

ZONING ORDINANCE DATA

PROJECT DESCRIPTION

THE DEVELOPMENT OF AN AIRCRAFT HANGAR WITH SUPPORTING OFFICE AND STORAGE SPACES AT THE CITY OF SCOTTSDALE MUNICIPAL AIRPORT

PROJECT ADDRESSES: 16061 N. 81ST STREET
SCOTTSDALE, ARIZONA 85260

PROJECT OWNER: ARIZTARI LLC
(SEND ALL CORRESPONDENCE THROUGH THE ARCHITECT)

PROJECT ARCHITECT: LARSON ASSOCIATES ARCHITECTS, INC.
3801 NORTH 24TH STREET #200
PHOENIX, ARIZONA 85016
602-955-9929
602-954-4790 FAX
EMAIL: jlarson@larson-architects.com

BOOK-MAP-PARCEL: APN 215-48-054

CURRENT ZONING: I-1 (SCOTTSDALE MUNICIPAL AIRPORT)

NET SITE AREA: 52,521 SF. (1206 AC)

LOT COVERAGE: 2122%

OCCUPANCY GROUP: B-1 - AIRCRAFT HANGAR (PER IBC SECTION 3102)
B-1 OFFICE SPACES (PER IBC SECTION 304.1)
-NEW CONSTRUCTION

CONSTRUCTION TYPE: V-B, FULLY SPRINKLED (PER CITY OF SCOTTSDALE AMENDMENT)

FLOOR AREAS PROVIDED:
OCCUPANCY B:
OCCUPANCY B-1:
OCCUPANCY B-1:
SITE COVERAGE TOTAL:
TOTAL OFFICES:
TOTAL HANGARS:
2ND FLOOR PATIO:
2ND FLOOR VIEWING DECK:
GRAND TOTAL:

2,844 SF.
1,234 SF.
14,298 SF.
4,825 SF.
1,234 SF.
413 SF.
635 SF.
17,101 SF.

OPEN STAGING AREA: 12,449 SF.

PARKING REQUIRED:
OFFICE AREA: 4,825 / 300 = 16 SPACES
HANGAR AND SUPPORT AREA: 0 SPACES
REQUIRED
TOTAL SPACES REQUIRED = 16 SPACES

PARKING PROVIDED:
SPACES PROVIDED = 11 + OK

ACCESSIBLE SPACES REQUIRED:
VAN SPACES REQUIRED:
ACCESSIBLE SPACES PROVIDED:
VAN SPACES PROVIDED:

16 SPACES X 4 = 1 SPACE
1 SPACE
SPACES PROVIDED = 1 SPACE + OK
1 SPACE + OK

BICYCLE PARKING REQUIRED:
BICYCLE PARKING PROVIDED:
EV PARKING REQUIRED:
EV PARKING PROVIDED:

16/20 = 2 SPACES
2 + OK
11 SPACES X 4% = 1 SPACE
2 SPACES PROVIDED

FRONTAGE YARDS REQUIRED:
EAST (TAXILANE):
WEST (81ST STREET):

50'-0"
25'-0"

YARD PROVIDED:
EAST:
WEST:

149'-8"
25'-0"

ON GRADE PAVING AREA:
REQUIRED:
PROVIDED:

9,954 SF.
9,954 SF. + 1,431 SF.
2,108 SF. + OK

INTERIOR PARKING LOT LANDSCAPING:
REQUIRED:
PROVIDED:

10,215 X 33 = 339,82 SF.
163 SF. + OK

OPEN FRONT SPACE:
REQUIRED:
PROVIDED:

20x206 = 4,120 SF.
4,508 SF. + OK

OPEN SPACE REQUIRED:
FIRST 12' FEET
AREAS ABOVE 12'
SUB-TOTAL:
STAGING AREA REDUCTION:
TOTAL REQUIRED:

10x49,555x4,955 SF.
215x,003x4,955 = 4,055 SF.
9,191 SF.
4,055x50.1 = 2,0215 SF.
4,955 + 2,0215 = 6,943 SF.

TOTAL OPEN SPACE PROVIDED:
OPEN FRONT SPACE:
STAGING:
OTHER OPEN SPACE:
TOTAL:

4,508 SF.
17,449 SF.
2,366 SF.
15,520 SF. + OK

FLOOR AREA RATIO:
PER CITY OF SCOTTSDALE ZONING ORDINANCE SECTION 5.18.04.A
NET LOT AREA = 49,155 SF. (113 ACRES)
F.A.R. (ALLOWED) = NLA * 6/10 = 29,493 SF.
F.A.R. (ALLOWED) = 49,155 * 6/10 = 29,493 SF.
F.A.R. (PROVIDED) =
HANGAR: 11,234 SF.
OFFICES: 4,825 SF.
2ND FLOOR PATIO: 413 SF.
2ND FLOOR VIEWING DECK: 635 SF.
TOTAL GROSS FLOOR AREA: 17,101 SF. + OK
F.A.R. (ALLOWED) AS PERCENT = 60%
F.A.R. (PROVIDED) AS PERCENT = 34.80% + OK

