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July 15, 2022

Zoning Administrator
City of Scottsdale Arizona

Re: *Residential Variance to Figure 9.106.A for Scottsdale 101 Development*

Dear Zoning Administrator:

Walker Consultants has been requested by the Design Team and Owner of the Scottsdale 101 development project to provide a professional opinion in support of a request for variance regarding Figure 9.106.A in Section IX of the the City Zoning Code.

The Scottsdale 101 Project is located at the North East corner of East Mayo Blvd and North Scottsdale Road in Scottsdale, Arizona.

The project is proposed to provide over 2,000 parking spaces. There are 3 user groups within the project: private residential parking, private residential visitor parking and commercial parking and public parking. Most of the users in the facility will be familiar with where they park and will not have issue with the proposed variant from figure 9.106.A. and further, more than 85% of the vehicles on the road can effectively use this facility with the variation proposed.

Article IX of the City Zoning code, section 9.106 defines the requirements for column encroachment into a parking stall.

Sec. 9.106. Design standards for public and private on-site ingress, egress, maneuvering and parking areas.

A. *Standard Parking space dimension.*

1. *Vehicular.*

- a. Except for parallel parking spaces, as indicated below, and in Table 9.106.A. parking spaces shall have a minimum width of nine (9) feet and a minimum length of eighteen (18) feet. Parallel parking spaces shall have a minimum width of nine (9) feet and a minimum length of twenty-one (21) feet.

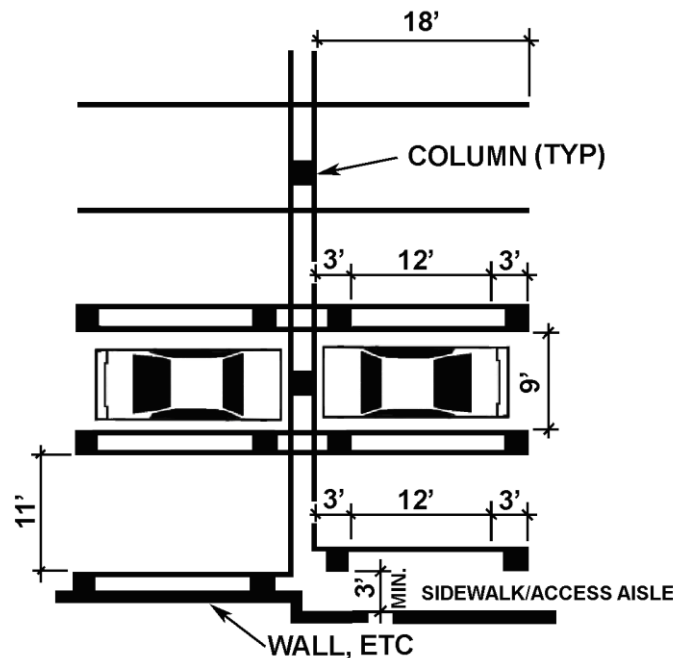
Compliance: all standard parking stalls will be 9ft x 18 ft. No variation requested.

- i. For new development and/or redevelopment constructed after July 9, 2010, when a side of a parking space is adjacent to a wall, column, or other obstruction, except as provided in Sections 9.106.A.1.a.ii. and 9106.A.1.a.iii., that is taller than six (6) inches, and where a minimum three-foot wide unobstructed pedestrian access aisle is not provided between the wall, column, or

other obstruction and the parking spaces, the width of the parking space shall be increased by two (2) feet on the obstructed side, as illustrated by Figure 9.106.A.

- (1). The entire required width and length of a parking space(s) shall not be obstructed by a column, or obstruction that is greater than six (6) inches in height, as illustrated by Figure 9.106.A.
- ii. For new development and/or redevelopment constructed after July 9, 2010, when a side of a parking space, excluding a parallel parking space, that is adjacent to a column that is taller than six (6) inches, the obstructed side shall be unobstructed for a minimum of twelve (12) feet, which is between the front three (3) feet and rear three (3) feet of the parking space, as further illustrated by Figure 9.106.A.

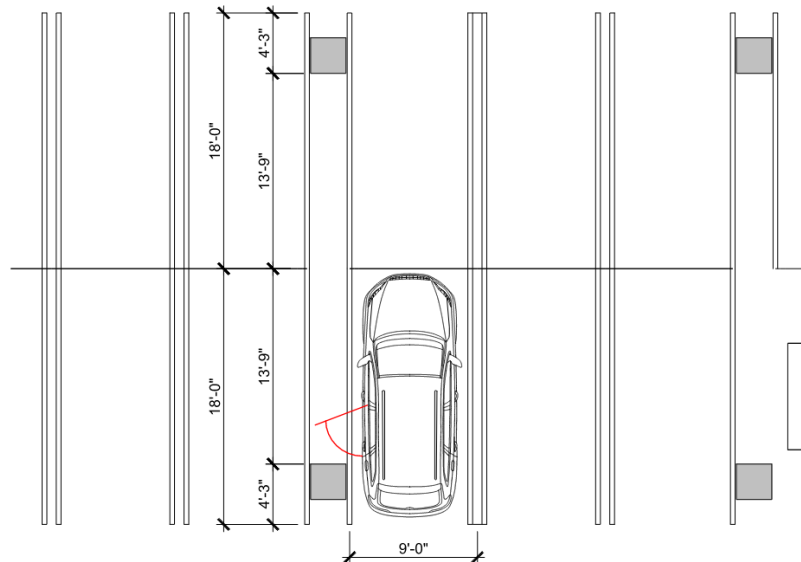
FIGURE 9.106.A. Column, etc. Obstructions



REQUEST: Allow for a limited increased encroachment at the rear/side of the stall beyond the 3'-0" shown in figure 9.106.A.

COMPLIANCE: The project configuration that is proposed is shown on the figure below and will have a column only at the back of every third stall. No obstructions will exist near the head of the stall or at the front/sides of the stalls.

For over 40 years, Walker has gathered all automobile manufacturer data regarding vehicles produced and their sizes. The design vehicle used by Walker represents the 85th percentile of all vehicles on the road, meaning, that 85% of the vehicles on the road are smaller than the design vehicle and will be more comfortable in a given parking stall or parking maneuver. As the below exhibit shows, the column at the back of the stall will be slightly more forward than allowed by Figure 9.106.A. in that it will be 4'-3" from the back of the stall in lieu of the 3'-0" per Figure 9.106.A. However, it is our professional opinion that having the column exceed the limit of the rear encroachment will not negatively impact the usability or functionality of the stall during parking and unparking maneuvers and will still allow for access to rear seat occupants. Further, there will not be columns near the front/side of the stall or at the direct head of the stall, whereby there will be a larger clear space provided along the sides of the stall (13'-9") as compared to that shown in Figure 9.106.A. (12'-0").



We respectfully request that this variance be accepted and approved for use.

Sincerely,

WALKER CONSULTANTS



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Vice President

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