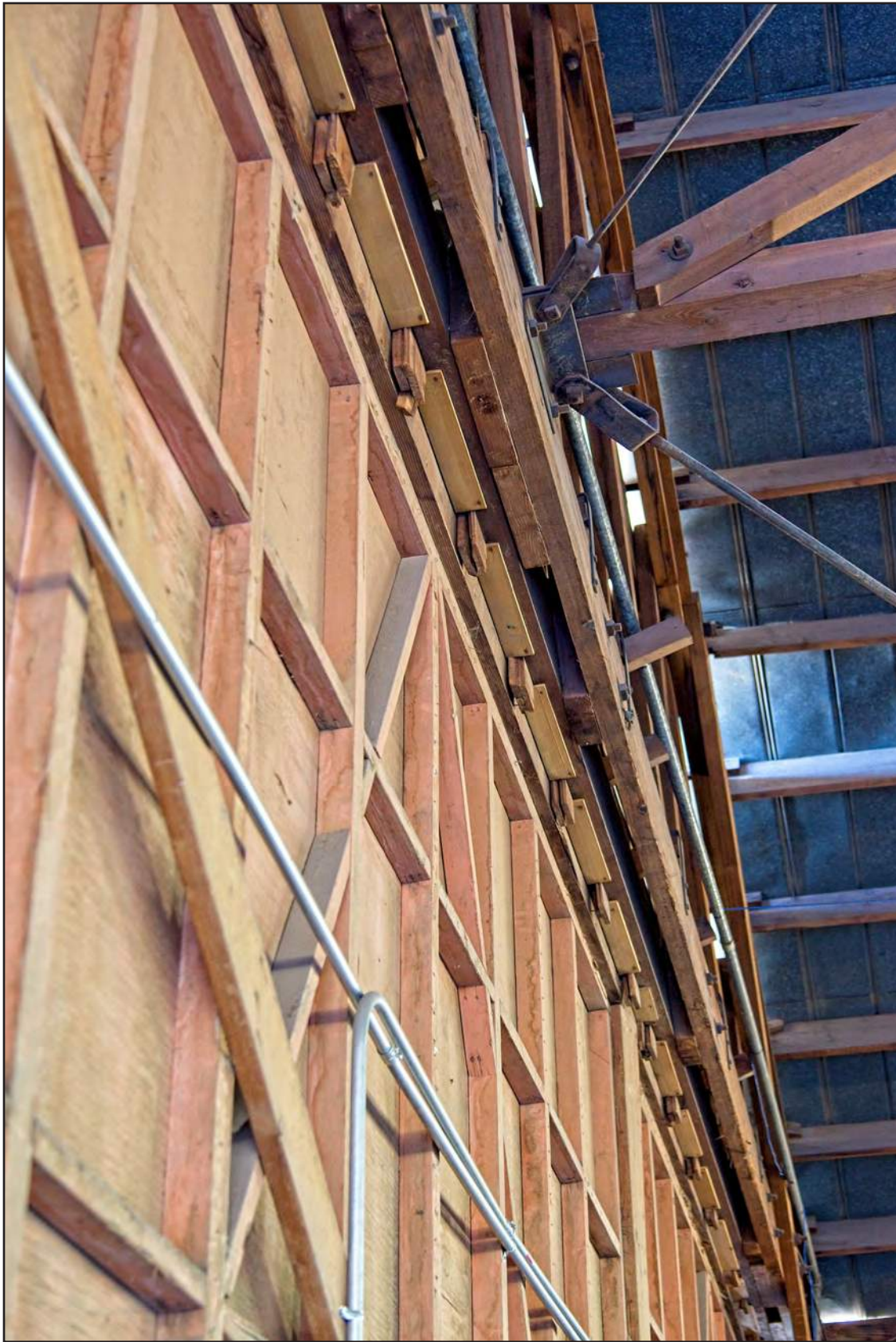
H1.12 (DSC\_7639)

Door Slide Wheel



H1.13 (DSC\_7642)

Detail of Door Slide Wheel



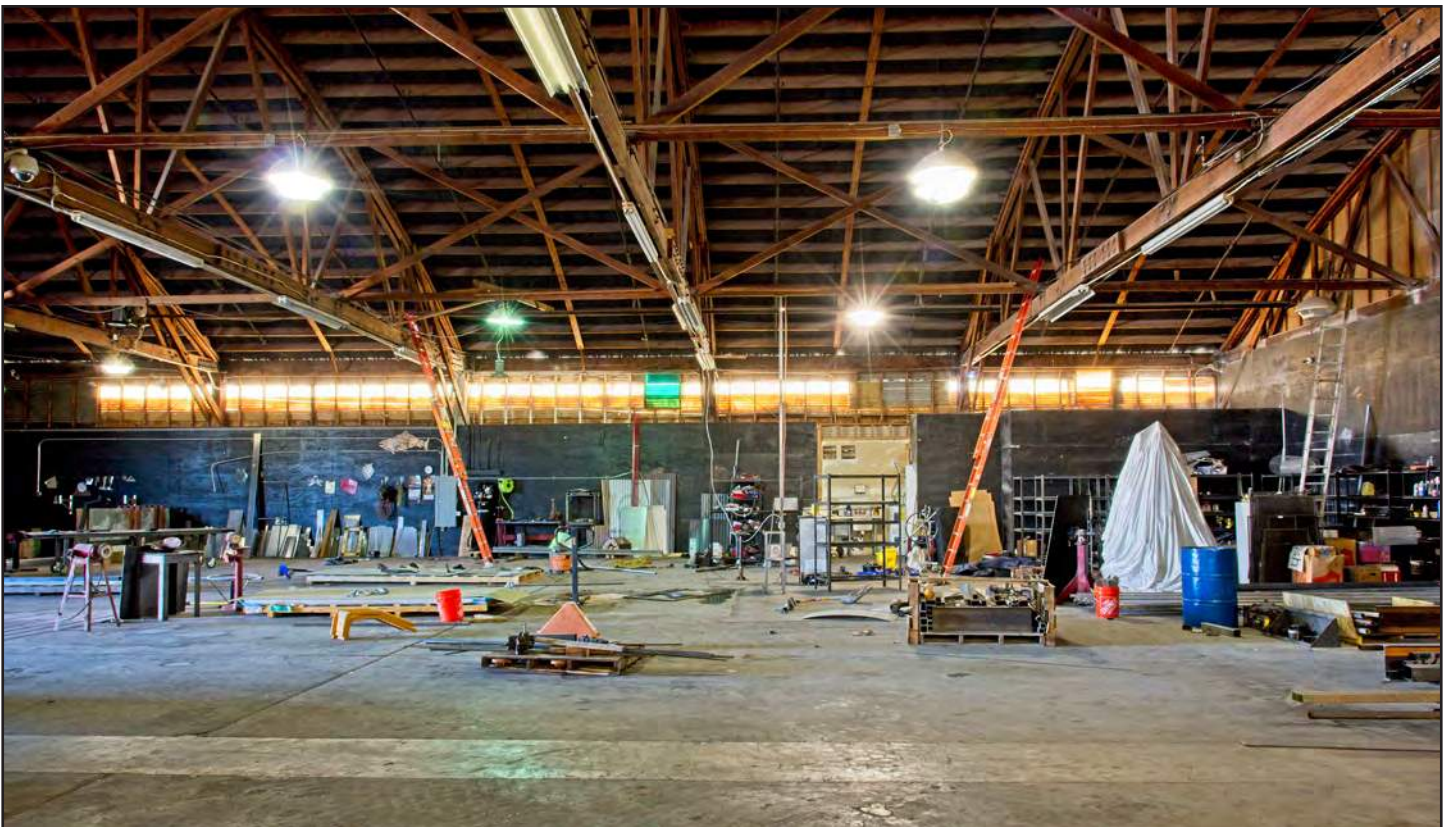
**H1.14 (DSC\_7643)**

**Looking North**



H1.15 (DSC\_7653)

Looking West



H1.16 (DSC\_7657)

Looking North



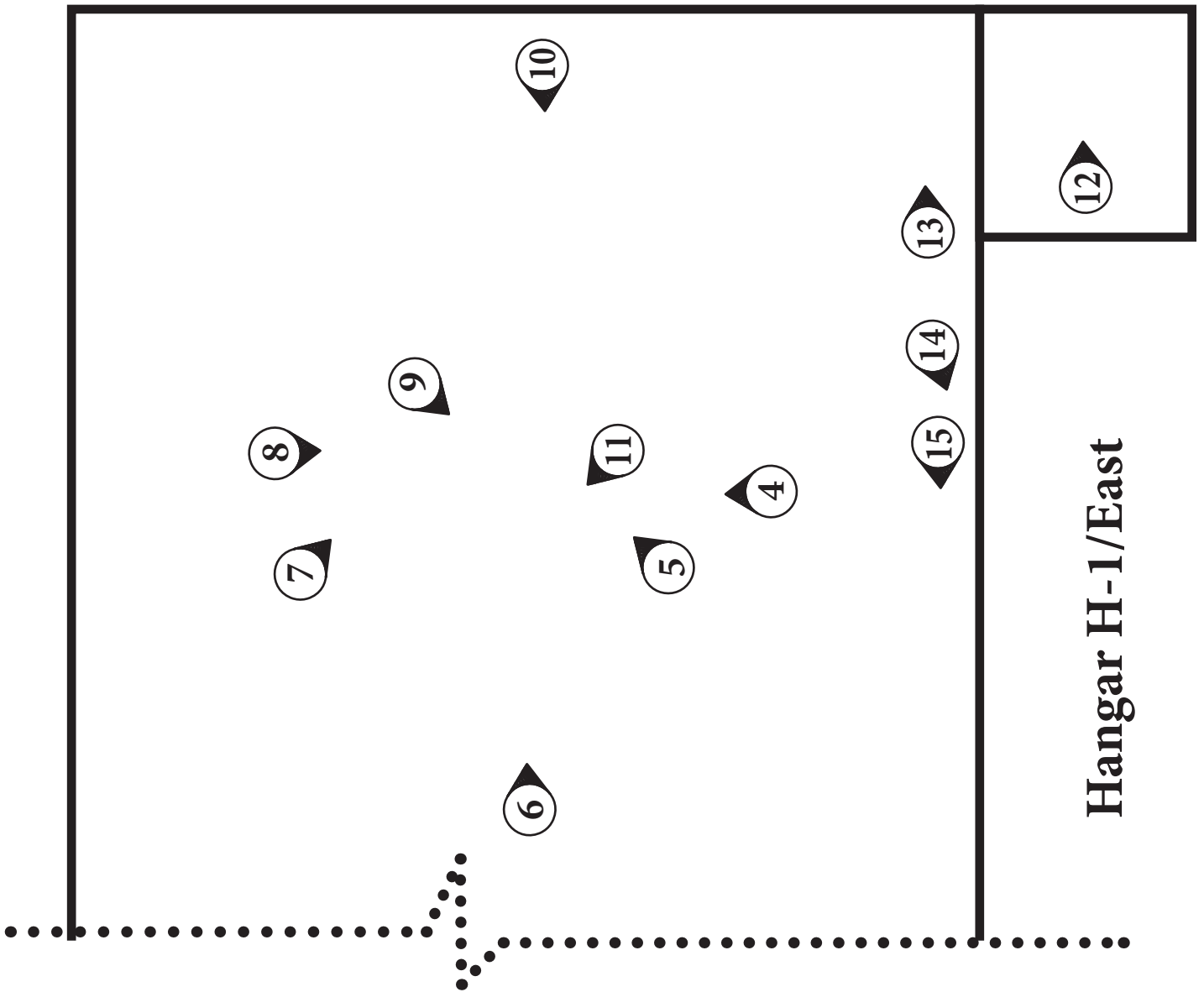
H1.17 (DSC\_7632)

Looking East



H1.18 (DSC\_7625)

Original Humidifier Mounted in Roof Joists



# HANGAR H-1/EAST



H1.21 (DSC\_7994)

Looking West



H1.22 (DSC\_7767)

Looking Northwest



H1.23 (DSC\_7791)

Looking West

### INTERIOR VIEWS - H-1 EAST



H1.24 (DSC\_7880)

Looking North



H1.25 (DSC\_7884)

Looking Northeast



H1.26 (DSC\_7897)

Looking East



H1.27 (DSC\_7890)

Looking Southeast



H1.28 (DSC\_7893)

Looking South



H1.29 (DSC\_7891)

Looking Southwest



H1.30 (DSC\_7899)

Looking West



H1.31 (DSC\_7879)