REQUIRED SITE PLAN NOTES

- SIGHT DISTANCE TRIANGLES SHALL BE SHOWN ON FINAL PLANS FOR DRIVEWAYS FROM COMMERCIAL SITES AND ANY INTERSECTIONS. AREA WITHIN THE SAFETY TRIANGLE IS TO BE CLEAR OF LANDSCAPING, SIGNS, OR OTHER VISIBILITY OBSTRUCTIONS WITH A HEIGHT GREATER THAN 15 FEET. TREES WITHIN THE SAFETY TRIANGLE SHALL HAVE A CANOPY THAT BEGINS AT 8 FEET IN HEIGHT UPON INSTALLATION. ALL HEIGHTS ARE MEASURED FROM NEAREST STREET LINE ELEVATION.
- ALL RIGHT-OF-WAY ADJACENT TO THIS PROPERTY SHALL BE LANDSCAPED AND MAINTAINED BY THE PROPERTY OWNER.
- ALL SIGNS REQUIRE SEPARATE PERMITS AND APPROVALS.
- NO EXTERIOR VENDING OR DISPLAY SHALL BE ALLOWED.
- FLAG POLES, IF PROVIDED SHALL BE ONE PIECE CONICAL TAPERED.
- NO EXTERIOR PUBLIC ADDRESS OR SPEAKER SYSTEM SHALL BE ALLOWED.
- ALL EXTERIOR MECHANICAL, UTILITY, AND COMMUNICATION EQUIPMENT SHALL BE SCREENED TO A HEIGHT OF THE TALLEST UNIT BY PARAPET OR SCREEN WALL THAT MATCHES THE ARCHITECTURAL COLOR AND ARCHITECTURAL FINISH OF THE BUILDING. GROUND MOUNTED MECHANICAL, UTILITY, AND COMMUNICATION EQUIPMENT SHALL BE SCREENED BY A SCREEN WALL THAT MATCHES THE ARCHITECTURAL COLOR AND ARCHITECTURAL FINISH OF THE BUILDING, WHICH IS A MINIMUM OF 1'-0" HIGHER THAN THE HIGHEST POINT OF TALLEST UNIT.
- ALL EQUIPMENT, UTILITIES, OR OTHER AFFURANCES ATTACHED TO THE BUILDING SHALL BE AN INTEGRAL PART OF THE BUILDING DESIGN IN TERMS OF FORM, COLOR AND TEXTURE.
- NO EXTERIOR VISIBLE LADDERS SHALL BE ALLOWED.
- ALL POLE MOUNTED LIGHTING SHALL BE A MAXIMUM OF 20 FEET IN HEIGHT.
- NO CHAIN LINK OR DOOLEY WALL FENCING SHALL BE ALLOWED.
- ALL EXTERIOR CONDUIT AND RACEWAYS SHALL BE PAINTED TO MATCH THE BUILDING.
- UPON REMOVAL OF THE SALVAGEABLE NATIVE PLANTS THE SALVAGE CONTRACTOR SHALL COMPLETE NATIVE PLANT TRACKING FORM AS WELL AS A LIST IDENTIFYING THE TAG NUMBERS OF THE PLANTS SURVIVING SALVAGE OPERATIONS TO THE CITY'S INSPECTION SERVICES UNIT WITHIN 3 MONTHS FROM THE BEGINNING OF SALVAGE OPERATIONS AND/OR PRIOR TO THE ISSUANCE OF THE CERTIFICATE OF OCCUPANCY.
- ALL EXTERIOR LUMINARIES SHALL MEET ALL IESNA REQUIREMENTS FOR FULL CUTOFF, AND SHALL BE AIMED DOWNWARD AND AWAY FROM PROPERTY LINES EXCEPT SIGN, PARKING LOT CANOPY AND LANDSCAPE LIGHTING.
- CITY STAFF MAY AT ANY TIME REQUEST THE DEVELOPER TO SUBMIT AS-BUILT PLANS TO THE INSPECTION SERVICES DIVISION. AS BUILT PLANS SHALL BE CERTIFIED IN WRITING BY A REGISTERED PROFESSIONAL ENGINEER, USING AS-BUILT DATA FROM A REGISTERED LAND SURVEYOR. AS-BUILT PLANS FOR DRAINAGE FACILITIES AND STRUCTURES SHALL INCLUDE, BUT ARE NOT LIMITED TO STREETS, LOT GRADING, STORM DRAIN PIPES, VALLEY GUTTERS, CURBS AND GUTTER, FLOOD WALLS, CULVERTS, INLET AND OUTLET STRUCTURES, DAMS, BERTS, LINED AND UNLINED OPEN CHANNELS, STORM WATER STORAGE BASINS, UNDERGROUND STORM WATER STORAGE TANKS, AND BRIDGES AS DETERMINED BY CITY STAFF.
- TEMPORARY/SECURITY FENCING THAT IS REQUIRED OR IS OPTIONAL PROVIDED SHALL BE IN ACCORDANCE WITH THE ZONING ORDINANCE AND THE DESIGN STANDARDS AND POLICIES MANUAL.
- THE TEMPORARY/SECURITY FENCE LOCATION SHALL NOT BE MODIFIED OR THE TEMPORARY/SECURITY FENCE SHALL NOT BE REMOVED WITHOUT THE APPROVAL OF THE PLANNING AND DEVELOPMENT SERVICES/INSPECTION SERVICES DIVISION.
- NO TURF AREAS SHALL BE PROVIDED.
- NO IRRIGATION SHALL BE PROVIDED TO UNDISTURBED NATURAL OPEN SPACE (NAOS) AREAS.
- NO HELICOPTER ACCESS OF HELIPORT APPROVED UNDER THIS REVIEW.
- COORDINATE ALL OFF-SITE TRAFFIC CONTROLS WITH THE CITY OF SCOTTSDALE TRAFFIC DEPARTMENT AND ADOT DURING THE CONSTRUCTION PROCESS.
- THE FACE OF THE SERVICE ENTRY SECTION SHALL BE FLUSH WITH THE BUILDING FACADE AND PAINTED TO MATCH THE BUILDING.
- ROOF DRAINAGE SYSTEMS SHALL BE INTERIOR.
- NO EXTERIOR VENDING OR DISPLAY SHALL BE ALLOWED.
- WALL ENCLOSURE FOR REFUSE BINS AND TRASH COMPACTORS SHALL BE CONSTRUCTED OF MATERIALS, WITH COLORS AND TEXTURES WHICH ARE COMPATIBLE WITH THE BUILDING ON THE SITE.
- ALL SITE WALLS SHALL MATCH THE ARCHITECTURAL COLOR, MATERIALS AND FINISH OF THE BUILDING.
- THE MAXIMUM HEIGHT FROM FINISH GRADE TO THE TOP OF ANY EXTERIOR LUMINARIES SHALL NOT EXCEED TWENTY (20) FEET.
- ALL LUMINARIES SHALL BE RECESSED OR SHIELDED SO THE LIGHT SOURCE IS NOT DIRECTLY VISIBLE FROM THE PROPERTY LINE.

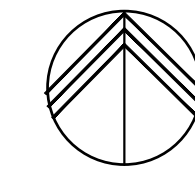
IGCC CHECK COMPLIANCE

- HEAT ISLAND MITIGATION: OVER 50% OF THE HARDSCAPE ACCOMMODATED BY OPTION (E) PARKING UNDER A BUILDING, IN ACCORDANCE WITH 2021 IGCC SECTION 5013.51 (BELOW GRADE PARKING GARAGE).
- ELECTRIC VEHICLE CHARGING: UNDERGROUND PARKING GARAGE WILL ACCOMMODATE 4% OF TOTAL REQUIRED PARKING SPACES OR NOT LESS THAN 8% OF DESIGNATED EMPLOYEE ONLY PARKING SPACES OR 10% OF REQUIRED TOTAL PARKING SPACES.
- ENERGY COMPLIANCE PATH: PRESCRIPTIVE BASED COMPLIANCE - COMPLY WITH MANDATORY AND PRESCRIPTIVE REQUIREMENTS IN 2021 IGCC OR ASHRAE 90.1-2019.
- ONSITE RENEWABLE ENERGY SYSTEM (SOLAR PV) - NOT LESS THEN 3% OF ANNUAL ESTIMATED ENERGY OR 2 WATTS PER SQ. FT. MULTIPLIED BY THE GROSS ROOF AREA IN ACCORDANCE WITH IGCC AMENDED SECTION 1013 - SOLAR PANELS WILL BE ROOF MOUNTED SEE KEYNOTE 39/5P1.
- REFUSE AND RECYCLING COLLECTION - LOCATION NOTED PER KEYNOTE 39/5P1.