Looking Northwest



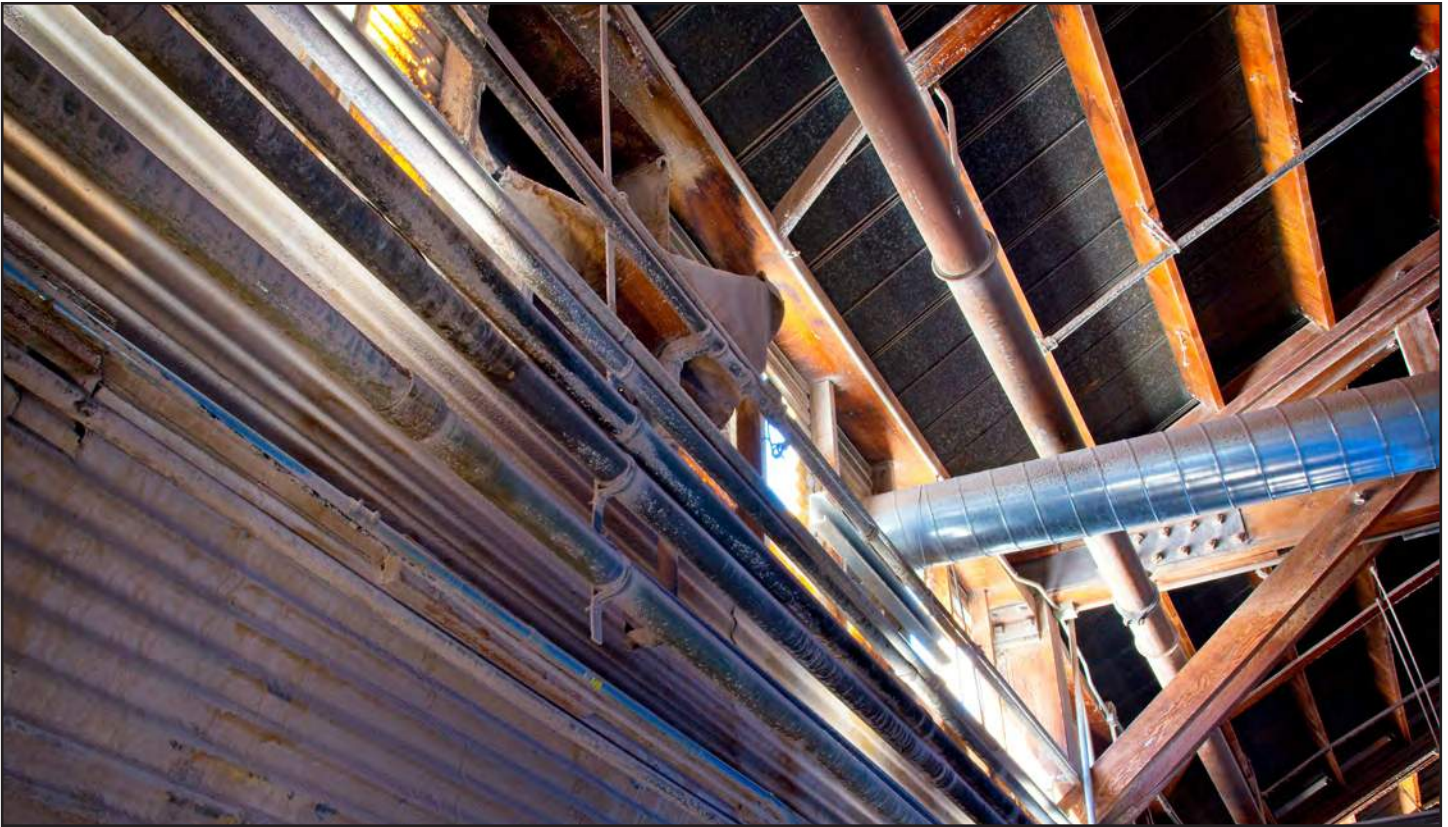
H1.32 (DSC\_7872)

Electrical Room



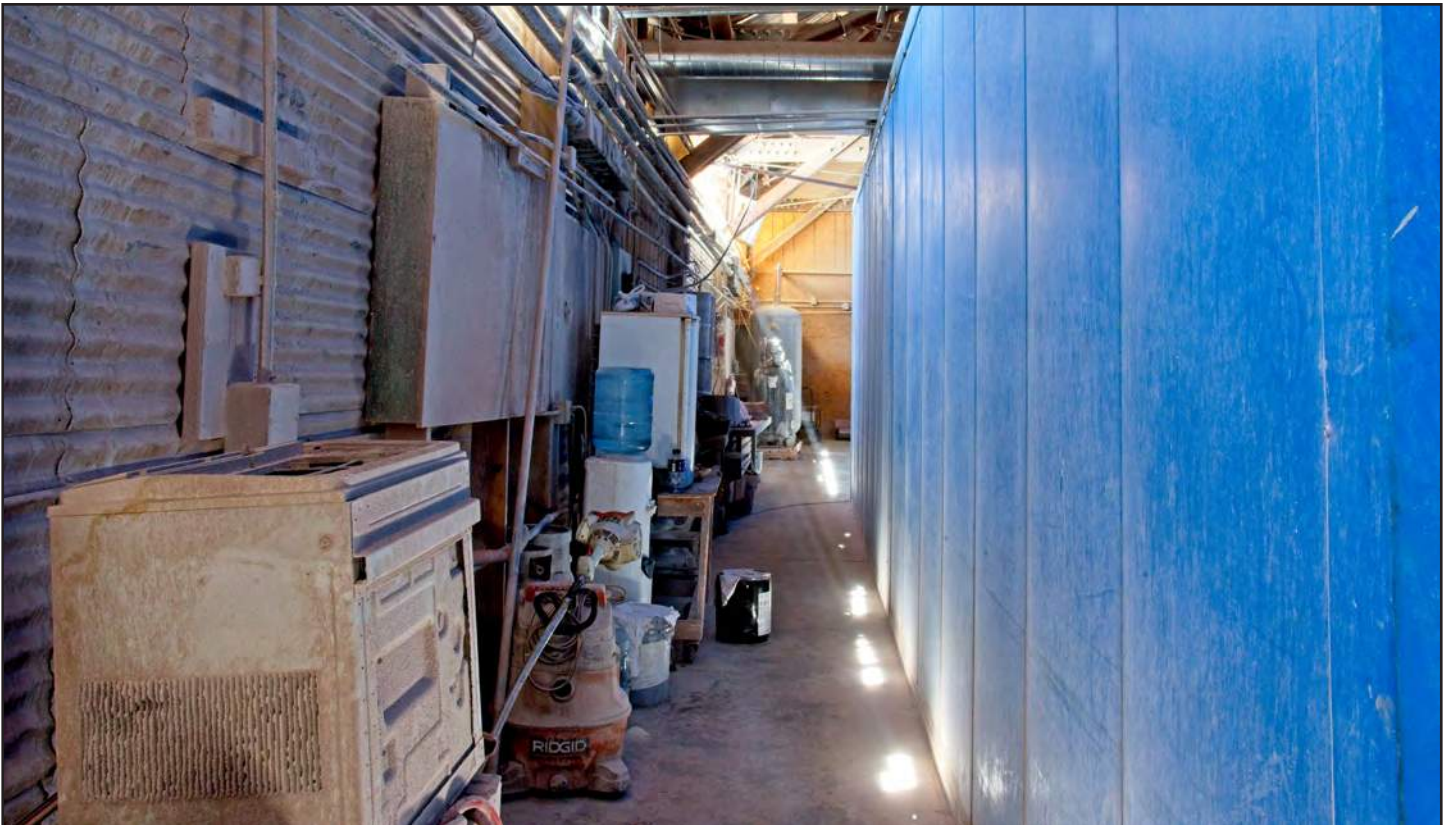
H1.33 (DSC\_7875)

Looking East



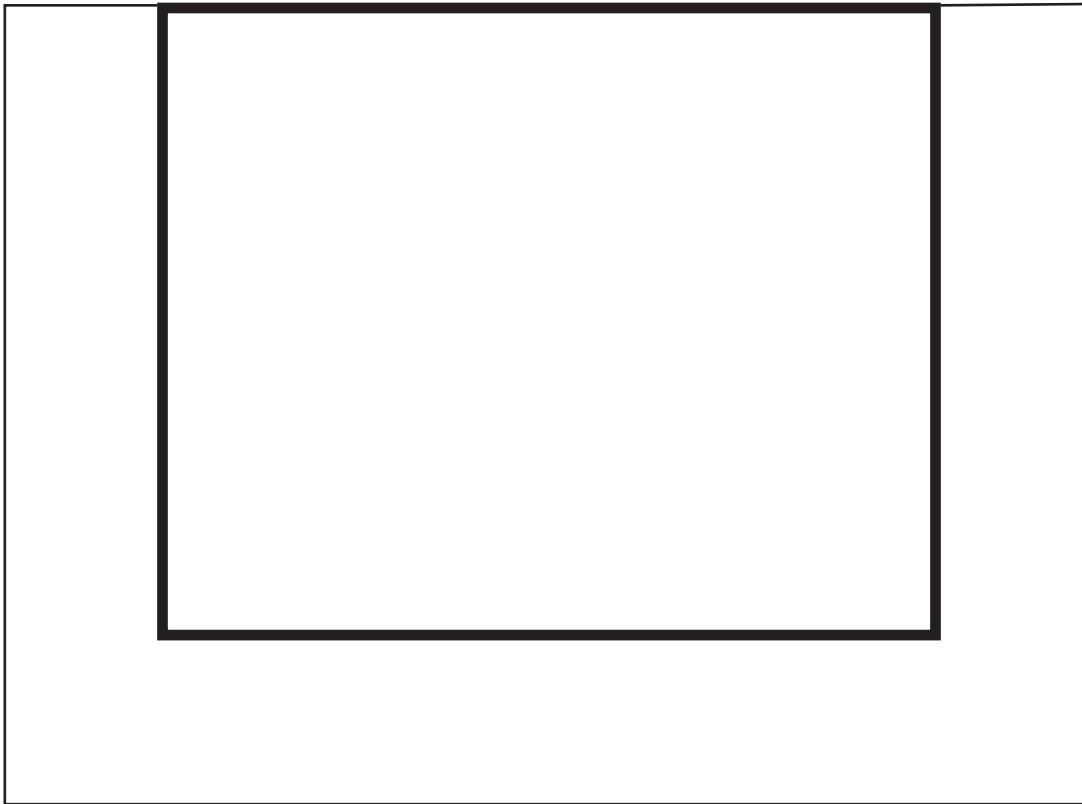
H1.34 (DSC\_7877)

Looking West



H1.35 (DSC\_7878)

Looking West



## Hangar H-2

## HANGAR H-2



H2.1 (DSC\_7728)

Looking South



H2.2 (DSC\_7663)

Looking West



H2.3 (DSC\_7726)

Looking Northwest



H2.4 (DSC\_7727)

Looking Southwest



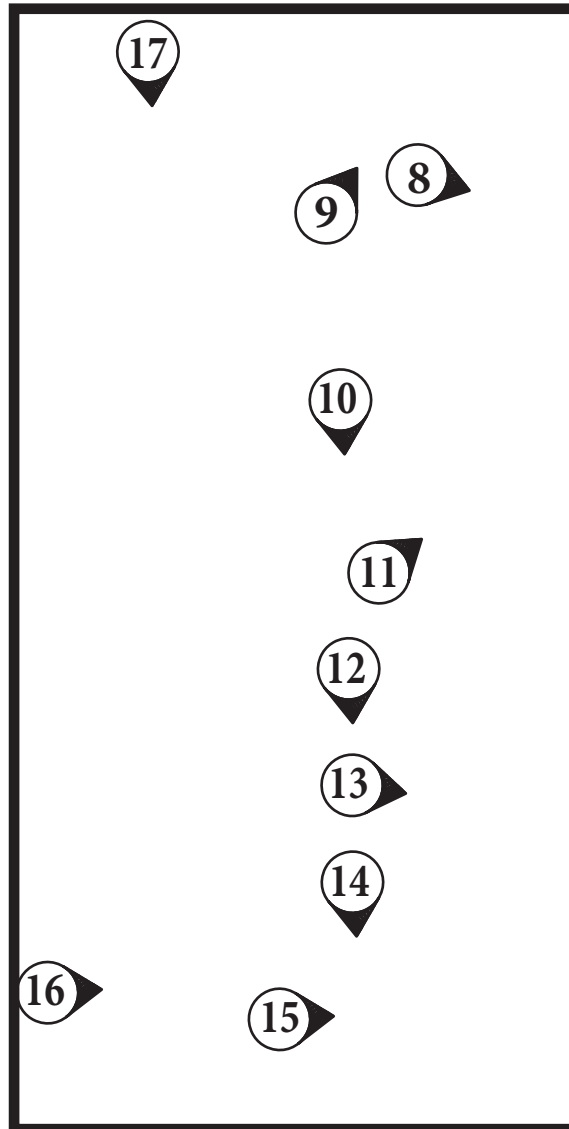
H2.5 (DSC\_7724)

Looking West



H2.6 (DSC\_7960)

Looking South



## Workshop W-1

## WORKSHOP W-1



W1.1 (DSC\_7774)

Looking West



W1.2 (DSC\_8436)

Looking Southeast



W1.3 (DSC\_7942)

Looking Southwest



W1.4 (DSC\_7919)

Looking South



W1.5 (DSC\_8426)

Looking Southeast



W1.6 (DSC\_8429)

Looking East



W1.7 (DSC\_7979)

Looking Southeast



W1.8 (DSC\_7903)

Looking Southwest



W1.9 (DSC\_7671)

Looking Southeast



W1.10 (DSC\_7907)

Looking West



W1.11 (DSC\_7908)

Looking Southeast



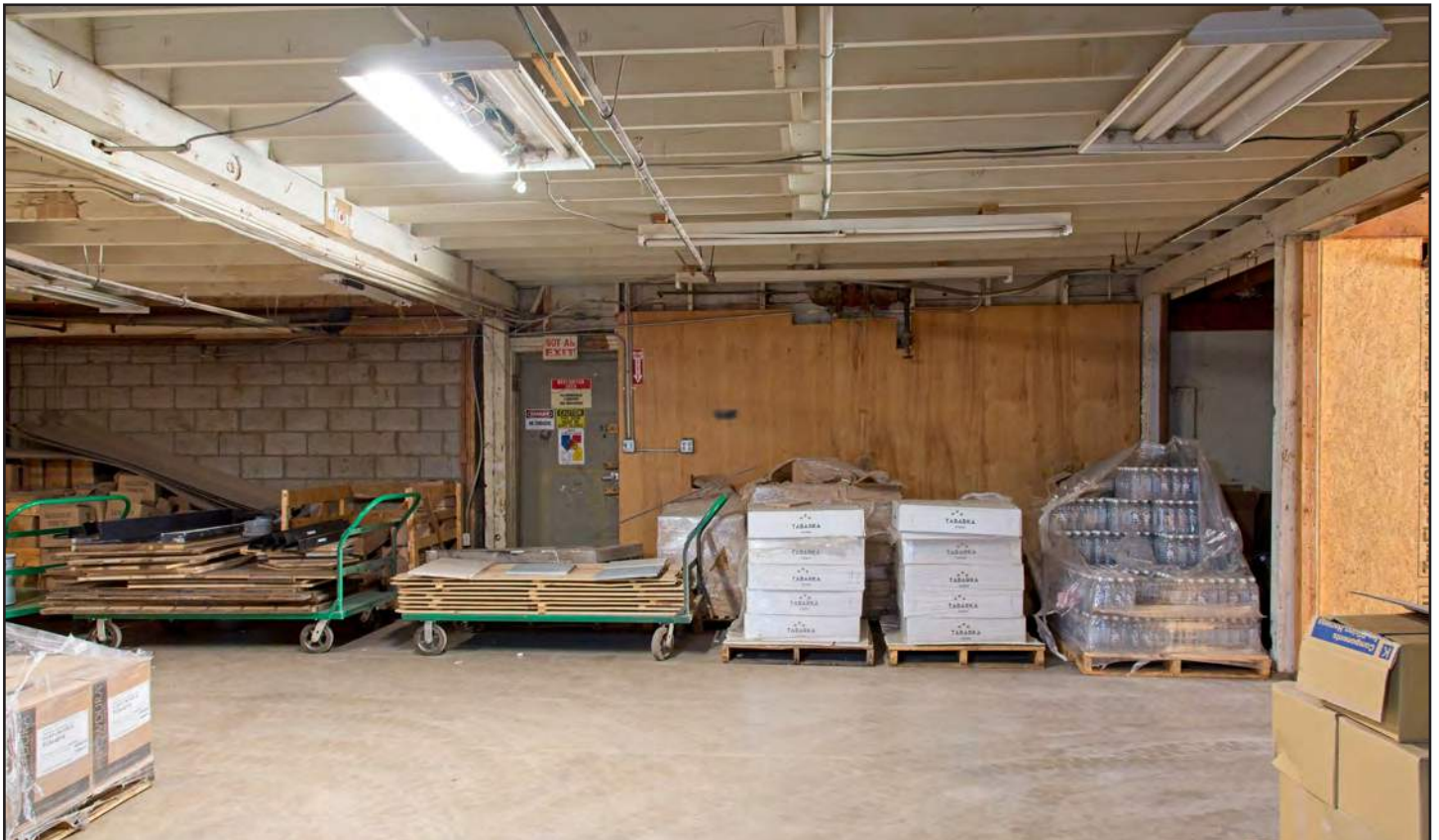
W1.12 (DSC\_7910)

Looking West



W1.13 (DSC\_7913)

Looking South



W1.14 (DSC\_7915)

Looking West



W1. 15 (DSC\_7918)

Looking South



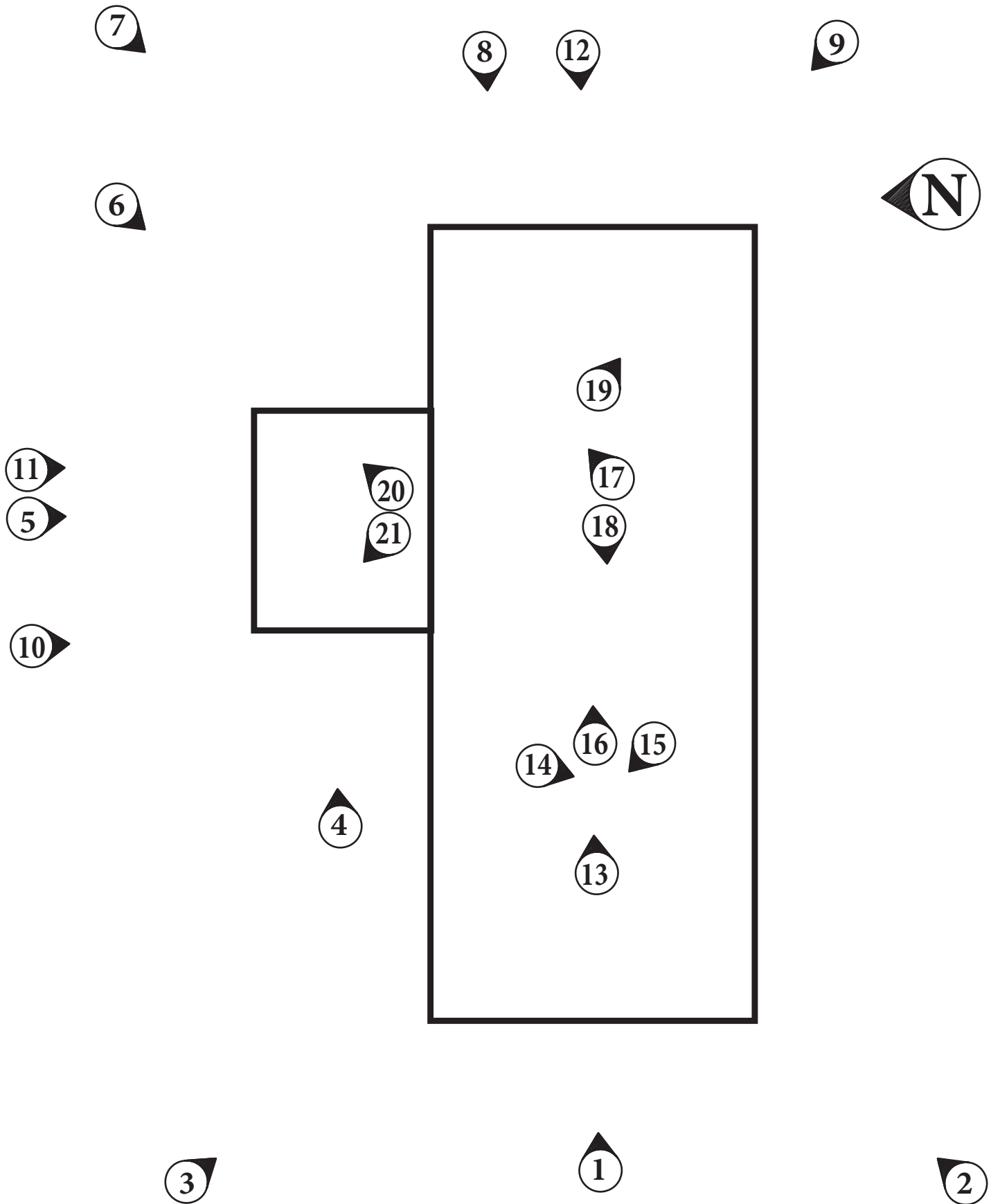
W1.16 (DSC\_7917)

Looking South



W1.17 (DSC\_7906)

Looking West



## Workshop W-2

## WORKSHOP W-2



W2.1 (DSC\_8439)

Looking East



W2.2 (DSC\_7699)

Looking Northeast



W2.3 (DSC\_7702)

Looking Southeast



W2.4 (DSC\_8008)

Looking East



W2.5 (DSC\_7790)

Looking South



W2.6 (DSC\_8442)

Looking Southwest



W2.7 (DSC\_7786)

Looking Southwest.



W2.8 (DSC\_7784)

Looking West



**W2.9 (DSC\_7780)**

**Looking Northwest**



**W2.10 (DSC\_8006)**

**Looking South**



W2.11 (DSC\_7973)

Looking South



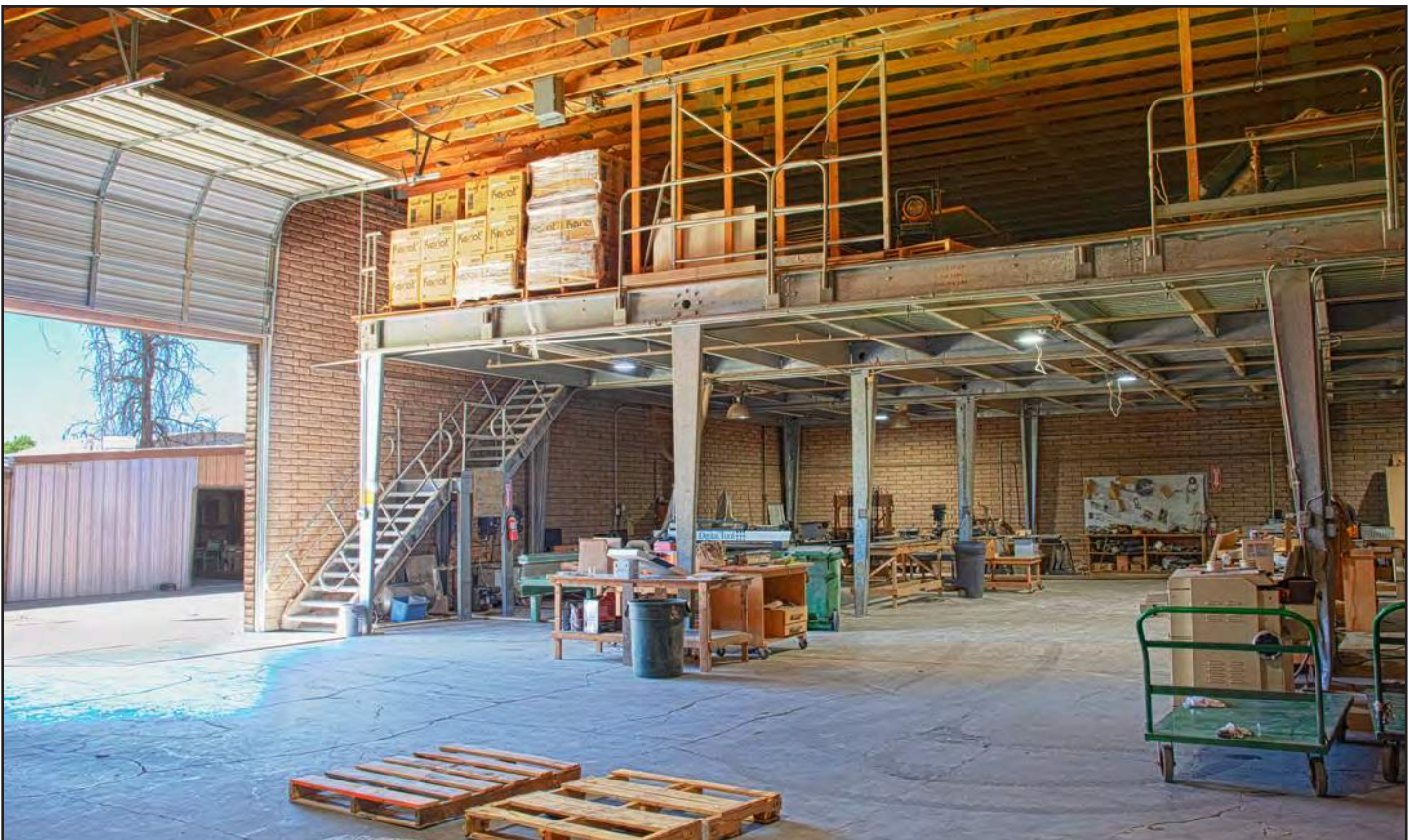
W2.12 (DSC\_7976)

Looking West



W2.13 (DSC\_7927)

Looking East



W2.14 (DSC\_7930)

Looking Southwest



W2.15 (DSC\_7928)

Looking Northwest



W2.16 (DSC\_7924)

Looking East



W2.17 (DSC\_7936)

Looking Northeast



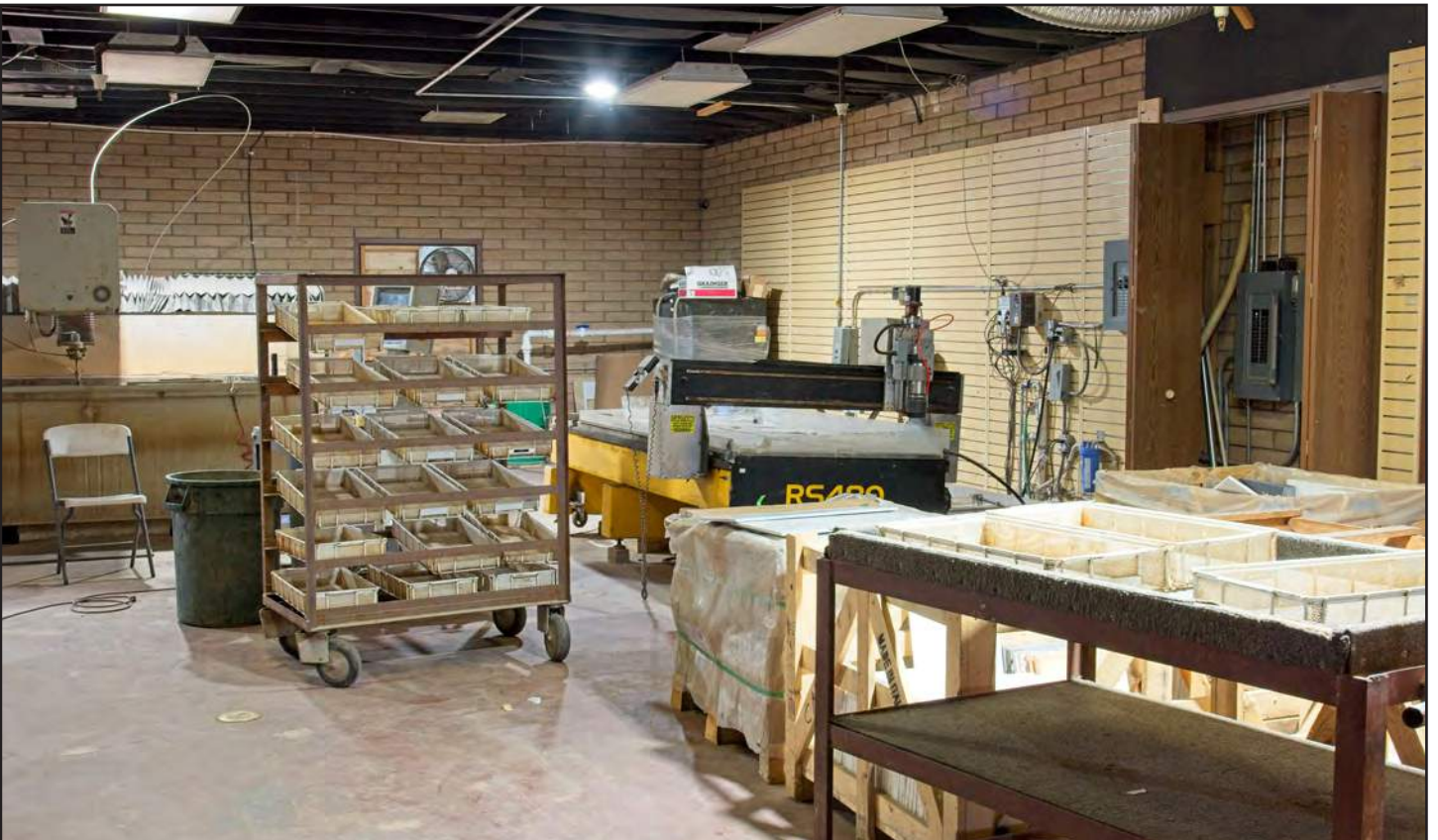
W2.18 (DSC\_7938)