SITE PLAN KEYNOTES

- NEW GROUND-UP BUILDING - SEE FLOOR PLAN.
- SITE SCREEN WALL "TYPE A" - SEE DETAIL 15 4 16 /8P2.4 (TOP OF WALL 3'-6" A.F.G.)
- DUMPSTER ENCLOSURE AND CONCRETE PAD TO MATCH BUILDING WALLS - SEE DETAIL 11/8P22 4 14/8P23 - (C.O.S. STD. DTL. #146-1)
- CONCRETE CURB PER CIVIL PLANS
- TYPICAL PEDESTRIAN CONCRETE SIDEWALK WITH LIGHT BROOM FINISH - W/ 5'-0" LANDINGS OUTSIDE BUILDING EXIT DOORS WITH MAX. 2% SLOPE AND CROSS SLOPES - MAX. 2% CROSS SLOPES ALONG LENGTH OF WALK - MAX. RUN SLOPE OF 1" PER 1' FOOT ALONG LENGTH OF WALK - SEE CIVIL PLANS AND DETAILS 1, 6, AND 7/8P2.0
- TRAFFIC VIABILITY TRIANGLE TO REMAIN CLEAR OF OBSTRUCTIONS FROM 2'-0" TO 7'-0" ABOVE GRADE - SEE LANDSCAPE AND CIVIL PLANS
- NEW DRIVEWAY CURB CUT PER CIVIL PLANS 4 30/8P2.11 (C.O.S. STD. DTL. 2250)
- H.C. ACCESSIBLE PARKING SPACE PER ADA STANDARDS - SEE DETAIL 10/8P2.1
- H.C. ACCESSIBLE PARKING SPACE SIGN PER ADA STANDARDS - SEE DETAIL 24/8P2.1
- INDICATES ACCESSIBLE ROUTE OF TRAVEL - MAX. 1" IN 1' FOOT RUN SLOPE - MAX. 2% CROSS SLOPES ALONG RUN - SEE CIVIL DRAWINGS
- FIRE TRUCK LANE AND TURNING RADIUS PER CITY OF SCOTTSDALE REQUIREMENTS (INSIDE RADIUS OF 25', OUTSIDE RADIUS OF 49', OVERHANG RADIUS OF 55')
- BIKE PARKING RACKS PER CITY OF SCOTTSDALE STANDARDS WITH 6'-6" X 9'-6" CLEAR AREA CONC. PAD - SEE DETAIL 27/8P2.8 (C.O.S. STD. DTL. 2285)
- DRIVEWAY CURB CUT W/ H.C. ACCESSIBLE RAMP - MAX SLOPE 1:12 - SEE 30/8P2.11 AND CIVIL PLANS - (MIN. 4'-0" WIDTH @ APPROX)
- SITE LIGHT - SEE DETAIL 25/8P2.1 AND STRUCTURAL AND ELECTRICAL DRAWINGS
- FAA READY LINE - COORDINATE SIZE, COLOR, ETC. WITH THE CITY OF SCOTTSDALE AIRPORT. PAINT LINE OUTSIDE THE TAXILANE SAFETY AREA (AREA EXTENDS 50'-0" FROM THE CENTERLINE OF THE TAXILANE). LINE SHALL BE PAINTED IN ACCORDANCE WITH FAA-AC 5340-1 (AS CURRENTLY AMENDED).
- MOTORIZED TRAFFIC CONTROL GATE PER CITY OF SCOTTSDALE AIRPORT STANDARDS - WROUGHT IRON GATE TO COMPLETE SEPARATION OF AIRSIDE FROM LANDSIDE (SECURE AIRPORT) - SEE DETAIL 17 4 18/8P2.4 - G.C. TO PROVIDE A KEY SWITCH AND PREEMPTION SENSOR - SEE ELECTRICAL DRAWINGS AND SPECIFICATIONS FOR FURTHER GATE AND OPERATOR INFORMATION AND REQUIREMENTS
- CONTROL PANEL FOR STAGING AREA MOTORIZED GATE PER SPECIFICATIONS AND CITY OF SCOTTSDALE AIRPORT AUTHORITY STANDARDS - SEE ELECTRICAL PLANS
- NEW 8'-0" A.F.G. HIGH SITE WALL COORDINATE CONSTRUCTION WITH THE ADJACENT PROPERTY OWNER AS REQUIRED - SEE DETAIL 20/8P2.3 4 21/8P2.6
- LANDSCAPE AREA - SEE LANDSCAPE PLANS
- FIRE RISER ROOM - SEE FLOOR PLAN SHEET A1.0 AND DEFERRED FIRE PROTECTION PLAN SUBMITTAL
- EXISTING CURB TO REMAIN AT EXISTING ROADWAY
- PDC PER CITY OF SCOTTSDALE STANDARDS - SEE BUILDING PLANS AND FIRE PROTECTION DRAWINGS
- DEMOLISH EXISTING 5'-0" SIDEWALK AT RIGHT-OF-WAY AND PROVIDE A NEW 6'-0" SIDEWALK PER CITY OF SCOTTSDALE
- 4" PARKING LOT STRIPING - SEE 4/8P2.0 AND CIVIL DRAWINGS
- TYPICAL H.C. ACCESSIBLE RAMP MAX SLOPE 1:12 - SEE DETAILS 6 4 7/8P2.0 AND CIVIL PLANS
- AIRCRAFT FUEL PUMP - TO BE POP-UP STYLE FULL UNDERGROUND ENCASMENT - UNDER SEPARATE PERMIT - SEE SHOP DRAWINGS
- AIRCRAFT FUEL TANK TO BE COMPLETELY UNDERGROUND - UNDER SEPARATE PERMIT - SEE SHOP DRAWINGS
- ASPHALT PARKING LOT PER CIVIL DRAWINGS
- FIRE LANE MARKINGS AT CURB - CURB TO BE COLORED PER SCOTTSDALE STANDARDS AND SIGNS POSTED PER C.O.S. STANDARD DETAIL 2365 - (DETAIL 28/8P2.5
- SEES LOCATED BEHIND EXTERIOR DOORS - SEE FIRST FLOOR PLANS
- DRIVEWAY TO BE 24'-0" WIDE TWO WAY - DIMENSION NOTED ON PLAN
- PROPOSED GROUND MOUNTED TRANSFORMER LOCATION - SUBJECT TO POWER COMPANY APPROVAL - SCREENED BY PROPERTY SITE WALL
- INSTALL 1 1/2" PAINTED STEEL SQUARE TUBE GUARDRAIL - 42" HIGH (W/ HANDRAILS PER ADA #34" MAX. TO TOP OF HANDRAIL PER ADA REQUIREMENTS - MAX. 3/8" CLEAR OPENINGS IN SYSTEM (4" SPACES CAN NOT PASS THRU) - SEE 2/8P2.0 AND CIVIL DRAWINGS. - ANCHOR PER MANUFACTURER'S RECOMMENDATIONS.
- INSTALL 8'-0" HIGH WROUGHT IRON SECURITY FENCE - PAINTED BLACK - SEE SPECIFICATION AND CIVIL - RUN FROM BUILDING TO PROPERTY LINE ALONG TAXILANE
- NEW ADA COMPLIANT 6'-6" WIDE CONCRETE SIDEWALK W/ LIGHT BROOM FINISH AND DROP OFF EDGES - ENSURE 5'-0" LANDING OUTSIDE BUILDING EXIT DOOR WITH MAX. 2% SLOPE AND CROSS SLOPE - MAX. 2% CROSS SLOPE ALONG LENGTH OF WALK - AND MAX. RUN SLOPE OF 1" IN 1' FOOT - SEE CIVIL DRAWINGS
- 2 HOUR RATED EXTERIOR HANGAR WALLS (SOUTH AND WEST HANGAR WALLS) PER IBC SECTION 412.4.1 (WITHIN 30' OF PROPERTY LINE)
- SEE STRUCTURAL DETAIL 22/81.4 FOR CONCRETE RETAINING CONDITION
- 90 GALLON CITY OF SCOTTSDALE RECYCLING CONTAINER PER IGCC 9013.41
- ROOF MOUNTED ON-SITE RENEWABLE ENERGY SYSTEM (SOLAR PV) - NOT LESS THEN 3% OF ANNUAL ESTIMATED ENERGY OR 2 WATTS PER SQ. FT. MULTIPLIED BY THE GROSS ROOF AREA IN ACCORDANCE WITH IGCC AMENDED SECTION 1013
- ELECTRIC VEHICLE CHARGING INFRASTRUCTURE PER IGCC - MIN. NUMBER OF EV INSTALLED SPACES 4% OF TOTAL REQUIRED PARKING SPACES OR NOT LESS THAN 8% OF DESIGNATED EMPLOYEE ONLY PARKING SPACES 10% OF TOTAL REQUIRED PARKING SPACES - MIN. NUMBER OF EV CAPABLE SPACES 10% OF TOTAL REQUIRED PARKING SPACES

NOTE:
ALL ON-SITE PARKING AND CIRCULATION AREAS TO ACCOMMODATE C.O.S. ON-SITE DRIVE REQUIREMENTS - SEE 29/8P2.10.



VICINITY MAP

NOT TO SCALE

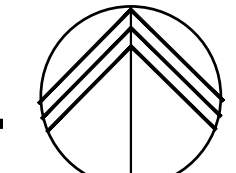
CITY OF SCOTTSDALE APPROVAL BLOCK

NOTE: G.C. TO ENSURE THAT ALL BUILDING SYSTEMS FROM ALL TRADES MUST BE CLEAN, NEAT, COORDINATED AND ORGANIZED IN A CRISP AND ORDERLY FASHION. ALL EXPOSED CONDUIT, PIPING, DUCTWORK, STRUCTURE, LIGHTING, ETC. TO BE NEAT AND ORDERLY.

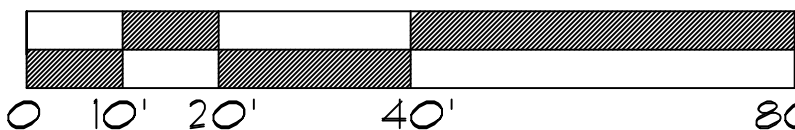
SITE PLAN

SCALE: 1"=20'-0"

NORTH



GRAPHIC SCALE 1"=20'-0"



LEGAL DESCRIPTION

LOT 34, OF NORTH SCOTTSDALE AIRPARK UNIT 1, ACCORDING TO BOOK 321 OF MAPS, PAGE 12, AND CERTIFICATE OF CORRECTION RECORDED IN DOCUMENT NO. 94-268055, RECORDS OF MARICOPA COUNTY, ARIZONA.

APN: 215-48-054

AIRCRAFT STIPULATION:
1. FUEL DISPENSING AND STORAGE BY SEPARATE SUBMITTAL AND PERMIT MUST HAVE SPECIFIC DOCUMENTED AIRPORT REVIEW AND APPROVAL TO CONFIRM COMPLIANCE WITH AIRPORT RULES AND REGULATIONS.