Looking West



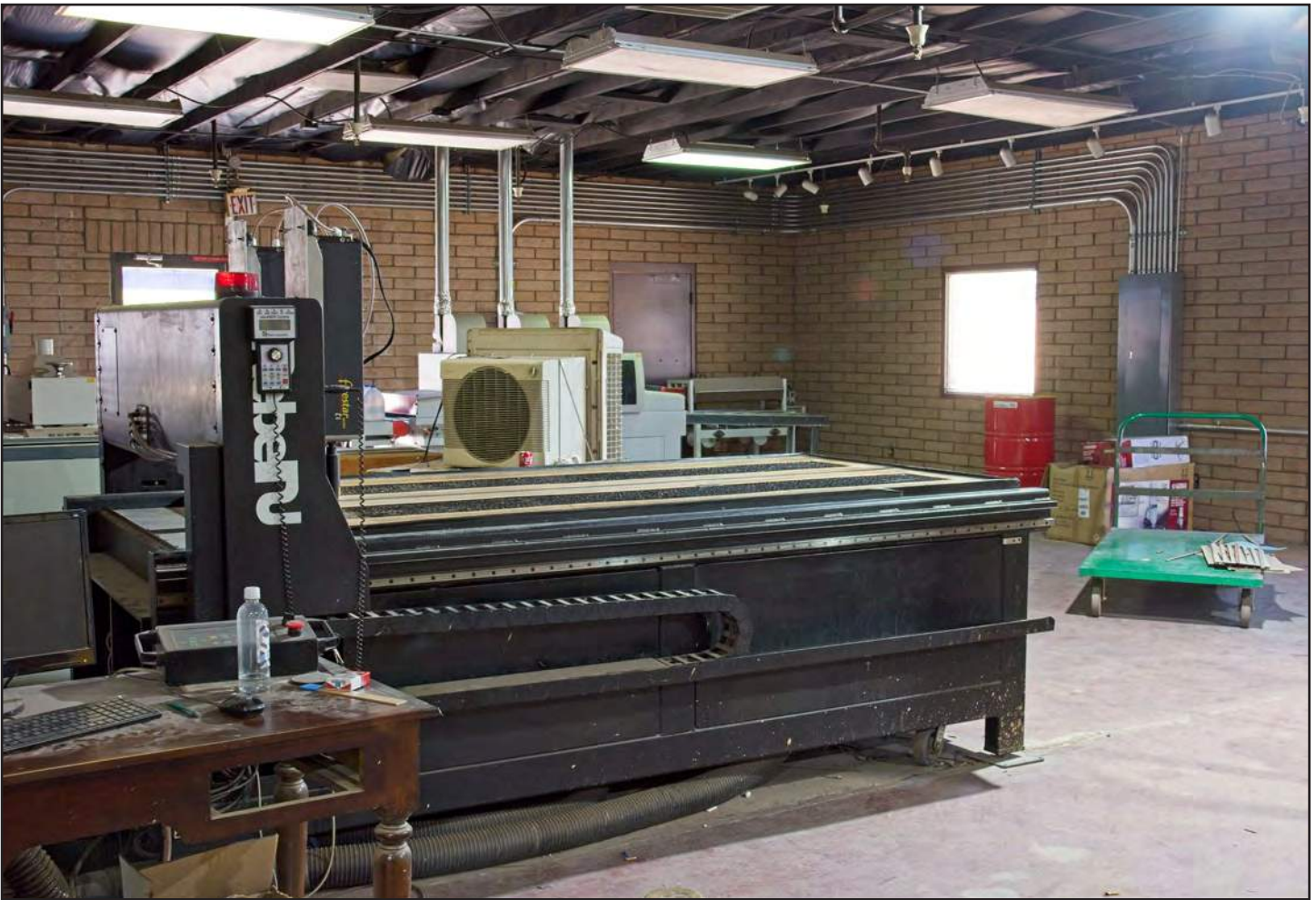
W2.19 (DSC\_7937)

Looking Southeast



W2.20 (DSC\_7940)

North room looking North

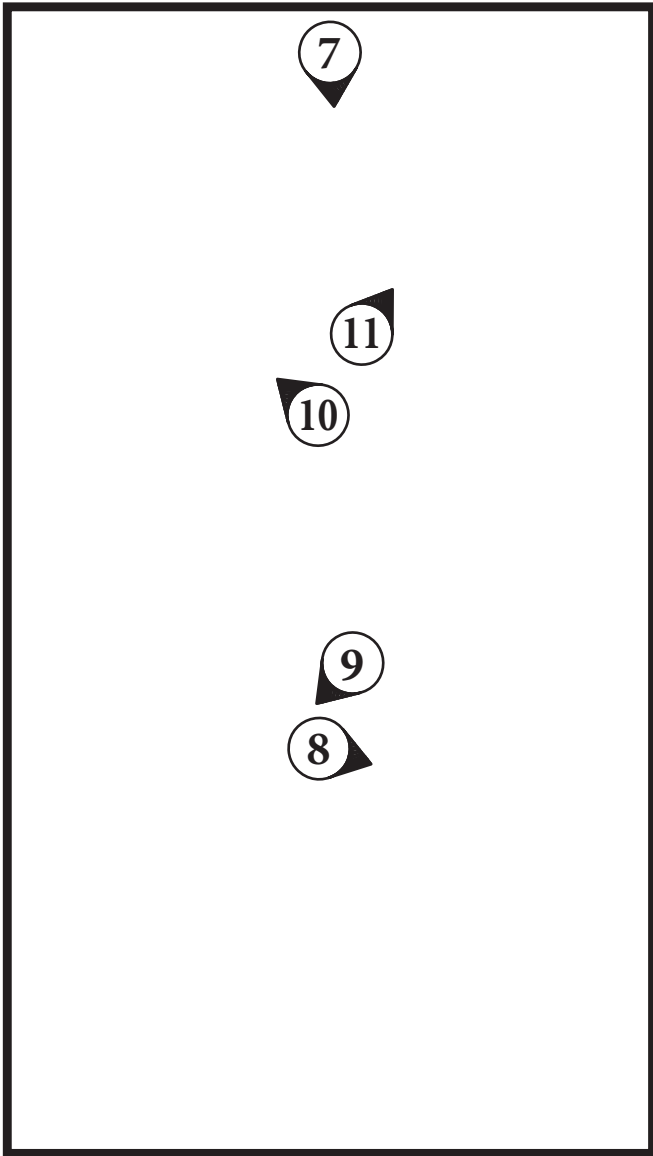


W2.21 (DSC\_7941)

North room looking Northwest

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# Workshop W-3

## WORKSHOP W-3



W3.1 (DSC\_7739)

Looking North



W3.2 (DSC\_7701)

Looking Northeast



W3.3 (DSC\_8421)

Looking East



W3.4 (DSC\_7700)

Looking Southeast

RYDEN ARCHITECTS, INC. – Don W. Ryden, AIA © 2022  
2241 E. Mountain View Rd. – Phoenix, AZ 85028  
602-253-5381 w; don@rydenarchitects.com

Historic Significance Evaluation & Documentation  
Military/Industrial Bldgs. at Thunderbird Adventist Academy  
Final Report . 28 September 2022

[ A2.51 ]



W3.5 (DSC\_7921)

Looking South



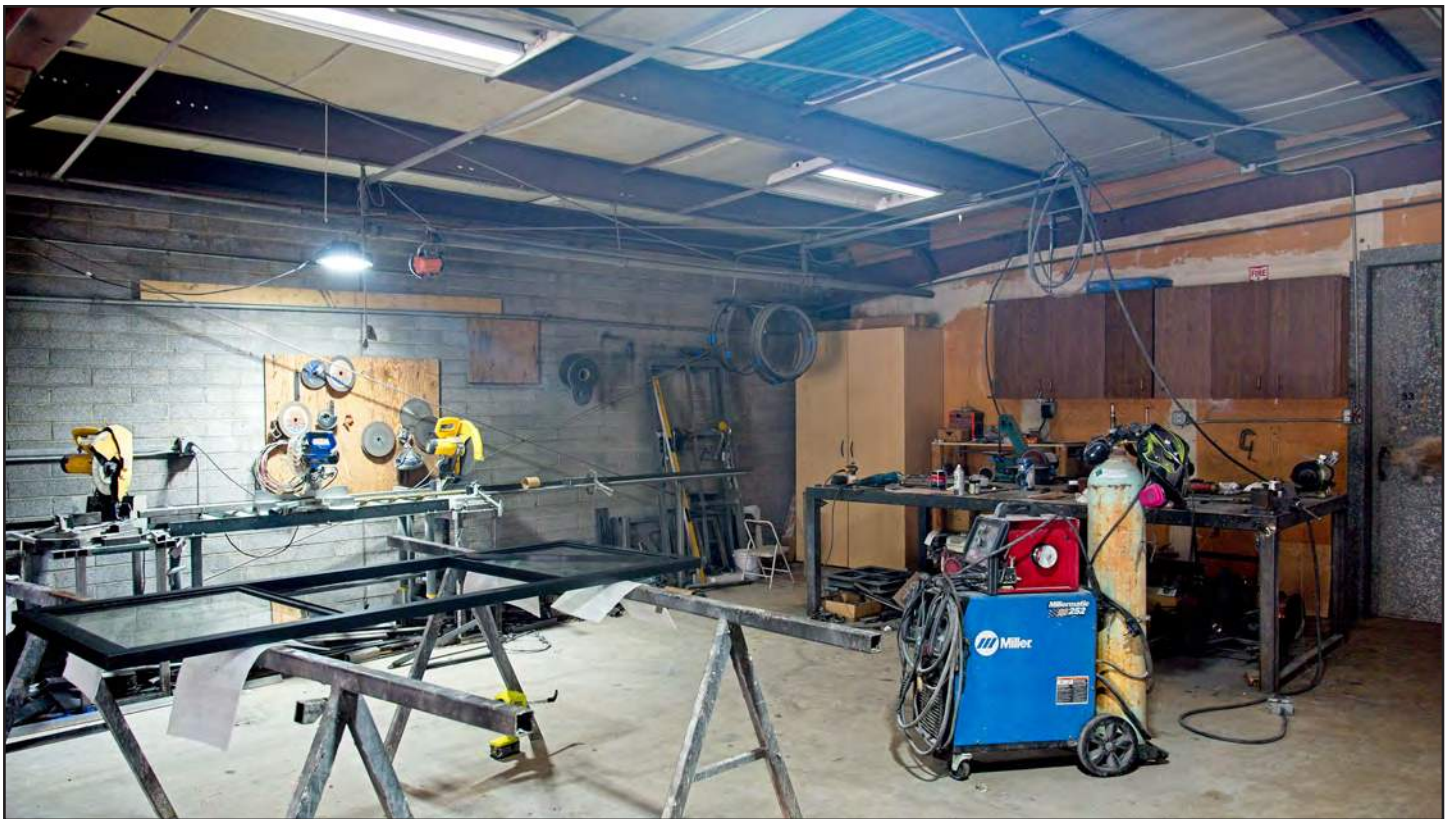
W3.6 (DSC\_7964)

Looking North



W3.7 (DSC\_7676)

Looking South



W3.8 (DSC\_7678)

Looking Southeast



W3.9 (DSC\_7680)

Looking Southwest



W3.10 (DSC\_7682)

Looking Northwest



W3.11 (DSC\_7684)

Looking Northeast

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# Laundry B-1

5

# LAUNDRY B-1



B1.1 (DSC\_7837)

Looking West



B1.2 (DSC\_7813)

Looking Southwest



B1.3 (DSC\_7866)

Looking Northwest



B1.4 (DSC\_7868)

Looking North



B1.5 (DSC\_7693)

Looking Northeast



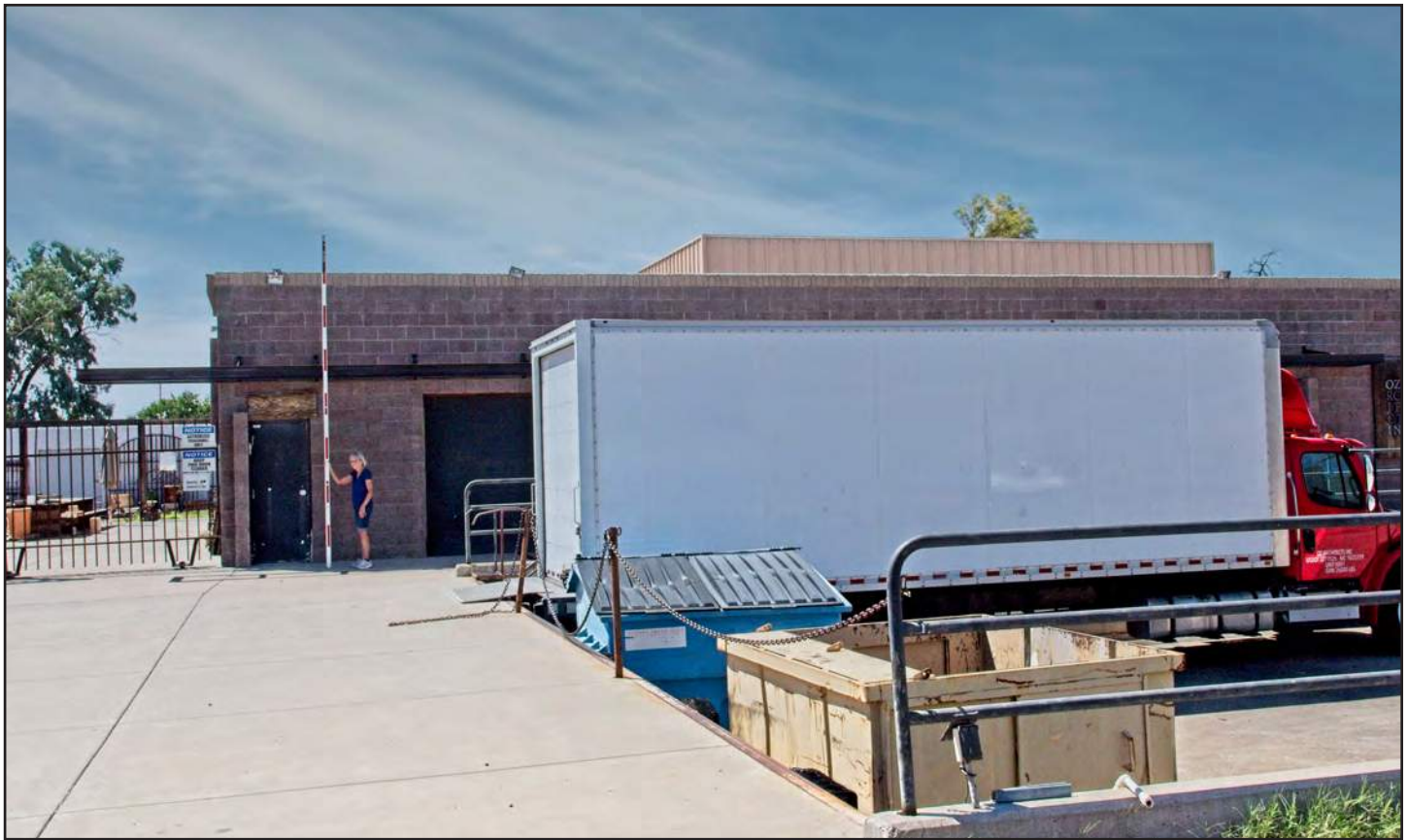
B1.6 (DSC\_7692)

Looking Southeast



B1.7 (DSC\_7816)

Looking South



B1.8 (DSC\_7999)

Looking South



B1.9 (DSC\_7686)

Looking Northeast



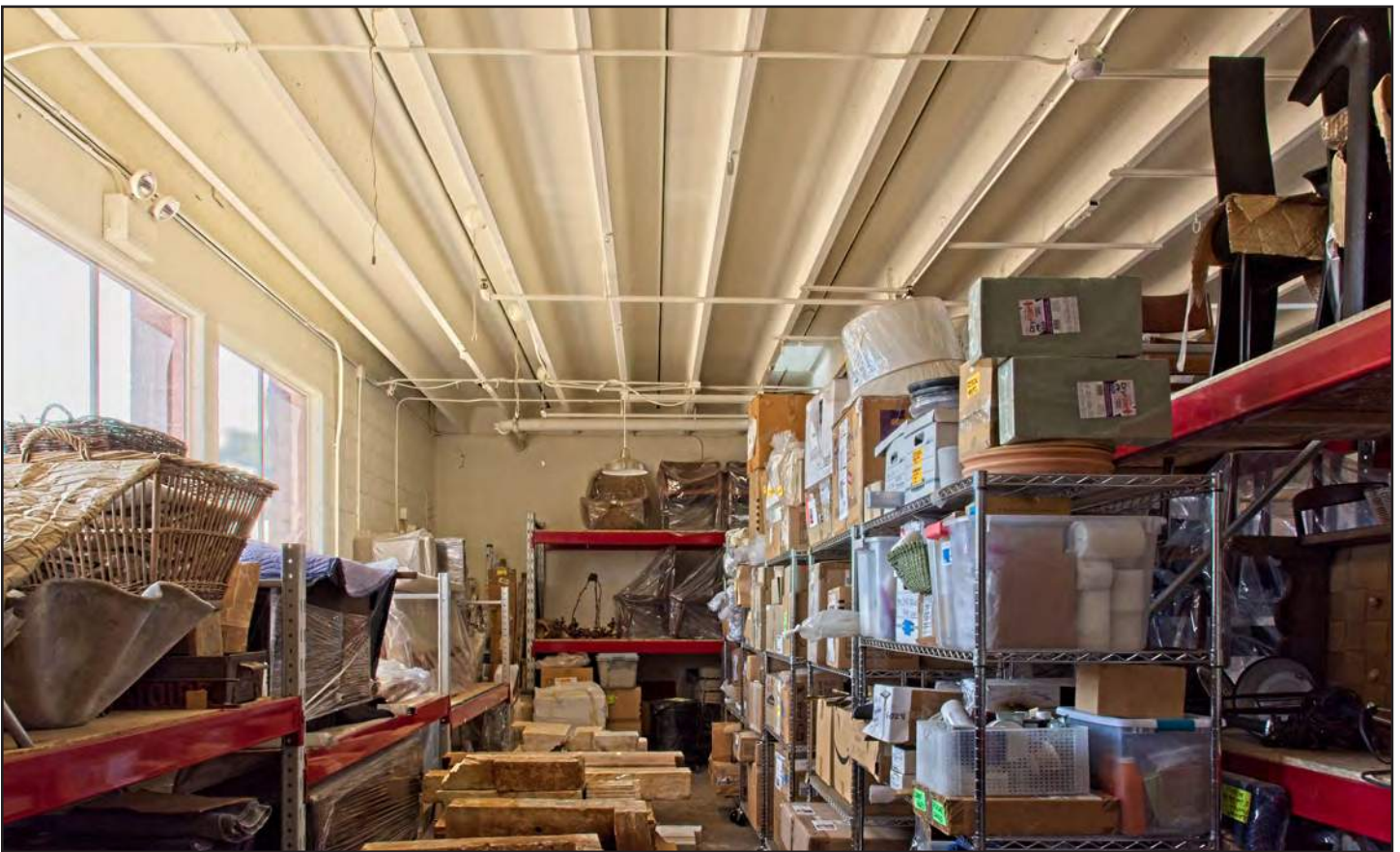
B1.10 (DSC\_7856)

Looking Northeast



B1.11 (DSC\_7854)

Looking North.



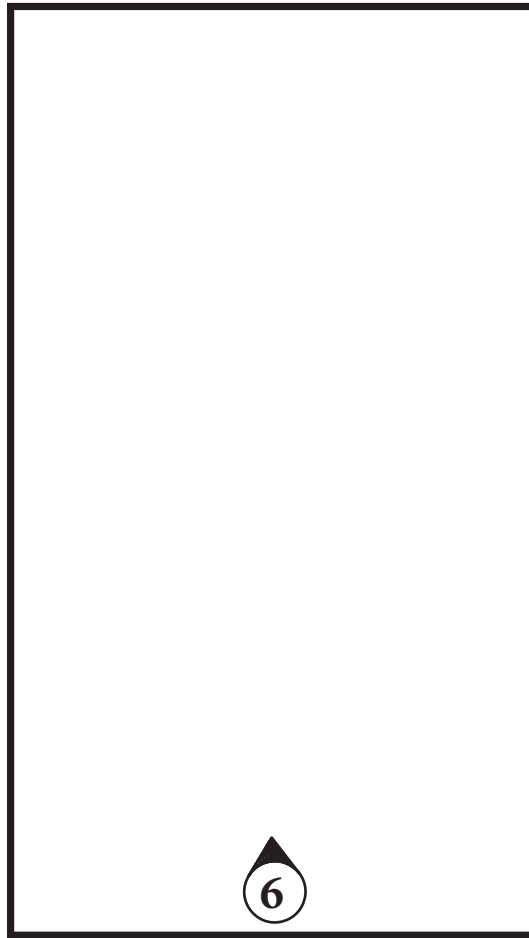
B1.12 (DSC\_7850)

Looking South



**B1.13 (DSC\_4317)**

**Looking Northwest**



# Raw Materials Shed R-1

# RAW MATERIALS SHED R-1



R1.1 (DSC\_8419)

Looking East



R1.2 (DSC\_7698)

Looking Northeast



R1.3 (DSC\_7763)

Looking North



R1.4 (DSC\_7746)

Looking Northwest



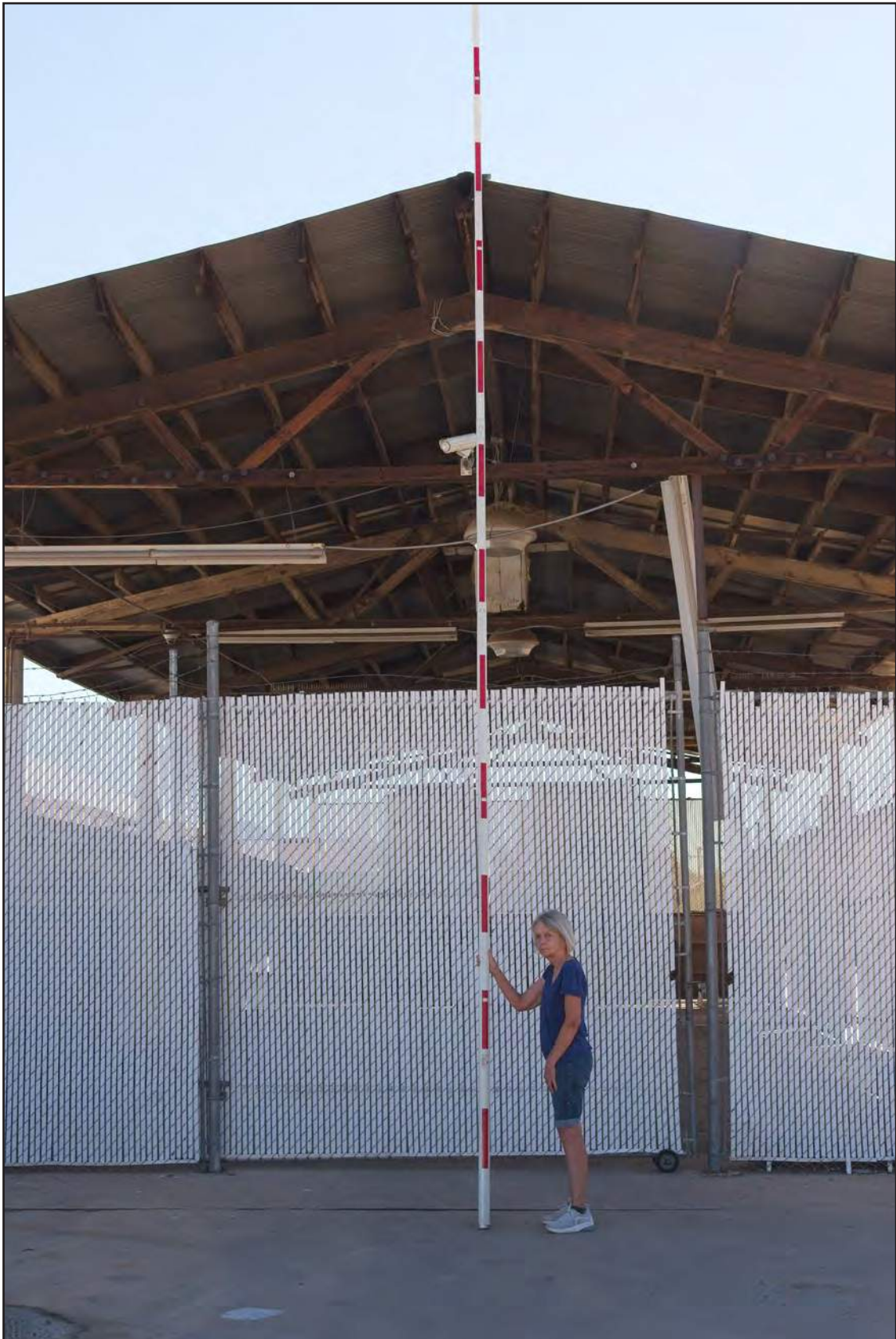
R1.5 (DSC\_8417)

Looking West



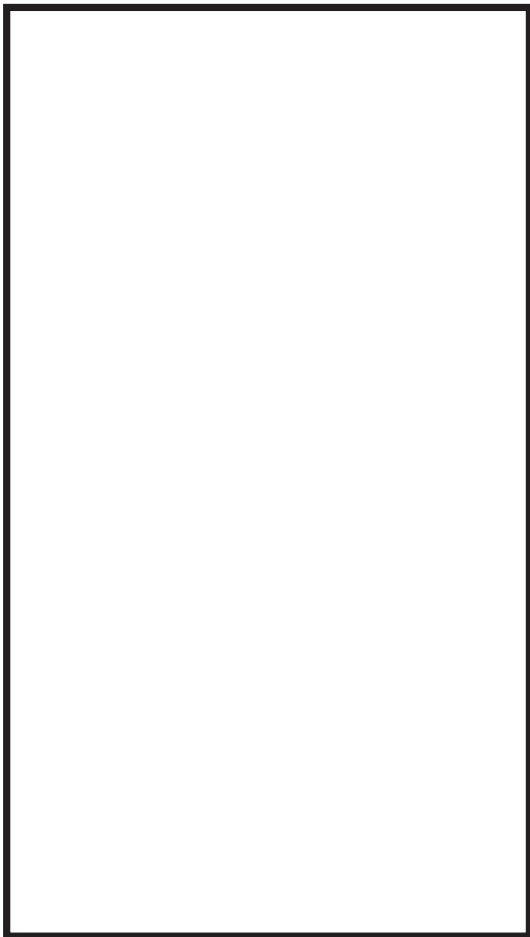
R1.6 (DSC\_7666)

Looking East



**R1.7 (DSC\_7965)**

**Looking East**



## Storage Shed R-2

## STORAGE SHED R-2



R2.1 (DSC\_7810)

Looking West



R2.2 (DSC\_7811)

Looking Northwest



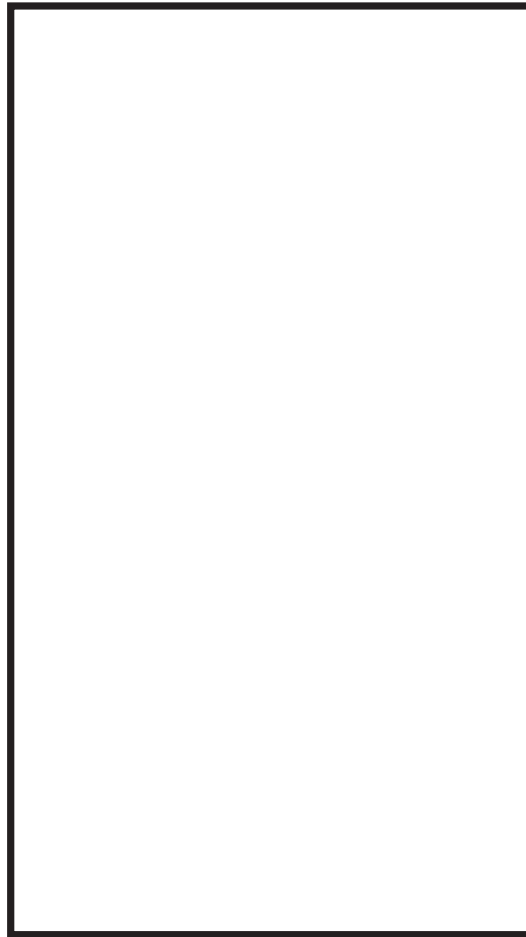
R2.3 (DSC\_7809)

Looking Southwest



R2.2 (DSC\_7968)