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TY JENKINS HANGAR
16061 NORTH 81ST STREET
SCOTTSDALE, AZ
APN: 215-48-054

APPROVED AND
SEAL
JAMES LAWRENCE
Oct 29, 2024
Arizona, U.S.A.

EXPIRES: 6/30/2024

REGISTERED PROFESSIONAL
ARCHITECT
JAMES LAWRENCE
Oct 29, 2024
Arizona, U.S.A.

EXPIRES: 6/30/2027

Drawing Name:
SITE PLAN

Revisions

Date: 11/6/2020

Project Number:
17-029

Drawing No:

SP1.0

659-PA-2024

ZONING ORDINANCE DATA

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THE DEVELOPMENT OF AN AIRCRAFT HANGAR WITH SUPPORTING OFFICE AND STORAGE SPACES AT THE CITY OF SCOTTSDALE MUNICIPAL AIRPORT

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SCOTTSDALE, ARIZONA 85260

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PROJECT ARCHITECT: LARSON ASSOCIATES ARCHITECTS, INC.
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EMAIL: jlarson@larson-architects.com

BOOK-MAP-PARCEL: APN 215-48-054

CURRENT ZONING: I-1 (SCOTTSDALE MUNICIPAL AIRPORT)

NET SITE AREA: 52,521 SF. (1206 AC)

LOT COVERAGE: 2122%

OCCUPANCY GROUP: S-1 - AIRCRAFT HANGAR (PER IBC SECTION 3112)
B - OFFICE SPACES (PER IBC SECTION 304.1)
-NEW CONSTRUCTION

1 HOUR SEPARATION IS REQUIRED BETWEEN OCCUPANCIES B AND S-1 PER NFPA 409

CONSTRUCTION TYPE: V-B, FULLY SPRINKLED (PER CITY OF SCOTTSDALE AMENDMENT)

FLOOR AREAS PROVIDED:
OCCUPANCY B:
FIRST FLOOR: 2,844 SF.
OCCUPANCY S-1:
FIRST FLOOR: 11,234 SF.
SITE COVERAGE TOTAL: 14,078 SF.
TOTAL OFFICES: 4,825 SF.
TOTAL HANGARS: 11,234 SF.
2ND FLOOR PATIO: 413 SF.
2ND FLOOR VIEWING DECK: 635 SF.
GRAND TOTAL: 17,101 SF.

OPEN STAGING AREA: 12,449 SF.

PARKING REQUIRED:
OFFICE AREA: 4,825 / 300 = 16 SPACES
HANGAR AND SUPPORT AREA: 0 SPACES
REQUIRED
TOTAL SPACES REQUIRED = 16 SPACES

PARKING PROVIDED:
SPACES PROVIDED = 17 = OK

ACCESSIBLE SPACES REQUIRED:
VAN SPACES REQUIRED: 16 SPACES X 4% = 1 SPACE
1 SPACE
SPACES PROVIDED = 1 SPACE = OK
1 SPACE = OK

ACCESSIBLE SPACES PROVIDED:
VAN SPACES PROVIDED: 1 SPACE = OK

BICYCLE PARKING REQUIRED: 16/20 = 2 SPACES

BICYCLE PARKING PROVIDED: 2 = OK

FRONTAGE YARDS REQUIRED:
EAST (TAXILANE): 50'-0"
WEST (81ST STREET): 25'-0"

YARD PROVIDED:
EAST: 149'-8"
WEST: 25'-0"

ON GRADE PAVING AREA: 9,954 SF.

PARKING LOT LANDSCAPING:
REQUIRED: 9,954x.15 = 1,493 SF
PROVIDED: 2,108 SF. = OK

INTERIOR PARKING LOT LANDSCAPING:
REQUIRED: 1028.75 x 33 = 33962 SF.
PROVIDED: 163 SF. = OK

OPEN FRONT SPACE:
REQUIRED: 20x206 = 4,120 SF.
PROVIDED: 4,505 SF. = OK

OPEN SPACE REQUIRED:
FIRST 12' FEET: 10x49.55x49.55 SF.
AREAS ABOVE 12': 215x.003x49.55 = 4,055 SF.
SUB-TOTAL: 9,155 SF.
STAGING AREA REDUCTION: 4,055/50 = 2,021.5 SF.
TOTAL REQUIRED: 4,915.5 + 2,021.5 = 6,943 SF.

TOTAL OPEN SPACE PROVIDED:
OPEN FRONT SPACE: 4,505 SF.
STAGING: 12,449 SF.
OTHER OPEN SPACE: 2,566 SF.
TOTAL: 19,520 SF. = OK

FLOOR AREA RATIO:
PER CITY OF SCOTTSDALE ZONING ORDINANCE SECTION 5.1804.A
NET LOT AREA = 49,155 SF. (113 ACRES)
F.A.R. (ALLOWED) = NLA = 6/10
F.A.R. (ALLOWED) = 49,155 * 6/10 = 29,493 SF.
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HANGAR: 11,234 SF.
OFFICES: 4,825 SF.
2ND FLOOR PATIO: 413 SF.
2ND FLOOR VIEWING DECK: 635 SF.
TOTAL GROSS FLOOR AREA: 17,101 SF. = OK
F.A.R. (ALLOWED) AS PERCENT = 60%
F.A.R. (PROVIDED) AS PERCENT = 34.820% = OK