Looking West



# Dock Canopy R-3

## DOCK CANOPY R-3



R3.1 (DSC\_7808)

Looking South



R3.2 (DSC\_7943)

Looking Southeast



R3.3 (DSC\_7944)

Looking Northeast



R3.4 (DSC\_7946)

Looking North



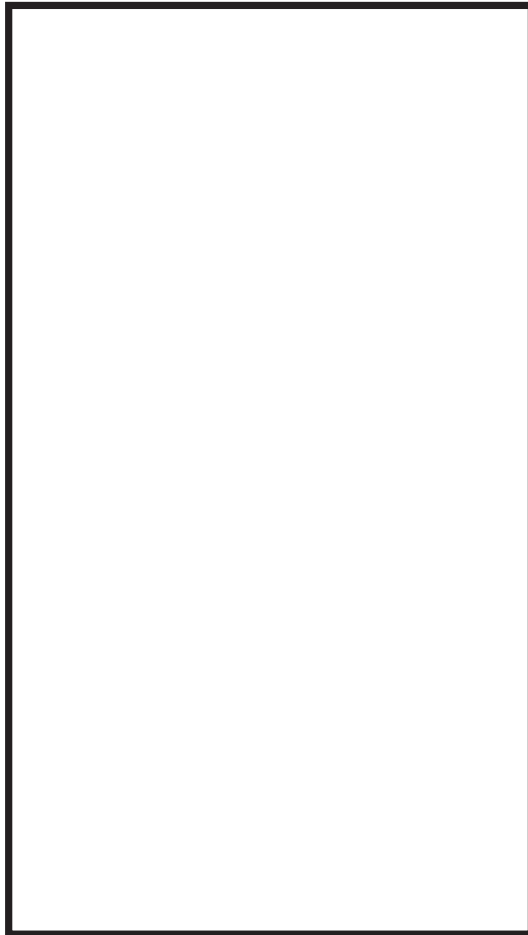
R3.5 (DSC\_7806)

Looking West



R3.6 (DSC\_7982)

Looking West



# East Storage Canopy R-4

## EAST STORAGE CANOPY R-4



R4.1 (DSC\_7805)

Looking West



R4.2 (DSC\_7945)

Looking Southeast



R4.3 (DSC\_8441)

Looking Northwest



R4.4 (DSC\_8415)

Looking North



R4.5 (DSC\_8414)

Looking Southwest

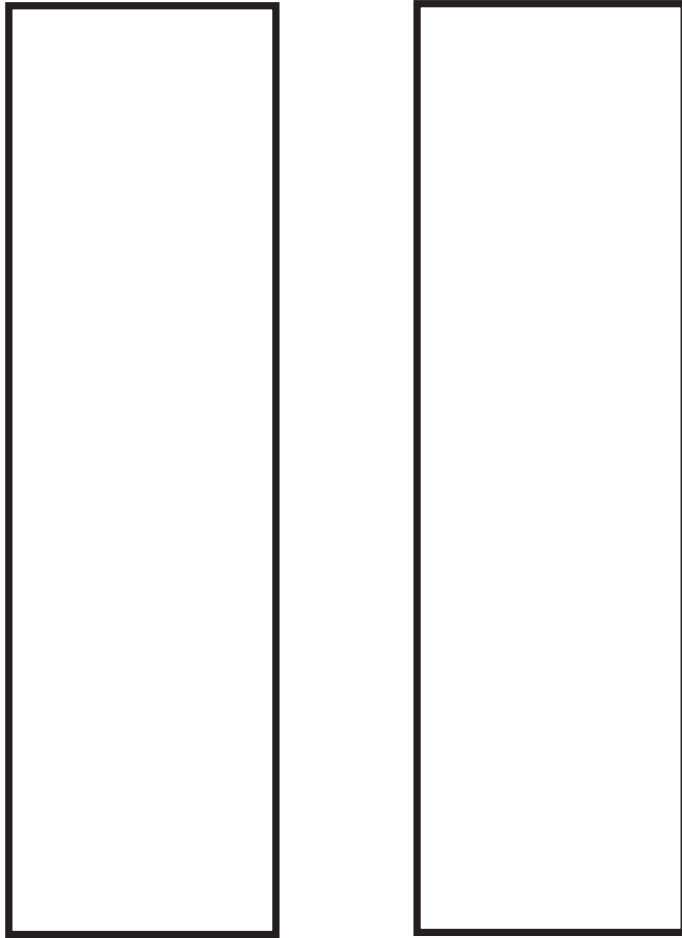


R4.6 (DSC\_7985)

Looking West

1 5

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# Twin Canopies R-5/East & West

## TWIN CANOPIES R-5/EAST & WEST



R5.1 (DSC\_8400)

Looking South



R5.2 (DSC\_7772)

Looking Southwest



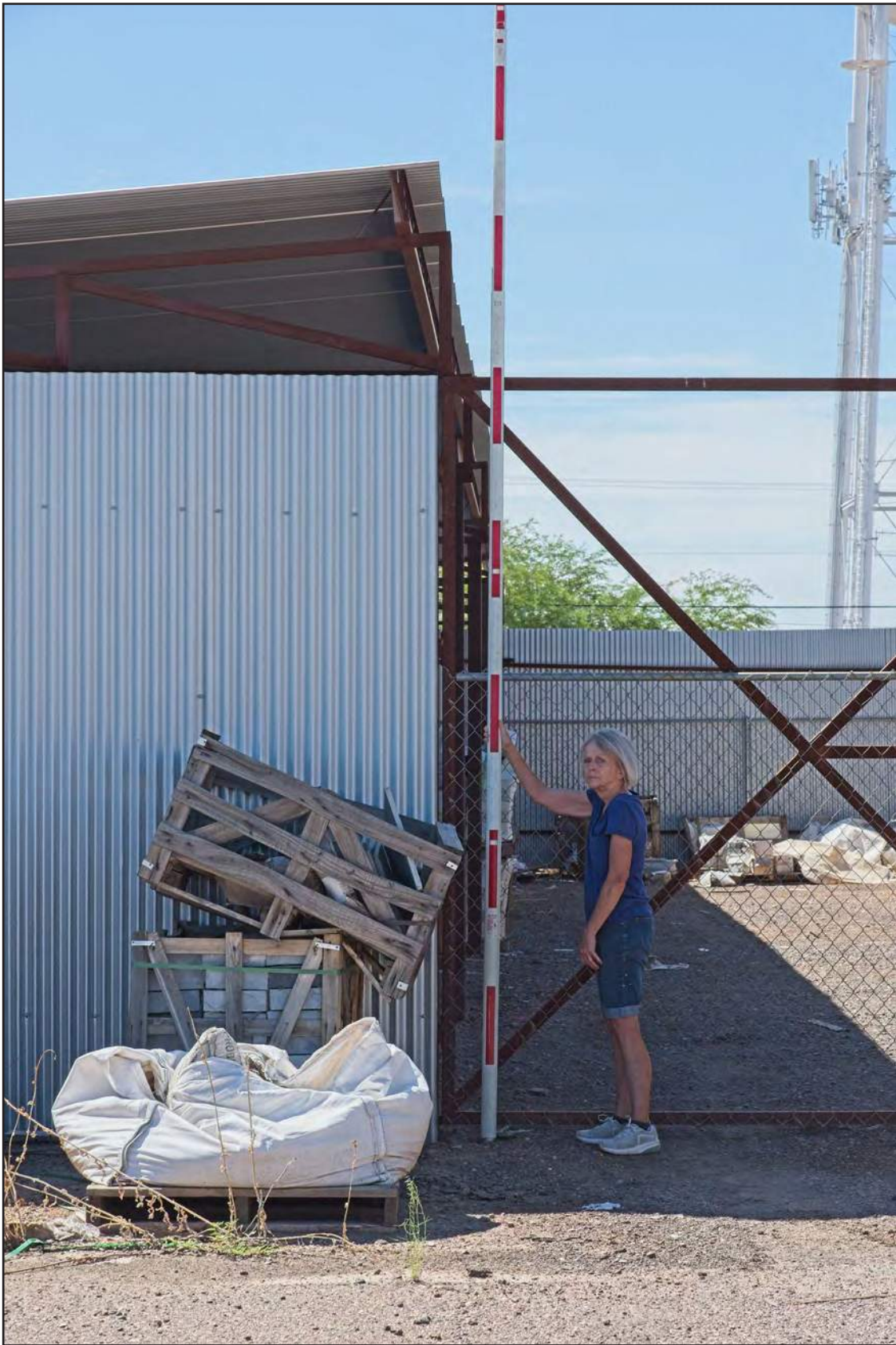
R5.3 (DSC\_7773)

Looking West



R5.4 (DSC\_7947)

Looking Northeast



**R5.5 (DSC\_7987)**

**Looking South**



# Slab of Missing Control Tower C-1

# SLAB OF MISSING CONTROL TOWER C-1



C1.1 (DSC\_7828)

Looking Northeast



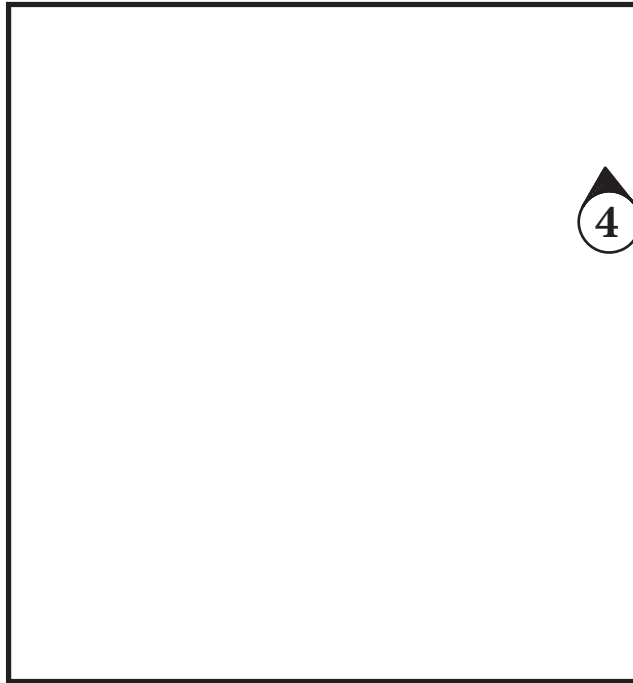
C1.2 (DSC\_7829)

Looking Northwest



C1.3 (DSC\_7831)

Looking Northeast



# Laundry Dock C-2

## LAUNDRY DOCK C-2



C2.1 (DSC\_7690)

Looking East



C2.2 (DSC\_7691)

Looking Northwest



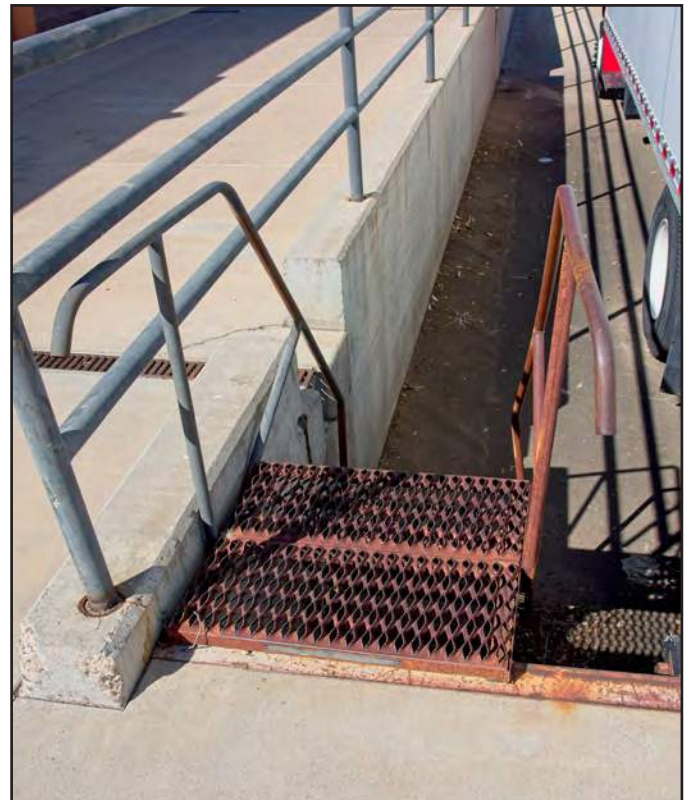
C2.3 (DSC\_8443)

Looking West



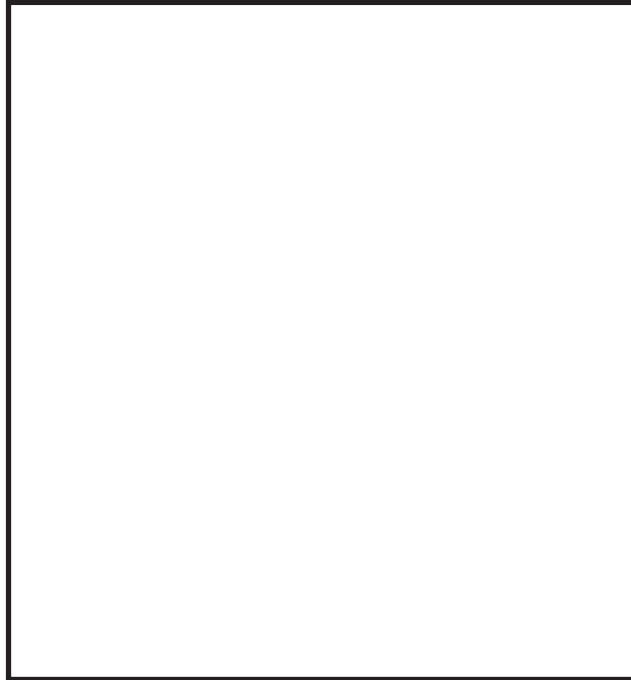
C2.4 (DSC\_8444)

Looking East



C2.5 (DSC\_8445)

Looking West



## Furniture Dock C-3

## Furniture Dock C-3



C2.1 (DSC\_8408)

Looking West



C2.2 (DSC\_8402)