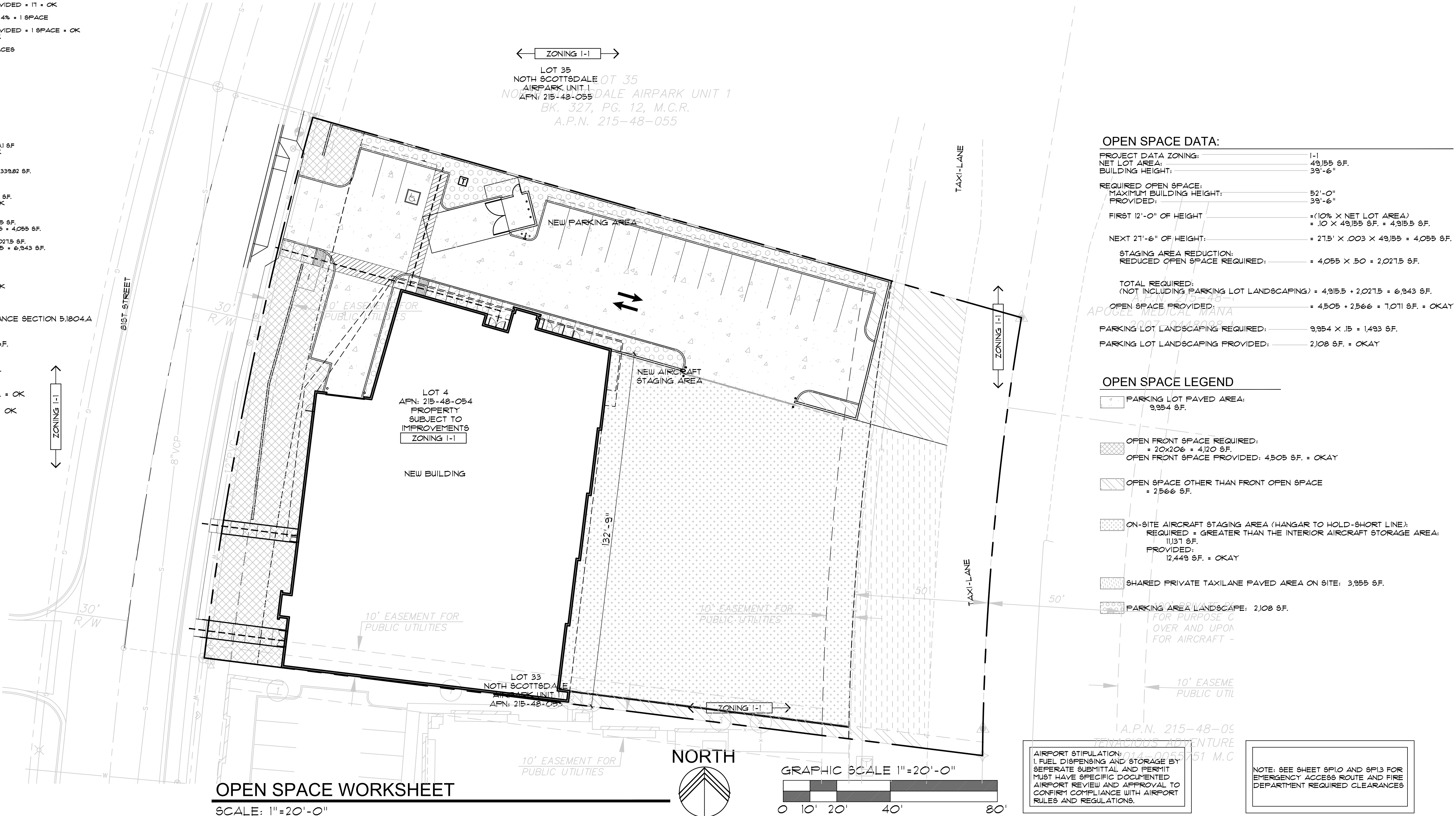
REQUIRED SITE PLAN NOTES

- RIGHT DISTANCE TRIANGLES SHALL BE SHOWN ON FINAL PLANS FOR DRIVEWAYS FROM COMMERCIAL SITES AND ANY INTERSECTIONS. AREA WITHIN THE SAFETY TRIANGLE IS TO BE CLEAR OF LANDSCAPING, SIGNS, OR OTHER VISIBILITY OBSTRUCTIONS WITH A HEIGHT GREATER THAN 15 FEET. TREES WITHIN THE SAFETY TRIANGLE SHALL HAVE A CANOPY THAT BEGINS AT 8 FEET IN HEIGHT UPON INSTALLATION. ALL HEIGHTS ARE MEASURED FROM NEAREST STREET LINE ELEVATION.
- ALL RIGHT-OF-WAY ADJACENT TO THIS PROPERTY SHALL BE LANDSCAPED AND MAINTAINED BY THE PROPERTY OWNER.
- ALL SIGNS REQUIRE SEPARATE PERMITS AND APPROVALS.
- NO EXTERIOR VENDING OR DISPLAY SHALL BE ALLOWED.
- FLAG POLES, IF PROVIDED SHALL BE ONE PIECE CONICAL TAPERED.
- NO EXTERIOR PUBLIC ADDRESS OR SPEAKER SYSTEM SHALL BE ALLOWED.
- ALL EXTERIOR MECHANICAL, UTILITY, AND COMMUNICATION EQUIPMENT SHALL BE SCREENED TO A HEIGHT OF THE TALLEST UNIT BY PARAFET OR SCREEN WALL THAT MATCHES THE ARCHITECTURAL COLOR AND ARCHITECTURAL FINISH OF THE BUILDING. GROUND MOUNTED MECHANICAL, UTILITY, AND COMMUNICATION EQUIPMENT SHALL BE SCREENED BY A SCREEN WALL THAT MATCHES THE ARCHITECTURAL COLOR AND ARCHITECTURAL FINISH OF THE BUILDING, WHICH IS A MINIMUM OF 1'-0" HIGHER THAN THE HIGHEST POINT OF TALLEST UNIT.
- ALL EQUIPMENT, UTILITIES, OR OTHER AFFURANCES ATTACHED TO THE BUILDING SHALL BE AN INTEGRAL PART OF THE BUILDING DESIGN IN TERMS OF FORM, COLOR AND TEXTURE.
- NO EXTERIOR VISIBLE LADDERS SHALL BE ALLOWED.
- ALL POLE MOUNTED LIGHTING SHALL BE A MAXIMUM OF 20 FEET IN HEIGHT.
- NO CHAIN LINK OR DOOLEY WALL FENCING SHALL BE ALLOWED.
- ALL EXTERIOR CONDUIT AND RACEWAYS SHALL BE PAINTED TO MATCH THE BUILDING.
- UPON REMOVAL OF THE SALVAGEABLE NATIVE PLANTS THE SALVAGE CONTRACTOR SHALL COMPLETE NATIVE PLANT TRACKING FORM AS WELL AS A LIST IDENTIFYING THE TAG NUMBERS OF THE PLANTS SURVIVING SALVAGE OPERATIONS TO THE CITY'S INSPECTION SERVICES UNIT WITHIN 3 MONTHS FROM THE BEGINNING OF SALVAGE OPERATIONS AND/OR PRIOR TO THE ISSUANCE OF THE CERTIFICATE OF OCCUPANCY.
- ALL EXTERIOR LUMINARIES SHALL MEET ALL IEBCA REQUIREMENTS FOR FULL CUTOFF, AND SHALL BE AIMED DOWNWARD AND AWAY FROM PROPERTY LINES EXCEPT SIGN, PARKING LOT CANOPY AND LANDSCAPE LIGHTING.
- CITY STAFF MAY AT ANY TIME REQUEST THE DEVELOPER TO SUBMIT AS-BUILT PLANS TO THE INSPECTION SERVICES DIVISION. AS BUILT PLANS SHALL BE CERTIFIED IN WRITING BY A REGISTERED PROFESSIONAL ENGINEER, USING AS-BUILT DATA FROM A REGISTERED LAND SURVEYOR. AS-BUILT PLANS FOR DRAINAGE FACILITIES AND STRUCTURES SHALL INCLUDE, BUT ARE NOT LIMITED TO STREETS, LOT GRADING, STORM DRAIN, PIPE, VALLEY GUTTERS, CURB AND GUTTER, FLOOD WALLS, CULVERTS, INLET AND OUTLET STRUCTURES, DAMS, BERMS, LINED AND UNLINED OPEN CHANNELS, STORM WATER STORAGE BASINS, UNDERGROUND STORM WATER STORAGE TANKS, AND BRIDGES AS DETERMINED BY CITY STAFF.
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- THE TEMPORARY/SECURITY FENCE LOCATION SHALL NOT BE MODIFIED OR THE TEMPORARY/SECURITY FENCE SHALL NOT BE REMOVED WITHOUT THE APPROVAL OF THE PLANNING AND DEVELOPMENT SERVICES' INSPECTION SERVICES DIVISION.
- NO TURF AREAS SHALL BE PROVIDED.
- NO IRRIGATION SHALL BE PROVIDED TO UNDISTURBED NATURAL OPEN SPACE (NAOS) AREAS.
- NO HELICOPTER ACCESS OF HELIPORT APPROVED UNDER THIS REVIEW.
- COORDINATE ALL OFF-SITE TRAFFIC CONTROLS WITH THE CITY OF SCOTTSDALE TRAFFIC DEPARTMENT AND ADOT DURING THE CONSTRUCTION PROCESS.
- THE FACE OF THE SERVICE ENTRY SECTION SHALL BE FLUSH WITH THE BUILDING FACADE AND PAINTED TO MATCH THE BUILDING.
- ROOF DRAINAGE SYSTEMS SHALL BE INTERIOR.
- NO EXTERIOR VENDING OR DISPLAY SHALL BE ALLOWED.
- WALL ENCLOSURE FOR REFUSE BINS AND TRASH COMPACTORS SHALL BE CONSTRUCTED OF MATERIALS, WITH COLORS AND TEXTURES WHICH ARE COMPATIBLE WITH THE BUILDING ON THE SITE.
- ALL SITE WALLS SHALL MATCH THE ARCHITECTURAL COLOR MATERIALS AND FINISH OF THE BUILDING.
- THE MAXIMUM HEIGHT FROM FINISH GRADE TO THE BOTTOM OF ANY EXTERIOR LUMINARIES SHALL NOT EXCEED TWENTY (20) FEET.
- ALL LUMINARIES SHALL BE RECESSED OR SHIELDED SO THE LIGHT SOURCE IS NOT DIRECTLY VISIBLE FROM THE PROPERTY LINE.

LEGAL DESCRIPTION

LOT 34, OF NORTH SCOTTSDALE AIRPARK UNIT 1, ACCORDING TO BOOK 321 OF MAPS, PAGE 12, AND CERTIFICATE OF CORRECTION RECORDED IN DOCUMENT NO. 94-268055, RECORDS OF MARICOPA COUNTY, ARIZONA.

APN: 215-48-054



OPEN SPACE DATA:

PROJECT DATA ZONING: I-1
NET LOT AREA: 49,155 SF.
BUILDING HEIGHT: 39'-6"

REQUIRED OPEN SPACE:
MAXIMUM BUILDING HEIGHT: 52'-0"
PROVIDED: 39'-6"

FIRST 12'-0" OF HEIGHT = (10% X NET LOT AREA) = .10 X 49,155 SF. = 4,915.5 SF.
NEXT 21'-6" OF HEIGHT: = 21.5' X .003 X 49,155 = 4,055 SF.

STAGING AREA REDUCTION:
REDUCED OPEN SPACE REQUIRED: = 4,055 X .50 = 2,021.5 SF.

TOTAL REQUIRED: (NOT INCLUDING PARKING LOT LANDSCAPING) = 4,915.5 + 2,021.5 = 6,943 SF.
OPEN SPACE PROVIDED: = 4,505 + 2,566 = 7,071 SF. = OKAY

PARKING LOT LANDSCAPING REQUIRED: = 9,954 X .15 = 1,493 SF.
PARKING LOT LANDSCAPING PROVIDED: = 2,108 SF. = OKAY

OPEN SPACE LEGEND

- PARKING LOT PAVED AREA: 9,954 SF.
- OPEN FRONT SPACE REQUIRED: = 20x206 = 4,120 SF.
OPEN FRONT SPACE PROVIDED: 4,505 SF. = OKAY
- OPEN SPACE OTHER THAN FRONT OPEN SPACE = 2,566 SF.
- ON-SITE AIRCRAFT STAGING AREA (HANGAR TO HOLD-SHORT LINE): REQUIRED = GREATER THAN THE INTERIOR AIRCRAFT STORAGE AREA: 11,131 SF.
PROVIDED: 12,449 SF. = OKAY
- SHARED PRIVATE TAXILANE PAVED AREA ON SITE: 3,955 SF.
- PARKING AREA LANDSCAPE: 2,108 SF.

A.P.N. 215-48-05

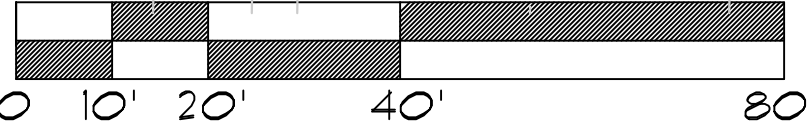
AIRCRAFT STIPULATION:
1. FUEL DISPENSING AND STORAGE BY SEPARATE SUBMITTAL AND PERMIT MUST HAVE SPECIFIC DOCUMENTED AIRPORT REVIEW AND APPROVAL TO CONFIRM COMPLIANCE WITH AIRPORT RULES AND REGULATIONS.

NOTE: SEE SHEET SP1.0 AND SP1.3 FOR EMERGENCY ACCESS ROUTE AND FIRE DEPARTMENT REQUIRED CLEARANCES

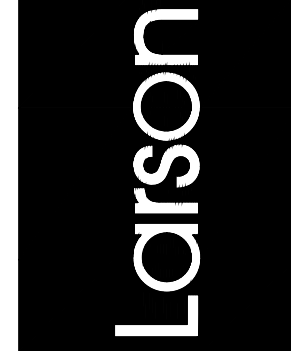
OPEN SPACE WORKSHEET

SCALE: 1"=20'-0"

GRAPHIC SCALE 1"=20'-0"



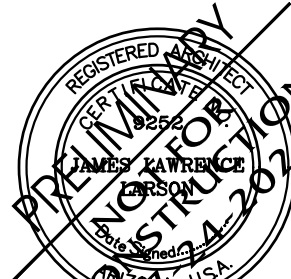
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602.955.9929 602.954.4790 FAX
design@larson-architects.com



TY JENKINS HANGAR
16061 NORTH 81ST STREET
SCOTTSDALE, AZ
APN: 215-48-054



EXPIRES: 6/30/2024



EXPIRES: 6/30/2027

Drawing Name:
OPEN SPACE
SITE PLAN

Revisions

Date: 11/6/2020

Project Number:
17-029

Drawing No:

SP1.1

659-PA-2024

659-PA-2024

REQUIRED SITE PLAN NOTES

- SIGHT DISTANCE TRIANGLES SHALL BE SHOWN ON FINAL PLANS FOR DRIVEWAYS FROM COMMERCIAL SITES AND ANY INTERSECTIONS AREA WITHIN THE SAFETY TRIANGLE IS TO BE CLEAR OF LANDSCAPING, SIGNS, OR OTHER VISIBILITY OBSTRUCTIONS WITH A HEIGHT GREATER THAN 15 FEET. TREES WITHIN THE SAFETY TRIANGLE SHALL HAVE A CANOPY THAT BEGINS AT 8 FEET IN HEIGHT UPON INSTALLATION. ALL HEIGHTS ARE MEASURED FROM NEAREST STREET LINE ELEVATION.
- ALL RIGHT-OF-WAY ADJACENT TO THIS PROPERTY SHALL BE LANDSCAPED AND MAINTAINED BY THE PROPERTY OWNER.
- ALL SIGNS REQUIRE SEPARATE PERMITS AND APPROVALS.
- NO EXTERIOR VENDING OR DISPLAY SHALL BE ALLOWED.
- FLAG POLES, IF PROVIDED SHALL BE ONE PIECE CONICAL TAPERED.
- NO EXTERIOR PUBLIC ADDRESS OR SPEAKER SYSTEM SHALL BE ALLOWED.
- ALL EXTERIOR MECHANICAL, UTILITY, AND COMMUNICATION EQUIPMENT SHALL BE SCREENED TO A HEIGHT OF THE TALLEST UNIT BY PARAPET OR SCREEN WALL THAT MATCHES THE ARCHITECTURAL COLOR AND ARCHITECTURAL FINISH OF THE BUILDING. GROUND MOUNTED MECHANICAL, UTILITY, AND COMMUNICATION EQUIPMENT SHALL BE SCREENED BY A SCREEN WALL THAT MATCHES THE ARCHITECTURAL COLOR AND ARCHITECTURAL FINISH OF THE BUILDING, WHICH IS A MINIMUM OF 1'-0" HIGHER THAN THE HIGHEST POINT OF TALLEST UNIT.
- ALL EQUIPMENT, UTILITIES, OR OTHER APPURTENANCES ATTACHED TO THE BUILDING SHALL BE AN INTEGRAL PART OF THE BUILDING DESIGN IN TERMS OF FORM, COLOR, AND TEXTURE.
- NO EXTERIOR VISIBLE LADDERS SHALL BE ALLOWED.
- ALL POLE MOUNTED LIGHTING SHALL BE A MAXIMUM OF 20 FEET IN HEIGHT.
- NO CHAIN LINK OR DOOLEY WALL FENCING SHALL BE ALLOWED.
- ALL EXTERIOR CONDUIT AND RACEWAYS SHALL BE PAINTED TO MATCH THE BUILDING.
- UPON REMOVAL OF THE SALVAGEABLE NATIVE PLANTS THE SALVAGE CONTRACTOR SHALL COMPLETE NATIVE PLANT TRACKING FORM AS WELL AS A LIST IDENTIFYING THE TAG NUMBERS OF THE PLANTS SURVIVING SALVAGE OPERATIONS TO THE CITY'S INSPECTION SERVICES UNIT WITHIN 3 MONTHS FROM THE BEGINNING OF SALVAGE OPERATIONS AND/OR PRIOR TO THE ISSUANCE OF THE CERTIFICATE OF OCCUPANCY.
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- ALL SITE WALLS SHALL MATCH THE ARCHITECTURAL COLOR, MATERIALS AND FINISH OF THE BUILDING.
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LEGAL DESCRIPTION

LOT 34, OF NORTH SCOTTSDALE AIRPARK, UNIT 1, ACCORDING TO BOOK 321 OF MAPS, PAGE 12, AND CERTIFICATE OF CORRECTION RECORDED IN DOCUMENT NO. 94-268035, RECORDS OF MARICOPA COUNTY, ARIZONA.