Looking Southwest



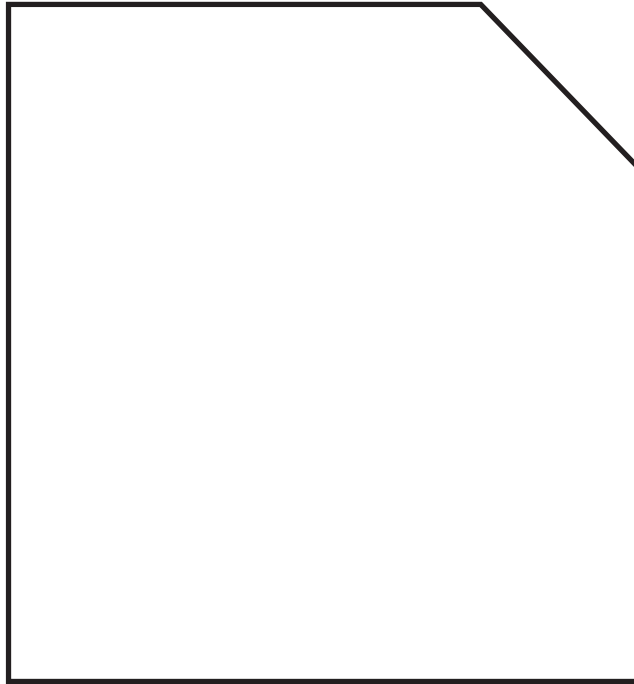
C2.3 (DSC\_7774)

Looking West



C2.4 (DSC\_7979)

Looking West



## Scored Slab C-4

**SCORED SLAB C-4**



**C4.1 (DSC\_0834)**

**Looking Northeast**



**C4.2 (DSC\_7738)**

**Looking Northwest**



## Hangar H-3

## HANGAR H-3



H3.1 (DSC\_7820)

Looking West



H3.2 (DSC\_7819)

Looking Southwest

## ARCHIVAL PHOTOGRAPHS AND DOCUMENTS



Fig. 1 Aerial view of T2 airfield and campus; to northeast toward McDowell Mountains, c.1942-44.  
SHS-photo



Fig. 2 Aerial view of T2 airfield and campus; to north on primary axis of site, c.1942-44. SHS-photo 86.004.4.jpg

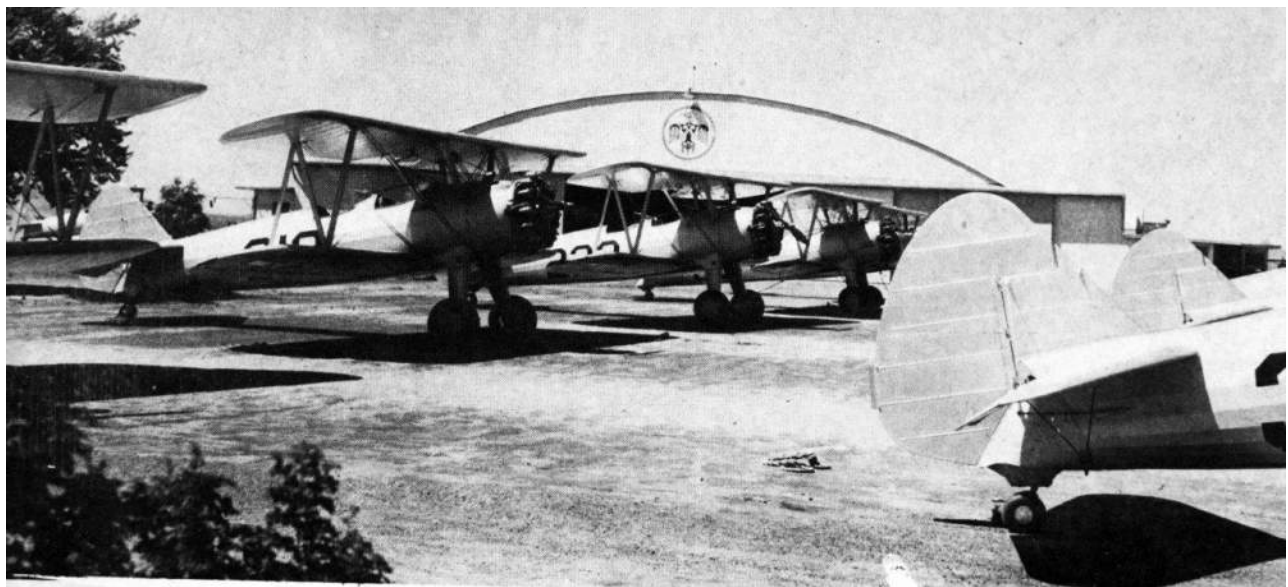


Fig. 3 Stearman PT-17s on T2 ramp; to east, c.1942-44. T2Yearbook.jpg



Fig. 4 Stearman PT-17s in monsoon mud of T2 unpaved ramp; to east, 2 Aug 1943. T2Yearbook.jpg

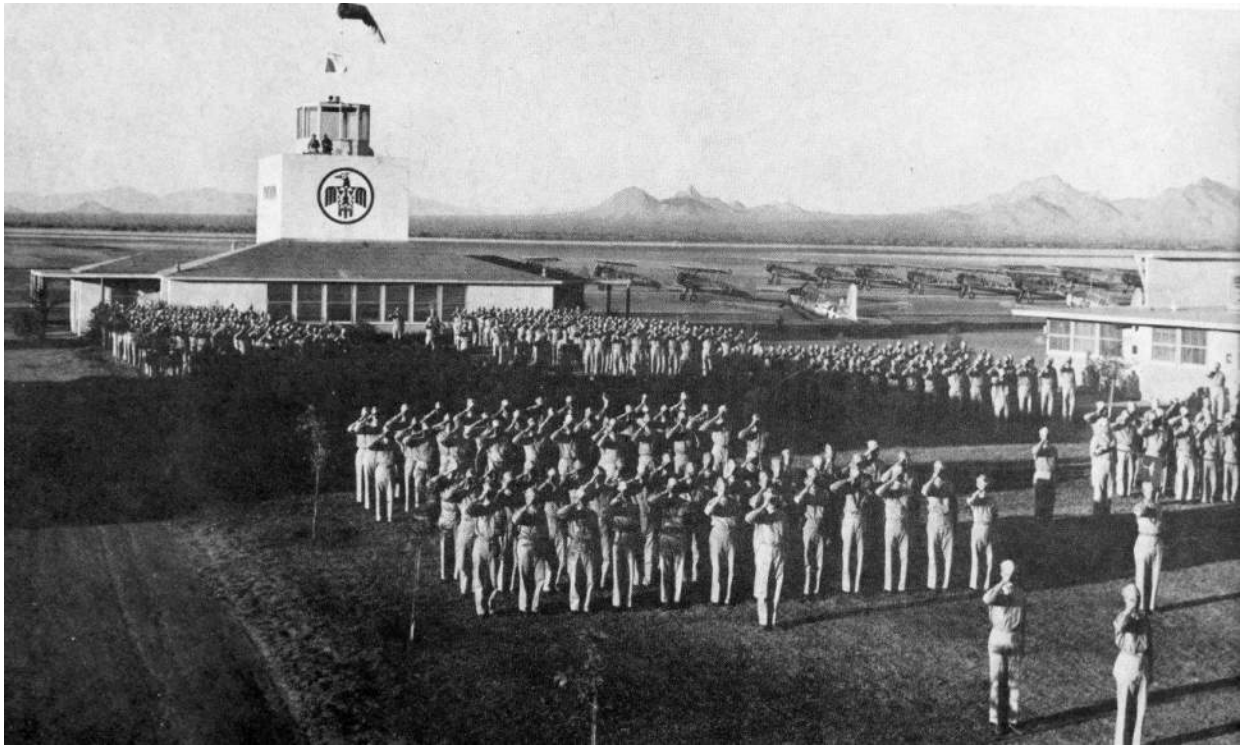


Fig. 5 Cadets with hand salute in formation at Control Tower lawn; to north, c.1942-44. SHS-photo

T2

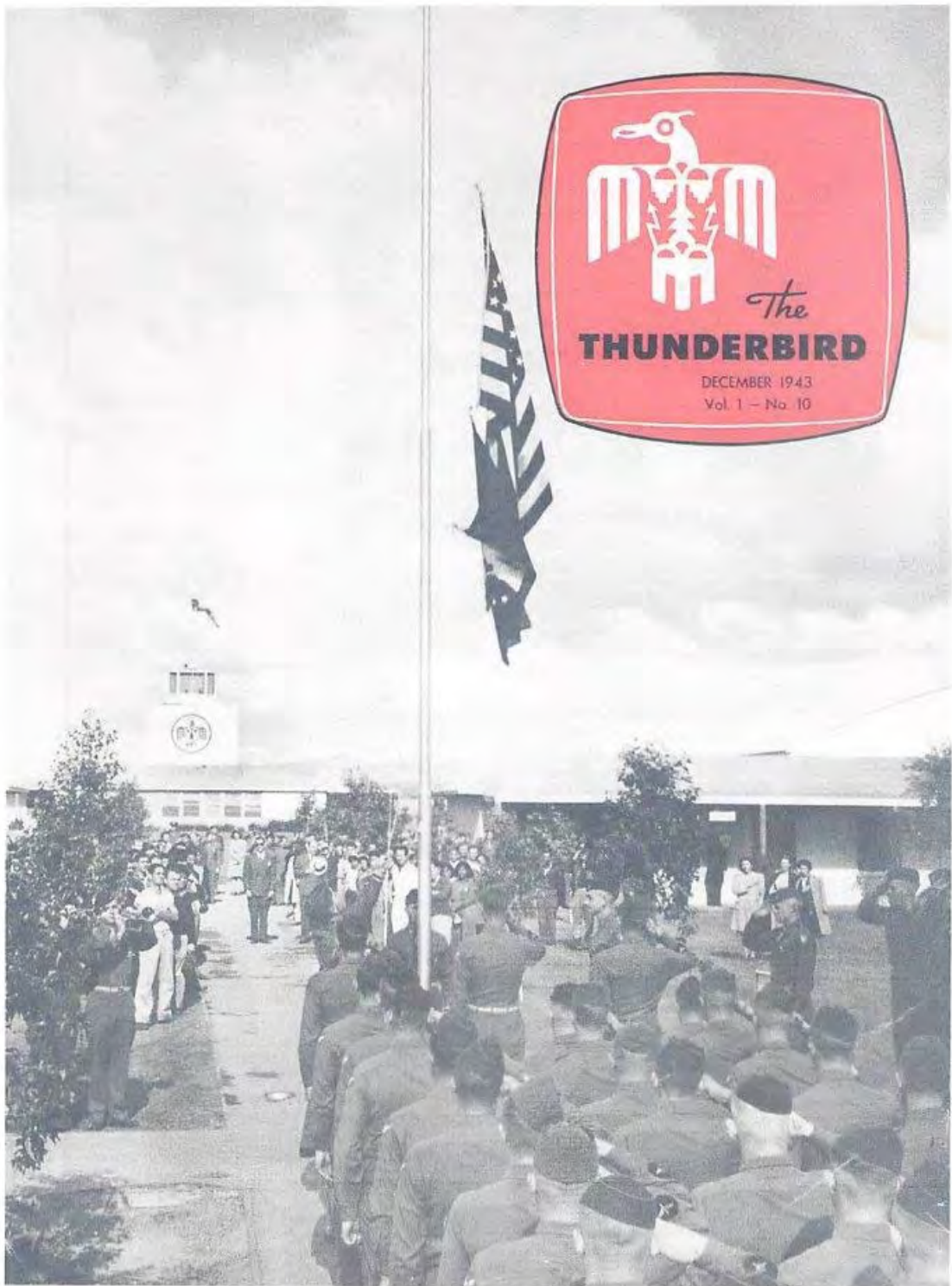


Fig. 6 Public flag ceremony at campus flagpole; to north, December 1943. DLAPRarchives(1).jpg



# BEGINS RIGHT HERE IN ARIZONA

## Proudly Flies The Thunderbird

The Thunderbird is known to many as the emblem of Southwest Airways . . . and recognized over the world as a symbol of another powerful weapon in the arsenal of the United Nations!

*But the Thunderbird is more than just a symbol!*

It represents the thousands of young Yanks, British, Chinese, and Latin and South Americans who first flew under its watchful wings, here in Arizona's Valley of the Sun . . . who now fly and fight for last Victory. It is Thunderbird Field . . . one of the nation's largest primary training schools . . . and also Sky Harbor, Thunderbird II and Falcon fields. And it is all the Southwest Airways employees who night and day "keep 'em flying" . . . who never forget their pledge:

*the finest fighters today, the finest fliers tomorrow*

## SOUTHWEST AIRWAYS

THUNDERBIRD FIELD      FALCON FIELD      SKY HARBOR      THUNDERBIRD II  
Phoenix, Arizona  
CONTRACTORS TO THE UNITED STATES GOVERNMENT      UNITED STATES ARMY      UNITED KINGDOM GOVERNMENT

There is no particular front from which Victory springs . . . not from Guadalcanal alone, nor North Africa alone. But it does BEGIN right here in Arizona, U. S. A.—in the mines and forests, the ranches and farms, the factories and training fields, of which we are all so justly proud.

There are no particular individuals from which Victory springs . . . not from the soldier alone, nor the worker alone. For this is everyone's war, and "we are all in it . . . all the way . . . every man, woman and child" (President Roosevelt).

Those who fight on the home front, fight best when they are putting ten percent of their salary in War Stamps and Bonds EVERY MONTH. Every purchase helps to buy another gun, tank or plane, to save another soldier life. This is the American way to preserve freedom . . . to safeguard home, family and future. It is the All-American way to fight!