APN: 215-48-054

AIRPORT SITE PLAN KEYNOTES

- FAA READY LINE ("HOLD SHORT" LINE)- COORDINATE SIZE, COLOR, ETC. WITH THE CITY OF SCOTTSDALE AIRPORT AND THE FEDERAL AVIATION ADMINISTRATION
- TRAFFIC CONTROL GATE PER CITY OF SCOTTSDALE AIRPORT STANDARDS AND THE SCOTTSDALE NORTHWEST AIRPARK PROPERTY OWNER ASSOCIATION'S CCAR'S.
- NEW PAVING PER FAA ADVISORY CIRCULARS, AIRCRAFT STAGING AREA WITH SLOPE NOT TO EXCEED 2% - SEE CIVIL DRAWINGS.
- 8'-0" HEIGHT SITE WALL TO MATCH ADJACENT BUILDING MATERIALS AND COLOR - COORDINATE CONNECTION TO ADJACENT PROPERTY TAXIWAY BARRIER WITH THE ADJACENT PROPERTY OWNER, THE CITY OF SCOTTSDALE AIRPORT, THE CITY OF SCOTTSDALE BUILDING DEPARTMENT, AND THE SCOTTSDALE NORTHWEST AIRPARK PROPERTY OWNER ASSOCIATION
- EXTERIOR LIGHT - SEE ELECTRICAL SITE PLANS - LIGHTS SHALL ILLUMINATE DOWNWARD TO PREVENT GLARE TO PILOTS AND/OR AIR TRAFFIC CONTROLLERS.
- PROPOSED LOCATION FOR UNDERGROUND 20,000 GAL. JET 'A' FUEL TANKS BY SEPERATE SUBMITTAL AND PERMIT - SUPPLIER TO PROVIDE COMPLETE DRAWINGS TO THE CITY OF SCOTTSDALE AIRPORT - NO PORTION OF THE WORK FOR THE FUELING SHALL BE ALLOWED WITHOUT THE AIRPORT'S APPROVAL
- FUEL DISPENSING LOCATION BY SEPERATE SUBMITTAL AND PERMIT - SUPPLIER TO PROVIDE COMPLETE DRAWINGS TO THE CITY OF SCOTTSDALE AIRPORT - NO PORTION OF THE WORK FOR THE FUELING SHALL BE ALLOWED WITHOUT THE AIRPORT'S APPROVAL (IN-GROUND POP-UP PUMPS W/ WEATHERPROOF LIDS)
- TAXILANE CENTERLINE
- TAXILANE SAFETY EASEMENT AREA - NO DROP-OFFS, OBJECTS EXCEEDING 3" IN HEIGHT OR VEGETATION ALLOWED IN THIS AREA. GRADE SLOPE DOES NOT EXCEED 5%. VERIFY EXISTING PAVING IS LIGHT BEARING.
- CITY OF SCOTTSDALE TRASH REFUSE ENCLOSURES, ACCESS SHALL NOT TRANSVERSE THE STAGING AREAS OR TAXILANE.
- NEW DRIVEWAY PAVING - SEE CIVIL PLANS.
- NEW LANDSCAPE AREA (ALL ROCK WITH THIS AREA SHALL BE GREATER THAN 2" IN DIAMETER) - SEE LANDSCAPE PLANS.

AIRPORT AUTHORITY DATA

PROJECT DESCRIPTION

THE DEVELOPMENT OF AN AIRCRAFT HANGAR WITH SUPPORTING OFFICE AND STORAGE SPACES AT THE CITY OF SCOTTSDALE MUNICIPAL AIRPORT

PROJECT ADDRESS:	16061 N. 81ST STREET SCOTTSDALE, ARIZONA 85260
PROJECT OWNER:	ARIZTAR LLC (SEND ALL CORRESPONDENCE THROUGH THE ARCHITECT)
PROJECT ARCHITECT:	LARSON ASSOCIATES ARCHITECTS, INC. 3807 NORTH 24TH STREET, SUITE 100 PHOENIX, ARIZONA 85016 602-955-9929 602-954-4790 FAX EMAIL: jlarson@larson-architects.com
BOOK-MAP-PARCEL	APN 215-48-054
CURRENT ZONING:	I-1 (SCOTTSDALE MUNICIPAL AIRPORT)
NET SITE AREA:	52,521 SF. (1206 AC)
LOT COVERAGE:	21.22%
OCCUPANCY GROUP:	S-1 - AIRCRAFT HANGAR (PER IBC SECTION 312) B - OFFICE SPACES (PER IBC SECTION 304.1) -NEW CONSTRUCTION 1 HOUR SEPERATION IS REQUIRED BETWEEN OCCUPANCIES B AND S-1 PER NFPA 409
CONSTRUCTION TYPE:	V-B, FULLY SPRINKLED (PER CITY OF SCOTTSDALE AMENDMENT)
FLOOR AREAS PROVIDED:	
OCCUPANCY B:	2,844 SF.
FIRST FLOOR:	
OCCUPANCY S-1:	11,234 SF.
SITE COVERAGE TOTAL:	14,299 SF.
TOTAL OFFICE:	4,234 SF.
TOTAL HANGARS:	11,234 SF.
2ND FLOOR PATIO:	413 SF.
2ND FLOOR VIEWING DECK:	629 SF.
GRAND TOTAL:	17,101 SF.
USABLE STAGING AREA:	12,449 SF. + GREATER THAN ASB - OK
OTHER AIRCRAFT AREAS (HOLD SHORT TO PROPERTY LINE):	3,955 SF. PAVED PRIVATE TAXILANE
OPEN SPACE:	19,520 SF.

AIRPORT AUTHORITY GENERAL NOTES

- AIRPARK RULES AND REGULATIONS AND AIRPARK MINIMUM OPERATING STANDARDS SHALL REGULATE THE ACTIVITIES ALLOWED ON THIS PROPERTY.
- NO AIRCRAFT SHALL BE STORED OUTSIDE OF THE HANGARS ON THIS PROPERTY.
- ALL TENANTS SHALL BE RESPONSIBLE FOR COMPLIANCE WITH THE AIRPORT SLOT PROGRAM FOR THEIR LEASED SPACE.
- AIRCRAFT WITH WING SPANS OF MORE THAN 66' SHALL OBTAIN PERMISSION FROM THE AIRPORT PRIOR TO BEING BROUGHT TO THIS SITE.
- THE STAGING AREA SHALL NOT HAVE A SLOPE IN EXCESS OF 2%.
- THE STAGING AREA SHALL BE FULLY PAVED TO MEET THE FAA PAVEMENT STANDARDS AND THE REQUIREMENTS OF THE SOILS REPORT.
- THE FAA READY LINE ("HOLD SHORT" LINE) SHALL MEET FAA STANDARDS AS REQUIRED AT THE TAXILANE EASEMENT BOUNDARY.
- NO DROP-OFFS OR OBJECTS EXCEEDING 3" IN HEIGHT SHALL BE LOCATED WITHIN 50' OF THE TAXIWAY CENTERLINE. OBJECTS INCLUDE BUT ARE NOT LIMITED TO TRANSFORMERS, SWITCHING CABINETS, FUEL FACILITIES, CURBS, HEADWALLS, RIVER ROCK, CULVERTS, ETC.
- ALL TAXILANE EASEMENTS SHALL BE LIGHT BEARING SURFACES.
- GROUND COVER AT THE STAGING AREA SHALL BE PAVED.
- A FAA STANDARD EDGE LINE MUST BE PAINTED WHERE THE TAXILANE EDGE IS NOT OBVIOUS.
- NO STORM WATER SHALL BE RETAINED IN THE TAXIWAY.
- VEHICLES MUST OBTAIN PERMISSION PRIOR TO ENTERING THE STAGING AREA.
- PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL MEET WITH AIRPORT STAFF TO PLAN ALL CONSTRUCTION RELATED ACTIVITIES INCLUDING THE INSTALLATION OF CONSTRUCTION FENCING, PAVING ADJACENT TO THE TAXILANE, SITE SECURITY, AND ANY OTHER ITEMS AS DEEMED NECESSARY BY THE AIRPORT STAFF.
- ALL FUEL TANKS, EQUIPMENT, AND CONTROLS SHALL BE BY SEPERATE SUBMITTAL AND PERMIT. ALL FUEL TANKS, EQUIPMENT, AND CONTROLS SHALL BE APPROVED BY THE CITY OF SCOTTSDALE AIRPORT AUTHORITY PRIOR TO BEING SUBMITTED TO THE CITY FOR PERMITTING.
- THE CONTRACTOR SHALL SUBMIT ALL TAGS FOR THE FAA FOR TEMPORARY CRANES AND OTHER EQUIPMENT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT.