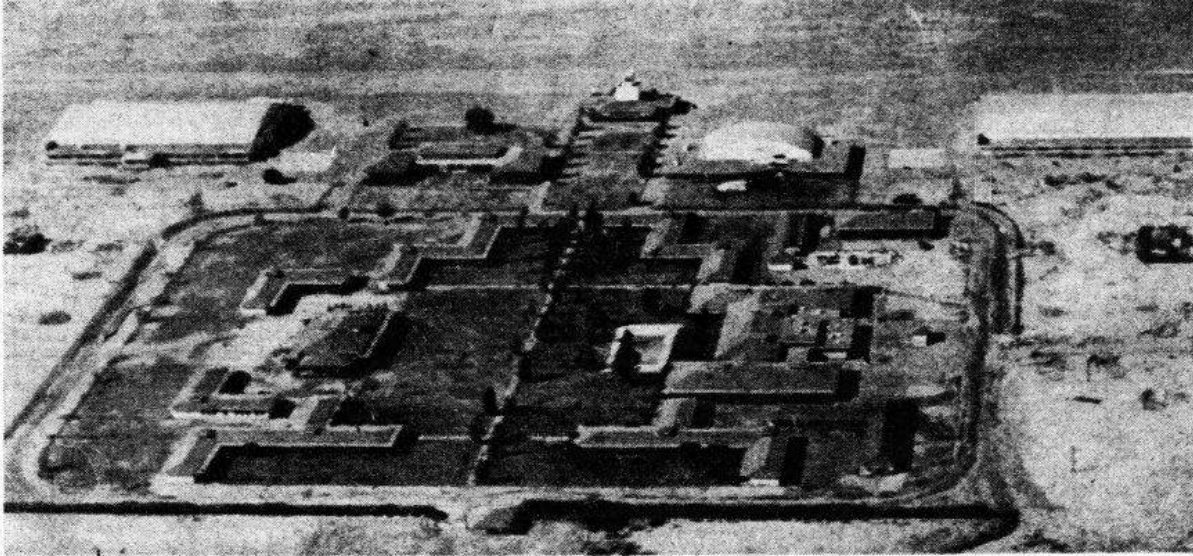
Yes, let each of us remember: this is MY war . . . to fight, to finance . . . to finish; and Victory begins right here in Arizona!

Fig. 7 Southwest Airways ad "Victory begins right here in Arizona;" 22 Nov 1942. AZRep.jpg

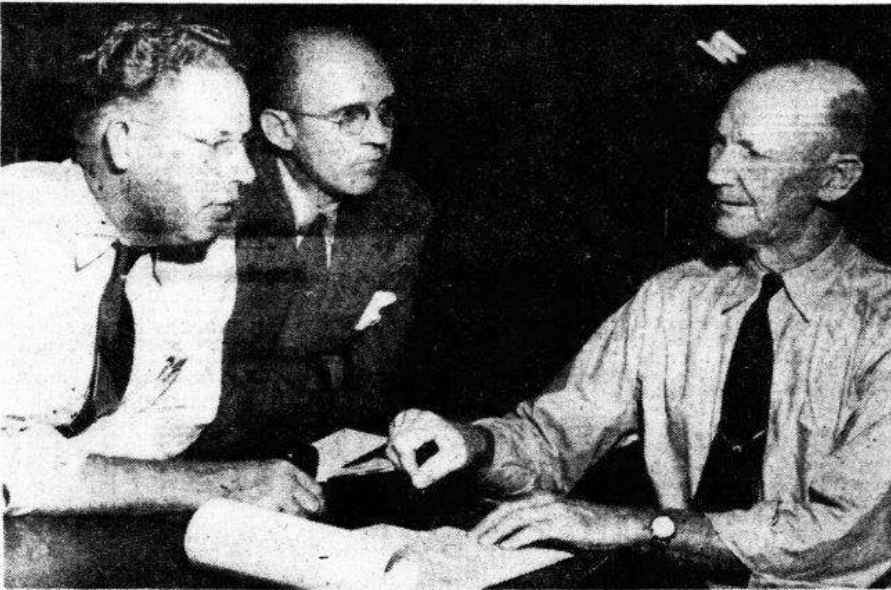


Fig. 8 Civilian staff group photo at Control Tower lawn; to north, c.1942-44. T2YrBK.jpg

## Tempe College Officials File Deed For Airfield



Officials of Arizona State College at Tempe have big plans for student activity at Thunderbird Field II, above, title to which became final for the school yesterday with filing of the deed in the office of Roger Laveen, county recorder. After months of negotiation with the War Assets Administration, the 700-acre former army installation was obtained at a 100 per cent discount. On hand for the final step in acquiring the field yesterday were, as they visited the field yesterday left to right, Gilbert L. Cady, college director of business management; George C. Yates, director of special services, and Mr. Laveen.—(Republic Staff Photos.)



## U.N. Tally Of Troops Is Urged

LAKE SUCCESS, Sept. 12—(AP) An immediate world-wide troop census was proposed by Britain today in a surprise pre-assembly maneuver on the year-old Molotov arms reduction scheme.

The British proposal was outlined before a committee of the United Nations commission for conventional armaments by Sir Alexander Cadogan as delegates were gathering for the opening of the 55-nation general assembly at Flushing Meadow park in New York Tuesday.

**THIS WAS** the first time the controversial troop census question had come up since the 1946 assembly in which debate became so bitter that the issue had to be side-stepped in order to get agreement or even a nebulous start for an arms reduction program.

The program was originally put before the assembly last fall in a surprise move by V. M. Molotov, Soviet foreign minister.

Britain's new surprise package on the arms question was pushed aside temporarily today but it appeared headed toward vigorous

Fig. 9 News article "T2 Now ASC Property;" 21 May 1950. The\_Arizona\_Republic (2).jpg



Fig. 10 News article "ASC Vocational School," 21 May 1950. The\_Arizona\_Republic.jpg

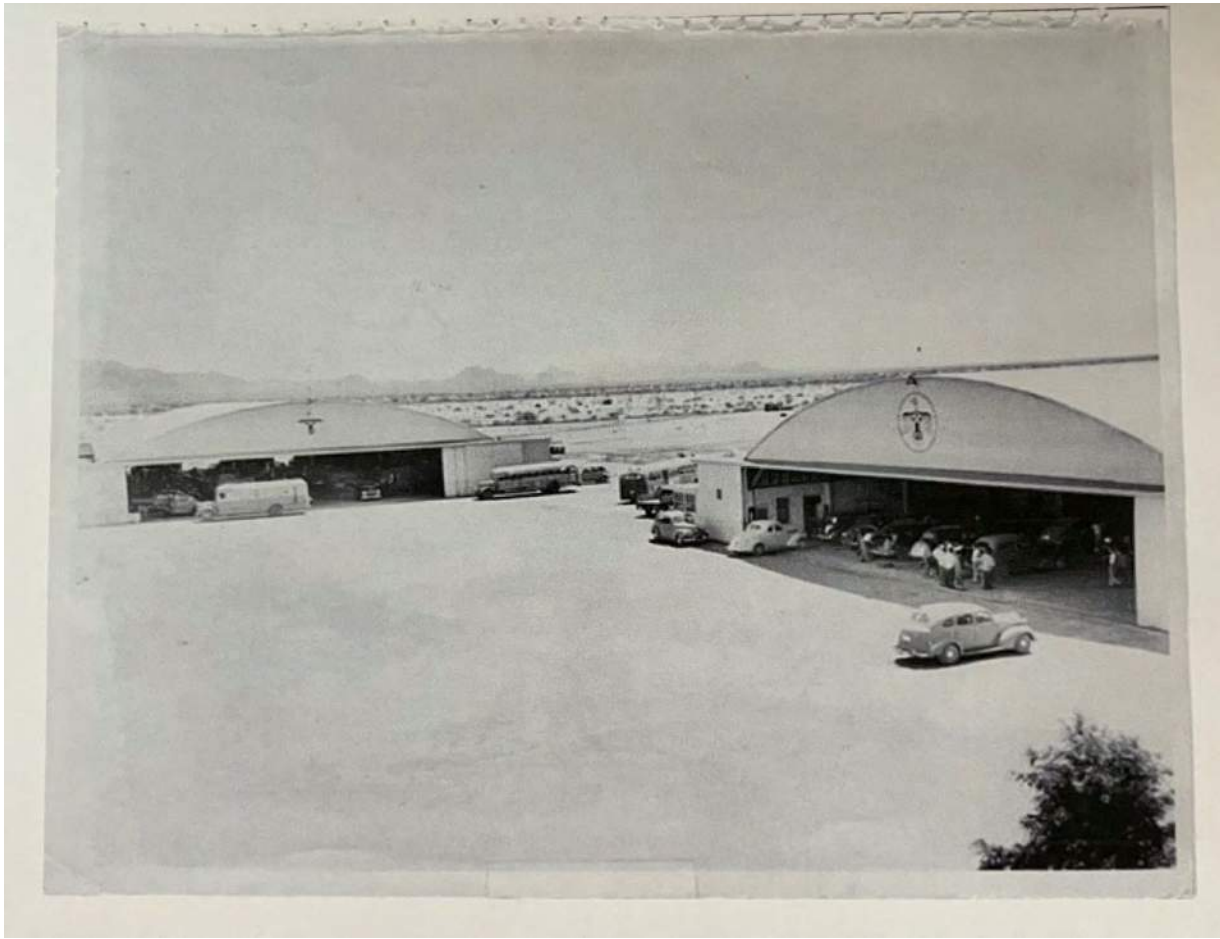


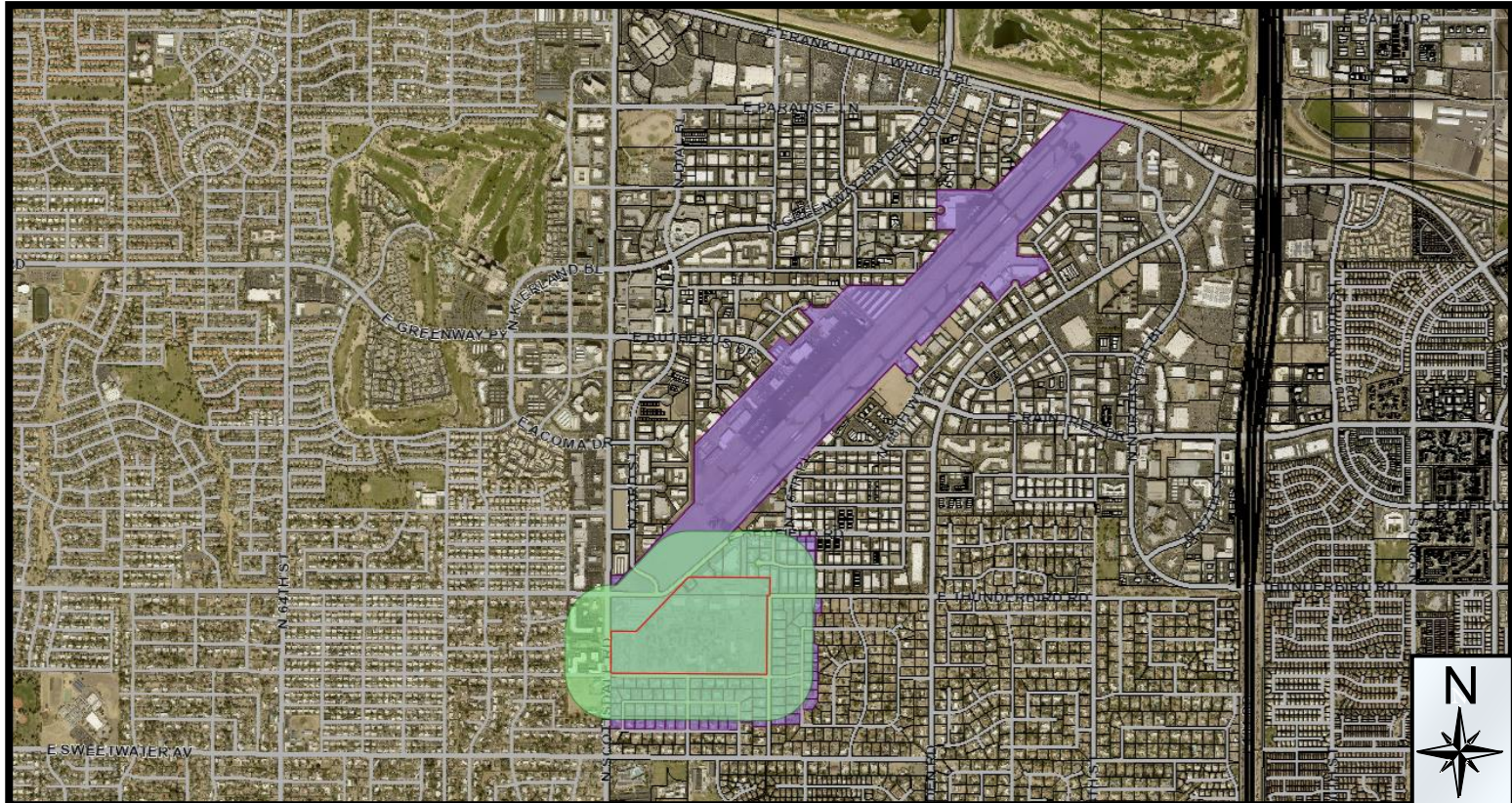
Fig. 11 T2 hangars adapted for ASC Vocational School auto shops; c.1948. UP\_ASU.D.T48\_89\_14.jpg



Fig. 12 Aerial view of Thunderbird Adventist Academy campus; c.1964. SHSpic.jpg

# City Notifications – Mailing List Selection Map

## Seventh Day Adventist Warehouse Building



### Additional Notifications:

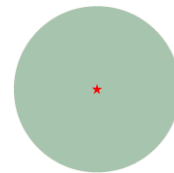
- Interested Parties List
- Adjacent HOA's
- P&Z E-Newsletter
- Facebook
- Nextdoor.com
- City Website-Projects in the hearing process

Pulled Labels  
June 8, 2022

### Map Legend:



Site Boundary



Properties within 750-feet

Postcards: 255

14-DR-2022

## Barton, Wayland

---

**From:** Chris Shaw <chris@tripluspartners.com>  
**Sent:** Tuesday, June 21, 2022 8:14 AM  
**To:** Barton, Wayland  
**Subject:** Case 14-DR-2022

**⚠ External Email: Please use caution if opening links or attachments!**

Wayland,

I live in the adjacent neighborhood to the Adventist Academy at 7629 E. Thunderbird Road. I read through the project narrative, but wanted to clarify a couple of matters.

1. Height – The building is proposed at 40'. The original case had a 32-40' allowance based on setbacks. Could you confirm this is in compliance with that requirement?
2. Phasing – I believe this area is considered phase II under the original case. Can you advise what landscape and buffer elements will be completed with this phase, including undergrounding the power lines? Also, will the sewer line be extended along Miller Road.

Finally, could you send me a copy of the final ordinance for the 2019 rezoning?

Thanks.Chris

Chris Shaw  
TriPlus Partners  
5350 N. 16th St. #106  
Phoenix, AZ 85016  
Phone: (602) 604-2400  
Fax: (602) 604-2401  
[www.tripluspartners.com](http://www.tripluspartners.com)