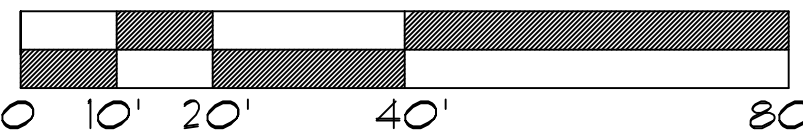
CITY OF SCOTTSDALE APPROVAL BLOCK

AIRPORT STIPULATION:
1. FUEL DISPENSING AND STORAGE BY SEPERATE SUBMITTAL AND PERMIT MUST HAVE SPECIFIC DOCUMENTED AIRPORT REVIEW AND APPROVAL TO CONFIRM COMPLIANCE WITH AIRPORT RULES AND REGULATIONS.

NOTE: SEE SHEET SP1.0 AND SP1.3 FOR EMERGENCY ACCESS ROUTE AND FIRE DEPARTMENT REQUIRED CLEARANCES



GRAPHIC SCALE 1"=20'-0"



AIRPORT AUTHORITY SITE PLAN

SCALE: 1"=20'-0"

Larson Associates Architects, Inc.

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Larson

TY JENKINS HANGAR

16061 NORTH 81ST STREET

SCOTTSDALE, AZ

APN: 215-48-054



EXPIRES: 6/30/2024



EXPIRES: 6/30/2027

Drawing Name:

SITE PLAN FOR

AIRPORT AUTHORITY

Revisions

Date: 11/6/2020

Project Number:

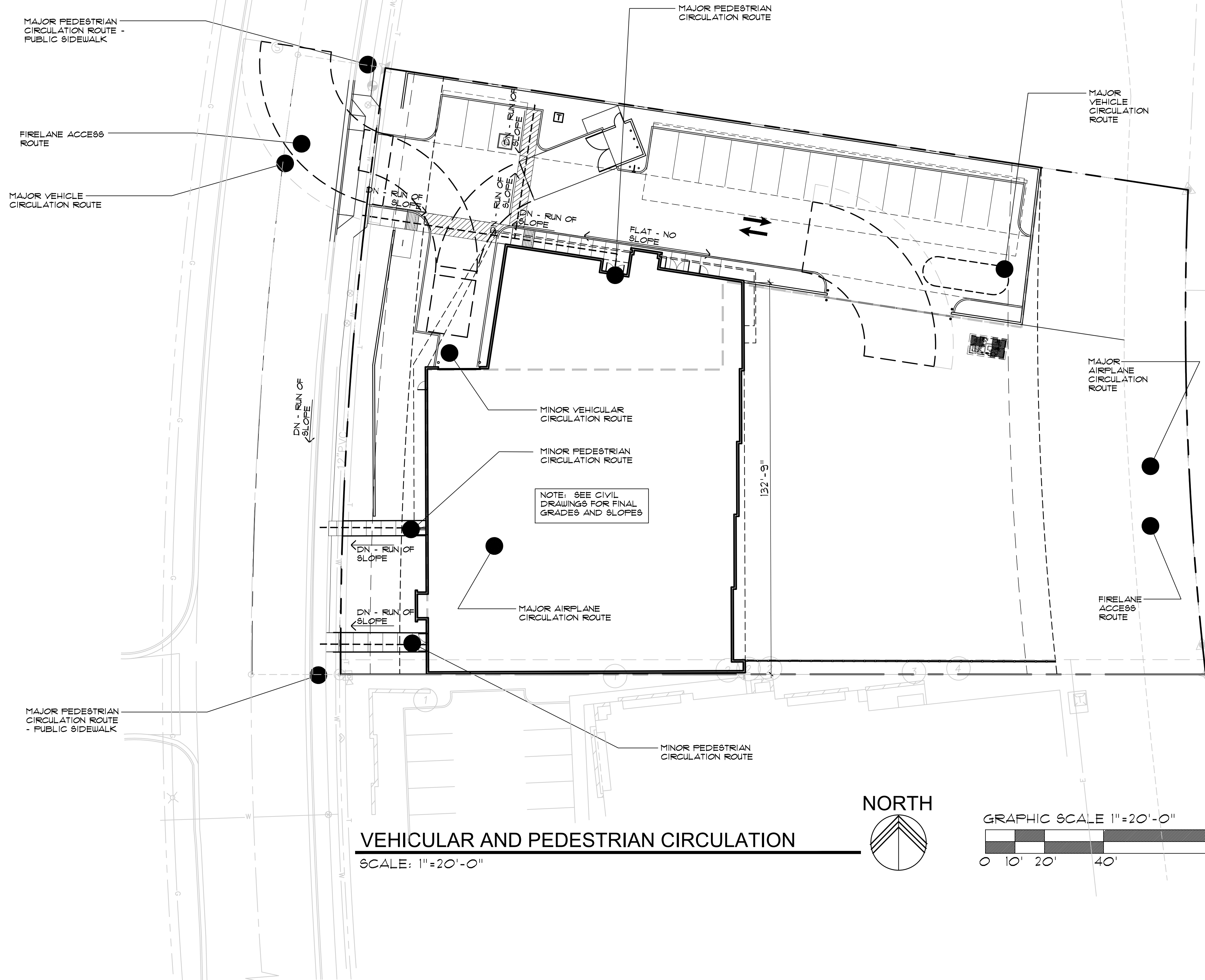
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Drawing No:

SP1.2

659-PA-2024

659-PA-2024

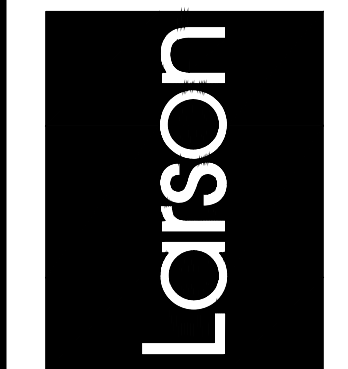
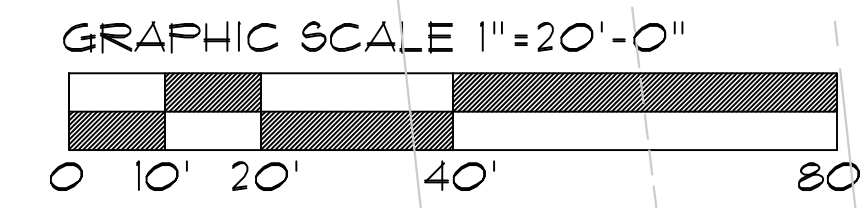


VEHICULAR AND PEDESTRIAN CIRCULATION LEGEND

- FIRE LANE ACCESS ROUTE
- MAJOR VEHICLE CIRCULATION ROUTE
- MINOR VEHICLE CIRCULATION ROUTE
- MAJOR PEDESTRIAN CIRCULATION ROUTE - MAX. 1" PER FOOT SLOPE ALONG RUN OF ROUTE - MAX. 2% CROSS SLOPE TO RUN - MAX. 2% SLOPE AT ANY LANDINGS BY EXIT DOORS OR ON SITE RUNS ETC... - SEE CIVIL DRAWINGS FOR FINAL GRADES
- MINOR PEDESTRIAN CIRCULATION ROUTE - MAX. 1" PER FOOT SLOPE ALONG RUN OF ROUTE - MAX. 2% CROSS SLOPE TO RUN - MAX. 2% SLOPE AT ANY LANDINGS BY EXIT DOORS OR ON SITE RUNS ETC... - SEE CIVIL DRAWINGS FOR FINAL GRADES
- MAJOR AIRPLANE CIRCULATION ROUTE
- FAA SECURITY LINE

NOTE:
ALL ON-SITE PARKING AND
CIRCULATION AREAS TO
ACCOMMODATE C.O.S. ON-SITE DRIVE
REQUIREMENTS - SEE 29/SF2.10.

VEHICULAR AND PEDESTRIAN CIRCULATION
SCALE: 1" = 20'-0"



Drawing Name:
PED. & VEHIC
CIRC. SITE PLAN

Revisions

Date: 11/6/2020
Project Number:
17-029
Drawing No:

SP1.3